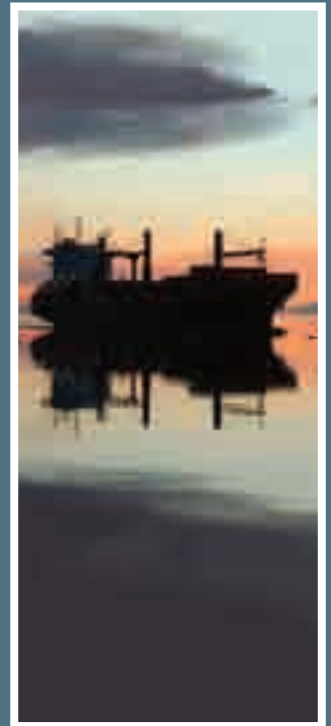
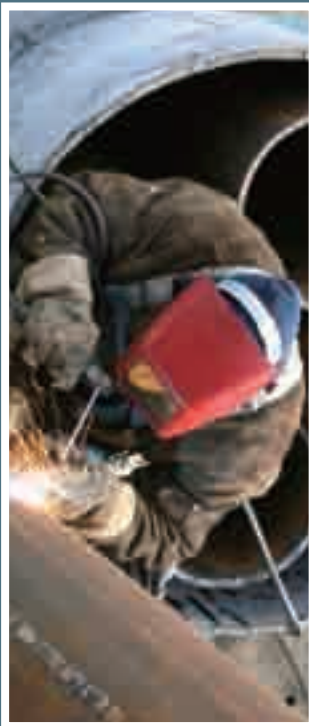


VOLUME ONE

January 2008

# Preliminary Environmental and Alternatives Report

*Port MacKenzie Rail Extension Project*



Prepared for the Matanuska-Susitna Borough and the Alaska Railroad Corporation



**Preliminary Environmental and Alternatives Report  
for the Port MacKenzie Rail Extension Project**

**Finance Docket No. 35095**

Matanuska-Susitna Borough  
350 E. Dahlia Avenue  
Palmer, Alaska 99645

Alaska Railroad Corporation  
327 West Ship Creek Avenue  
Anchorage, Alaska 99501

Prepared by HDR Alaska, Inc.,  
and TNH-Hanson, LLC

January 2008

**Table of Contents**

Executive Summary ..... viii

1.0 Introduction ..... 1-1

    1.1 Project Overview ..... 1-1

    1.2 Project Objective ..... 1-1

    1.3 Project Description ..... 1-2

    1.4 Project Setting ..... 1-2

    1.5 Background Information ..... 1-4

    1.6 Public and Agency Coordination ..... 1-5

    1.7 Report Objectives and Contents ..... 1-6

2.0 Alternatives Development ..... 2-1

    2.1 Design Criteria ..... 2-1

    2.2 Environmental Constraints ..... 2-5

    2.3 Constraints Analysis ..... 2-16

3.0 Description of Alternatives ..... 3-1

    3.1 Past Corridors ..... 3-1

    3.2 Alternatives under Evaluation ..... 3-4

        3.2.1 *South Segments* ..... 3-5

        3.2.2 *Connectors* ..... 3-7

        3.2.3 *North Segments* ..... 3-8

    3.3 No Action Alternative ..... 3-13

    3.4 Construction ..... 3-13

4.0 Environmental Factors ..... 4-1

    4.1 Transportation Systems ..... 4-1

    4.2 Natural Environment ..... 4-5

        4.2.1 *Wetlands* ..... 4-5

        4.2.2 *Hydrology* ..... 4-6

        4.2.3 *Fish and Essential Fish Habitat* ..... 4-11

        4.2.4 *Wildlife* ..... 4-12

        4.2.5 *Geology and Soils* ..... 4-15

    4.3 Human Environment ..... 4-21

        4.3.1 *Land Use and Land Use Plans* ..... 4-21

        4.3.2 *Parks and Open Space* ..... 4-28

        4.3.3 *Archaeological and Historical Properties* ..... 4-34

        4.3.4 *Socioeconomics* ..... 4-36

        4.3.5 *Air Quality* ..... 4-38

        4.3.6 *Noise* ..... 4-38

        4.3.7 *Coastal Consistency* ..... 4-40

        4.3.8 *Energy* ..... 4-41

        4.3.9 *Public Input* ..... 4-42

        4.3.10 *Environmental Justice* ..... 4-43

    4.4 Safety ..... 4-44

        4.4.1 *Crossings – roads and trails* ..... 4-44

        4.4.2 *Hazardous Materials and Petroleum Products* ..... 4-45

    4.5 Possible Actions Leading to Indirect and Cumulative Effects ..... 4-46

4.5.1	<i>Actions with Possible Indirect Effects</i> .....	4-46
4.5.2	<i>Actions with Potential Cumulative Effects</i> .....	4-46
5.0	Alternatives Study.....	5-1
5.1	Criteria Identification.....	5-1
5.2	Topics Not Covered or Removed from Matrix.....	5-7
6.0	Cost Estimates.....	6-1
6.1	Assumptions for Estimate Development .....	6-1
6.2	Estimate Items.....	6-1
6.2.1	Civil.....	6-1
6.2.2	Earthwork.....	6-2
6.2.3	Track .....	6-2
6.2.4	Structures .....	6-3
6.2.5	Right-of-Way .....	6-4
6.2.6	Additives.....	6-4
7.0	Permits .....	7-1
7.1	Implications for Permitting.....	7-1
8.0	References.....	8-1

**Tables**

Table 3-1: Corridors Considered in the 2003 MSB Study..... 3-2

Table 3-2 Estimated Crews and Equipment Needs for Construction ..... 3-15

Table 4-1: Road Crossings..... 4-2

Table 4-2: Preliminary Wetland Impacts..... 4-6

Table 4-3: Identified Waterbodies Impacted ..... 4-9

Table 4-4: Unsuitable soils identified by project alternative ..... 4-16

Table 4-5: Proposed Structures..... 4-20

Table 4-6: Land Impacted by Ownership Type ..... 4-22

Table 4-7: Parcels by Development Status within 200 feet of the Alternative  
Centerline..... 4-23

Table 4-8: Summary of Land Use..... 4-24

Table 4-9: Summary of Officially-Recognized Trail Crossings..... 4-34

Table 4-10: Summary of Population, Race and Poverty by Alternative..... 4-37

Table 4-11: Summary of Housing Units<sup>2</sup> by Alternative..... 4-37

Table 4-12: Number of Parcels with Appraised Building Values within 500 Feet of  
Proposed Alignments ..... 4-39

Table 4-13: Number of New At-Grade Crossings Where Locomotive Horn  
Noise Would be Heard..... 4-39

Table 4-14: Comparison of Energy Consumed by One Train Traveling in One Direction  
..... 4-41

Table 5-1: Description of criteria used in the matrix..... 5-2

Table 5-2: Final matrix evaluation..... 5-5

Table 5-3: Criteria not included in the evaluation matrix..... 5-7

Table 5-4: Alternatives strengths and weaknesses..... 5-9

Table 6-1: Conceptual Opinion of Probable Cost..... 6-7

**Figures**  
*(On or after page listed)*

1.1	Project Area .....	1-2
2.1	Proposed Track and Maintenance Road .....	2-2
2.2	Typical 28-foot Bridge.....	2-3
2.3	Grade-separated Crossing .....	2-3
2.4	Proposed Trail Crossing Alternatives .....	2-4
2.5	Waterbodies and Anadromous Streams .....	2-6
2.6	Wetlands and Wetland Banks .....	2-9
2.7	Limiting Soils.....	2-10
2.8	Developed Parcels, Land Value, and Agricultural Project .....	2-12
2.9	Parks, Refuges, and Recreation Facilities.....	2-13
2.10	Land Ownership (large landowners).....	2-15
2.11	Summary of Composite Map Development Process .....	2-16
2.12	Composite Constraints Map and Preliminary Corridors.....	2-17
3.1	Past Corridors.....	3-4
3.2	Alternatives Under Evaluation.....	3-4
3.3	Mac East and Mac West Segments.....	3-6
3.4	Connection Segments.....	3-7
3.5	Willow Segment.....	3-9
3.6	Houston Segment.....	3-10
3.7	Houston North and South Segments .....	3-11
3.8	Big Lake Segment.....	3-12
4.1	Locations of Possible New Road/Rail Intersections.....	4-2
4.2	NWI Wetlands .....	4-6
4.3	Mapped Floodplains.....	4-8
4.4	Anadromous Fish Streams .....	4-12
4.5	Game Management Units .....	4-12
4.6	Moose Habitat.....	4-12
4.7	Major Faults Along Railbelt .....	4-17
4.8	Castle Mountain Fault.....	4-18
4.9	Land Ownership.....	4-22
4.10	Land Use .....	4-22
4.11	Parks, Refuges, and Recognized Trails.....	4-28
4.12	Project Area Population Density.....	4-36
4.13	Percent Non-white Population by Census Block.....	4-44
4.14	Percent Below Poverty Level by Census Block .....	4-44

## **Appendices**

- Appendix A: Relevant Past MSB Assembly Resolutions and Actions
- Appendix B: NWI Wetland Categories in the Project Area
- Appendix C: Preliminary Letters Received from Regulatory Agencies
- Appendix D: Minutes of Preliminary Regulatory Agency Meetings
- Appendix E: ARRC Preliminary Voluntary Mitigation Measures
- Appendix F: Road Crossing Hazard Indices
- Appendix G: Preliminary Hydrology Report
- Appendix H: Preliminary Geological Investigations
- Appendix I: Cultural Resource Probability Modeling
- Appendix J: Train Energy Summary

## EXECUTIVE SUMMARY

The Matanuska-Susitna Borough (MSB) and the Alaska Railroad Corporation (ARRC) propose to connect Port MacKenzie to ARRC's rail system by constructing and operating a new rail line. The new rail line would extend from Port MacKenzie to the ARRC's existing main line between Wasilla and north of Willow. Port MacKenzie is a deepwater port on the north side of Knik Arm in upper Cook Inlet, located in Southcentral Alaska.

The purpose of the Port MacKenzie Rail Extension project is to establish a rail link between Port MacKenzie and the Alaska Railroad, providing Port MacKenzie customers and shippers cost effective rail transportation between the Port and Interior Alaska. Presently, the only surface mode of freight transport available to the Port is trucking. The construction of a rail line would satisfy the need for an additional mode of transportation to Port users, providing an economical alternative for movement of bulk materials.

The MSB began investigating development of Port MacKenzie and supporting infrastructure in the 1970s. In 1993, the MSB established the Port area as a Designated Major Energy Facility intended to facilitate the growth and development of the Port. The 1997 MSB Long Range Transportation Plan described the need for rail and improved road access to the Port.

In 2007, the State of Alaska granted to the MSB an appropriation to perform conceptual engineering and environmental documentation for the Port MacKenzie rail extension, which is the subject of this document. In November 2007, the MSB Assembly passed a resolution recognizing the need for further study and asking the Surface Transportation Board (STB) to include a thorough evaluation of local issues in the National Environmental Policy Act document that would be prepared.

Major elements of the Port MacKenzie Rail Extension Project would include between 30 and 45 miles of new railroad track; a 200-foot-wide right-of-way; crossings of local roads, streams, trails, and utility corridors; sidings; and ancillary facilities. The anticipated train traffic would be two trains daily, which would entail one train traveling in each direction.

The STB must examine the potential environmental impacts of a proposed action under the NEPA in considering whether to grant authority to construct and operate the new rail line. This report provides background information on the project area and evaluation of preliminary alternatives and environment of the project area.

Based on a constraints analysis approach, eight possible alignment configurations were developed. The alignments are composed of two southern and three northern segments (with one northern segment having two variants). The southern and northern segments are linked by connectors.

The project team developed a preliminary description of the natural and human environment and preliminary evaluation of potential impacts of the alternative alignments based on environmental factors and issues specific to the project area.

Surface and shallow subsurface drainage in the area of the alignment alternatives is generally to the west and south. Each of the project alternatives crosses wetlands, which may require excavation and filling for construction of a rail bed. Wetlands are widely distributed throughout the project area and complete avoidance of wetlands is not possible for any alternative. Each of the alternatives would cross anadromous fish streams.

The relatively undisturbed portions of the project area provide habitat for wildlife, including the main large species: moose, black bear, and brown bear. Upland and wetland bird species are also common in the project area.



Land in the project area is owned by government, as well as private and institutional land holders. Government land is owned primarily by the State and MSB. The MSB does not have a Borough-wide zoning code but it regulates land use through special land use districts, residential land use districts, and other mechanisms. A large percentage of the land in the project area has not been developed. For the developed parcels, the current land uses are diverse, but the dominant land use based on number of parcels is residential. Other land uses include commercial, industrial, institutional, and agricultural.

Designated parks, refuges, and recreation areas in the project area include the Willow Creek State Recreation Area (SRA), Nancy Lake SRA, Little Susitna River Recreation River, Little Susitna Public Use Facility, Susitna Flats State Game Refuge (SGR), Goose Bay SGR, Fish Creek Park, Big Lake North and South State Recreation Sites, and Rocky Lake State Recreation Site. In the MSB, trails are an important recreational and transportation resource. Trails serve as recreational, training, and competition areas for snowmachining, dog mushing, skiing, and skijoring. They also serve as the primary means of accessing many of the cabins and other recreational properties throughout the area. For the purpose of this project, the project team has defined officially-recognized public trails as those located on publicly-owned land or located within a properly obtained easement. Crossings of the official trails are intended to be grade-separated.

The MSB is the fastest growing area in the State. Approximately 4,300 people live in the project area, the majority living in the Willow, Houston, and Big Lake areas. The rest of the project area is more sparsely populated. There are approximately 1,600 households in the project area. The largest minority population in the area is American Indian and Alaska Native. The mean median household income of the project area is \$40,162. It is less than both the state as a whole and the MSB as a whole.

The current noise environment in the project area ranges from undeveloped areas with minimal human-generated noise, to rural areas with minimal or occasional human noise impacts, to residential and moderately urban areas with higher levels of road traffic noise, particularly adjacent to the Parks Highway.

The entire project area is located within the coastal zone as identified by the MSB Coastal Zone Management District and the Alaska Coastal Zone District boundaries.

A preliminary review of known contaminated sites indicates that there is one contaminated site located within the current range of alternatives. This site is located within existing ARRC right-of-way (ROW) near the tie-in of the Houston South alternative with the existing main line.

The project team used quantitative measures to compare the strengths and weaknesses of the Port MacKenzie Rail Extension project alternatives. The project team developed a matrix evaluation based on STB criteria and environmental and engineering criteria specific to the project area. Criteria were selected based on availability of a quantifiable measure and differences in measurements for alternatives that allowed comparison. Criteria included in the matrix included poorly or highly compressible soils, number of new road crossings, land availability, number of developed parcels, designated land use, train energy, wetlands, number of anadromous fish stream crossings, high potential for archaeological sites, fragmentation of a designated refuge and recreation area, and construction costs. Based on the results of the matrix, the various alternatives were described by their strengths and weaknesses.

## 1.0 Introduction

### 1.1 Project Overview

The Matanuska-Susitna Borough (MSB) and the Alaska Railroad Corporation (ARRC) jointly propose to connect Port MacKenzie to ARRC's rail system by constructing and operating a new rail line. The new rail line would be approximately 30 to 45 miles long, depending on the route selected, extending from Port MacKenzie to a point on the ARRC's existing main line between Wasilla and north of Willow.

Port MacKenzie is a deepwater port on the north side on Knik Arm. It lies about 30 miles southwest of Wasilla and 5 miles north of Anchorage across Knik Arm. Capable of serving large ships (such as Cape Class and Panamax vessels), the deep draft dock is enhanced by the port's 8,940 upland acres and 1,300 tideland acres (the Port MacKenzie District). These resources make Port MacKenzie an excellent area for bulk storage, transport, and processing facilities.

Previous studies have noted that good surface transportation access is necessary to accommodate growth at Port MacKenzie and develop as a strong economic driver in the MSB. Currently, the Port is only connected to the transportation network via roadways.

A rail line serving Port MacKenzie has been considered for nearly 30 years. Most recently, in 2003, the MSB completed a preliminary study of road and rail corridor alternatives that would connect Port MacKenzie to the Alaska Railroad. Rail line extensions such as this fall under the jurisdiction of the Surface Transportation Board (STB). As part of the process for authority to construct and operate the new line, the STB will serve as the lead federal agency in the conduct of environmental review under the National Environmental Policy Act (NEPA).

The MSB and ARRC have jointly prepared preliminary engineering and environmental studies for the new rail line. The information developed during this phase has been collected and is presented in this Preliminary Environmental and Alternatives Report. This report will:

- Re-evaluate the findings of the previous 2003 rail corridor study.
- Provide information in support of the NEPA process that the STB will be conducting, including alternatives development and study, preliminary design and engineering data, and anticipated construction cost estimates.

It is anticipated that the NEPA process should be completed, a decision whether to grant authority to construct and operate the line should be issued, and project design should be completed as early as 2009. If authority to construct and operate the line is issued, construction likely would extend from 2009 to 2011, and the rail extension should be operational in 2011 or 2012.

### 1.2 Project Objective

As freight traffic moving from upper Cook Inlet to the Alaska Interior continues to grow, Port MacKenzie has developed facilities to participate in the intermodal movement of goods. Presently, the only surface mode of freight available to the Port is trucking. The purpose of the Port MacKenzie Rail Extension project is to establish a rail link between Port MacKenzie and the Alaska Railroad, providing Port MacKenzie customers and shippers cost efficient rail transportation between the Port and Interior Alaska. The construction of a rail line would offer an additional mode of transportation available to Port users providing an economical alternative for movement of bulk materials.

The project would provide rail service to Port MacKenzie as an alternative means of surface transport and is consistent with MSB economic development plans (MSB 2006).

The proposed rail link is consistent with ARRC's enabling statute to foster and promote long-term economic growth and development of the State. It draws upon over 30-years of planning documents and studies relating to potential port development and related access. The project would represent a milestone in the continued development of the port.

Specifically, the project proposes to support Port MacKenzie's continuing development as a multi-modal and bulk material resources export and import facility. The project would also provide an alternative mode of surface transportation to Port MacKenzie.

### **1.3 Project Description**

The ARRC operates and maintains a 470-mile main line that runs from the port of Seward, Alaska, generally north through many communities including Anchorage, Wasilla, Houston, and Willow to a terminal in Fairbanks, Alaska. The Port MacKenzie rail extension would begin at the existing Port MacKenzie facility and tie into ARRC's existing main line track at a location between Mile 167 north of Wasilla and Mile 190 north of Willow. Major elements of the project would include:

- Between 30 and 45 miles of new railroad track depending on the alignment
- A 200-foot-wide right-of-way (ROW)
- Crossings (depending on the alignment) of the Little Susitna River, Lake Creek, and Willow Creek, along with many other small stream crossings
- The crossing of local roads and streets, including grade-separations as required
- Pipeline, utility, and recreational trail crossings
- Sidings along the route
- Ancillary railroad support facilities including, but not limited to, communication towers and facilities, maintenance, power, signals, and access roads

The anticipated foreseeable train traffic would be two trains daily, which would entail one train traveling in each direction. There is a possibility of an incremental increase in train traffic over time, although such increase is not anticipated during the reasonably foreseeable future.

### **1.4 Project Setting**

The project area is within the Susitna River valley and extends between the Susitna River, Cook Inlet, Knik Arm, and the existing Alaska Railroad main line (Figure 1.1).

The dominant climate for all of Southcentral Alaska, including the project setting, is classified as "maritime." Summers and winters are milder than what is normally seen in continental climates of similar latitude, with average temperatures ranging from 64.8 °F in July to 19.2 °F in January. In addition to relatively mild temperatures, the maritime climate of Alaska is characterized by heightened precipitation and persistent winds (Knik Arm Bridge and Toll Authority [KABATA] 2006a).

The area is generally composed of upland boreal forest—characterized by mixed stands of paper birch and white spruce, with occasional balsam poplar, quaking aspen, willow, and alder—interspersed with lakes, ponds, and wetland complexes associated with glacial tills and outwash deposits. Freshwater bogs and fens are the dominant wetlands in the project area. Coastal estuaries also occur at Susitna Flats (near the mouths of the Susitna and Little Susitna rivers) and Goose Bay. The wetlands are fed by multiple drainages that originate in the surrounding mountains, several of which are large, glacially fed, braided rivers with heavy sediment loads draining into the Susitna River and Knik Arm, which feed into Cook Inlet. The National Wetlands Inventory (NWI) has classified more than 200 wetland types in the project area. These

**Figure 1.1  
PROJECT AREA**

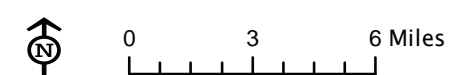


**LEGEND**

- ARRC Milepost
- +— ARRC Track
- Highway
- Major Rd.
- Medium Rd.
- Minor Rd.
- City Boundary
- Park or Refuge



This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: August 29, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

Z:\09585-ARRC\617879 P1 Mackenzie Rail Environmental Services\GIS\map\_docs\mxd\FINAL\chapter\_1\project\_area.mxd

*(This page intentionally left blank.)*

can be categorized as forested, scrub-shrub, and sedge or grass emergent. Common wetland plants in project area wetlands include black spruce, Labrador tea, dwarf birch, sweet gale, a variety of sedges, and sphagnum mosses. Wetlands are described in more detail in Section 4.2.1, and the distribution of wetlands and uplands in the project area is shown in Figure 4.2.

Drainage in the project area is generally to the west and south, with wetland complexes and small streams draining into larger stream systems (Susitna River and Little Susitna River) and into Knik Arm and Cook Inlet. In addition to the Little Susitna River, the primary streams in the project area include Cottonwood, Fish, Goose, Wasilla, and Willow creeks. These primary streams and many of the smaller associated streams provide habitat for anadromous and resident fish. All five species of Pacific salmon (Chinook, sockeye, coho, chum, and pink) occur in the project area and resident fish in the area include Dolly Varden, eulachon, northern pike, rainbow trout, arctic char, arctic grayling, lake trout, burbot, and whitefish (Alaska Department of Fish and Game [ADF&G] 2007a, KABATA 2006a). Water resources and fisheries are described in greater detail in sections 4.2.2 and 4.2.3, respectively, and streams are shown in Figure 4.4.

The project area provides habitat for numerous species of mammals and birds. The project area has one of the largest concentrations of moose in the State, including habitat for general year-round use, calving, rutting, and wintering. Both brown and black bears are common in the project area (KABATA 2006a). Numerous fur bearing and small mammals are also found in the project area. The diversity of upland forests, freshwater wetlands, and coastal marshes provide habitat for a wide variety of bird species, including shorebirds, waterfowl, other wetland and water dependent species, migratory and resident songbirds, raptors, and other upland species (KABATA 2006a).

The Castle Mountain Fault is one of several major east-northeast-striking faults in Southcentral Alaska, and the western Susitna lowlands portion of the Castle Mountain Fault runs through the middle of the project area. It is an active fault that presents an earthquake hazard. Two earthquakes are known to have occurred along the fault in areas where there is no surface expression.

Land use in the area is a mix of public recreation uses and wildlife habitat on State lands, residential uses, industrial uses, commercial enterprises, aviation uses, forestry, agriculture, and mineral resource development. In general, the amount of public land greatly exceeds the amount of privately owned land. Public land is owned by MSB, City of Houston, City of Wasilla, State of Alaska, Native corporations, public university, and the Alaska Mental Health Trust Authority (The Trust). Residential, urban, and commercial areas are concentrated near the Parks Highway and along the ARRC main line in the northern portion of the project area. Currently, the Alaska Department of Corrections (ADOC) owns and operates a rehabilitation facility in the area—the Pt. MacKenzie Correctional Farm. ADOC and MSB also have plans to construct and operate a large \$300 million medium-security prison at a site north of the Port MacKenzie District. This prison project is planned to be operational by 2010. Land use and land ownership are discussed in greater detail in Section 4.3, and Figure 4.10 shows general land ownership in the project area.

The Point MacKenzie Agricultural Project is also located in the area. The project, initiated by the State in the 1980s, is a group of privately-owned agricultural lands that were sold by the State with agricultural covenants. Owners are required to submit conservation plans for each parcel to the Division of Agriculture (DOA) to ensure that the agricultural resources in the area are preserved. Agricultural use of the project area is discussed in greater detail in Section 4.3.1 and shown in Figure 4.10.

Recreation is one of the area's major land uses. The project area includes State parks and refuges, such as the Nancy Lake State Recreation Area (SRA), Susitna Flats State Game Refuge (SGR), Willow Creek SRA, Little Susitna State Recreation River, and Goose Bay SGR (Figure 1.1).

State lands in the project area that are not dedicated for a specific purpose currently are primarily used for recreation. There are numerous year-round and winter trails that loop through the project area or act as a starting point for access across the Susitna River to remote locations. The major trails providing access across the Susitna River include the Iditarod National Historic Trail (INHT), RS 2477 trails<sup>1</sup> (Nancy Lake-Susitna and Knik-Susitna trails), the Iron Dog trail, and the Flathorn Lake trail. There are numerous loop trail systems in the project area, including the West Gateway trails that originate in Willow and the Aurora Dog Mushing trails near Knik. Recreation and trails are discussed further in Section 4.3.2, and trails are shown in Figure 4.11. The primary trail uses are winter dog mushing and snowmachining. Some trails are dry enough for year round use by all-terrain vehicles. Additional trail uses include cross-country skiing, biking, and hiking (Alaska Department of Natural Resources [ADNR] 2007).

Land in the area is also commonly used for both sport and subsistence hunting and fishing. Wildlife habitat and water features are extensive in the MSB. Privately-owned recreational cabins and properties are located along many of the lakes that are scattered throughout the project area including Big Lake, Red Shirt Lake, Delyndia Lake, Flat Horn Lake, Cow Lake, Horseshoe Lake, Papoose Twins Lakes, and Beaver Lakes.

## 1.5 Background Information

Over 35 years ago, the leaders of the MSB realized that conditions at the Port of Anchorage were such that significant expansion would not be feasible. The MSB embarked on a program to provide an alternate deepwater port facility easily accessible by both rail and highway. This facility, known as Port MacKenzie, is now in service and is located almost directly across Knik Arm from the Port of Anchorage. The location has access to deep water and offers significant uplands for port and industrial development.

Route specific access to Port MacKenzie has been addressed at least three times since 1990; as part of coastal zone planning, in the 1997 Long Range Transportation Plan (LRTP), and in a preliminary study of road and rail access to the port. In response to these studies, the MSB Assembly has passed resolutions supporting road and rail development. These resolutions are included in Appendix A.

In 1993, the MSB Assembly adopted the “Point MacKenzie Area Which Merits Special Attention (AMSA) Plan.” The AMSA was updated in 2006 (ADNR 2006a). Through the AMSA the MSB established the Port MacKenzie area as a Designated Major Energy Facility intended to facilitate the growth and development of the port while maintaining wise use of coastal resources. This plan supports development of a deepwater port at Point MacKenzie and refines the proposals for roadway access to the area. In the short term, road access was envisioned as improving and using the existing Point MacKenzie access road, Burma Road, and South Big Lake Road. A long-term alternative crossed the Little Susitna River and extended north to the Willow area.

The MSB LRTP was adopted in September 1997 (MSB 1997a). This document is a Borough-wide transportation plan which includes elements addressing the development of a deepwater port at Point MacKenzie and improved roadway and rail access to that facility. The LRTP states that the MSB approved the East Port site (Point MacKenzie) as the preferred deepwater port site. Access to the port area included in the LRTP echoes the recommendations of the Point MacKenzie AMSA (ADNR 2006a). The 1997 LRTP re-emphasizes the need for a rail connection between the port and the Alaska Railroad if the port is to meet its full potential. The LRTP

---

<sup>1</sup> RS 2477 trails, named from Revised Statute 2477 of the Mining Act of 1866, refer to the grant of public ROW access across unreserved federal land to guarantee access as land is transferred to State or private ownership.

specifies a rail connection to the port connecting with the Alaska Railroad south of the Little Susitna River near Houston.

Construction of an access road and barge dock at Port MacKenzie began in the fall of 1999. A deep-draft dock was added and became operational in 2005.

Currently, Knik-Goose Bay Road and the Point MacKenzie Road serve the port. Knik-Goose Bay Road is a two-lane, paved road with 4-foot shoulders. For the most part the road operates under a 55-mph rural speed limit with frequent driveways, side road intersections and frequent passing restrictions because of curves and hills. The route is approximately 22 miles long, which extends northeasterly to connect with the Parks Highway in Wasilla.

The MSB is undertaking improvements to access Port MacKenzie. The Alaska Department of Transportation and Public Facilities (ADOT&PF) executed a rehabilitation project for Knik-Goose Bay Road in 2005. The project improved the northerly 19.8 miles, providing a new typical section with two 12-foot lanes throughout and established turn lanes at the appropriate locations. The last 14 miles of the Point MacKenzie Road is still a gravel surface. The MSB has scheduled the paving of the road for 2008.

Currently, the bulk of the freight movement for the Alaska Railroad is in the Anchorage-Fairbanks corridor passing through Wasilla.

In 2003, the MSB commissioned a study to locate separate or combined corridors for roadway and railway access to Port MacKenzie (MSB 2003). The scope of the study did not include an in-depth analysis of the environmental impacts of the project. Rather, the study team summarized and considered apparent existing conditions and provided a preliminary review of constraints and probable impacts. The intent was to determine corridor feasibility. The primary areas of concern, identified from public meetings, were wetlands, geotechnical considerations, and the amount of private property to be acquired for ROW.

For the 2003 effort, a constraints analysis was conducted and 11 alternatives were initially identified. A more thorough description of all the 2003 alternatives that were originally considered can be found in Section 3.1 and Table 3-1. The study recommended two alignments as separate corridors for rail and road access to the port, which would need further study. The recommendations were approved by the MSB Assembly. Appendix A lists the MSB resolutions.

During the 2007 Alaska state legislative session, the MSB received an appropriation to perform conceptual engineering and environmental documentation for the Port MacKenzie rail extension, which is the subject of this document. The intent of this new effort is to take a fresh and more detailed look at the project area for a designated rail-only corridor, including those corridors identified in the 2003 MSB study.

In November 2007, the MSB Assembly passed a resolution (No. 07-139) recognizing the need for further study and asking the STB to include a thorough evaluation of local issues in the NEPA document that would be prepared (Appendix A).

## **1.6 Public and Agency Coordination**

The MSB and ARRC jointly conducted public open houses and agency overview meetings to support the development of this document. A summary can be found in the Public Involvement Activities Summary Report in Volume 4. Several agency consultations and public meetings were held with stakeholders and interested parties between September through December 2007. Area residents, landowners, businesses, native corporations, tribal entities, community groups, MSB representatives, and agency officials were invited to comment on the proposed project during a formal public involvement period and preliminary design scoping process. Communities



potentially impacted by the project include: Wasilla, Big Lake, Houston, Knik, Point MacKenzie, and Willow. At these meetings, several issues and concerns were raised by the public and regulatory agencies, including:

- Impacts to private property
- Impacts to floodplains, wetlands and hydrology, specifically natural drainage from Beaver Lakes, Horseshoe Lake, and West Lake
- Impacts to anadromous streams, such as the outlet streams from Crooked Lake flowing into the Little Susitna River
- Potential archaeological sites, specifically in the Red Shirt Lake area
- Impacts to public property and parks
- Impacts to recreation
- Access to undeveloped areas
- Impacts to moose and sandhill cranes in the project area
- Impacts to socio-economic issues
- Noise impacts
- Wetlands
- Impacts to farms and agricultural parcels
- Seismic concerns
- Traffic impacts to urban centers potentially effected by the alignments

Although this report considers practicable alignment alternatives and their potential impacts on the natural and human environment, the issues raised during the preliminary public and agency coordination process were used to focus the review of this report and help describe impacts and develop preliminary voluntary mitigation measures. Therefore, some of these areas identified in this preliminary public process have been developed in more detail throughout the document.

## **1.7 Report Objectives and Contents**

The STB must examine the potential environmental impacts of a proposed action under NEPA in considering whether to grant authority to construct and operate the new rail line. This report is structured to provide background information on the preliminary alternatives and environment of the project area. Section 1.0 of this report provides project background and overview. Section 2.0 explains the methodology for developing the alternatives through a constraints analysis and details the engineering and environmental constraints that were used in the analysis. Section 3.0 describes the alternatives. Section 4.0 describes the natural and human environment in the project area and develops a preliminary evaluation of the potential impacts of the alternatives considered. Section 5.0 compares the alternatives using a matrix and presents the strengths and weaknesses of the alternatives. Section 6.0 provides cost estimates for the alternatives. Section 7.0 describes permits and clearances that would likely be required for project construction.

This Preliminary Environmental and Alternatives Report represents Volume 1 of 4 prepared for this project. In addition to this report, there are three other volumes. Volume 2 contains the associated appendices to this document, which includes the following:

- Appendix A: Relevant Past MSB Assembly Resolutions and Actions
- Appendix B: NWI Wetland Categories in the Project Area
- Appendix C: Preliminary Letters Received from Regulatory Agencies
- Appendix D: Minutes of Preliminary Regulatory Agency Meetings
- Appendix E: ARRC Preliminary Voluntary Mitigation Measures
- Appendix F: Road Crossing Hazard Index Calculations
- Appendix G: Preliminary Hydrology Report

- Appendix H: Preliminary Geological Investigations
- Appendix I: Cultural Resource Probability Modeling
- Appendix J: Train Energy Calculations

Volume 3 contains the conceptual plan set (engineering drawings). Volume 4 contains the Public Involvement report which details the public involvement activities during the fall of 2007.

## 2.0 Alternatives Development

Alternatives development for this project started with an evaluation of previous studies. The design criteria for the project were assessed (Section 2.1) and available technical and environmental information was evaluated (Section 2.2). Based upon review of this available information, a constraints analysis was performed to develop a reasonable range of corridor alternatives that minimized apparent impacts to the natural and human environment (Section 2.3). The corridors developed during the constraints analysis were then further refined taking into account technical and environmental information to address potential conflicts and environmental impacts.

### 2.1 Design Criteria

The various alternatives identified for evaluation during the study process, while being largely railroad alignments, also included some road and trail elements. The basic design criteria selected for each class of facility are addressed below.

Consistent with ARRC main line design practice, all routes are aligned to facilitate 60-mph freight operations. Though not all trains may operate at this speed, time sensitive traffic, including intermodal and potentially passenger traffic, would likely require transit time which should not be limited by track geometry. Speed criteria limits horizontal curvature to approximately two degrees (a radius of 2,865 feet), with a minimum of nearly 200 feet between reversing curves.

Generally, grade changes would be kept to a minimum to maximize fuel efficiency and lessen long term maintenance costs associated with the track. The potential for heavy unit train traffic limits the alignment gradient. The ruling grade between Wasilla and Gold Creek is 1.00%. The design criteria for this project would limit grades to a maximum of 1.00% to maintain consistency in train components and lessen the need for additional wayside facilities for helper locomotives. Specific operating needs, including sidings and terminal facilities would limit the grades even further in specific areas.

The proposed ROW would be 200-feet wide, providing for signals, utility lines, sidings, and other facilities. This width also provides a reasonable safety buffer along the proposed route and is consistent with ARRC standards. The railroad typical cross section is based upon ARRC standard plan 2-11.05 which provides for a 40-foot embankment section to accommodate the railroad track and a track-level access road. The access road would facilitate construction of modern railroad track incorporating welded rail and concrete ties. Further, the roadway would provide access for railroad maintenance crews during operations. The proposed profile of the ROW is shown in Figure 2.1.

All bridges would be designed for Cooper's E-80 loading and would be consistent with ARRC and American Railway Engineering and Maintenance-of-Way Association (AREMA) bridge design practice. Bridges would be comprised of standard 28-foot spans as shown in Figure 2.2. For alternatives analysis, culverts were preliminarily sized to accommodate flows resulting from an estimated 100-year return period storm. For final design of the project, culverts would be hydraulically designed as specified by AREMA:

- Pass a 25-year storm flow without static head at the culvert entrance
- Pass a 100-year storm flow with a headwater condition that (a) is 2-feet or more below the base of rail or (b) does not exceed 1.5 times the culvert diameter/rise, whichever is less

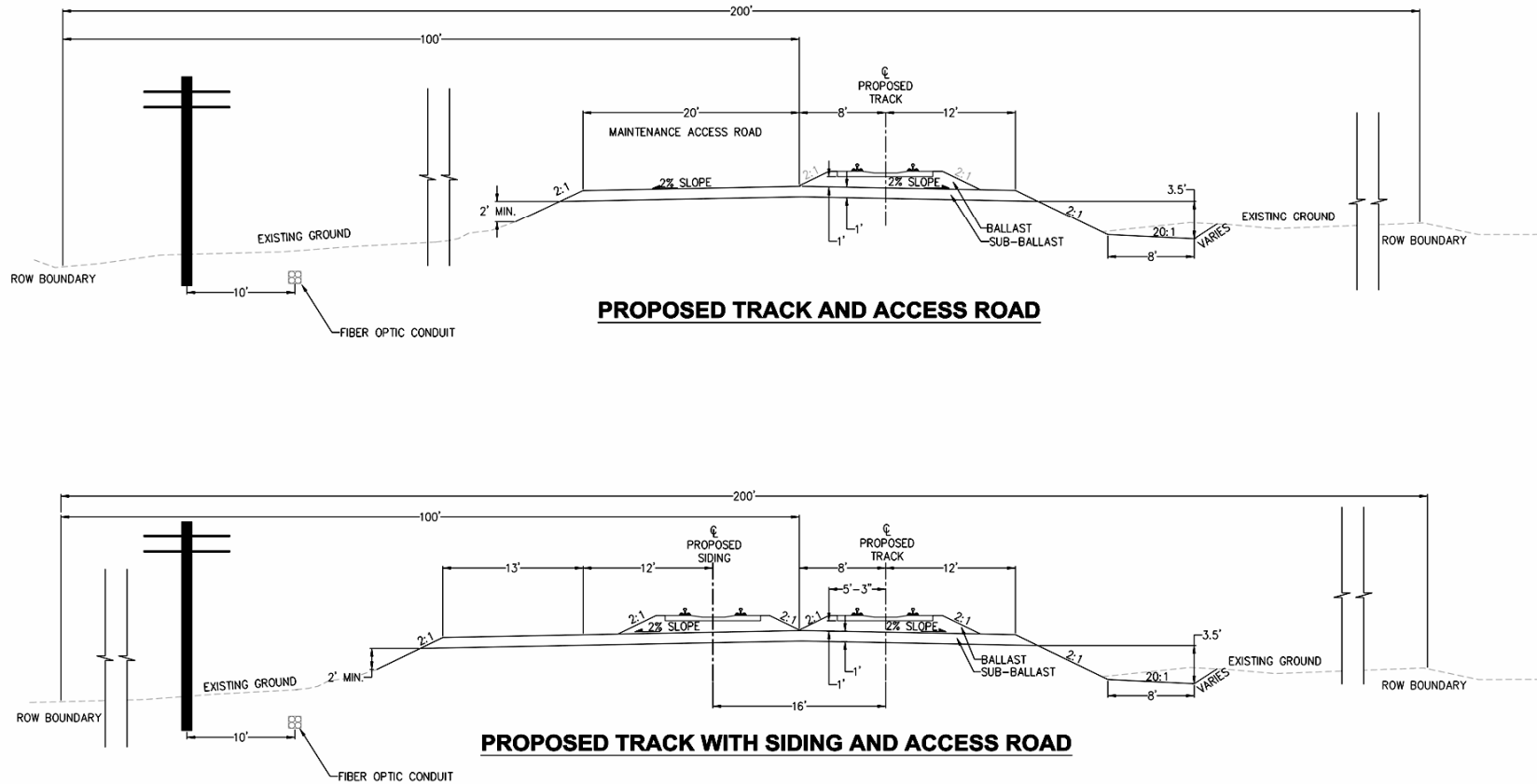
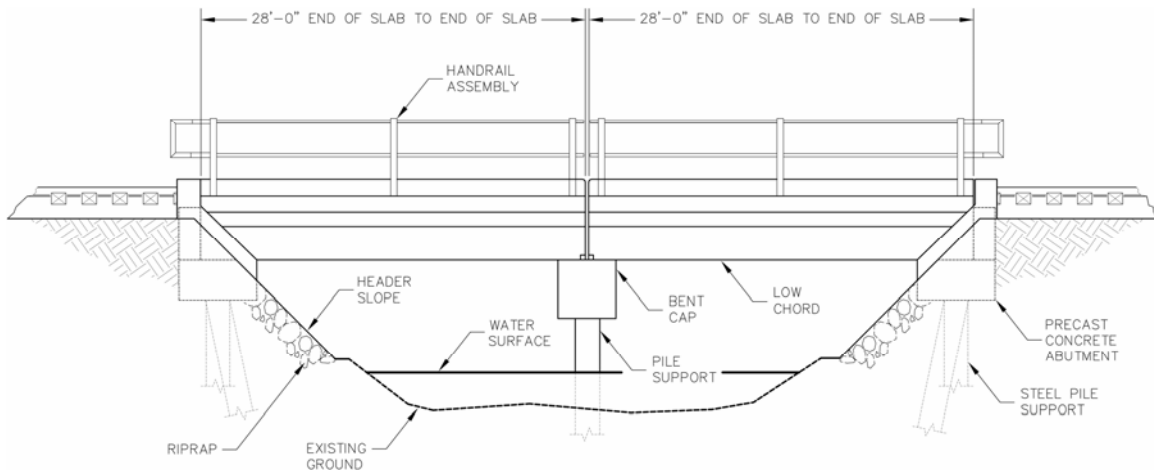


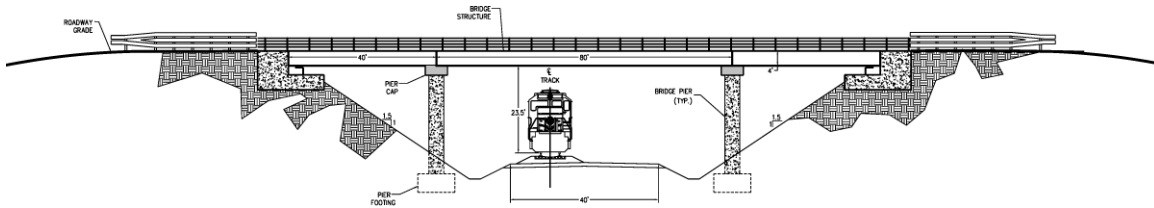
Figure 2.1 Proposed Track and Access Road



**Figure 2.2 Typical 28-foot Span Bridge**

Depending on the alignment ultimately selected, the project would cross, in some way, at least three classifications of roadway. Two of the alternatives cross the Parks Highway, which is functionally classified as a “Rural Principal Arterial” and listed as part of the National Highway System and Interstate Highway System. Each of the alternatives intersects roads that are part of the Alaska Highway System and/or the MSB collector and/or local road systems. The design criteria for crossings vary for each classification of road and follow the recommended standards as set forth in the ADOT&PF Preconstruction Manual and existing ARRC standard practices. A possible grade-separated interchange with the Parks Highway is depicted in Figure 2.3.

There are a multitude of official and unofficial trails traversing the project area. With the exception of bicycle pathways that parallel highway facilities (Parks Highway and Big Lake Road), the trails are undeveloped and primarily used by all-terrain vehicles, snowmachines, and dog sleds. The ARRC proposes to grade separate all officially-recognized trail crossings (Section 4.3.2 defines officially-recognized trails) not associated with roadways. Generally these crossings would provide for adequate horizontal and vertical clearance for typical trail users. Possible culvert and bridge trail crossing configurations are depicted in Figure 2.4.



**Figure 2.3 Grade-separated Crossing**

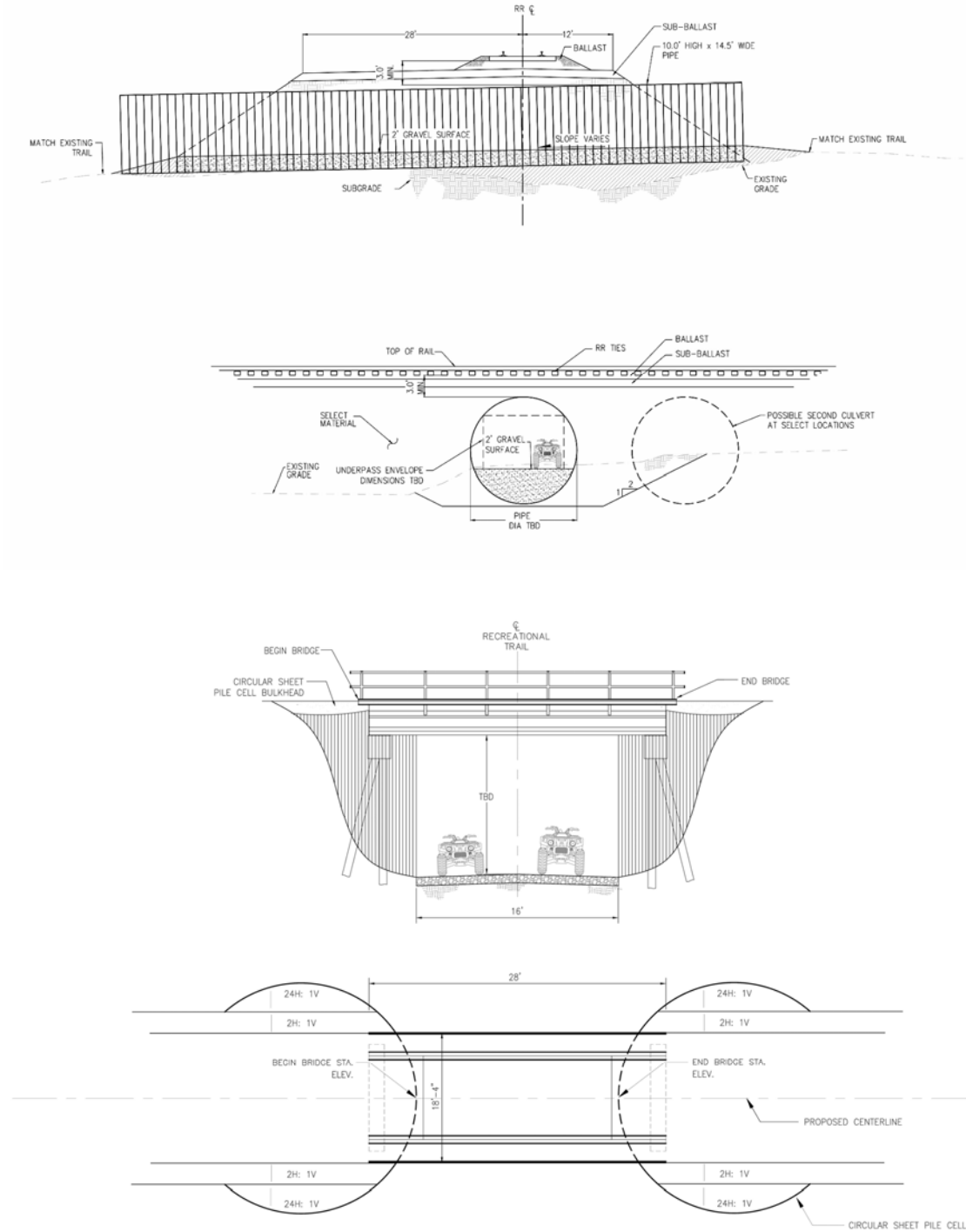


Figure 2.4 Proposed Trail Crossing Alternatives

## 2.2 Environmental Constraints

### 2.2.1 Methodology

To identify and refine alternatives for the proposed rail extension to Port MacKenzie, the project team developed a composite environmental constraints map. The purpose of this map was to identify potential corridors, based on the constraints considered to be less suitable for locating a rail line. Moreover, the methodology allowed the identification of corridors (and then more specific alignments) that avoided or minimized the potential environmental impacts of the construction and operation of the proposed rail project.

Each constraint was considered separately and then all were considered collectively to determine if there were opportunities to avoid or minimize the potential impacts of the project. The composite constraints map revealed logical corridors that serve as the platform to identify rail line alternative alignments.

To develop this overall understanding of the area's constraints, the project team used a modern version of an overlay process introduced in the 1960s by landscape architect Ian McHarg. McHarg developed this process so that a project's environmental impacts should be considered in the early stages of project development. The process entails mapping environmental resources separately and then combining them in a layering process to develop a map that reveals the overall environmental constraints of an area.

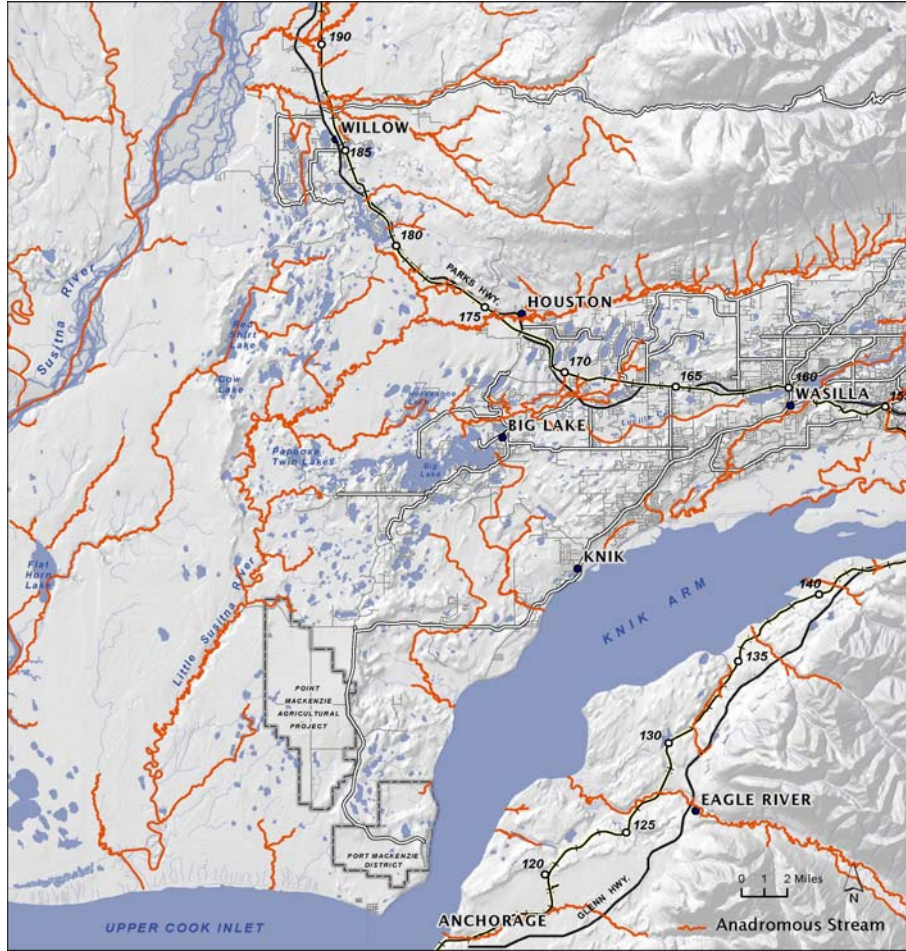
The evaluation process starts with the identification of the factors or resources to be considered. For each factor, a layer was created using a geographic information system (GIS), with dark gradations representing areas with the greatest value (or greatest constraint) and the lightest gradations representing the areas with the lowest values (or least constraint). The layers were digitally superimposed on each other to form a composite constraints map. The darkest areas showed the areas with the greatest overall values (or constraints), and the lightest with the least. The layering process enabled the project team to identify broad corridors with the least amount of environmental constraint to rail development.

### 2.2.2 Description of Constraints

The project team identified a number of factors that influence development of a rail line, such as government land management or the presence of physically limiting environmental conditions (e.g., wetlands or unsuitable soils). The project team identified eleven factors that had readily available information for environmental evaluation in a GIS format. The factors used to develop the composite constraints map are:

- Waterbodies
- Anadromous streams
- Archaeological and historic properties
- Native allotments
- Parks and refuges
- Wetlands
- Wetland banks
- Limiting soils
- Prison facilities
- Developed parcels (less than 20 acres) & land value (\$2,000/acre and higher with structures)
- Land ownership

Each factor is discussed in more detail below.



**Figure 2.5 Waterbodies and Anadromous Streams**

### Waterbodies

Waterbodies such as lakes and rivers are generally environmentally sensitive areas. The MSB requires a vegetated buffer<sup>2</sup> around waterbodies (ADNR 2006b) for the protection of water quality and fish and wildlife habitat. Bridges associated with the crossing of any waterbody would also increase project costs. Consequently, water crossings and waterbody buffer areas were deemed important to avoid when possible.

The information on waterbodies in the project area was provided by the MSB GIS and National Hydrography datasets. The project team used the waterbodies coverage and buffered those layers. The resulting map is depicted in Figure 2.5.

<sup>2</sup> The MSB Coastal Management Plan Enforceable Policies (RDA 7) indicates the size and extent of buffers shall be determined on a case-by-case basis and shall be commensurate with reasonably foreseeable impacts of the development on recreational uses and activities.



### **Anadromous fish streams**

Fish, particularly salmon, are an important resource in Alaska for economic, subsistence, and recreational purposes and as part of the ecosystem. As a result, the State has developed regulations designed to protect fish habitat, particularly those streams that support anadromous fish. Activities that can impact anadromous fish streams such as culvert and bridge construction, gravel removal, or stream bank disturbances, require an ADNR Office of Habitat Management & Permitting (OHMP) Title 41 Fish Habitat permit. Avoiding anadromous fish streams when possible is preferred because it reduces potential impacts to fish resources, decreases construction cost, and eliminates the need for a Title 41 Fish Habitat permit.

The project team mapped anadromous fish streams as identified by the ADF&G Anadromous Fish Stream Catalog (ADF&G 2007a).

### **Archaeological and Historic Properties**

Section 106 of the National Historic Preservation Act (NHPA) of 1966 (36 Code of Federal Regulations [CFR] 800) requires all Federal agencies to take into account how their projects would affect historic properties. A historic property is a site that is included or eligible for listing in the National Register of Historic Places. Historic properties can include buildings, other structures, archaeological sites, sacred sites, traditional cultural properties, and historic and cultural landscapes and districts.

The project team contacted the State Historic Preservation Officer (SHPO), the Knik Tribal Council, Knik Tribal Council<sup>3</sup> Cultural and Historic Preservation Committee, and the MSB Cultural Resources Office to identify potential archaeological, historical, and cultural sites in the project area. Records maintained in the Alaska Heritage Resource Survey (AHRS) database indicate that there are currently more than 100 recorded sites in the project area. Avoiding recorded sites to the extent possible would minimize the project's potential impact to historic properties.

The Knik Tribal Council and its members are located on the northwest bank of Knik Arm, northwest of Anchorage, in the MSB. Knik has long been home to Tanaina and Dena'ina Athabascans.

The project team used the latitude/longitude information listed in the AHRS to locate each site using GIS. Due to the sensitive nature of archaeological and historic properties, the resulting figure is not included in this report.

Concurrent with this project development, the MSB Cultural Resources Office developed a model to predict areas of high probability for archaeological sites within the project area. These areas were based on the attributes and context of recorded sites (including, for example, terrain, topography and distance to water), and would provide a basis for prioritizing further field survey and investigation. The modeling effort and the results are described in Section 4.3.3.

The INHT is also an identified historic resource that has national designation and functions across both historic and recreational spheres. Given the linear east-west orientation of the INHT which is located between Port MacKenzie and the ARRC main line, crossing the trail with any alternative is unavoidable. The design of the crossing would be done in consultation with the Bureau of Land Management (BLM), ADNR, SHPO, public trails councils, and interested parties and would be done in a manner that best serves the public interest.

---

<sup>3</sup> Knik Tribal Council is the only federally-recognized tribe in the project area.

## **Native Allotments**

The Native Allotment Act of 1906 authorized individual Indians, Aleuts, and Eskimos in Alaska to acquire an allotment consisting of one or more parcels of land not to exceed a total of 160 acres. Alaska Natives filed approximately 10,000 allotment applications for almost 16,000 parcels of land statewide under this Act before its repeal in 1971. The Alaska Native Veterans Allotment Act of 1998 (Veterans Allotment Act) provided certain Alaska Native Vietnam-era veterans, who missed applying for an allotment due to military service, the opportunity to apply under the terms of the 1906 Native Allotment Act as it existed before its repeal.

Land from Native allotments cannot be acquired through eminent domain. Eminent domain is the ability of a government entity to acquire private property at fair market value without the owner's consent. Eminent domain is typically used as a last resort means of property acquisition in situations where the owner is not willing to sell but the property is determined necessary for a project to proceed. Because selecting a corridor that requires land from a Native allotment may complicate the ROW acquisition process, avoiding Native allotments when possible is desirable.

According to the BLM, there are eleven Native allotment applications in the project vicinity. All eleven have been adjudicated, and five applicants have received a certificate of allotment (BLM 2007). The project team used the information provided by the BLM to identify the location of each allotment in GIS.

## **Wetlands**

Under most circumstances, wetlands and other “waters of the U.S.” are regulated by the U.S. Army Corps of Engineers (USACE) under authority of Section 404 of the Clean Water Act (CWA) or under authority of Section 10 of the Rivers and Harbors Act of 1899. By Federal law (CWA) and associated policy, it is necessary to avoid project impacts to wetlands wherever practicable, minimize impact where impact is not avoidable, and in some cases compensate for the impact. Construction in Waters of the U.S., including wetlands, requires a permit process whereby any work proposed in wetlands must comply with the CWA. Before a permit to work in a wetland is granted by the USACE, the project proponent must demonstrate that no practicable alternatives exist that would avoid impacts to wetlands altogether and still meet the same overall project purpose. Alternatives are typically evaluated to determine whether wetlands have been avoided where possible.

The project team classified uplands and wetlands within the project area into four categories based on their relative importance to other wetland types within the local region (Appendix B). These categories were based on the wetland types identified on NWI maps and the general wetland functions these wetland types may perform.

Category 1 includes all non-wetland areas.

Category 2 includes forested; scrub/shrub; and excavated, diked, partially drained, or ditched vegetated wetlands. These wetland types likely perform a variety of important functions, however, not to the high degree as other wetland types. This category was considered a moderate constraint because forested and scrub/shrub wetlands are relatively common throughout Southcentral Alaska and the Matanuska-Susitna Valley and are often considered to be of moderate overall ecological value to regulatory agencies. The vegetated but previously disturbed wetlands (excavated, diked, partially drained, or ditched) were also placed in this category because their natural functions are likely to have been somewhat diminished as a result of human alteration.

Category 3 represents emergent wetlands—wetlands dominated by sedges and grasses. This wetland type generally has a higher ecological value than other types in the region and hence is

considered a high constraint. The functions of emergent wetlands can be highly variable depending on their topographic position and level of inundation or saturation. In general, however, this wetland type provides groundwater discharge and recharge, storm water runoff attenuation, and habitat for water-dependent wildlife. In addition, many emergent wetlands improve surface water quality, tend to be more productive habitat, and export organic material to support downstream systems.

Category 4 includes open water habitats, estuarine habitats, riparian habitats, and coastal (tidally influenced) swamps and marshes. In general, these wetlands represent the most unique wetland types within the project area, and therefore, were considered the greatest constraint. Estuarine and other coastal marshes contribute to shoreline stability by binding sediments and protecting against erosion. These are usually highly productive habitats, with the organic matter exported to support marine ecosystems. These areas generally provide important fish and wildlife habitat. Wetlands adjacent to creeks and streams, known as riparian wetlands, were also assigned to this category because they typically provide important wildlife movement corridors, enhance stream water quality, often provide fish habitat, and usually stabilize stream banks against erosion. Riparian wetlands often export nutrients to downstream environments and are likely to attenuate flood flows.

Category 3 and 4 wetlands were mapped for the constraints analysis and avoided to the extent possible (Figure 2.6).

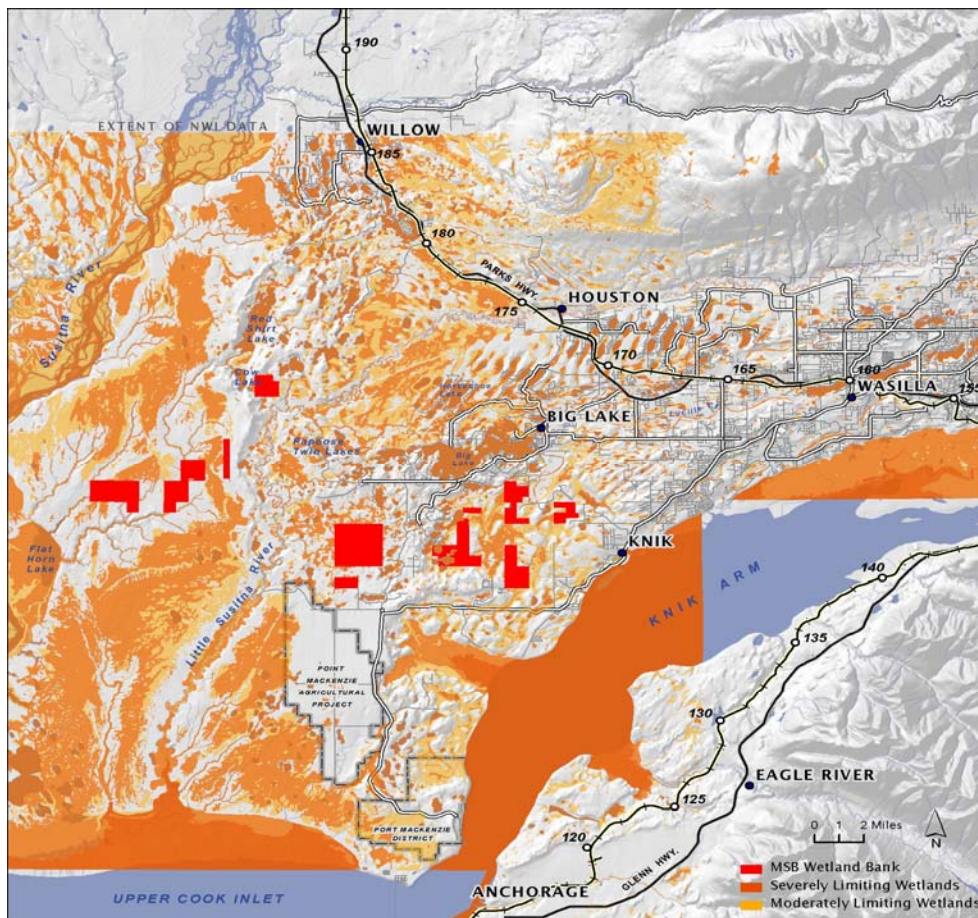


Figure 2.6 Wetlands and Wetland Banks

### Wetland Bank Lands

Recognizing the importance of preserving sensitive wetlands, the MSB is classifying some of the more sensitive MSB-owned wetland areas as “Wetland Bank Lands.” The MSB has two purposes for these banks; to protect and preserve valuable wetlands, and to allow developers an opportunity to purchase banked wetlands to offset unavoidable wetland impacts incurred by their projects (MSB and Sustainable Environments LLC 2007a, 2007b). While the wetland banks are in the process of being certified by the USACE, the MSB has already identified wetland areas for this purpose (Tracy McDaniel, pers. comm.). These lands should be avoided if possible by project alternatives. Wetland land bank locations are from the MSB GIS database (Figure 2.6).

### Limiting Soils

Underlying soil conditions influence construction feasibility and cost, and the presence of highly compressible soils is a wetland indicator. The Natural Resources Conservation Service (NRCS) (U.S. Department of Agriculture [USDA] 1998) classified soils in the Matanuska-Susitna Valley by multiple characteristics, including three indicators of constructability: drainage, suitability for roads, and suitability for building structures. Lands within the project area with poor drainage and not considered suitable for building structures or roads were considered a constraint because these areas are likely in wetlands or on steep slopes and would likely require extensive site preparation work.

Using the NRCS GIS data, the project team mapped soils that were not suitable for road development, soils that were unsuitable for buildings, and poorly drained soils to show soils that should be avoided if possible. The resulting map is shown in Figure 2.7.

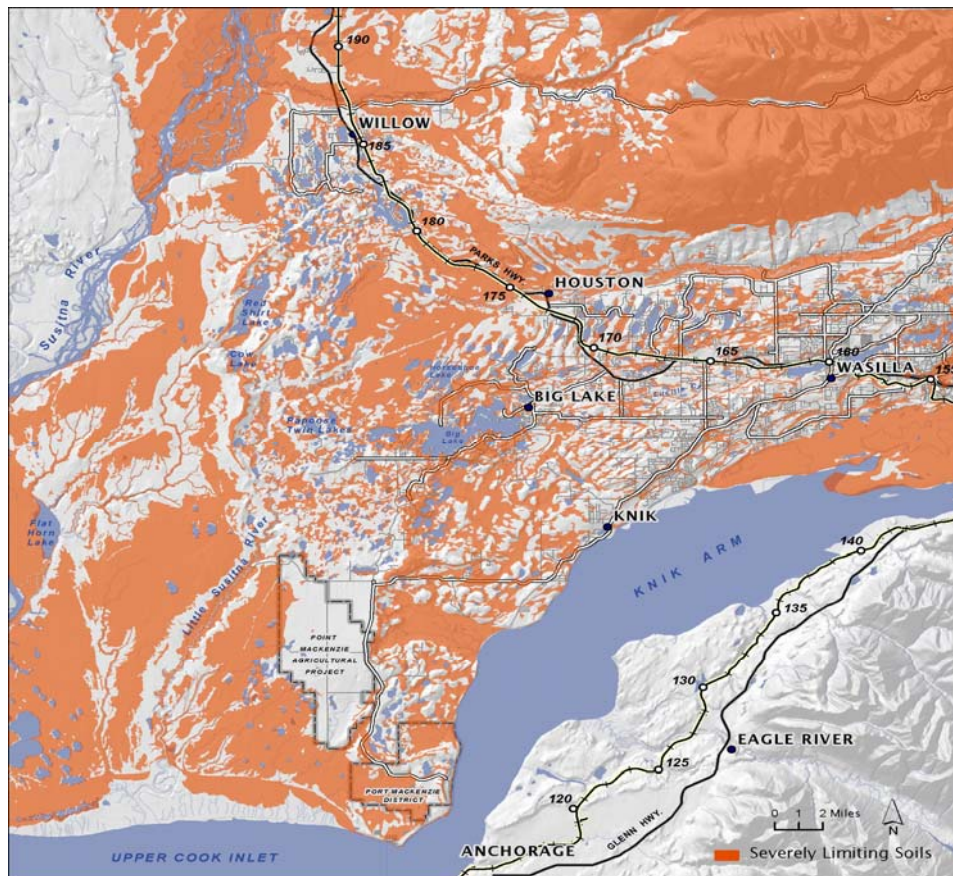


Figure 2.7 Limiting Soils

Concurrent with the development of the environmental constraints, an initial geotechnical reconnaissance was performed. The intent of this program was to determine the approximate depth of highly compressible soils and the potential for frozen soils in the vicinity of the alignments. The geotechnical field work results were not available during the alternatives development process. These data were used, however, for evaluating the alignments as discussed in Section 4.2.5.

**Developed Parcels (less than 20 acres) & Land Value (\$2,000/ acre and higher with structures)**

Avoiding and minimizing potential impacts to developed parcels would minimize the effects of the project on residents, businesses, and overall community cohesion. Allowing some separation between the rail corridor and developed areas also reduces other potential impacts such as noise, vibration, and visual impacts.

Given the predominately undeveloped nature of the project area, it was assumed that large parcels (over 20 acres in size) are not fully built out. The project team considered large parcels to have a higher opportunity to accommodate a rail corridor in a way that avoids the developed portion. Because of this, developed parcels over 20 acres were not considered a constraint.

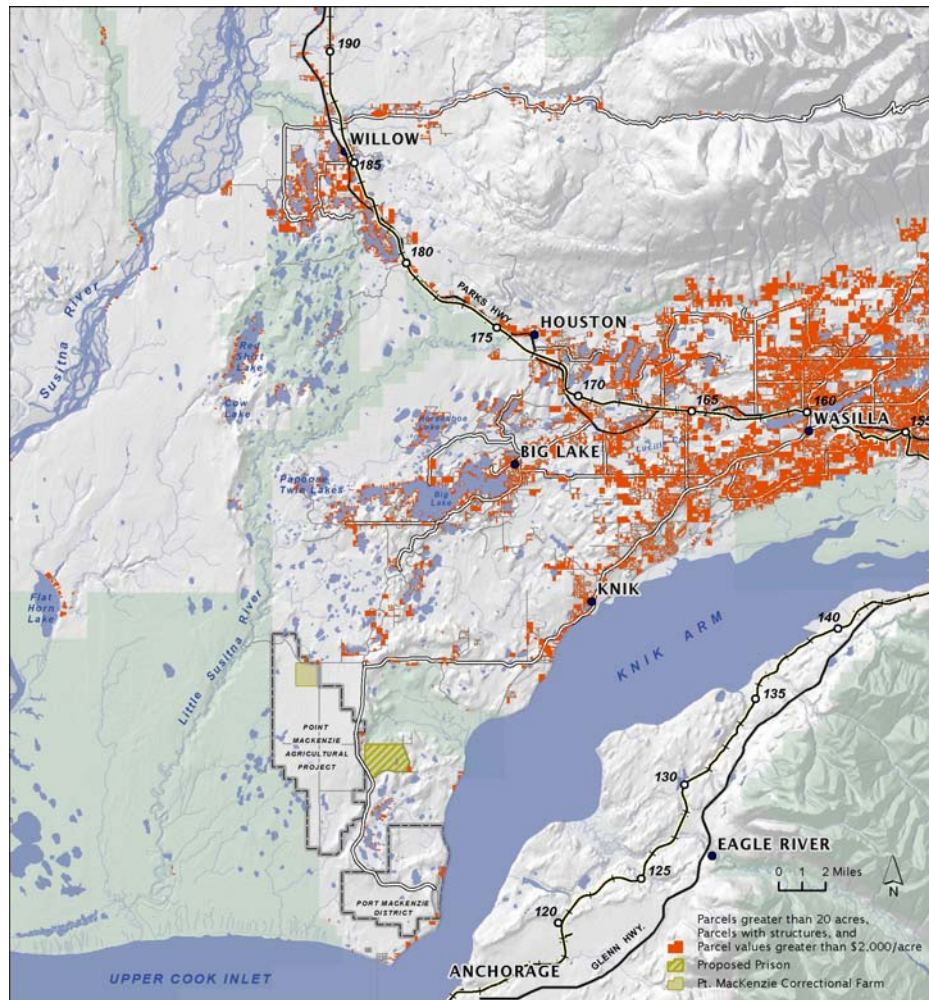
Avoiding densely populated areas, represented by developed parcels with a value greater than \$2,000 per acre, is important for the same reasons as for avoiding parcels of less than 20 acres in size. Land in more densely populated areas tends to be more expensive than land in more rural areas. In addition, there are also more people to be potentially impacted by the project in more densely populated areas.

The MSB Division of Assessment provided 2007 building and land values of the parcels considered for the constraints analysis. Using the provided tax values, the project team identified developed parcels as those with a MSB tax assessed structure value. From that dataset, the project team identified parcels less than 20 acres in size. The resulting information was mapped as a constraint. In addition, the project team calculated the value of the land per acre. The result of that calculation was used as a constraint instead of using the value of the parcel to account for different parcel sizes. The resulting parcels are depicted in Figure 2.8.

**Agricultural Lands**

Agricultural lands considered a constraint are those properties located within the Point MacKenzie Agricultural Project. While the area's designation as an agricultural project does not confer special status on these parcels beyond the parcel's agricultural restrictions, the area is the largest contiguous agricultural area in Alaska. As a result, preserving the commercial agriculture viability of this area is important to the DOA (Steve Trickett, pers. comm.). Easements specifically reserved for railroad development exist throughout the agricultural area; however, these easements are dis-contiguous and generally cut through the middle of the aerable land. The potential need for multiple crossings and the dis-contiguous nature does not constitute a practicable corridor.

Using information regarding agricultural lands provided by the DOA and the Knik Arm Crossing Draft Environmental Impact Statement (KABATA 2006a), the project team mapped the boundary of the Point MacKenzie Agricultural Project. The resulting map is shown in Figure 2.8.



**Figure 2.8 Developed Parcels, Land Value, and Agricultural Project**

### **Parks, Refuges, and Recreation Sites**

State parks and wildlife refuges represent important public recreation and wildlife resources. These public lands were designated for primary purposes ranging from protecting fish and wildlife habitat to providing public recreation opportunities.

The primary parks, refuges, and recreation sites in the project vicinity include:

- Willow Creek SRA
- Nancy Lake SRA
- Little Susitna Recreation River
- Susitna Flats SGR
- Goose Bay SGR

Development within these areas is generally incompatible with their primary purposes. Impacting these properties could result in Section 4(f) and 6(f) impacts. Section 4(f) of the Department of Transportation Act of 1966 (now re-codified as 49 U.S. Code 303 but still commonly referred to as Section 4(f)) provides protection for publicly-owned parks, recreation areas, historic sites, wildlife, and/or waterfowl refuges from conversion to a transportation use. A U.S. Department of Transportation (USDOT) agency may not approve the use of land from a

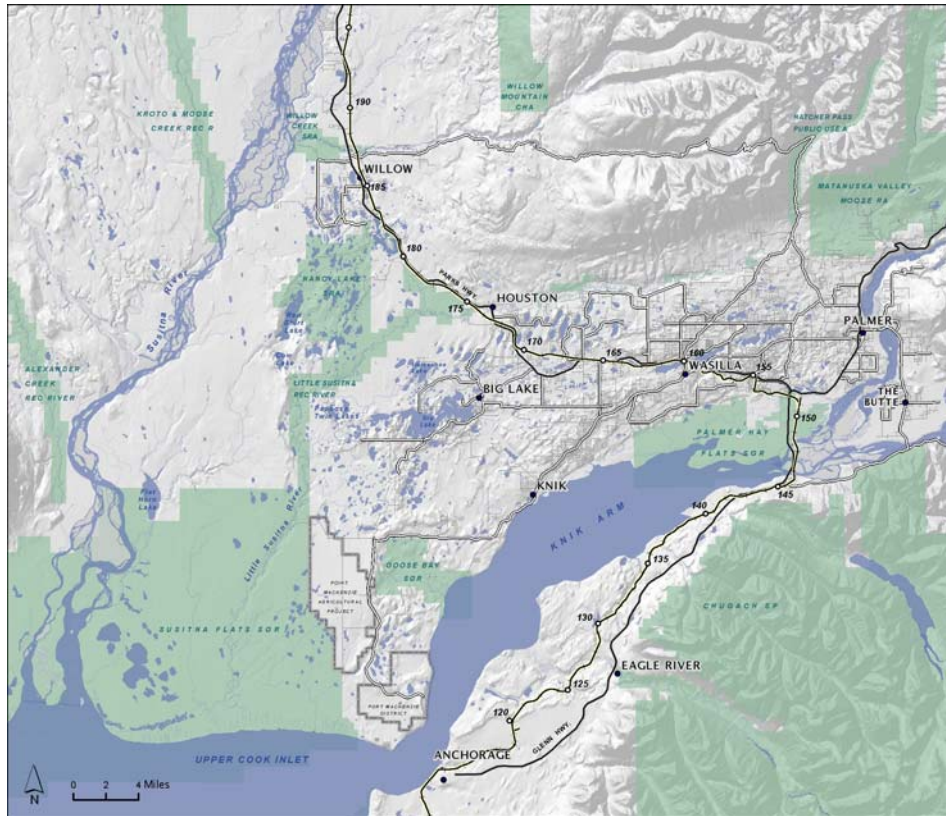
significant publicly-owned park, recreation area, or wildlife and waterfowl refuge or any significant historic site unless the following determination is made:

- There is no feasible and prudent alternative to the use of land from the property.
- The proposed action includes all possible planning to minimize harm to the property resulting from such use.

The STB is not subject to Section 4(f) restrictions; however, it is possible that an agency subject to Section 4(f) may contribute funding to the project in the future, resulting in the need to comply with Section 4(f). The project team would like to preserve the opportunity for other funding sources by including Section 4(f) properties in this report.

Section 6(f) refers to properties developed under the Land and Water Conservation Fund (LWCF) Act. The LWCF Act prohibits the conversion of properties acquired or developed with these funds to a non-recreational use without the approval of the National Park Service. To allow ARRC to convert this type of property, replacement land of equal value, location, and usefulness must be provided. Within the project area, the Nancy Lake SRA and the Big Lake North and South State Recreation Sites were identified as Section 6(f) properties (Kristi Gray, pers. comm.).

The project team used information from the ADNR Administrative Large Parcel dataset to identify state parks and refuges in the project area. Parks and refuges are shown in Figure 2.9.



**Figure 2.9 Parks, Refuges, and Recreation Facilities**

**Prison Facilities**

One of the most challenging community facilities to site is a correctional facility due to its security and space requirements as well as public issues. The MSB, in collaboration with the

Alaska Housing Finance Corporation and the ADOC, recently completed a site selection process to identify the location for a new MSB medium security prison. The selected site is in the project area near the existing Point MacKenzie Correctional Farm. The location of the proposed prison was a very contentious issue for the State and MSB and required a great deal of public involvement to resolve. Therefore, it was deemed important to avoid the selected prison site.

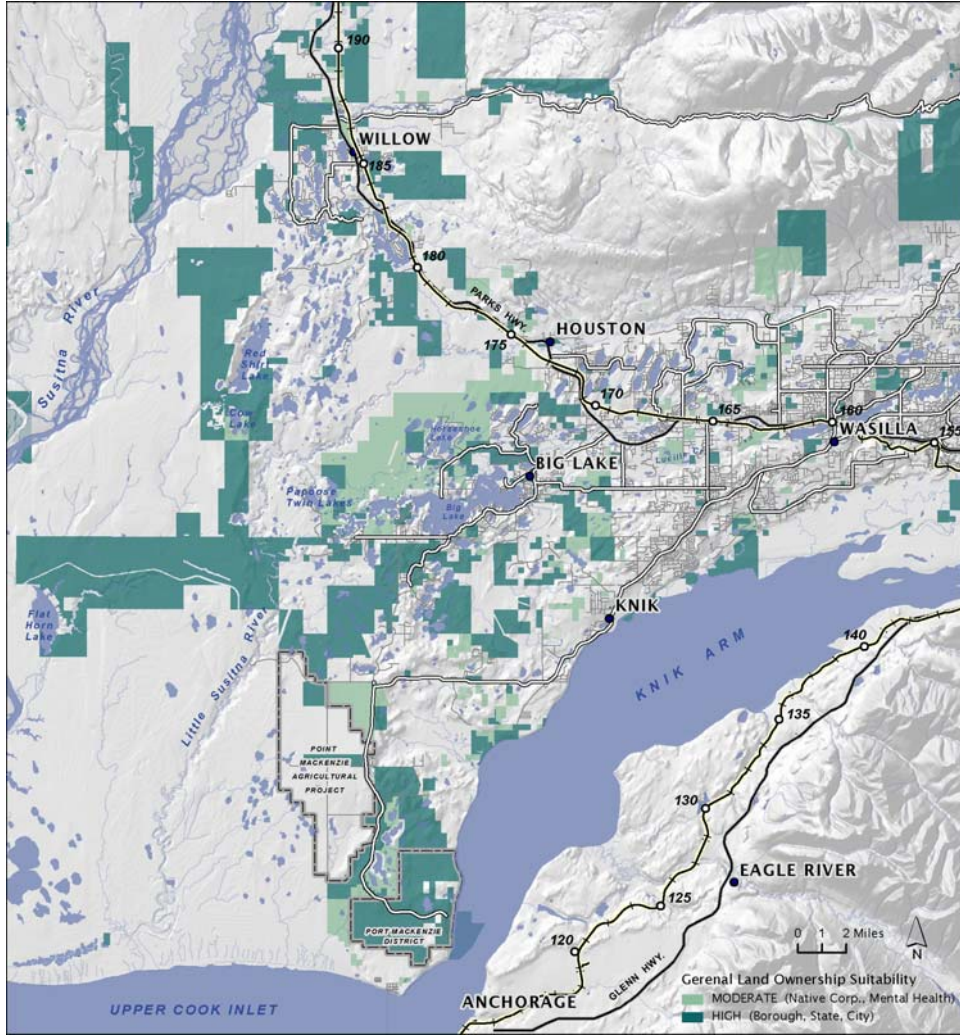
The location information for the existing Point MacKenzie Correctional Farm came from the MSB GIS dataset. The location of the proposed prison was obtained from the MSB and was located in GIS by the project team (Figure 2.8).

### **Land Ownership**

Land ownership can be viewed as both an opportunity and a constraint because the motivation for owning land can vary. Some entities own land with the intent to make a profit from the development or sale of that land. For example, the State's Trust Land Office, which manages The Trust land, and the University of Alaska generally manage their lands to derive income to support their organizations. Land owned by Native corporations, The Trust, the University of Alaska system, or a government agency (excluding land designated as a State park, recreation area, or game refuge) were considered an opportunity. In addition, government-owned land tends to consist of large parcels (see the Public Involvement Activities Summary (Vol.4) for description of stakeholder meetings). Buying land from a few owners is preferable to buying small amounts of land from multiple land owners because it simplifies the ROW acquisition process. Institutions that have lands for the primary purpose of generating income tend to be more willing sellers compared to private owners.

Information on land ownership was obtained from the MSB GIS dataset. The dataset listed all State-owned land as the same code. The project team further refined the State-owned land based on the ADNR Administrative Large Parcels dataset to remove State land that was previously designated for another purpose, such as state recreation areas and refuges. Land ownership opportunity is depicted on Figure 2.10.

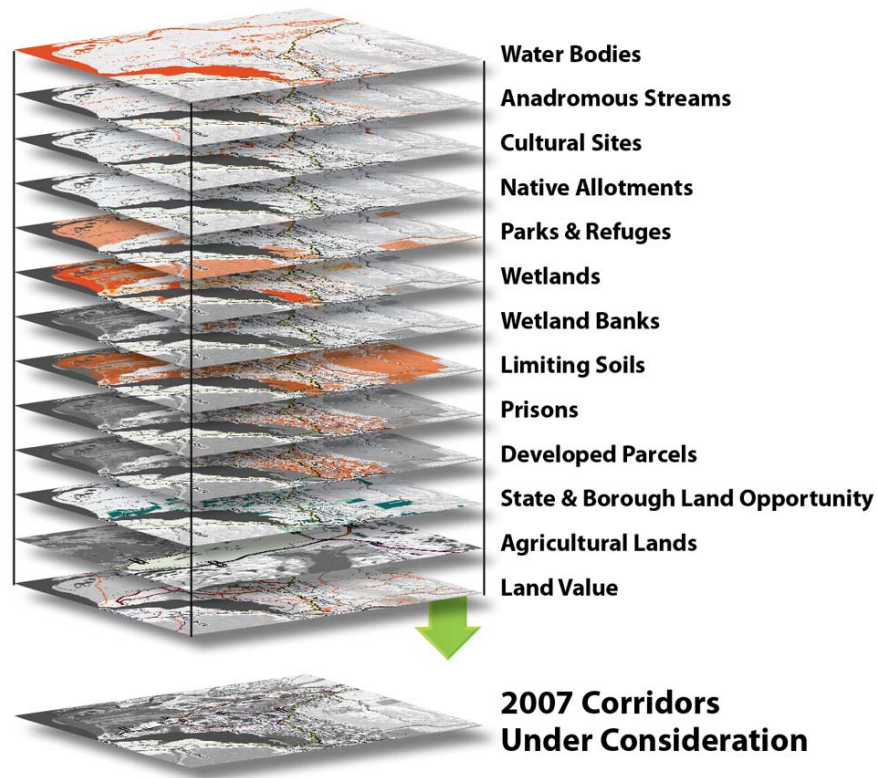




**Figure 2.10 Land Ownership (Large Landowners)**

**Composite Map**

After each factor was added to the GIS, the layers were combined to create a composite map showing the constraints and opportunities for a new rail corridor. The process is summarized in Figure 2.11. The composite map, with corridors identified by the project team, is shown in Figure 2.12.

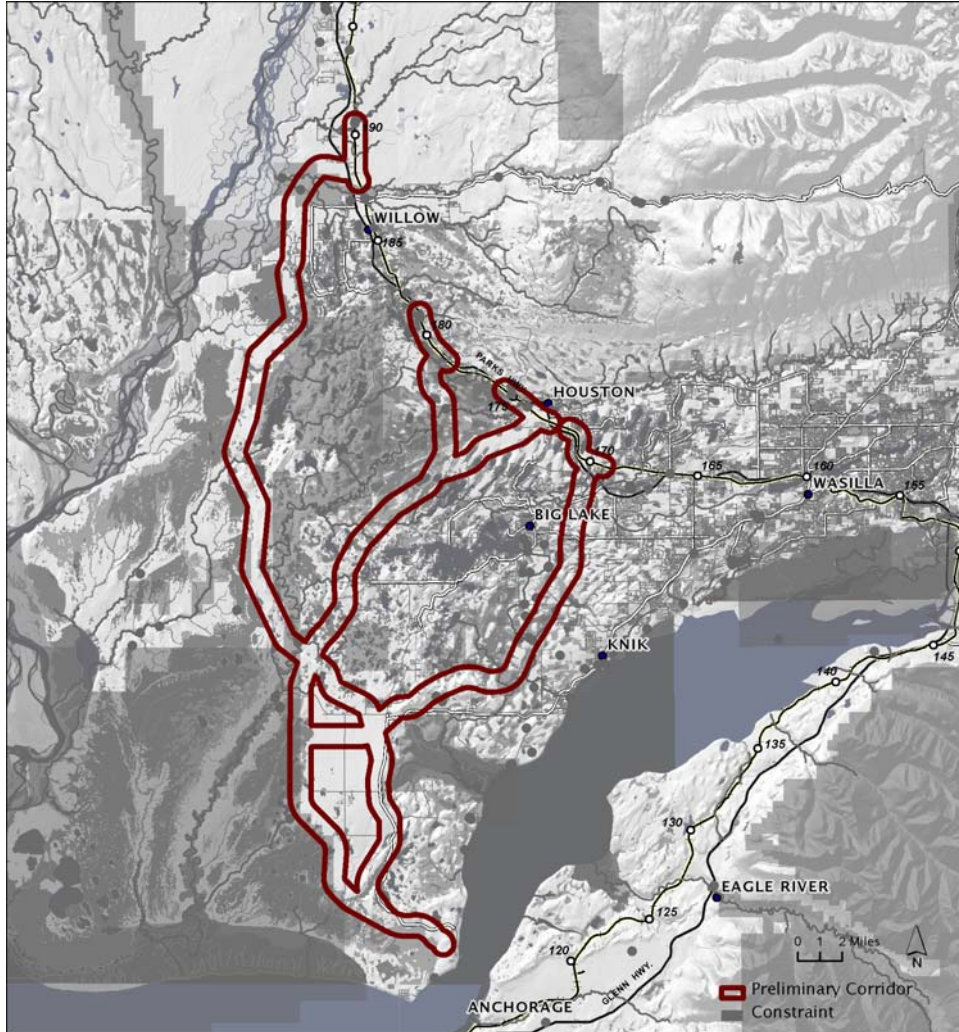


**Figure 2.11 Summary of Composite Map Development Process**

### **2.3 Constraints Analysis**

The purpose of conducting the constraints analysis was to determine corridors that would best avoid and minimize potential impacts to the environment. The composite constraints were combined and displayed digitally in GIS. White or lightly colored areas in the GIS depicted areas of least overall constraint that were identified as the best locations to overlay 5,000-foot-wide corridors. The 5,000-foot corridor width was chosen as an initial rough-order-of-magnitude width to enable the alignments to be shifted based on the given constraints and areas where generally there would be the least impact to the environment.

Based on this analysis, in the southern portion of the project area, the least constrained corridors were located on the east and west sides of the Point MacKenzie Agricultural Project. In the northern portion of the project area, three general corridors of least constraint appeared—a western corridor leading to Willow, a southeastern corridor south of Big Lake, and a central corridor leading towards Houston (Figure 2.12).



**Figure 2.12 Composite Constraints Map and Preliminary Corridors**

Upon reviewing the constraints within the 5,000-foot-wide corridors, the area of potential impact was assessed using a 200-foot-wide corridor centered upon an engineered route. Whereas the 2003 study considered an 800-foot-wide corridor, which enabled co-location of both road and rail, a 200-foot wide corridor was chosen for this project because only rail was considered. A 200-foot-wide corridor would need to be reserved to ensure the ARRC has a 200-foot ROW for the track and associated features, such as sidings and access roads. Plans for the eventual development of a parallel roadway within the corridor have not been advanced by the MSB, and are not expected to be advanced further in the foreseeable future.

The corridors were refined into alignment alternatives with a 200-foot-wide ROW through an iterative process using GIS and engineering considerations. GIS layers of environmental constraints were used to avoid and minimize potential impacts where possible and preliminary engineering criteria were used to further refine the alignments to meet project design and operation requirements.

The alignments resulting from this process minimize potential environmental impacts to the extent possible while meeting preliminary engineering criteria. Final alignment will continue to be refined as the NEPA and design processes evolve.

## 3.0 Description of Alternatives

### 3.1 Past Corridors

As summarized in Section 1.5, the 2003 rail corridor study (MSB 2003a) identified 11 potential corridors between Port MacKenzie and the existing rail mainline. The corridors generally adhered to the premise of preserving an 800-foot wide corridor to incorporate sufficient ROW for the railroad and periodic sidings, a four-lane divided highway, utilities, and bicycle pathways. Figure 3.1 depicts the 11 corridors.

For the 2003 effort, a constraints analysis was conducted to identify alternatives that may need to be adjusted or eliminated based on the concerns identified from the public meeting: private property and wetlands. Corridors that affected large amounts of private property and wetlands were eliminated and other corridors were adjusted to avoid these areas where feasible. Several similar corridors were combined to create more desirable routes. Table 3-1 lists the eleven original corridors studied in 2003 and whether they were eliminated from further study, combined with other alternatives, or adjusted and retained.

Corridor 3, leading to a junction with the existing ARRC mainline just north of Willow, received a considerable amount of support from the public and the MSB as an 800-foot-wide combined railroad, expressway, and utility corridor. Corridor 7 was designed to be a roadway-only corridor and utilized existing planned improvements by the MSB. The outcome of this study was the recommendation of further study of Corridors 3 and 7 as separate corridors for rail and road access to the port. This recommendation was supported by a resolution of the MSB Assembly.

**Table 3-1: Corridors Considered in the 2003 MSB Study**

<b>Corridor</b>	<b>Description</b>	<b>Report Recommendation</b>
Corridor 1	Corridor 1 was a combination of a rail/roadway corridor that extended west from Point MacKenzie, skirted the east boundary of the Susitna Flats SGR, and then turned west across the northern boundary of the refuge. Near the Susitna River and the community of Susitna, the corridor followed the east bank of the Susitna River and connected with the Parks Highway Corridor north of Willow Creek.	Eliminated. This was the longest alternative and traversed a considerable amount of wetlands.
Corridor 2	Corridor 2 was a combined rail/roadway corridor. This corridor was the same as Corridor 1 from Point MacKenzie north to the crossing of the Little Susitna River near the northeast corner of the Susitna Flats SGR. From there, it turned north and followed a glacial moraine west of Red Shirt Lake and then tied back into Corridor 1, north of Rolly Creek.	Eliminated. Corridor 2 was eventually combined with the modified Corridor 3 because they were close together.
Corridor 3	Corridor 3 was initially a rail/roadway corridor, with a westerly extension of the end of the Little Susitna River access road into the northeast corner of the Susitna Flats SGR before turning north. From there, it followed the glacial moraine traveling west of Red Shirt Lake and skirting west of the boundary of the Nancy Lake SRA, tying back in Corridor 1 before crossing Willow Creek. Corridor 3 was modified to shift west as it left the port area (near but outside of the Susitna Flats SGR boundary). It then extended north to cross the Little Susitna River, following a moraine deposit north on a line west of Red Shirt Lake, and the boundary of the Nancy Lake SRA, crossing Willow Creek and connecting with the Parks Highway/ARRC corridor north of Willow Creek.	Retained. Much of the corridor is public land, with some private land, mainly immediately north of Point MacKenzie and near Willow Creek. This corridor received considerable public support as the rail corridor, and there were numerous public and agency comments recommending inclusion of a roadway in the corridor.
Corridor 4	Corridor 4 was a combined rail/roadway corridor. It left the port area in a westerly direction, passing into the Susitna Flats SGR before turning north around Middle Lake. From there, it passed between Crooked Lake and the Papoose Twin Lakes, northwest of Horseshoe Lake and across a boggy area to connect with the Parks Highway corridor at Houston. This corridor, as originally defined, appeared to have the largest impact on wetlands and encroaches on the Susitna Flats SGR.	Eliminated. This alternative was modified from the original placement to avoid impact to the Susitna Flats SGR and minimize impact on private property; however, this alignment would still have the largest impact on wetlands and did not receive public support.

Corridor	Description - Continued	Report Recommendation
Corridor 5	This rail/roadway corridor extended west from the port area about four miles, then turned north along a section line through the Point MacKenzie Agricultural Project. The corridor continued west of Carpenter Lake and Diamond Lake before passing between Crooked Lake and Flat Lake, and then between Big Lake and Horseshoe Lake. It continued north of Beaver Lakes to meet the Parks Highway corridor south of Houston.	Eliminated. This corridor passed through a large amount of private land. It had limited public support and was excluded from further consideration.
Corridor 6	This rail/roadway corridor left the port area following the existing Point MacKenzie access road north to the Little Susitna River access road. It continued to the north on the east side of Carpenter Lake, along Burma Road, passed across the isthmus between Big Lake and Flat Lake, and tied back into Corridor 5 south of Horseshoe Lake. The corridor then followed Corridor 5 onto the Parks Highway corridor south of Houston.	Eliminated. This corridor passed through a large amount of private property and had limited public support.
Corridor 7	This rail/roadway corridor was initially coincident with Corridor 6 from the port north to the Little Susitna River access road. It then followed a slightly different route than Corridor 6 to a point just north of the South Big Lake Road, where it reconnected with and followed Corridor 6 to the Parks Highway. The corridor was then modified to be roadway only and realigned portions of the Burma Road to connect with the South Big Lake Road. It then followed South Big Lake Road east through the community of Big Lake to connect with the Parks Highway. This corridor was presented as roadway only; it was deliberately designed to take advantage of roadway improvements under design and/or ROW acquisition.	Retained. This would be roadway only access, with the least private property impacts, limited wetlands impacts, and the least construction costs of any build alternative. Selection of this alternative eliminated the need for an entire new roadway corridor.
Corridor 8	This roadway-only corridor was coincident with Corridors 6 and 7 from the port north to South Big Lake Road. It then followed South Big Lake Road easterly around the south side of Big Lake, and through the community of Big Lake. It continued four miles to a connection with the Parks Highway corridor.	Eliminated. This corridor was determined to be similar to the modified Corridor 7 and previously identified for improvements by the MSB.
Corridor 9	This rail/roadway corridor was coincident with Corridors 7, 8, and 11, leaving the port area and following the Point MacKenzie access road north to the Little Susitna access road. From that point, Corridor 9 went to the northeast and was positioned roughly half-way between Corridor 8 and Corridor 10. The corridor connected with the Parks Highway corridor at Pittman Road.	Eliminated. There was public concern about passing through a large amount of private property.

**Figure 3.1  
PAST CORRIDORS**



**LEGEND**

2003 Corridors

- Corridor 1
- Corridor 2
- Corridor 3
- Corridor 4
- Corridor 5
- Corridor 6
- Corridor 7
- Corridor 8
- Corridor 9
- Corridor 10
- Corridor 11

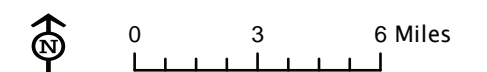
Recommended Corridor 3 (2003)

2007 Corridors

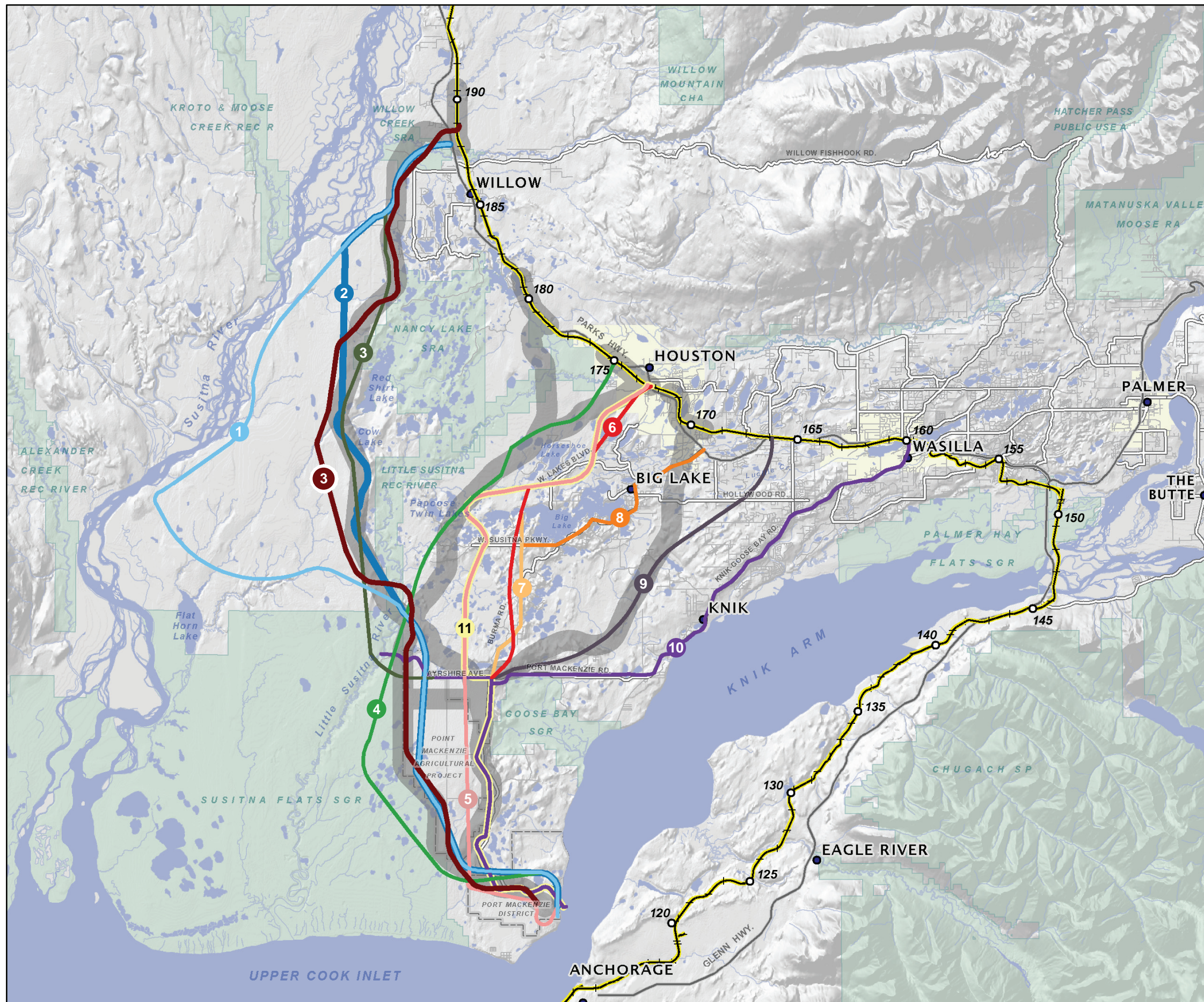
- ARRC Milepost
- ARRC Track
- Highway
- Medium Rd.
- Minor Rd.
- City Boundary
- Park or Refuge

\*These lines generally represent corridors which are subject to further refinement.

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: January 7, 2008  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.



*(This page intentionally left blank.)*



Corridor	Description – Continued	Report Recommendation
Corridor 10	This road-only corridor followed the Point MacKenzie access road and Knik-Goose Bay Road to the Parks Highway in Wasilla. This corridor was carried forward as the No Build alternate in that it is the current access to Port MacKenzie and would continue in that role if not other action were taken. This facility has the capacity to handle the projected increases in traffic generated by Port MacKenzie and is already programmed for improvements by the ADOT&PF and by the MSB.	Retained as the no build alternative. However, this corridor draws additional freight traffic into the urban Wasilla area and results in an increase in miles traveled for traffic with an origin or destination north of Wasilla.
Corridor 11	This rail/roadway corridor is not new but combined portions of Corridors 5 and 6. This corridor was approved by the MSB assembly in 1992.	Eliminated. This corridor passed through a large amount of private property.

Source: MSB 2003a

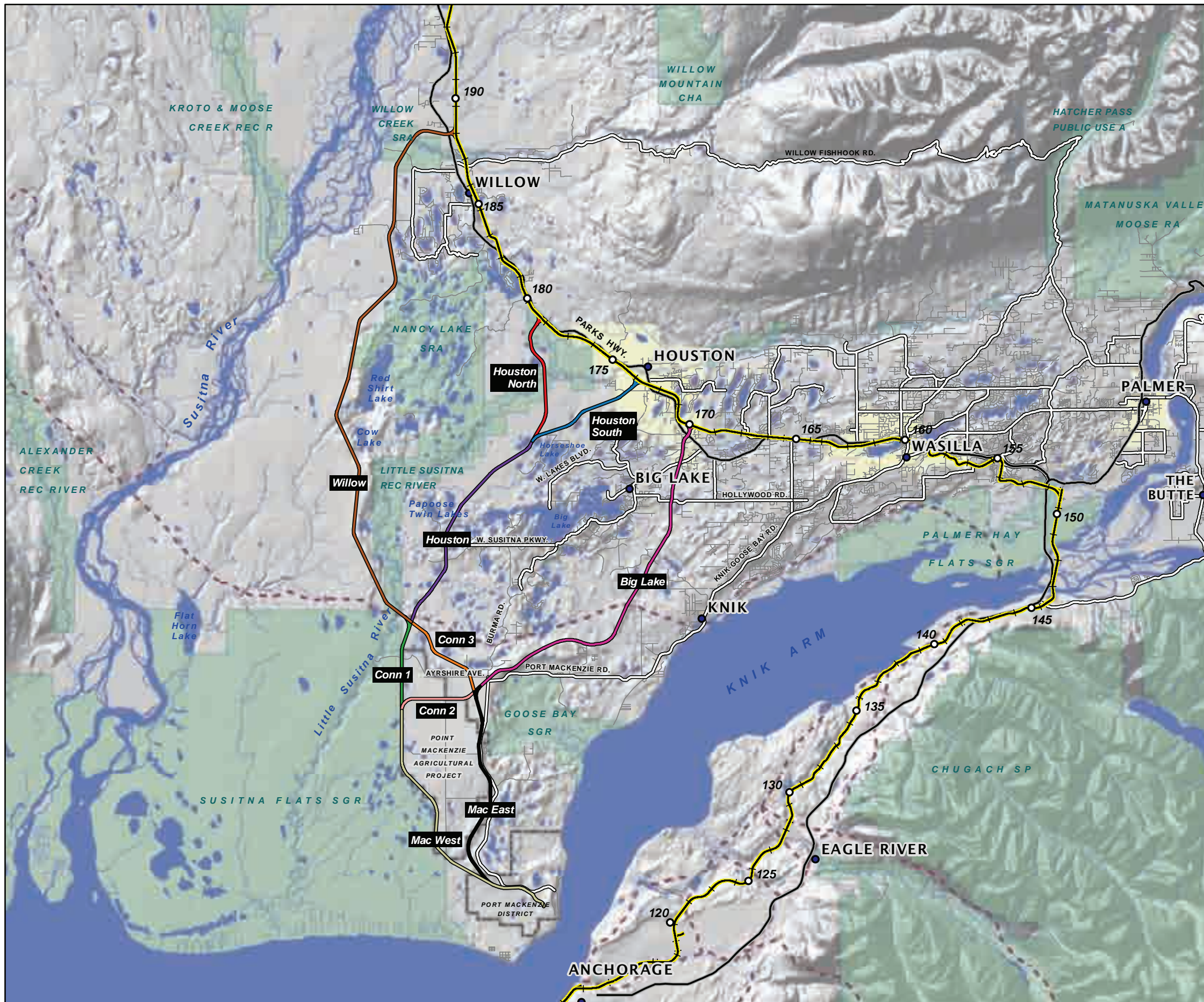
### 3.2 Alternatives under Evaluation

Following the 2007 constraints analysis, the alternatives considered in 2003 were re-evaluated to confirm that the findings from the 2003 report were still relevant, and that the conditions had not changed to make one or more of the alternatives previously eliminated more preferable. The 2003 corridors were also evaluated to ensure that there were no other alternatives that should be considered that had been overlooked in the earlier study.

Alternative corridors were developed based on reviewing the 2003 corridors, the constraints analysis, and engineering and environmental considerations described in Sections 1 and 4. Current alternatives are shown on Figure 3.1 with the alternatives considered in the 2003 study for comparison purposes. All the corridors start at Port MacKenzie at the south and connect to the existing mainline to the north. Each corridor is composed of a southern and northern segment with a possible connector tying the segments together. The southern segments run either east or west of the Point MacKenzie Agricultural Project. Just north of the agricultural area, there are three main northern segments – Willow, Houston, and Big Lake – with Houston having a north or south variant. Connectors link the north and south segments together to create eight possible alignment configurations, as listed below and depicted on Figure 3.2.

1. **Mac West – Connection 1 – Willow.** *This alignment would be 44.8 miles long. It is one of the longest alignments and contains the segments farthest west.*
2. **Mac West – Connection 1 – Houston – Houston North.** *This alignment would be 35.1 miles long, is one of the shorter alignments, and is geographically one of the middle alignments.*
3. **Mac West – Connection 1 – Houston – Houston South.** *This alignment would be 34.5 miles, is one of the shorter alignments, and is geographically one of the middle alignments.*
4. **Mac West – Connection 2 – Big Lake.** *This alignment would be 35.8 miles. It includes the southern segment along the west side of the Point MacKenzie Agricultural Project and the most eastern north segment going towards Big Lake.*
5. **Mac East – Connection 3 – Willow.** *This alignment would be 45 miles and is the longest. It includes the southern segment along the east side of the Point MacKenzie Agricultural Project and the most western north segment going towards Willow.*

**Figure 3.2**  
**ALTERNATIVES UNDER EVALUATION**



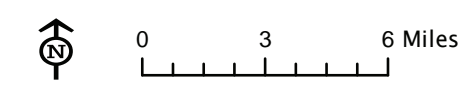
**LEGEND**

- |                           |                |
|---------------------------|----------------|
| Preliminary Alternatives* | ARRC Track     |
| Mac East                  | ARRC Milepost  |
| Mac West                  | Highway        |
| Conn 1                    | Medium Rd.     |
| Conn 2                    | Minor Rd.      |
| Conn 3                    | Iditarod Trail |
| Houston                   | City Boundary  |
| Houston North             | Park or Refuge |
| Houston South             |                |
| Willow                    |                |
| Big Lake                  |                |



\*These lines generally represent corridors which are subject to further refinement.

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: November 5, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

*(This page intentionally left blank.)*

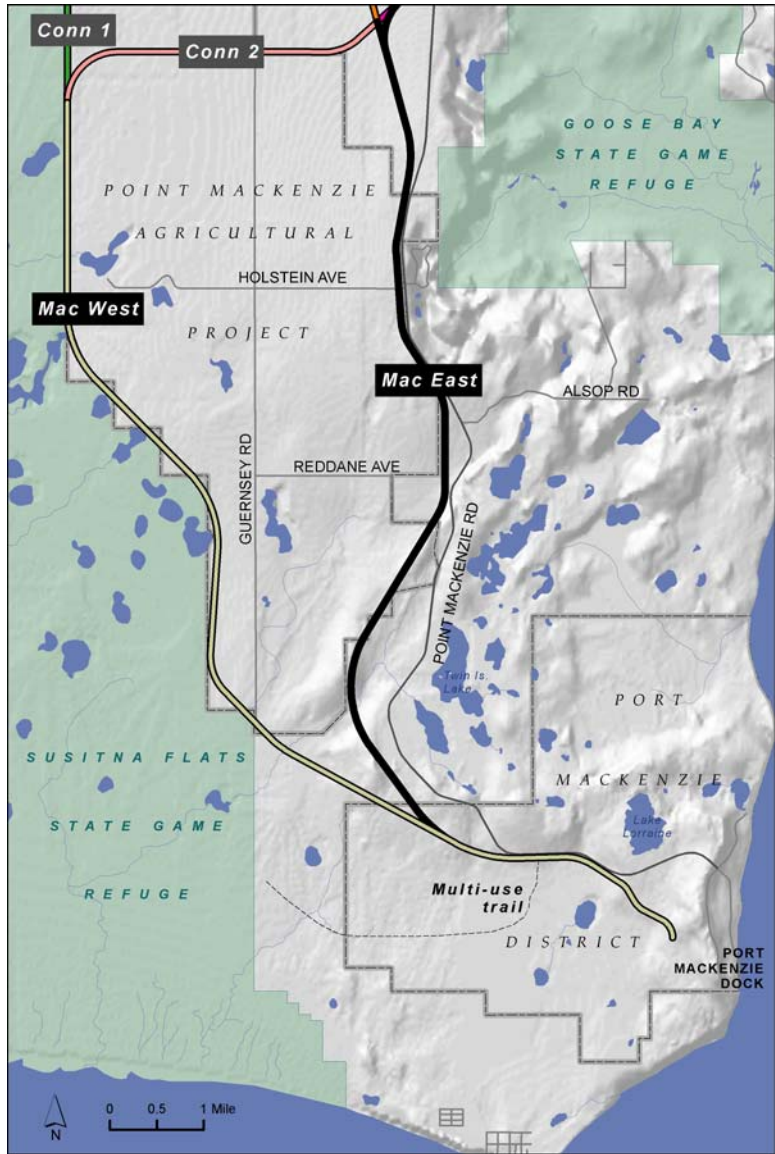
6. **Mac East – Connection 3 – Houston – Houston North.** *This alignment would be 35.3 miles, is one of the shorter alignments, and is geographically one of the middle alignments.*
7. **Mac East – Connection 3 – Houston – Houston South.** *This alignment would be 34.7 miles long, is one of the shorter alignments, and is geographically one of the middle alignments.*
8. **Mac East – Big Lake.** *This alignment would be 31.8 miles long and is the shortest alignment. It includes the southern segment along the east side of the Point MacKenzie Agricultural Project and the most eastern north segment going towards Big Lake.*

The alignments consist of a 200-foot wide ROW to allow for the track, periodic sidings, and a railroad maintenance access road. Additional engineering criteria used to develop the alignments are presented in Section 2.1. Conceptual-level plans have been developed for each of the alternatives. Plan sets are included in Volume 2.

### 3.2.1 South Segments

#### **Port MacKenzie Terminal Area**

The Mac West and Mac East segments have a common point of beginning, which is slightly less than one mile northwest of the Port MacKenzie dock facility as seen in Figure 3.3. In the terminal area, for the first 2.7 miles, both east and west segments would follow the same alignment. For approximately one mile, the track would curve north on an approximate 0.5% descending grade around existing port district development and would serve as a switching lead, where cars or sections of cars should be moved or switched among the terminal yard sidings. The ARRC's terminal yard at the Port would be located approximately between the first and third miles of the track, roughly parallel to and 150 feet south of the existing Port MacKenzie Road. The initial construction terminal would consist of the main track and two 8,000-foot siding tracks. Construction of a level terminal yard would require fills of approximately 10 to 25 feet deep. An insulated metal building, approximately 80-feet by 100-feet in size with a loading dock, would be constructed at the terminal site to facilitate rail line and equipment maintenance and train crew assignments. An existing MSB recreational trailhead near the end of the first mile of track would be relocated west of the terminal area. At approximately Milepost (MP) 2.7, the common Mac West and Mac East segments would separate.



**Figure 3.3 Mac East and Mac West Segments**

**Mac West**

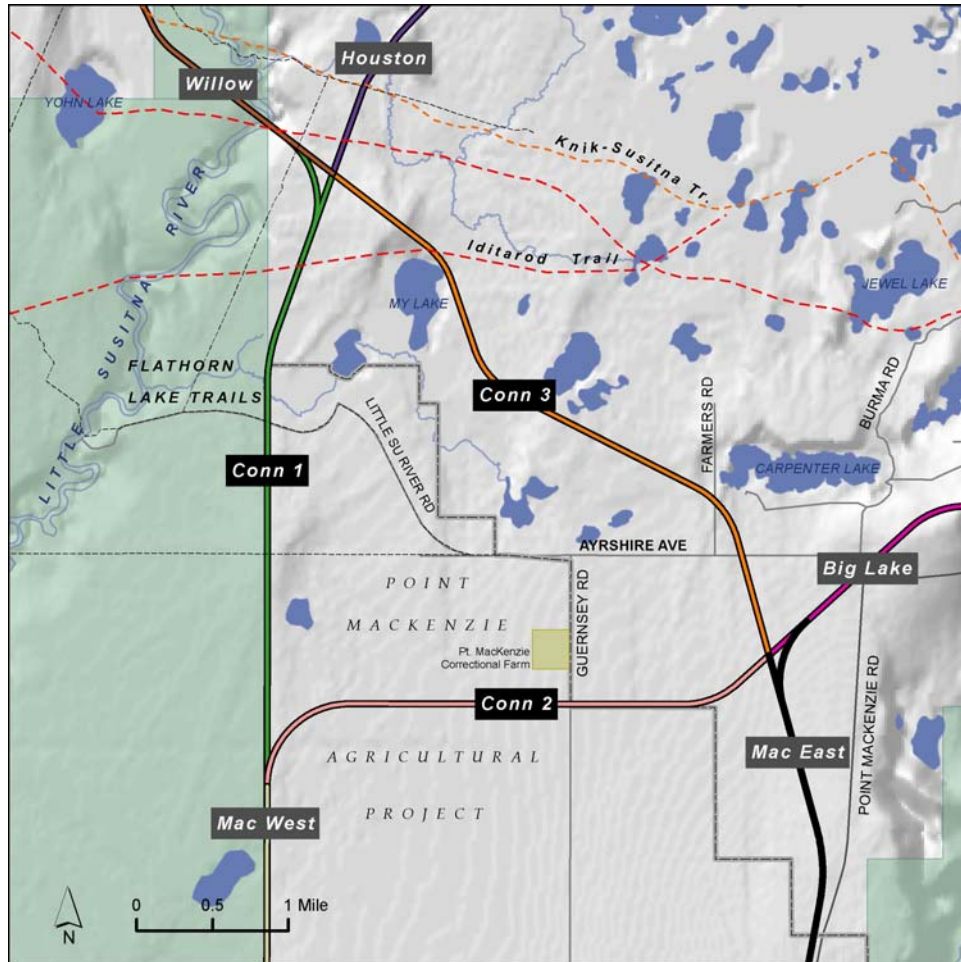
From MP 2.7, the Mac West segment would proceed northwesterly across relatively flat terrain towards the southwest corner of the Port MacKenzie Agricultural Project. The corridor continues west of the agricultural area traversing along the eastern boundary of Susitna Flats SGR. Minimal fill would be needed to elevate the track structure above the gently rising and falling terrain.

**Mac East**

From MP 2.7, the Mac East segment turns northward, gaining elevation on a 0.4% grade along the side of a ridge along the east side of the agricultural area. Near ME MP 4.7 the segment crosses a ravine that would require approximately 25 feet of fill, and then curves to the northeast along the top of another ridge. North of MP 6, the segment approximately follows the alignment of Port MacKenzie Road, offset 200 feet or more to the west. The track continues along undulating terrain before reaching the junction of the Big Lake segment or Connector 3 segment.

### 3.2.2 Connectors

Three connectors were developed to transition from the south segments to the north segments (Figure 3.4).



**Figure 3.4 Connection Segments**

#### Connector 1

This 4.1 mile segment is used to connect Mac West to the Willow or Houston segments. From Mac West, this segment would continue northward, with a slight descending grade, skirting the eastern boundary of the Susitna Flats SGR on level terrain. The segment crosses a tributary of the Little Susitna River. Approaches to the tributary stream along this alignment would require substantial fills approaching 45 feet in height. The final mile of the segment ascends at 0.4% grade with minimal cut and fill.

#### Connector 2

This 3.7 mile segment would connect Mac West to the Big Lake segment. At the northwestern end of the agricultural area, this segment shifts completely eastward, and travels along the southern boundary of the Point MacKenzie Correctional Farm. This segment requires a minimal amount of fill and is elevated slightly above the gently rising terrain.

### **Connector 3**

This 4.5 mile segment is used to connect Mac East to the Willow or Houston segments. At the northeastern end of the agricultural area, the alignment shifts northwestward and crosses two roads. The segment would require minor cuts and occasional fills up to approximately 25 feet high to descend the rolling terrain on a 0.5% grade. The segment goes north of My Lake, crossing an adjacent ravine that would require fills approximately 45 feet in height. The remaining mile of the segment is nearly level.

#### *3.2.3 North Segments*

##### **Willow**

From Connector 1 or 3, the Willow segment (Figure 3.5) descends on a 0.3% grade over the first mile, crossing the Little Susitna River with a bridge approximately 55 feet above the water. There are two conceptual options for bridge configurations (Figures 3.6 and 3.7). Over the next seven miles, the segment continues generally north, ascending on grades ranging up to 0.5% through rolling terrain that would require cuts and fills of up to approximately 35 feet. Fish Creek, the outlet for Red Shirt and Cow lakes, would be crossed with a bridge approximately 90 feet above the water. The segment would then proceed north, generally following the west facing slope of a glacial moraine that is located west of Red Shirt Lake. It continues northward staying west of the Nancy Lake SRA, and then descends off the glacial moraine. The segment would cross the outlet for Vera Lake, continue along rolling terrain requiring minor cuts and fills, and cross Willow Landing Road. The segment then crosses Willow Creek approximately 55 feet above the water and continues through the Willow Creek SRA. After crossing the Willow Creek valley, the segment curves to the east as it approaches the Parks Highway. The alignment would be grade-separated with the Parks Highway (road over railroad as conceptually depicted in Figure 3.9) before connecting to the existing mainline near existing ARRC MP 188.9. The alignment crosses a number of trails, including the Iron Dog Trail, the West Gateway Trails, the Nancy Lake-Susitna Trail, the Knik-Susitna Trail, the Crooked Lake Trail, and the INHT.

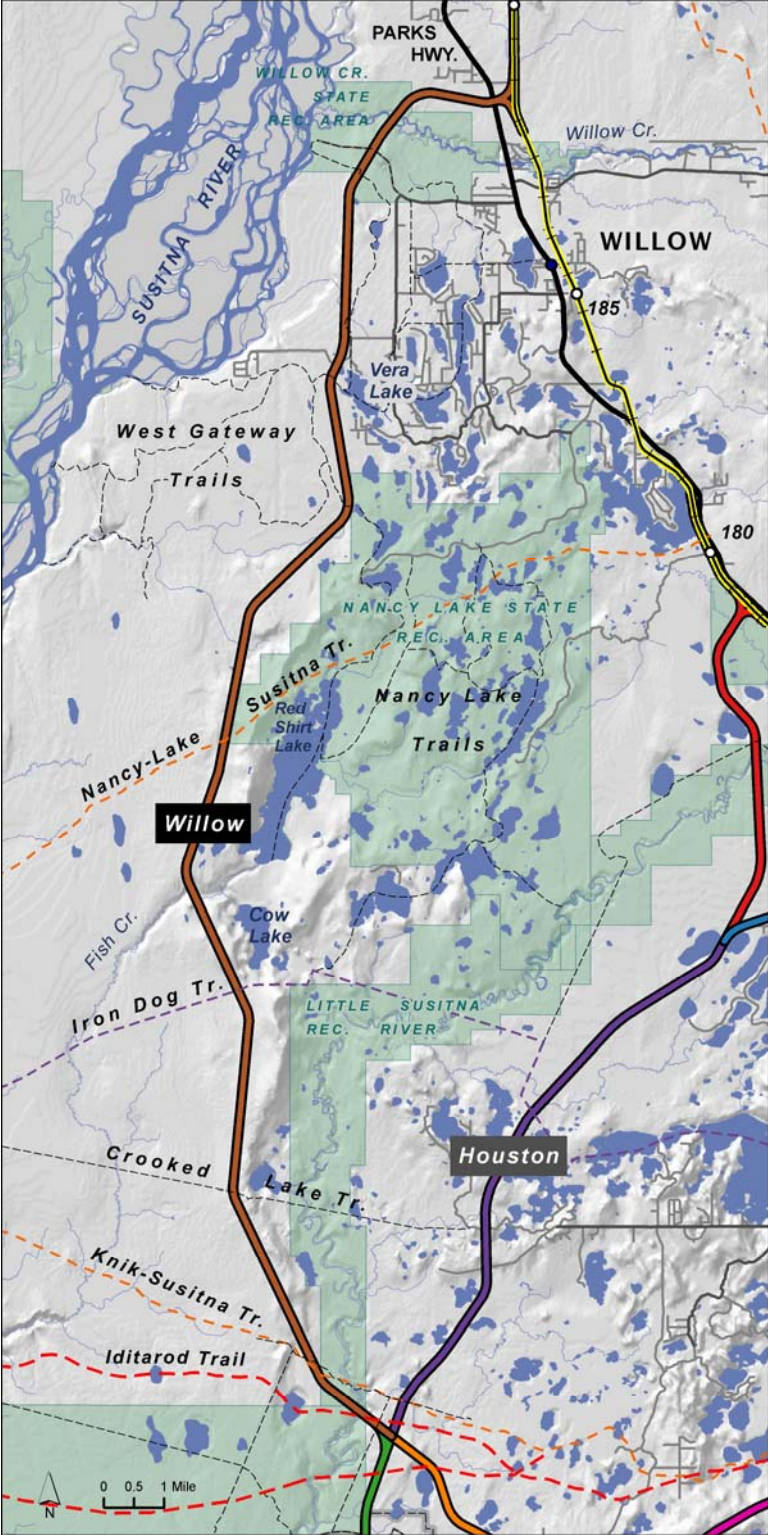


Figure 3.5 Willow Segment



## Houston

From Connector 1 or 3, this segment proceeds northeastwardly, traveling through slightly undulating terrain with areas of marsh (Figure 3.6). The segment passes between the Papoose Twins Lakes and Crooked Lake, traversing an area of hilly terrain that would require fills of approximately 30 feet and cuts approximately 35 to 75 feet deep. The remaining four miles of the segment are located in a gradually rising marshy area to a point near Muleshoe Lake and Little Horseshoe Lake where it would break into two variant of options: Houston North and Houston South. Three anadromous streams are crossed. Several trails are crossed, including the INHT, the Knik-Susitna Trail, the Iron Dog Trail, and the Muleshoe Trail.

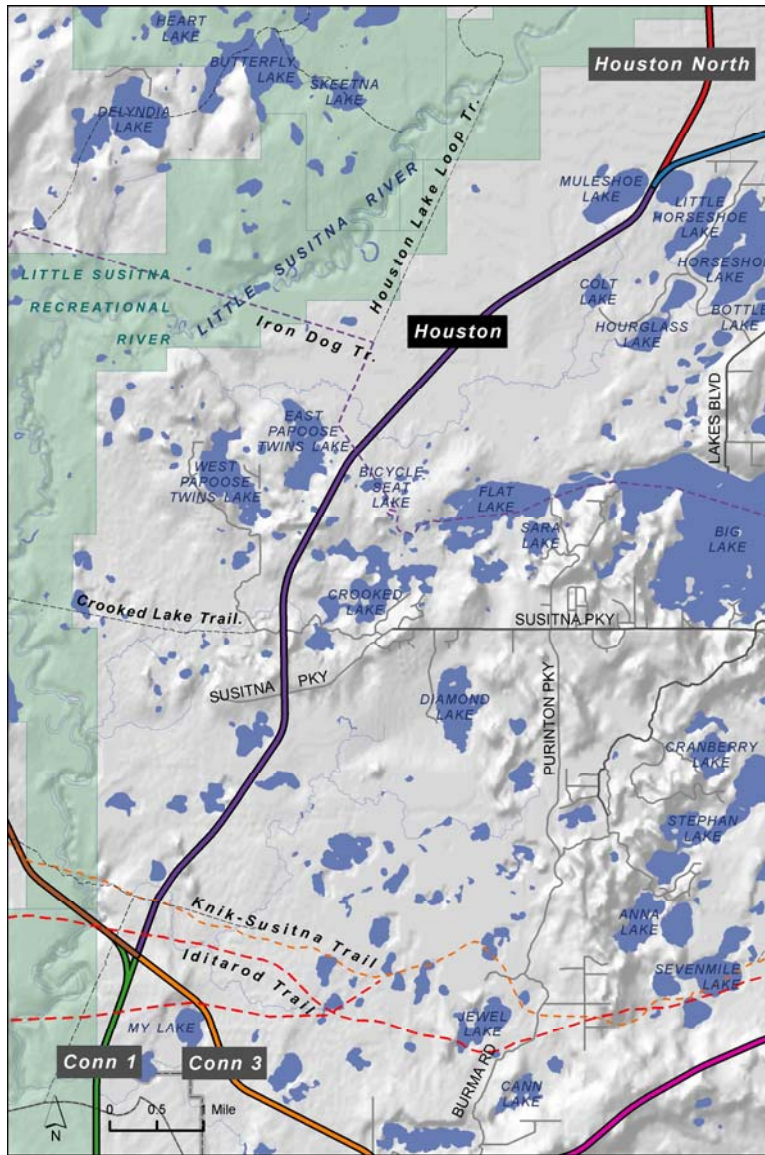


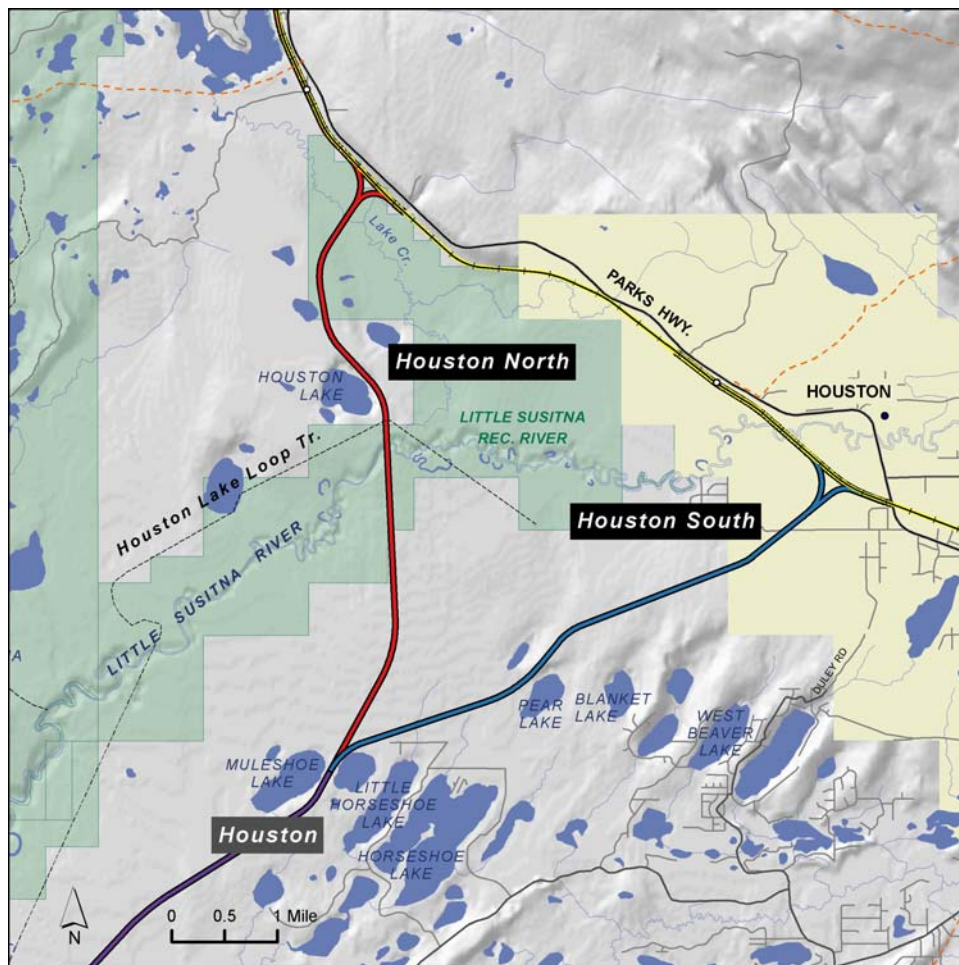
Figure 3.6 Houston Segment

**Houston North Variant**

Beginning between Muleshoe Lake and Little Horseshoe Lake, this variant of the alignment would traverse northward, crossing over the Castle Mountain Fault (Figure 3.7). This alignment would cross the Cow Lake Trail, which is part of the Houston Lake Loop Trail. The segment enters the Little Susitna Recreation River, and then crosses the Little Susitna River. The segment continues north on rolling terrain that would require two approximately 30 foot cuts, then passes between Little Houston Lake and Tiger Lake, descending gradually to the lower terrain adjacent to Lake Creek. The alignment ties in to the existing mainline near existing ARRC MP 179.3 without crossing the Parks Highway.

**Houston South Variant**

Also beginning between Muleshoe Lake and Little Horseshoe Lake, this proposed variant would traverse northeasterly, passing just west of Pear Lake (Figure 3.7). The segment traverses several gravel ridges that run parallel to the lakes in this area. The alignment crosses a number of trails including the Muleshoe Trail (crossed twice), Connecting Trail #2, the Houston Lake Loop Trail, and the Houston Power Line Trail. The alignment ties into the existing mainline near MP 174.0 without crossing the Parks Highway. The junction would be in the immediate vicinity of the Castle Mountain Fault.



**Figure 3.7 Houston North and South Segment**

## Big Lake

The southern end of the Big Lake segment begins from Mac West via Connector 2 or directly from Mac East. At the northeast corner of the agricultural area, this segment runs northeast for approximately 3 miles, crossing Burma Road and contouring around lower terrain to the southeast (Figure 3.8). The segment continues on rolling terrain towards Goose Creek. This segment continues down the north face of a ridge on a curving alignment, passing north of a residential area near Goose Creek Road. The segment continues along rolling terrain, crossing over Goose Creek, Fish Creek, Lucille Creek and tributaries of Lucille Creek, and Little Meadow Creek. From Burma Road to Big Lake Road the alignment would require cuts of up to approximately 60 feet and fills up to 40 feet to maintain a grade at or below 0.5%. The Big Lake Road Crossing would be grade-separated, rail above road. The segment continues northward through a residential area before crossing under the Parks Highway. Hawk Road, the connection from the Parks Highway to Houston Middle and High Schools, would be realigned away from the grade separation. The Big Lake segment connects with the existing main line near existing ARRC MP 170.3 in a marshy area surrounding a creek that feeds into Long Lake.

This alignment has many anadromous stream, road, and trail crossings. There are 10 anadromous stream crossings including the four named crossings. Some of the trail crossings include Knik-Susitna, Herning, Three Mile, and the INHT. The Aurora Dog Mushing trails are crossed several times, and several trails along roads are also crossed, including Hollywood Road, Big Lake Road, and Burma Road. The alignment also goes through two MSB proposed wetland banks located south of Big Lake.

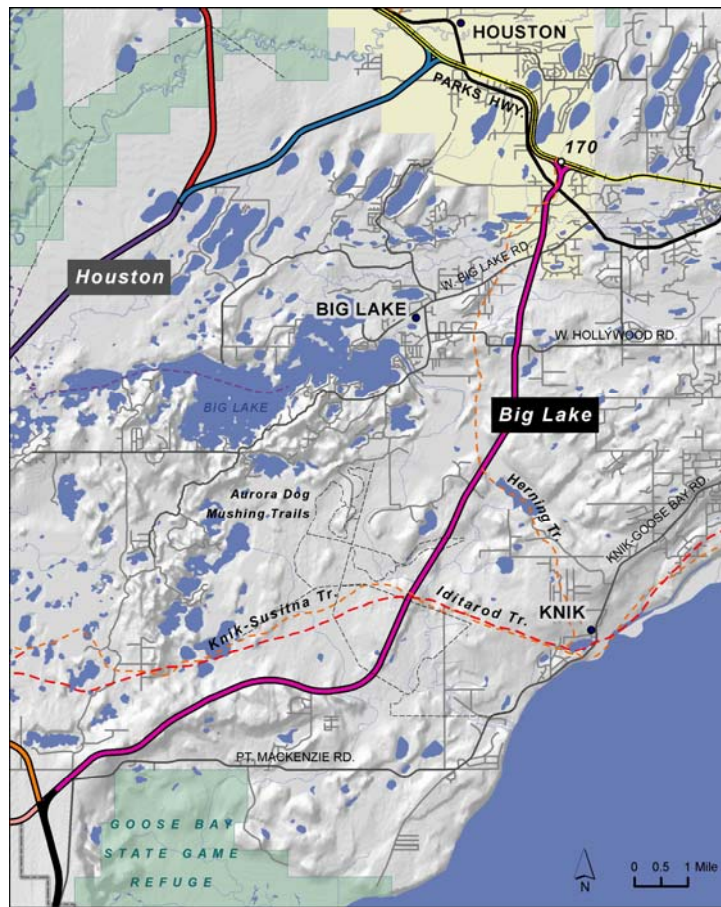


Figure 3.8 Big Lake Segment

### **3.3 No Action Alternative**

Additionally, consideration of the No Action (or “No Build”) Alternative is required under NEPA guidance as a benchmark for comparison of the potential environmental effects of the various build alternatives. The No Action Alternative would not change the existing access to Port MacKenzie, which is a two-lane road from Wasilla. Goods and materials would continue to move between Interior Alaska and the port by truck. The No Action Alternative will be considered during preparation of the environmental document by the third party contractor under STB direction. The No Action Alternative is not considered further in this report.

### **3.4 Construction**

Construction of the selected alternative would follow a sequence that begins with a survey of the clearing limits and final track centerline, followed by site clearing and grubbing, development of staging areas, embankment construction, track and structure construction, site cleanup, and reclamation. The typical design plan involves construction of a roadbed (subgrade) to accommodate a new track along the described alignment. A cross-sectional view of the roadbed is discussed in Section 2.1. The actual footprint of the fill placement, or excavation cut, would be a function of the existing topography and desired design grade of the roadbed. The minimum footprint width at the top of the subgrade would be 52 feet.

The most time-consuming construction activity would be the creation of the subgrade for the new track, support structures, and ancillary facilities. Proposed construction would consist of placing fill material, and/or removing existing material, to facilitate the proposed design grade. The proposed route would be a typical 100 feet of ROW designated at each side of the proposed track centerline location. It is ARRC’s intent to utilize the proposed route and accompanying ROW for all activities consistent with the existing uses of their current ROW. The proposed ROW would be larger in areas where fill slopes or excavation cut slopes extend outside of the 200-foot wide ROW cross-section. In these areas, the ROW boundary would encompass the toe and top of the slope for all cut and fill locations with an additional 20 foot buffer to facilitate construction and maintenance activities. The construction limits would be the boundary for construction-related activities associated with the railroad extension. The construction limits would be defined as the edge of the cut or fill plus an additional 20 feet to facilitate construction. Existing undeveloped areas outside the construction limits would not be directly impacted from construction of this project. Furthermore, only the necessary area required for construction and construction activities within the ROW would be impacted during the construction process. New access roads and staging areas would likely be required along the alignment. Previously disturbed areas would be utilized for these activities to the greatest extent feasible.

#### **Clearing and Grubbing**

Following the survey of construction limits, the projected footprint would be cleared and grubbed. Clearing would be completed to the edge of the construction limits; however, grubbing is not anticipated outside the described footprint. Clearing involves felling and removing trees and undergrowth from the construction area. Grubbing would involve the removal of roots and other vegetation within the same area. These tasks would be accomplished using bulldozers, loaders, excavators, and scrapers. If the natural ground in these areas is highly compressible or otherwise unsuitable, then soil excavation may be required. Organic soil and herbaceous vegetation removed for roadbed construction would be stored on-site for application to finished slopes, to facilitate re-vegetation and provide erosion control, or would be removed from the area and disposed of in an acceptable manner.

## **Grading**

Once clearing and grubbing are completed, excavation and fill activities would begin. Excavation and fill would be accomplished utilizing bulldozers, excavators, loaders, dump trucks, and scrapers. To the extent practicable, the design profile grade would be adjusted to balance cut and fill quantities. If the project requires additional fill, it would be generated from borrow sources within the ROW or off-site. The transport of cut material would be completed using dump trucks and/or scrapers at the cut site along the proposed route to the closest designated fill location. At fill locations, the soil material would be spread out in thin layers and compacted prior to the application of successive soil layers. Culvert pipes would be placed in areas requiring drainage from one side of the fill to the other. Should the project create excess fill material, it would be removed from the site, transported and deposited in appropriate locations.

To the extent possible, the construction ROW would be used as a haul route to transport fill. When this is not feasible, existing paved or unpaved roads would be enhanced as necessary and utilized as alternate haul routes. These roads would be restored to pre-construction conditions subsequent to project completion. Heavy equipment travel during construction would be limited to existing roads and the proposed and existing ROW. Additional access roads are not anticipated for this project based on the conceptual engineering information; however, it is possible that new roads may be necessary in certain areas to shorten haul distances for fill or track material once final engineering for the selected route is completed.

## **Culverts, Bridges, and Roads**

Numerous culverts would be installed and bridges would be constructed for the selected alignment. These structures would be installed concurrent with embankment construction. Some of the alignment alternatives have locations where established roads cross the railroad tracks. These crossings would become either grade crossings or grade separations as discussed in Section 4.4.1. Egress across proposed crossings would be maintained during construction and operation of the selected alignment.

## **Embankment and Track**

When the subgrade is near the desired finished elevation, a road grader would be utilized to provide a crown at the center of the subgrade section. A slope at the top surface of the subgrade would direct drainage to a ditch line on both sides of the proposed roadbed. Once the subgrade is constructed, crushed rock would be placed atop the finished subgrade to form up to a typical 12-inch thick layer based upon final engineering; referred to as the sub-ballast layer. This sub-ballast layer provides structural support as well as drainage for the ballast section on top of it, and distributes the load more uniformly over the subgrade. The side slopes of the cut and fill slopes along the new alignment would have native seed or other appropriate stabilization applied.

Following placement and compaction of the sub-ballast, the new track would be started from a tie-in location at the existing track alignment. Ties and rail would be placed with conventional construction and track-mounted equipment in successive application, until the track is completely from one end of the alignment to the other. Once the track is installed for a certain distance, ballast trains would be routed over the loose track to place crushed rock ballast. Then the track would be adjusted to final grade and alignment using a tamper. This rail-mounted machine is designed to compact the ballast under the ties and, thus, adjust the elevation and horizontal location of the rails. Several cycles of ballast dumping and tamping would be required to lift the track to the design elevation with the proper amount of ballast under the ties. The ballast serves to hold the ties in place and distribute the load from the track to the sub-ballast and subgrade. Additional ballast material would be placed and compacted on the outside edge of the ties to hold

the track in alignment. The ballast placing and tamping operation would follow the placement of track along the alignment.

**Construction Materials**

Materials such as ballast, ties, rail, and bridge beams represent the bulk of items that are not available in the local area and must be imported to complete the project. The majority of these items are expected to be transported to the site via ship and rail from out of state due to their weight and bulk. Materials such as ballast would be available from in-state sources.

**Construction Crew, Equipment, and Schedule**

Typical crew sizes and equipment needed for the various construction activities are shown in Table 3-1. ARRC anticipates that construction of the rail extension would be completed within 24 months. Construction should be contracted to experienced grading and rail construction firms, several of which have offices located in the local area. To meet a construction schedule, up to three crews may work on a 24-hour basis; however, significant portions of the project length would not have a 24-hour construction schedule because of environmental and human constraints. It is anticipated that the construction work force would vary from 66 persons during grading and embankment construction to 100 during ballast and track installation. Permanent new employment for future train operations is estimated to be approximately 4 persons for ARRC operations. It is anticipated that the Port MacKenzie rail extension would be completed and the line operational in 2012 (taking into account completion of the environmental and permitting processes and project funding).

**Table 3-2 Estimated Crews and Equipment Needs for Construction**

Construction Activity	Crews	Crew Size	Equipment Needed, per Crew
Clearing and Grubbing	3	6	1 Loader Excavator, 2 Articulated Trucks, 2 Dozers,
Grading/Embankment Construction	3	22	6 Scrapers, 6 Articulated Trucks, 2 Compactors, 2 Graders, 1 Water Truck, 3 Dozers
Infrastructure	3	25	4 Backhoes, 2 Cranes, 2 Forklifts, 4 Concrete Trucks,
Track	1	25	2 Excavators, 1 Speed Swing, 2 Production Tampers, 2 Ballast Regulators, 1 Rail Heater, 1 Anchor Applicator, 2 Ballast Trains
Site Cleanup	4	4	1 Pick-up Truck, 1 HyRail Truck

Source: TNH-Hanson

**Staging Areas**

Staging areas would be identified for construction of the selected alternative. These staging areas would be used as laydown areas for construction vehicles and supplies. Attempts would be made to locate the staging areas within the proposed ROW at relatively flat, previously disturbed areas with established access to existing public roads. All stockpiled materials would either be consumed by the project or would be removed from the staging areas after construction activities.

## **Maintenance**

ARRC employees would maintain the new alignment on a regular basis. Examples of ongoing maintenance activities include track lining and surfacing, rail repair and replacement, embankment upkeep, and sight triangle vegetation clearance. A sight triangle is determined from where a vehicle would stop to evaluate the potential approach of a train at an at-grade crossing. Operations and maintenance problems would typically be addressed by small repair crews transported to the site along existing roads or along the adjacent tracks by use of rail-mounted vehicles. With the exception of emergency repair or derailment, maintenance operations are not expected to impact the project area.

## 4.0 Environmental Factors

This section describes the natural and human environment of the project area and provides a preliminary evaluation of the potential impacts of the alternative alignments. The section is organized to address the STB's environmental factors and is expanded to include issues specific to Alaska and the project area. Each section begins with an overall description of the environmental factor within the project area and ends with an overview of potential impacts of each alternative and mitigation based on the design features common to all alternatives. See Appendix E for ARRC preliminary voluntary mitigation measures.

### 4.1 Transportation Systems

The existing transportation system in the project area is generally characterized by a network of local roads interspersed with a handful of arterial and collector roads, some of which include Port MacKenzie Road, Burma Road, Hollywood Road, Big Lake Road, Ayrshire Avenue, and Guernsey Road. The Parks Highway generally parallels the existing rail mainline to the north. Additionally, many small airstrips and floatplane bases are located throughout the area. A ferry linking Anchorage with Port MacKenzie is scheduled to begin service in late 2009.

The proposed project would have a potential impact on the local transportation system, primarily at the road crossings. New at-grade and grade separated road crossings would be created (Figure 4.1). The new at-grade crossings would cause a slight delay for vehicular traffic when the train is using the crossing. The amount of delay would vary depending on roadway traffic volume, and train length and speed. With two trains a day, the amount of delay caused should be minimal. Crossing protection would vary and would be determined by the amount of vehicular traffic at the crossing. Roads with 500 vehicles per day or more would have flashing lights and gates, and roads with less volume would have crossbuck signs. A crossbuck sign is an X-shaped sign used to indicate level railway crossings. The signs are typically white with the words "railroad crossing" in black lettering.

The proposed rail extension is not expected to create additional vehicular traffic. Instead, it is anticipated the amount of truck traffic in the area would be reduced as shippers would have the option of transporting goods by rail. To address coordination of future transportation planning in the project area, the project team held a meeting with DOT&PF and KABATA to introduce the project. Both entities were receptive to the project plans and did not anticipate any conflicts with their transportation planning efforts in the foreseeable future. See Appendix D for meeting minutes.

The 2007 MSB LRTP includes a rail extension from the existing mainline to Port MacKenzie. The alignment shown in the LRTP generally follows the Mac West – Willow alternative. This alternative was identified in the 2003 corridor study as the preferred alignment, and was subsequently supported by an amended ordinance from the MSB Assembly (No. 05-011 [AM], Appendix A). Prior to that study, a route more closely following the Houston corridor was also supported by the Assembly with an amended action memorandum (No. 92-109 [AM], Appendix A). Other planned improvements in the LRTP include reconstructing Burma Road between Big Lake Road and Point MacKenzie Road as a two-lane collector and upgrading Big Lake Road between Northshore Drive and the Parks Highway to a four-lane minor arterial (MSB 2007a). The Point MacKenzie Road, between Burma Road and the port, is scheduled to be paved in 2008.

In addition to being in the adopted LRTP, the Port MacKenzie Rail Extension project was also adopted by resolution into the MSB Comprehensive Plan (MSB 2005). For these reasons, the project is considered to be consistent with the plans in place for the MSB.



New road crossings are summarized in Table 4-1. To compare the corridors, each alternative was evaluated to a common point at the north end of the project area near Willow. Therefore, the “use” of “existing crossings” refers to the segment along the existing mainline to get to the common point near Willow and the connection point of each alternative with the mainline. This is reflected in the last column of Table 4-1. Hazard indices were computed for at-grade and grade-separated crossings (Appendix F).

**Table 4-1: Road Crossings**

<b>Alternative</b>	<b>Number of New Grade-Separated Crossings</b>	<b>Number of New At-Grade Crossings</b>	<b>Number of Crossings along the Existing Mainline (At-Grade/Separated)</b>
Mac West – Willow	1 (Parks Highway)	4	--
Mac West – Houston – Houston North	0	4	4 / 1
Mac West – Houston – Houston South	0	5	4 / 1
Mac West – Big Lake	2 (Parks Highway and Big Lake Road)	9	5 / 2
Mac East – Willow	1 (Parks Highway)	6	--
Mac East – Houston – Houston North	0	6	4 / 1
Mac East – Houston – Houston South	0	7	4 / 1
Mac East – Big Lake	2 (Parks Highway and Big Lake Road)	9	5 / 2

Source: TNH-Hanson 2007 (as shown in Appendix F), MSB

**Mac West – Willow**

The Mac West - Willow alternative requires the construction of a new grade-separated crossing with the Parks Highway. At this crossing, it is anticipated that the Parks Highway would be reconstructed to go over the railroad tracks. This section of the Parks Highway is a two-lane highway with an annual average daily traffic (AADT) of approximately 3,000 (ADOT&PF 2006). The alignment would also include four at-grade crossings (Guernsey Road, Little Su River Road, W. Deshka Landing, and Willow Creek Parkway). At a minimum, crossbuck signs would be installed at all four new at-grade crossings. Teeka Run Road would be closed short of the proposed wye track at the junction of the alternative with the mainline track, avoiding the creation of an at-grade crossing there. Currently, Teeka Run Road is a primitive/unpaved road that dead ends just west of the existing railroad track at the junction of the Willow segment.

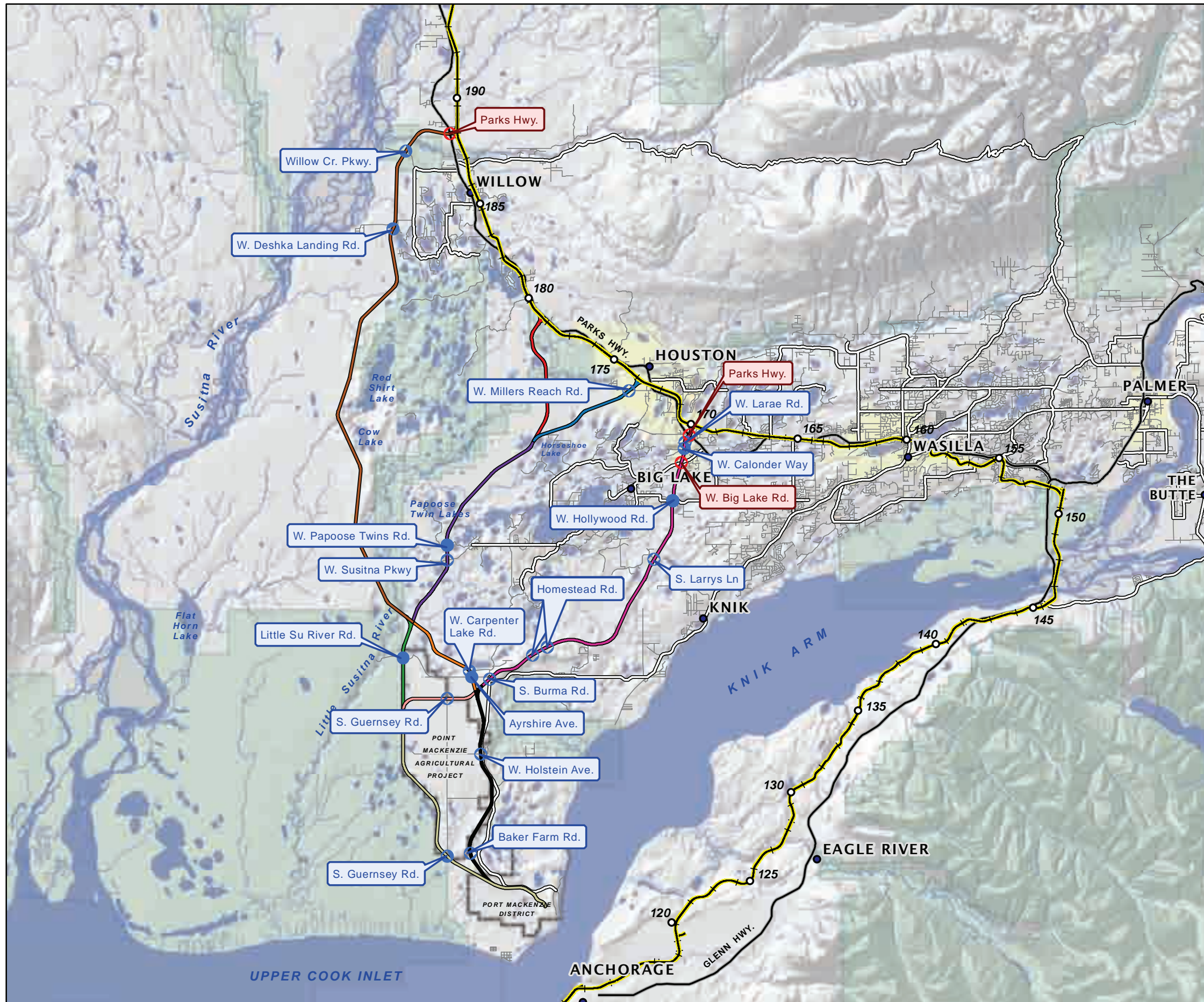
**Mac West – Houston – Houston North**

This alternative would use five existing (4 at-grade and 1 grade-separated) crossings along the mainline between MP 179.3 and Willow Creek. In addition, this alternative requires four new at-grade crossings (Guernsey Road, Little Su River Road, W. Susitna Parkway, and W. Papoose Twins Road). Crossbuck signs would be installed at all four new at-grade crossings.

**Mac West – Houston – Houston South**

This alternative would use five existing (4 at-grade and 1 grade-separated) crossings along the mainline between MP 174.0 and Willow Creek. In addition, this alternative requires five new at-

**Figure 4.1 LOCATIONS OF POSSIBLE NEW ROAD INTERSECTIONS**



**LEGEND**

Road Crossings

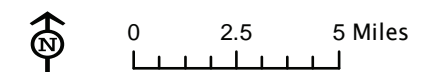
- Grade-separated
- At-grade

Preliminary Alternatives\*

- Mac East
- Mac West
- Conn 1
- Conn 2
- Conn 3
- Houston
- Houston North
- Houston South
- Willow
- Big Lake
- ARRC Milepost
- ARRC Track
- Highway
- Medium Rd.
- Minor Rd.
- - - Iditarod Trail
- City Boundary
- Park or Refuge

\*These lines generally represent corridors which are subject to further refinement.

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: January 9, 2008  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

*(This page intentionally left blank.)*

grade crossings (Guernsey Road, Little Su River Road, W. Susitna Parkway, W. Papoose Twins Road, and W. Millers Reach Road). Crossbuck signs would be installed at all five crossings at a minimum.

This alternative crosses the northern half of a private airfield (Kucera Residence) located at the north end of Little Horseshoe Lake. This gravel runway is 25 feet wide and 1,200 feet long. This is a private landing strip which is located on both public and private land. It would appear that no instrument exists for it to be located on public property. The Houston alternatives cross the runway at the north end as the alignment parallels the property line on the public (north) side. Extending the runway to the south is not a recommended option because it would require filling part of Little Horseshoe Lake. Even so, the railroad envelope would be afoul of the runway approach zone. Constraints related to wetlands, property and geotechnical concerns limit the re-alignment of the proposed rail line in the area. Further engineering analysis would be required to resolve the conflict between the railroad and the airfield, likely requiring the runway to be re-oriented east-west. In the event the necessary property instruments cannot be obtained, the runway would need to be closed.

### **Mac West – Big Lake**

This alternative would use seven (5 at-grade and 2 grade separated) existing crossings along the mainline between MP 170.3 and Willow Creek. To avoid traffic and safety impacts, this alternative would have two new grade separated crossings. One crossing is at Big Lake Road where the railroad would go over the roadway as shown in Figure 4.1. Big Lake Road is a minor arterial. The AADT along this section of Big Lake Road is approximately 4,600 (ADOT&PF 2006). The second new grade-separated crossing is of the Parks Highway. The AADT along this section of the Parks Highway is approximately 5,000 (ADOT&PF 2006). The highway is proposed to be elevated to go over the railroad.

In addition, this alternative requires nine new at-grade crossings (two crossings of Guernsey Road, S. Burma Road, two crossings of Homestead Road, S. Larrys Lane, W. Hollywood Road, W. Calonder Way, W. Larae Road). The new at-grade crossings would have at a minimum crossbuck signs for protection, except S. Burma Road and W. Hollywood Road, which would have flashing lights and gates due to greater traffic. The two at-grade crossings of Homestead Road are shown in Figure 4.1. Based on a fly-over and ground reconnaissance of the project area, members of the project team observed a new subdivision being built along this alignment between MPs 3 and 5. Based on their observations, it appears that Homestead Road in the subdivision would be crossed in two places by the alignment. Additional information about this subdivision is not known at this time.

This alternative is approximately one mile east of the Big Lake airport. No impacts to the airport approach or clear zone are anticipated.

### **Mac East – Willow**

To avoid traffic and safety impacts, this alternative would have a new grade separated crossing with the Parks Highway. For this crossing, the highway would be elevated to go under the railroad tracks. This section of the Parks Highway is a two-lane highway with an AADT of approximately 3,000 (ADOT&PF 2006).

The alignment would also include six new at-grade crossings (Baker Farm Road, W. Holstein Road, Ayrshire Avenue, W. Carpenter Lake Road, W. Deshka Landing, and Willow Creek Parkway). Crossbuck signs would be installed at five crossings at a minimum. Ayrshire Avenue would have flashing lights and gates due to greater traffic.

Currently, Teeka Run Road dead ends just west of the existing railroad tracks at the junction of the Willow segment. Teeka Run Road would be closed short of the proposed junction wye track, avoiding the creation of an at-grade crossing.

#### **Mac East – Houston – Houston North**

This alternative would use five existing (4 at-grade and 1 grade-separated) crossings along the mainline between MP 179.3 and Willow Creek. In addition, this alternative requires six new at-grade crossings (Baker Farm Road, W. Holstein Road, Ayrshire Avenue, W. Carpenter Lake Road, W. Susitna Parkway, and W. Papoose Twins Road). Five of the new at-grade crossings would have crossbuck signs installed at a minimum. Ayrshire Avenue would have flashing lights and gates due to greater traffic.

#### **Mac East – Houston –Houston South**

This alternative would use five existing (4 at-grade and 1 grade-separated) crossings along the mainline between MP 174.0 and Willow Creek. This alternative would require seven new at-grade crossings (Baker Farm Road, W. Holstein Road, Ayrshire Avenue, W. Carpenter Lake Road, W. Susitna Parkway, W. Papoose Twins Road, and W. Millers Reach Road). The new at-grade crossings would have minimally crossbuck signs, except for Ayrshire Avenue which would have flashing lights and gates.

This alternative crosses the northern half of a private airfield (Kucera Residence) located at the north end of Little Horseshoe Lake. This gravel runway is 25 feet wide and 1,200 feet long. This is a private landing strip which is located on both public and private land. It would appear that no instrument exists for it to be located on public property. The Houston alternatives cross the runway at the north end as the alignment parallels the property line on the public (north) side. Extending the runway to the south is not a recommended option because it would require filling part of Little Horseshoe Lake. Even so, the railroad envelope would be afoul of the runway approach zone. Constraints related to wetlands, property and geotechnical concerns limit the re-alignment of the proposed rail line in the area. Further engineering analysis would be required to resolve the conflict between the railroad and the airfield, likely requiring the runway to be re-oriented east-west. In the event the necessary property instruments cannot be obtained, the runway would need to be closed.

#### **Mac East – Big Lake**

This alternative would use seven (5 at-grade and 2 grade separated) existing crossings along the mainline between MP 170.3 and Willow Creek. To avoid traffic and safety impacts, this alternative would have two new grade-separated crossings. One crossing is at Big Lake Road, where the railroad would go over the roadway. Big Lake Road is a minor arterial. The AADT along this section of Big Lake Road is approximately 4,600 (ADOT&PF 2006). The second new grade-separated crossing is at the Parks Highway. At the proposed crossing location, the AADT along the Parks Highway is approximately 5,000 (ADOT&PF 2006). The highway would be elevated to go over the railroad).

This alternative requires nine new at-grade crossings (Baker Farm Road, W. Holstein Road, Burma Road, two crossings of Homestead Road, S. Larrys Lane, W. Hollywood Road, W. Calonder Way, and W. Larae Road). The two at-grade crossings of Homestead Road are shown in Figure 4.1. Based on a fly-over and ground reconnaissance of the project area, members of the project team observed a new subdivision being built along this alignment between MPs 3 and 5. Based on their observations, it appears that Homestead Road in the subdivision would be crossed in two places by the alignment. Additional information about this subdivision is not known at this time.

This alternative is approximately one mile east of the Big Lake airport. No impacts to the airport approach or clear zone are anticipated.

## 4.2 Natural Environment

### 4.2.1 Wetlands

The project area is within the Upper Cook Inlet region, and is characterized by stream, rivers, glacial features, and wetlands throughout the area. All of the project alternatives cross wetlands, which would require dredging and filling for construction of a rail bed.

The term “wetlands” refers to “those areas that are inundated by surface or ground water at a frequency and duration sufficient to support, and that under normal circumstances do support, a prevalence of vegetation typically adapted for life in saturated soil conditions. Wetlands generally include swamps, marshes, bogs, and similar areas” (33 CFR Part 328.3(b)). Other waterbodies are also regulated by the USACE as “Waters of the U.S.”; these include lakes, small ponds, and streams.

Section 2 describes the four divisions of wetland types in the project area:

- Category 1: Uplands
- Category 2: Forested, scrub/shrub, and excavated, diked, partially drained, or ditched vegetated wetlands
- Category 3: Emergent wetlands
- Category 4: Open water or estuarine habitats; coastal swamps and marshes

Wetlands in the project area classified by the NWI are shown in Figure 4.2.

Anticipated wetland impacts are presented in Table 4-2, based on a 200-foot-wide ROW. Potential project impacts to wetlands would be their long-term loss through placement of fill. This characterization of wetlands impacts is based on the best available design information and wetlands data at this stage of the project. As required under the Section 404(b)(1) of the CWA, detailed wetland identification and evaluation of impacts would be done as part of the NEPA and permitting processes to comply with the guidelines and demonstrate that the appropriate steps to minimize potential impacts to aquatic ecosystems have been taken. Per USACE regulatory responsibilities, the final alternative proposal should represent the least environmentally damaging practicable alternative (Appendix C, USACE letter). During the NEPA process, wetland impacts would need to be carefully weighed against other project impacts to identify the most practicable alternative.

Wetlands are widely distributed throughout the project area and complete avoidance of wetlands is not possible for any alternative; however, the project team worked to minimize impacts to wetlands and other environmental issues. For example, during the constraints analysis process (Section 2.0) the project team used wetlands data available from the NWI to develop alignments that minimized impacts to wetlands. The constraints analysis also sought to avoid or minimize impacts to potential archaeological and cultural resources, anadromous fish streams, parks and refuges, limiting soils, and populated areas.

Wetlands impacts are also being minimized as part of the conceptual engineering design. Embankment design width would reduce wetland impacts by controlling slopes to a minimum 2H:1V and removing ditch sections in wetland areas. Designs also include installing transverse equalization culverts at the base of embankment in wetlands to allow cross-drainage of surface and shallow subsurface water, thus maintaining natural drainage patterns across wetlands. During

construction, the ARRC would limit earth-moving equipment and fill-hauling trucks to disturbed areas (e.g., within the footprint of the embankment) and local roads whenever possible.

**Table 4-2: Preliminary Wetland Impacts**

<b>Alternative</b>	<b>Category 2: Forested and scrub/shrub (acres)</b>	<b>Category 3: Emergent (acres)</b>	<b>Category 4: Lakes, ponds, and marshes (acres)</b>	<b>Total (acres)</b>
Mac West – Willow	80	115	5	200
Mac West – Houston – Houston North	190	155	5	350
Mac West – Houston – Houston South	145	130	5	280
Mac West – Big Lake	110	110	1	221
Mac East – Willow	40	50	1	91
Mac East – Houston – Houston North	150	90	2	242
Mac East – Houston – Houston South	105	70	2	177
Mac East – Big Lake	95	65	0	160

As shown in Table 4-2, the Mac West – Houston alternatives (both Houston South and Houston North) impact the greatest total wetland acreage. Alternatively, the Mac East – Willow alternative impacts the least wetland acreage.

The wetland impacts shown in Table 4-2 represent calculations made solely using NWI digitized mapping data and the project alternatives. Digitized NWI data ends at the northern end of the project area just south of Willow, therefore, a small amount of the Willow segment (approximately 183.5 acres, or less than 5%) does not have NWI data. Consultation with the USACE has indicated that additional wetlands analysis would be necessary to supplement NWI data (USACE meeting notes in Appendix D). Any project alternative would be subject to a jurisdictional determination by the USACE, and would require submission of a Section 404 permit application and issuance of the necessary permit by the USACE.

**Wetland Bank Lands**

Two areas proposed by MSB as wetland banks (description in Section 2.2.2) are crossed by the Big Lake segment of the project alternatives, as shown on Figure 4.2. Assuming a 200-foot ROW, the Big Lake segment (and therefore the Mac West – Big Lake and Mac East – Big Lake alternatives) would impact approximately 36 acres of wetland bank lands. No other wetland bank lands are impacted by the project alternatives. Since the purpose of the wetland bank lands is to protect and preserve valuable wetlands as well as to allow developers an opportunity to purchase wetlands in these areas and “bank” them for preservation to offset wetland impacts elsewhere, different wetland areas would need to be identified to compensate for the impacts associated with either of the Big Lake alternatives.

*4.2.2 Hydrology*

A hydrologic review was conducted for this project to identify surface and subsurface water resources including pre- and post-project drainage patterns, flow rates, and floodplain limits and

**Figure 4.2  
NWI WETLANDS**

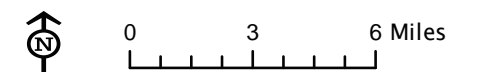


**LEGEND**

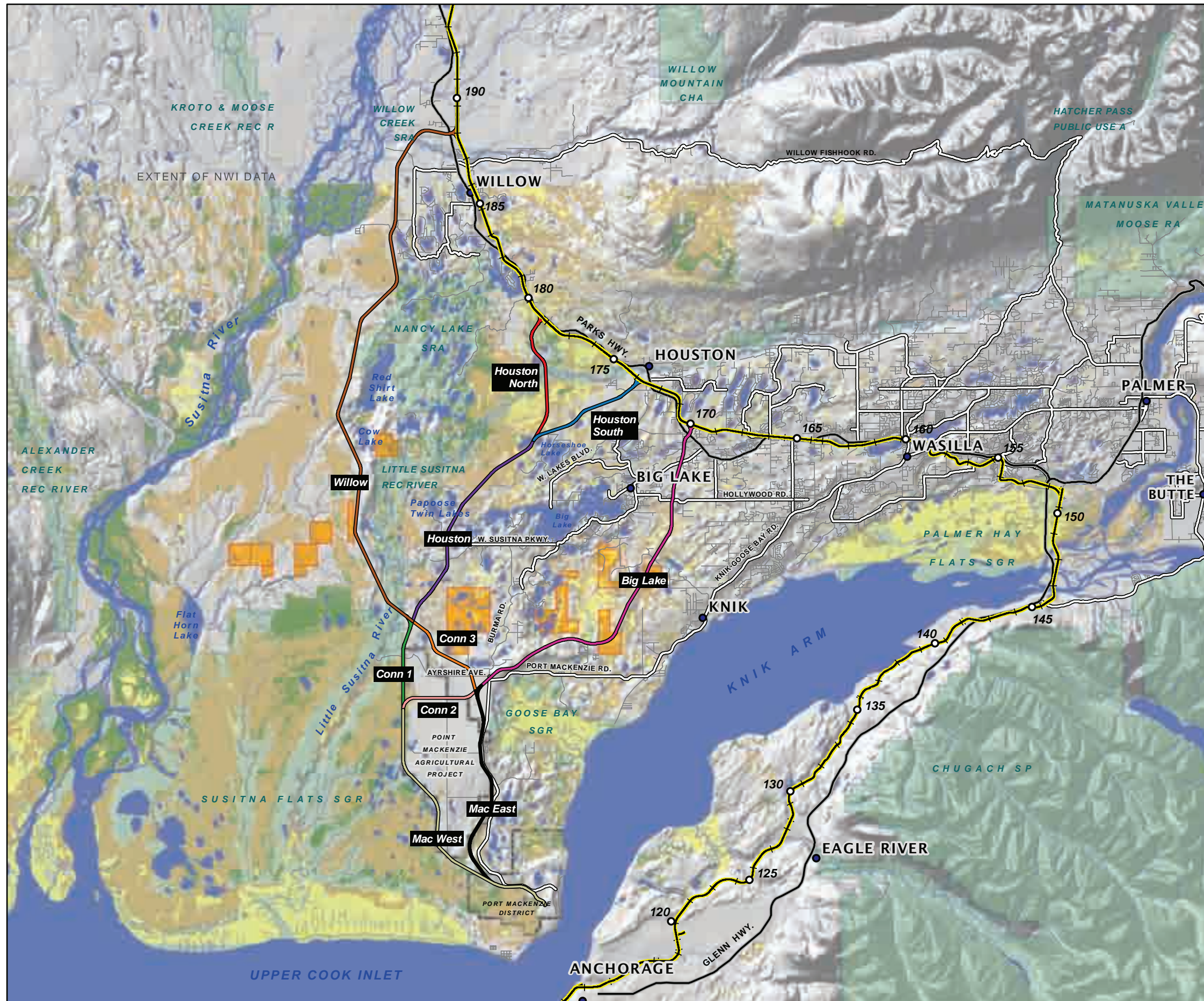
- |                                  |                          |
|----------------------------------|--------------------------|
| <b>Preliminary Alternatives*</b> | <b>Wetlands</b>          |
| — Mac East                       | — Forested Wetland       |
| — Mac West                       | — Scrub or Shrub Wetland |
| — Conn 1                         | — Sedge or Grass Wetland |
| — Conn 2                         | — MSB Wetland Bank       |
| — Conn 3                         | — ARRC Milepost          |
| — Houston                        | — ARRC Track             |
| — Houston North                  | — Highway                |
| — Houston South                  | — Medium Rd.             |
| — Willow                         | — Minor Rd.              |
| — Big Lake                       | — Park or Refuge         |

\*These lines generally represent corridors which are subject to further refinement.

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: November 5, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USF&W, USGS.





*(This page intentionally left blank.)*

encroachments (Appendix G). Special attention was given to anadromous streams as well as wetland and wetland bank areas. The following sections summarize the results of the hydrology assessments.

Five major watersheds are intersected by the alignment alternatives, which include the Susitna, Little Susitna, Willow Creek, Fish Creek, and an unnamed watershed in the project area. These watersheds range in size from just over 100 square miles (the unnamed watershed) to more than 6000 square miles (the Susitna watershed) (Lamke et al. 1995). Precipitation within these watersheds may vary greatly from the low to high elevations. Most precipitation measurements available from these watersheds are obtained from lower elevation areas near the population centers and very few measurements from the high elevation areas. Based on long-term (1971-2000) precipitation and temperature data from the Matanuska Experiment Station near Wasilla, annual precipitation as rainfall between April and October is 11 inches; average precipitation as snowfall snow fall between November and May is 4 inches (water equivalent) (Alaska Climate Research Center 2007). Snow depths vary greatly over the study area watersheds. For example, NRCS snow measurement data depths vary greatly over the study area watersheds. For example, NRCS snow measurement data shows April 1 (30-year average) water equivalent values at Independence Mine (elevation 3,550 feet) of 24.2 inches and at Point Mackenzie (elevation 250 feet) of 4.4 inches. At Independence Mine, the snow cover season extends to mid-June while at Point MacKenzie the snow cover season extends only to the beginning of May (USDA 2007).

#### 4.2.2.1 Surface Water

Surface drainage in the area of the alignment alternatives is generally to the west and south; areas either drain into Cook Inlet or the Knik Arm, bordering the project area to the south and east, or to the Susitna River, bordering the project area to the west and flowing south, which drains also to Cook Inlet. A watershed or drainage basin is a specific region of land draining into a river system, and divided from other watersheds by ridgelines or high points along its perimeter. Drainage basins potentially impacted by the alternatives were delineated using a combination of USGS 2-arc second digital elevation maps and USGS Quadrangle Maps. A more detailed explanation of the hydrologic and hydraulic analysis can be found in Appendix G. Drainage divides were difficult to locate in the flat, poorly-drained areas characterizing much of the project area, and some may have been altered by human activities, such as road construction. The delineations used for this preliminary analysis are the most accurate possible without extensive survey. The presence of lake inlets and outlets or stream courses located by future field investigations may warrant modification of the boundaries used here.

The project area is dominated by several distinct hydrologic areas; high-gradient, high-elevation mountainous areas and low-gradient, low-elevation areas dominated by lakes and wetlands. The Talkeetna Mountains, in the area to the north of the Little Susitna River, in the upper drainage area of Willow Creek, have greater relief and a better developed drainage pattern. This is due to the differential glacial erosion which took place in this area; however, drainage is still complicated by post glacial surface morphology (Collazzi et al. 1988). In the lower drainage area of the Little Susitna River and entirety of the project area south of the Little Susitna River, the landscape is dominated by hundreds of small, irregular lakes (Collazzi et al. 1988). The majority of these are seepage lakes, formed in kettle moraines where the land surface was shaped primarily by stagnant, melting ice. They are present throughout the area, vary widely in size and configuration, and are not usually associated with stream systems. There are also a large number of drainage and outlet lakes, typically found in the central areas of watersheds where one of the main streams or tributary flows through or out of the lake. The abundance of these lakes indicates that the water inputs to area lakes by precipitation, surface runoff, and ground-water inflow are typically greater than water losses by evaporation and ground-water outflow (Jokela et al. 1990).

These high- and low-gradient geomorphic areas have differing effects on the five major watersheds intersected by the alignment alternatives. Three of these watersheds, the Susitna, Little Susitna, and Willow Creek, have their headwaters in the Talkeetna Mountains. Around half of the Willow Creek watershed area is made up of mountainous terrain; its stream flow is dominated by high elevation snow fields and rapid response to summer storms. The Susitna and Little Susitna watersheds have a smaller portion of their area in the Talkeetna Mountains; a larger portion of their watersheds are dominated by low-lying, low-gradient areas which moderate the influence of the mountainous terrain. The Fish Creek and unnamed watershed in the Port MacKenzie area exclusively contain low-lying, low-gradient landforms which tend to retard runoff and reduce stream flow. All of the watershed areas can be characterized by increasing flows from spring breakup beginning in mid-April and snowmelt runoff continuing from May to July; rainfall runoff throughout the time from May to September; and fall freeze-up and stream flow recession through from October through April (USGS 2005a, USGS 2005b).

### **Impacts to Surface Waters**

Where possible, the numerous lakes, ponds, streams, and rivers in the project area have been avoided or alignments have been designed to minimize impacts to surface waters. The alignment alternatives would not impact any area lakes. For the siding construction along the Houston North segment, the existing rail line may need to be shifted to the northeast. This would be to accommodate the second track on the southwest, and would avoid placing fill into Nancy Lake.

Table 4-3 presents the named water bodies potentially impacted by each alternative, based on MSB and ADF&G GIS data.

The project would be consistent with applicable Federal, State, and local water quality standards. Best management practices (BMPs) to restrict non-point pollution sources would be conducted during construction. A National Pollutant Discharge Elimination System (NPDES) permit, consistent with Section 402 of the CWA, would be obtained and a Storm Water Pollution Prevention Plan (SWPPP) would be prepared prior to construction. Implementation of the BMPs during construction would minimize the potential for impacts to surface waters.

Maintenance and preservation of regional surface water hydraulic connectivity has been identified as a concern by area residents and resource agencies. Project design would incorporate culverts and hydraulic considerations into the rail bed construction to ensure existing surface water connections are maintained. This design would include sizing bridges and culverts for anadromous fish passage for designated streams in accordance with the ADNR OHMP Title 41 Fish Habitat Permit. In flat areas without defined drainage channels, additional culverts would be installed as necessary to prevent damming of water behind the railroad embankment. Final location and spacing of these culverts would occur during final design of the selected alternative.

As discussed in Section 4.2.1, there are wetlands throughout the project area, and complete avoidance is impossible for any of the alignment alternatives.

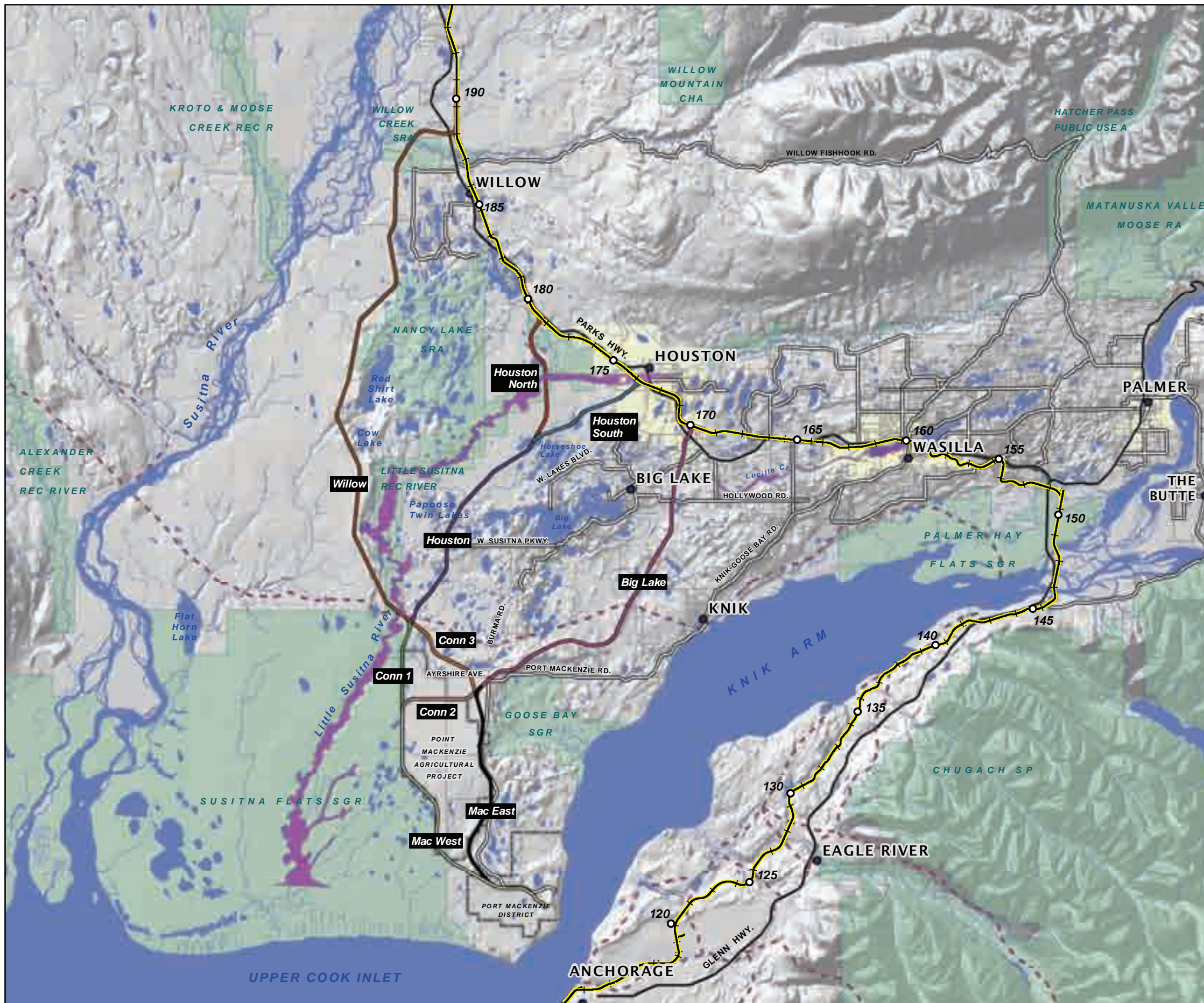
As shown in Figure 4.3, 100-year floodplains have been mapped by the Federal Emergency Management Agency (FEMA) for only the Little Susitna River and Lucile Creek. The Willow, Houston North, and Houston South segments all intersect the floodplain of the Little Susitna River at some point, although the Houston South segment appears to be adjacent instead of in the floodplain. The Big Lake alternative intersects the Lucile Creek floodplain. Any alternative that involves construction within a designated floodplain would be subject to permitting by the FEMA floodplain administrator (MSB).

**Figure 4.3  
MAPPED FLOODPLAINS**



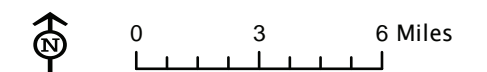
**LEGEND**

- |                           |                |
|---------------------------|----------------|
| Preliminary Alternatives* | ARRC Track     |
| Mac East                  | ARRC Milepost  |
| Mac West                  | Highway        |
| Conn 1                    | Medium Rd.     |
| Conn 2                    | Minor Rd.      |
| Conn 3                    | Floodplains    |
| Houston                   | Iditarod Trail |
| Houston North             | City Boundary  |
| Houston South             | Park or Refuge |
| Willow                    |                |
| Big Lake                  |                |



\*These lines generally represent corridors which are subject to further refinement.

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: November 5, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

*(This page intentionally left blank.)*

**Table 4-3: Identified Waterbodies Impacted**

Alternative	Waterbody name and known ADF&G anadromous stream ID numbers
Mac West – Willow	Unnamed stream, just west of Twin Island Lake Unnamed stream, just east of Middle Lake Unnamed stream, 247-41-10100-2080 (tributary of Little Susitna River) Little Susitna River, 247-41-10100 Fish Creek, 247-41-10200-2020 Unnamed stream (south tributary of Rolly Creek) Unnamed stream (north tributary of Rolly Creek) Unnamed stream (tributary of Little Susitna River) Willow Creek, 247-41-10200-2120 Rodgers Creek, 247-41-10200-2130-3020
Mac West – Houston – Houston North	Unnamed stream, just west of Twin Island Lake Unnamed stream, just east of Middle Lake Unnamed stream, 247-41-10100-2080 (tributary of Little Susitna River) Unnamed stream, 247-41-10100-2090 (tributary of Little Susitna River) Unnamed stream, 247-41-10100-2100 (tributary of Little Susitna River) Unnamed stream, 247-41-10100-2150 (tributary of Little Susitna River) Unnamed stream, between Muleshoe and Colt lakes Little Susitna River, 247-41-10100 Lake Creek, 247-41-10100-2231 Unnamed stream, 247-41-10100-2231-3050 (tributary of Nancy Lake)
Mac West – Houston – Houston South	Unnamed stream, just west of Twin Island Lake Unnamed stream, just east of Middle Lake Unnamed stream, 247-41-10100-2080 (tributary of Little Susitna River) Unnamed stream, 247-41-10100-2090 (tributary of Little Susitna River) Unnamed stream, 247-41-10100-2100 (tributary of Little Susitna River) Unnamed stream, 247-41-10100-2150 (tributary of Little Susitna River) Unnamed stream, between Muleshoe and Colt lakes Unnamed stream (tributary of Little Horseshoe Lake) Little Susitna River, 247-41-10100 Unnamed stream, 247-41-10100-2255 (tributary of Little Susitna River)
Mac West – Big Lake	Unnamed stream, just west of Twin Island Lake Unnamed stream, just east of Middle Lake Goose Creek, 247-50-10360 Fish Creek, 247-50-10330 Unnamed stream (south tributary of Lucile Creek) Unnamed stream (north tributary of Lucile Creek) Lucile Creek, 247-50-10330-2050-3030 Little Meadow Creek, 247-50-10330-2050-3050 Unnamed stream, 247-50-10330-2050-3025 (between Cheri and Long lakes; 3 crossings)

Alternative	Waterbody name and known ADF&G anadromous stream ID numbers
Mac East – Willow	Unnamed stream, just west of Twin Island Lake Little Susitna River, 247-41-10100 Fish Creek, 247-41-10200-2020 Unnamed stream (south tributary of Rolly Creek) Unnamed stream (north tributary of Rolly Creek) Unnamed stream (tributary of Little Susitna River) Willow Creek, 247-41-10200-2120 Rodgers Creek, 247-41-10200-2130-3020
Mac East – Houston – Houston North	Unnamed stream, just west of Twin Island Lake Unnamed stream, 247-41-10100-2090 (tributary of Little Susitna River) Unnamed stream, 247-41-10100-2100 (tributary of Little Susitna River) Unnamed stream, 247-41-10100-2150 (tributary of Little Susitna River) Unnamed stream, between Muleshoe and Colt lakes Little Susitna River, 247-41-10100 Lake Creek, 247-41-10100-2231 Unnamed stream, 247-41-10100-2231-3050 (tributary of Nancy Lake)
Mac East – Houston – Houston South	Unnamed stream, just west of Twin Island Lake Unnamed stream, 247-41-10100-2090 (tributary of Little Susitna River) Unnamed stream, 247-41-10100-2100 (tributary of Little Susitna River) Unnamed stream, 247-41-10100-2150 (tributary of Little Susitna River) Unnamed stream, between Muleshoe and Colt lakes Unnamed stream (tributary of Little Horseshoe Lake) Little Susitna River, 247-41-10100 Unnamed stream, 247-41-10100-2255 (tributary of Little Susitna River)
Mac East – Big Lake	Unnamed stream, just west of Twin Island Lake Goose Creek, 247-50-10360 Fish Creek, 247-50-10330 Unnamed stream (south tributary of Lucile Creek) Unnamed stream (north tributary of Lucile Creek) Lucile Creek, 247-50-10330-2050-3030 Little Meadow Creek, 247-50-10330-2050-3050 Unnamed stream, 247-50-10330-2050-3025 (between Cheri and Long lakes; 3 crossings)

#### 4.2.2.2 Ground Water

Ground water is widely used for water supply throughout the area and ground water resources are adequate to supply the existing development (KABATA 2005). Ground water and surface water are closely interconnected in the project area; ground water discharges into most lakes, streams, and wetlands, although the reverse relationship sometimes occurs (Jokela et al. 1990). Groundwater is fed by direct infiltration of precipitation and snowmelt and by streams infiltrating into foothills slopes. Examination of the regional water-table defined by well and surface data indicates that shallow ground water basins are generally concordant with topographic basins. A deep aquifer may underlie the shallow ground-water basins, with recharge occurring near the Talkeetna Mountains and discharge occurring near Knik Arm, although data to confirm its presence is currently lacking (Jokela et al. 1990). Deep groundwater flow is unlikely to be

affected by any alternative because the all disturbance would be within a few feet of the land surface.

Ground water quality throughout most of the area is good; it is of suitable quality for general domestic, agricultural, and commercial or industrial use (Jokela et al. 1990). Localized water quality impairment has occurred as a result of a few specific waste-disposal operations. Potential sources of ground water contamination include fuel storage tanks, septic systems, road-salt storage areas, agricultural chemicals, and urban runoff (Jokela et al. 1990).

Public drinking water wells (a well that provides water for 25 or more people) are regulated by Alaska Department of Environmental Conservation (ADEC). Identification of project area public water supply wells has not been performed and may be necessary through coordination with ADEC in order to minimize potential impacts to public water systems. Private wells are not regulated by ADEC and locations of such wells are not cataloged (Kellie Alvstad, pers.comm.). There are no EPA-designated sole source aquifers in the project area (<http://yosemite.epa.gov/r10/water.nsf/Sole+Source+Aquifers/Overview>).

### **Impacts to Ground Water**

Construction of the rail embankment would require relatively shallow excavation and is not expected to impact ground water resources within the project area. Implementation of BMPs during construction operations is expected to minimize the potential for impacts to surface water quality that influences ground water within the study area.

#### *4.2.3 Fish and Essential Fish Habitat*

The Magnuson-Stevens Fishery and Conservation Management Act (PL94-265), as amended by the Sustainable Fisheries Act of 1996 (PL104-267) and by the Reauthorization Act of 2006 (PL109-479), defines Essential Fish Habitat (EFH) as “those waters and substrate necessary to fish for spawning, breeding, feeding, or growth to maturity.” EFH designations emphasize the importance of habitat protection to healthy fisheries and serve to protect and conserve the habitat of federally managed fish resources.

The National Oceanic and Atmospheric Administration (NOAA) National Marine Fisheries Service (NOAA Fisheries) is responsible for designation of EFH and defers to the ADF&G Anadromous Fish Streams catalog for many inland EFH resources. Anadromous fish streams are those used by salmon and other sea-going fish for spawning and rearing. Additional EFH may exist in the project area beyond the streams mapped in the catalog; an EFH Assessment should be conducted to satisfy federal consultation requirements for the project. Correspondence with regulatory agencies is in Appendices C and D.

The project area hosts many species of both anadromous and freshwater fish, including all five species of Pacific salmon, Dolly Varden, eulachon, northern pike, rainbow trout, arctic char, arctic grayling, lake trout, burbot, and whitefish (ADF&G 2007a; KABATA 2006a).

Crossing anadromous fish streams requires a Title 41 Fish Habitat permit from the ADNR OHMP. Stream surveys would likely be required. OHMP correspondence is in Appendix C. Given the number of anadromous fish streams that cross the project area (Figure 4.4), it is not possible to avoid crossing anadromous fish streams. As shown in Table 4-3, the Mac West – Houston – Houston North, Mac West – Big Lake, and Mac East – Big Lake alternatives require the greatest number of anadromous fish stream crossings (7 each), and would have the highest potential to impact fish and EFH of the project alternatives. The Mac East – Willow alternative requires the fewest number of anadromous fish stream crossings (4).



#### 4.2.4 Wildlife

Excluding the highly developed areas, the relatively undisturbed portions of the project area provide habitat for numerous mammal species, including a wide variety large and small mammals. The main large species found in the area include moose, black bear, and brown bear.

Information contained here regarding terrestrial mammals was obtained through a literature review, relying largely on the information presented in the January 2006 document “Terrestrial Mammals Technical Report” prepared in support of the Knik Arm Crossing Project (KABATA 2006c), and correspondence with various state and federal resource agencies.

##### **Black Bear**

The exact size of the black bear population in the project area has not been identified, but is speculated to be between 500 and 1,000 bears within Game Management Unit (GMU) 14 (KABATA 2006c), which includes lands within the Anchorage Bowl as well as the project area (Figure 4.5). Black bear distribution during spring, summer, and fall is largely determined by food availability. Dense populations are found in the Susitna Valley, along the Susitna River west of Willow, and at the Susitna River mouth north to Susitna Station. Black bear habitats within the MSB include broadleaf forest, mixed needleleaf/broadleaf forest, tall and low shrub, sedge and grass wetlands, and alpine tundra communities (KABATA 2006c). Except for alpine areas, all these habitats exist in the project area.

##### **Brown Bear**

Brown bears require greater habitat area than black bears and are less common than black bears in the project area. The population in GMU 14 was last estimated to be between 125 and 232 bears. Brown bears are usually found along river drainages searching for fish and have feeding habitats similar to those of black bear. In the spring, they also use the salt marshes and sedge meadows at Susitna Flats for feeding (KABATA 2006c).

##### **Moose**

About 6,560 resident and migratory moose inhabit GMU 14A (Figure 4.5). With a winter moose population of between 10 and 20 moose per square mile, the Point MacKenzie area hosts one of the highest concentrations of moose in Alaska (KABATA 2006c). Moose populations are high, especially during winter, because of browsing opportunities and the opportunity for relief from deep snow. The Point MacKenzie Agricultural Project area and the 37,000-acre area burned during the 1996 Big Lake fire are ideal habitats for moose browsing because of the subsequent return of early vegetation. Moose in the project area primarily winter in lowland riparian and wetland areas, and in other upland areas supporting willow, birch, and other forage species. Many moose winter in the Susitna Flats, seeking food and refuge from the snow. Each spring, moose calving occurs in the inland portions of Goose Bay SGR, north of Point MacKenzie, and the brushy thickets of Susitna Flats. Moose spend summer and fall in lowlands throughout the project area. Figure 4.6 shows moose habitat in and around the project area.

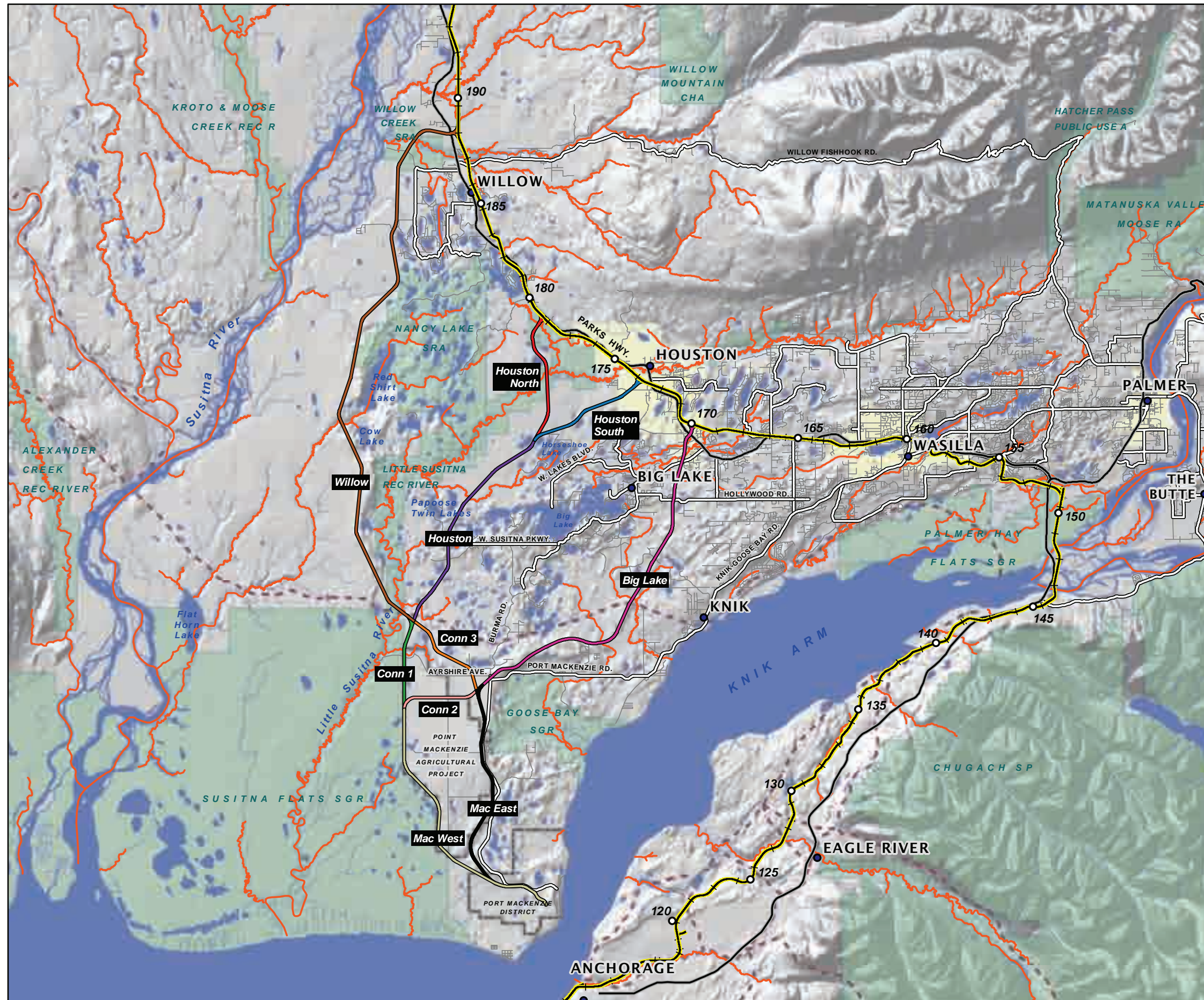
Migrating populations of moose move through the project area to and from seasonal habitat areas. The low lying areas of the Susitna Flats SGR to the west, and the Palmer Hay Flats SGR to the east, of the project area provide prime wintering habitat for moose in the region. While there are moose in the project area year round, density of moose population increases during the winter as moose come down from the higher elevations to escape deep snow. There is evidence that a moose migration may occur from the west to the east, across the Susitna River after freeze up in the fall and before break up in the spring (Tim Peltier, pers. comm.). Moose wintering in the Palmer Hay Flats SGR, east of the project area, generally migrate to this location from spring/summer/fall ranges 15-45 miles to the west and northwest (Masteller, unpublished data).

**Figure 4.4**  
**ANADROMOUS FISH STREAMS**



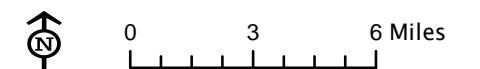
**LEGEND**

- |                           |                          |
|---------------------------|--------------------------|
| Preliminary Alternatives* |                          |
| — Mac East                | — Anadromous Fish Stream |
| — Mac West                | ○ ARRC Milepost          |
| — Conn 1                  | — ARRC Track             |
| — Conn 2                  | — Highway                |
| — Conn 3                  | — Medium Rd.             |
| — Houston                 | — Minor Rd.              |
| — Houston North           | — Iditarod Trail         |
| — Houston South           | ■ City Boundary          |
| — Willow                  | ■ Park or Refuge         |
| — Big Lake                |                          |



\*These lines generally represent corridors which are subject to further refinement.

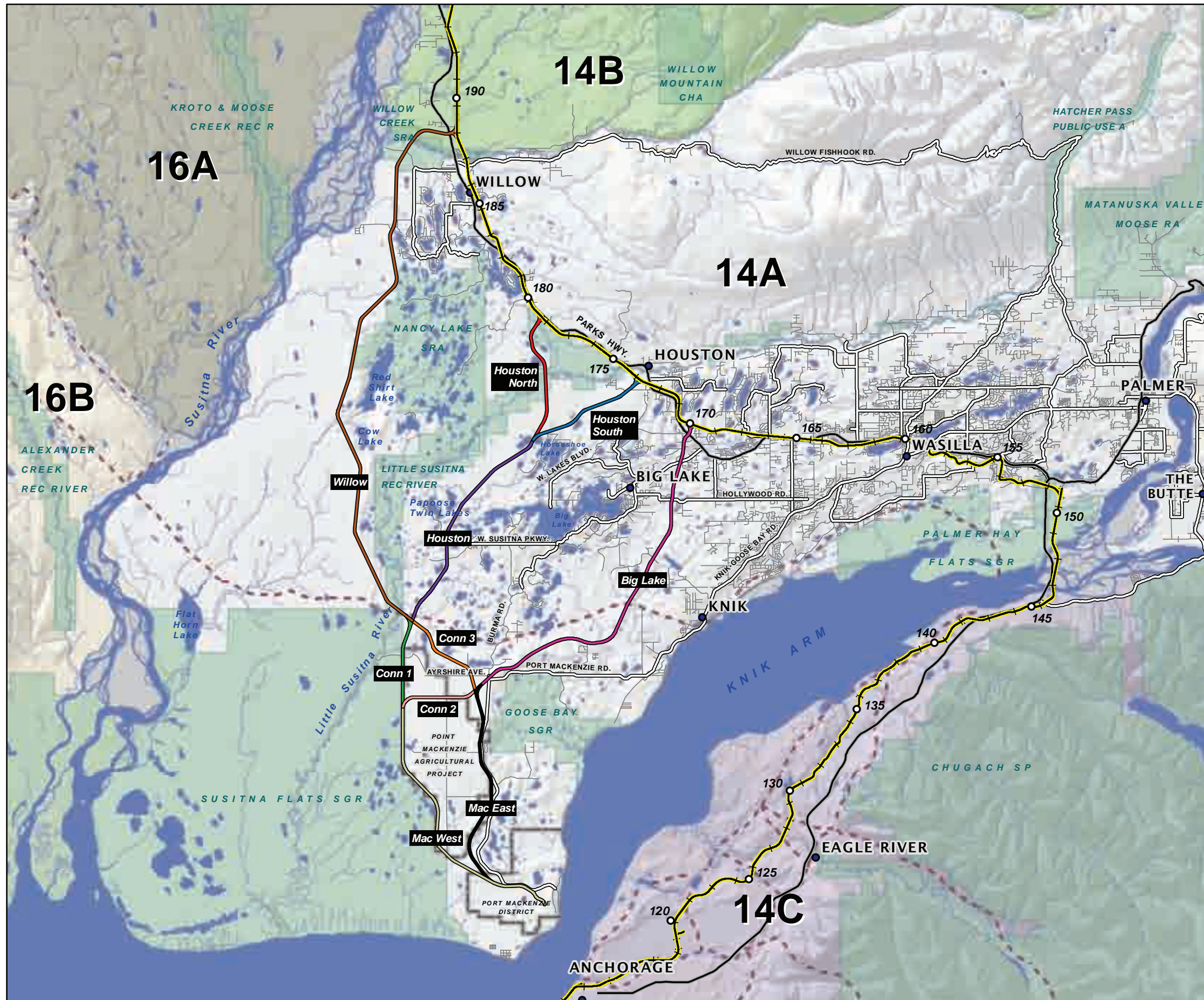
This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: December 10, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADF&G, ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

*(This page intentionally left blank.)*

**Figure 4.5**  
**GAME MANAGEMENT UNITS**

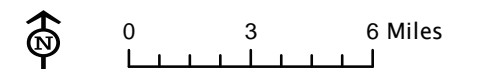


**LEGEND**

- | Preliminary Alternatives* | Game Management Units |
|---------------------------|-----------------------|
| Mac East                  | 14A                   |
| Mac West                  | 14B                   |
| Conn 1                    | 14C                   |
| Conn 2                    | 16A                   |
| Conn 3                    | 16B                   |
| Houston                   | ARRC Milepost         |
| Houston North             | ARRC Track            |
| Houston South             | Highway               |
| Willow                    | Medium Rd.            |
| Big Lake                  | Minor Rd.             |
|                           | Iditarod Trail        |
|                           | City Boundary         |
|                           | Park or Refuge        |

\*These lines generally represent corridors which are subject to further refinement.

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: December 10, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADF&G, ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

*(This page intentionally left blank.)*

**Figure 4.6  
MOOSE HABITAT**

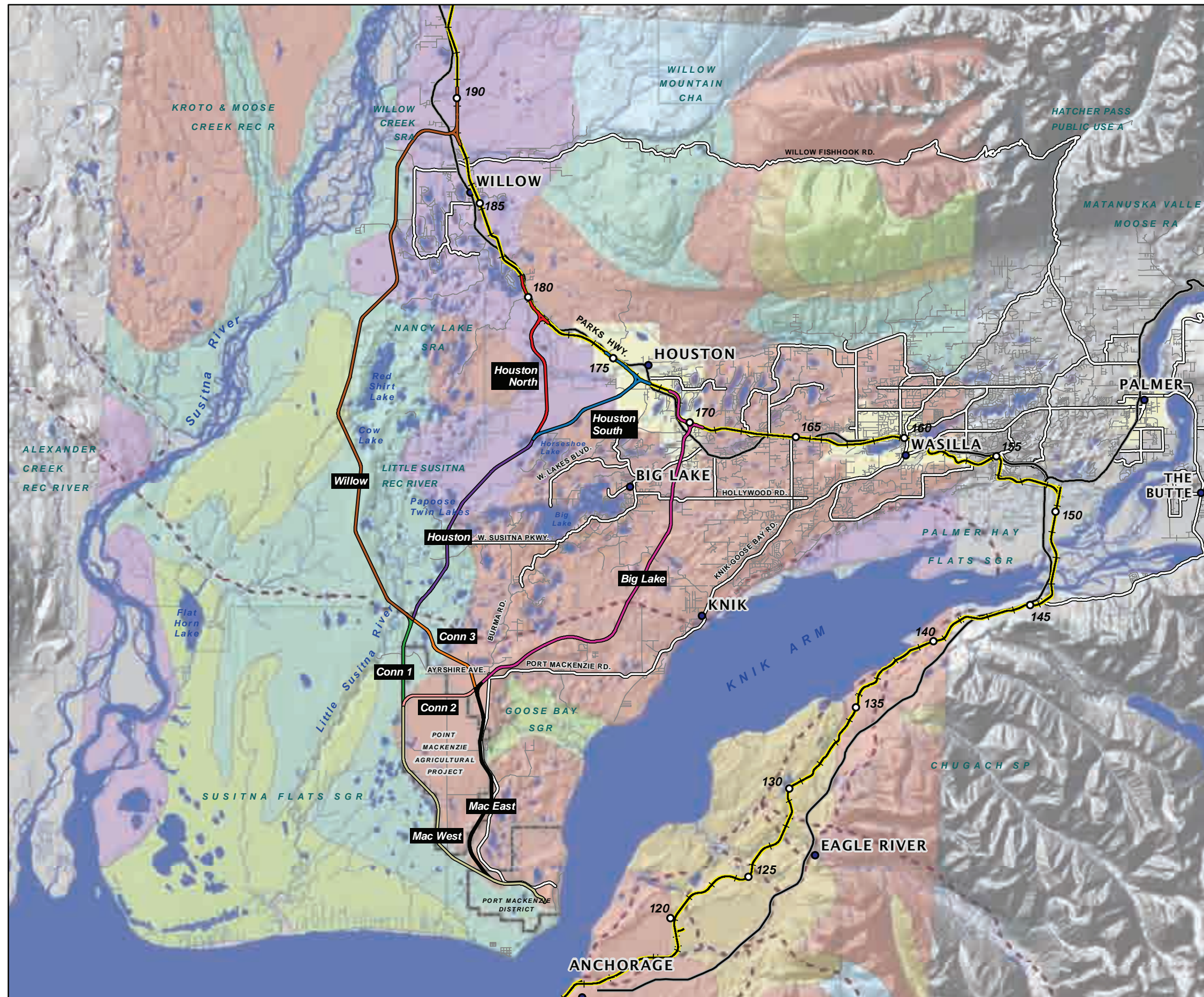


**LEGEND**

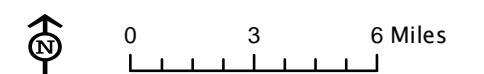
**Moose Habitat ADF&G 2005**

- Winter
- Rutting, Winter
- Calving
- Calving, Winter
- Calving, Rutting, Winter
- General

- ARRC Milepost
- ARRC Track
- Highway
- Medium Rd.
- Minor Rd.
- Iditarod Trail
- City Boundary
- Park or Refuge



This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: October 3, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

*(This page intentionally left blank.)*

According to a study conducted from 1995 through 1998, the area between Big Lake and Palmer Hay Flats SGR is considered important winter habitat for this migratory moose population (Masteller, unpublished data). The general east-west movements found in the moose populations suggest that this pattern may be prevalent for migratory moose living throughout the Susitna Valley.

Moose generally travel along major drainages throughout their seasonal habitat areas. These drainages often parallel and intersect transportation corridors (ADOT&PF 1995, BLM 2000). Alignment alternatives that intersect a larger number of drainages are likely to have a greater impact on migrating and resident moose populations in the project area.

During winters of heavy snowfall moose tend to use the plowed railway corridors for travel. When approached by a train, moose often fail to escape from these corridors as the snow may be piled too high on the sides of the railway. (Andersen et al. 1991).

### **Other Mammals**

Among others, the project area supports beaver, coyote, ermine, northern flying squirrel, river otter, lynx, mink, martin, muskrat, red fox, red squirrel, weasel, wolf, and wolverine. Most of these are found in riparian, wetland, or forested areas, which are abundant in the project area (KABATA 2006c).

### **Birds**

Freshwater bird habitats in the MSB portion of the project area include lakes and large ponds, small intermittent and perennial streams, and vegetated wetlands. These wetlands include sedge and grass bogs, and fens; shrub thickets, swamps, and bogs; and forested wetlands. Bird species that use these habitats in the project area include loons and grebes, Canada geese, several species of dabbling and diving ducks, sandhill cranes, and migrant and summer resident shorebirds, such as greater and lesser yellowlegs, short-billed dowitchers, Wilson's snipe, spotted sandpiper, and least sandpiper (KABATA 2006c).

The Migratory Bird Treaty Act (MBTA) protects all migratory birds from "take" through human-related activity. "Take" includes "taking" by any means or in any manner, including any attempt at hunting, pursuing, wounding, killing, possessing, or transporting any migratory bird, nest, egg, or part thereof. Clearing of habitat during sensitive nesting periods should be avoided. U.S. Fish and Wildlife Service [USFWS] agency letter is in Appendix C.

Terrestrial bird species include both resident and migrant species. Songbirds, or passerines, and neotropical migratory birds inhabit terrestrial habitats of the project area. Some of the common types include raptors (hawks and owls), woodpeckers, flycatchers, swallows, corvids (ravens, jay, magpies), chickadees and kinglets, thrushes, warblers, sparrows, and finches. The upland areas also are habitat for spruce grouse, and the ruffed grouse has been introduced to the Goose Bay area (KABATA 2006c).

Public input has identified impacts to sandhill cranes, loons, trumpeter swans, and migratory birds as a project concern.

Bald and golden eagles are protected by the Bald and Golden Eagle Protection Act and the MBTA. USFWS developed the National Bald Eagle Management Guidelines (May 2007), which should be followed to comply with the Eagle Act. Bald and golden eagles are present in the project area. Field surveys performed in 2004 and 2006 identified eagle nests in the area. The Willow segment is the only part of the project with mapped eagle nests that may be impacted (two nests). A project-specific nest survey may be needed to accurately map existing nests in the area. The USFWS has identified impacts to golden and bald eagles as a project concern (correspondence in Appendix C).



## Impacts to Wildlife

Each of the project alternatives would result in similar potential impacts on terrestrial mammals. Direct impacts would include the conversion of habitat to railway or rail ROW. The Mac West – Willow alignment should have the greatest conversion of habitat, since it has the greatest length. The Mac East – Big Lake alignment would have the least direct impact, for the opposite reason. Construction impacts would include the temporary or permanent displacement of mammals in the project area and potential mortality of smaller, slow-moving species (e.g. voles).

Introducing a rail corridor into a region with documented high concentrations of moose during winter months could have an impact on moose populations due to moose-train collisions. During the years 1963 to 1990, the ARRC documented an annual moose mortality ranging from 9 to 725, with the majority of moose-train kills occurring during the months of January through March (Modafferi 1991). Research points to the positive correlation between increased moose-train accidents during years of greater snowfall (Modafferi 1991; Andersen et al. 1991). Mitigation measures to reduce incidents of moose-train accidents have been implemented in various locations where frequency of such occurrences was high. Research done in Norway points to the effectiveness of vegetation removal around the railway (20-30m wide sector on each side of the railway) to reduce the number of train kills (Jaren et al. 1991). In 1988, the ARRC experimented with reducing train speeds from 49 mph to 25 mph along a 53-mile section of track between Talkeetna (MP 225) and Houston (MP 173). The experiment concluded that reducing train speed did not result in a significant reduction in the number of moose hit by trains (Becker and Grauvogel 1991).

The ARRC has been working under a Memorandum of Understanding with ADNR since 1990 in an effort to reduce moose mortality resulting from train-moose collisions. During this time period, the total annual moose deaths resulting from collisions have been reduced significantly. Significant reductions have been observed in areas where the embankment has been widened to accommodate a second track or a maintenance access road, providing a place for a moose to retreat as a train passes. For this project, the ARRC proposes a maintenance road adjacent to the track to provide such an area. This design, coupled with proactive clearing of snow and vegetation control is expected to minimize moose mortality resulting from collisions with trains.

Habitat fragmentation has been highlighted as a major concern of State and Federal resource agencies. The rail embankment would be unlikely to create a barrier to terrestrial wildlife movement, but it could be an obstacle to normal travel and could alter wildlife movement patterns across use areas. The ADF&G and ADNR OHMP have identified the Mac West – Willow alignment as causing the greatest amount of habitat fragmentation of the alternatives (agency letters in Appendix C). This is contrasted with the Big Lake, Mac East and Houston South alignments. The Big Lake alignment has sporadic human development along its length already substantially fragmenting habitat. The Mac East and Houston South alignments, traversing in between developed areas and un-developed areas, effectively provide delineation between habitat already fragmented, and habitat that is relatively undisturbed.

The MBTA protects migratory birds in the project area from “take” through human-related activity. The USFWS has suggested avoiding vegetation clearing during a timing window of May 1 through July 15 to avoid sensitive nesting areas (correspondence in Appendix C). Bald eagles nest in Alaska between approximately February and September, and nesting surveys may be required prior to construction. A 660-foot buffer around each active eagle nest is recommended if the construction activity would be visible from the nest, and 330 feet if it would not be visible. Some impacts to freshwater birds may occur because of the loss of wetland habitat resulting from the placement of fill in wetlands within the railbed footprint; however, this is not expected to be a significant impact since there is ample similar habitat in the vicinity.

Construction of any of the alternatives would result in the permanent loss of habitat for terrestrial birds within the railbed footprint and adjacent cleared areas. Direct impacts would include the loss of food sources, cover, breeding grounds, and roosting sites. Again, this is not expected to be a significant impact since there is ample similar habitat in the vicinity.

### **Threatened and Endangered Species**

No threatened or endangered species listed under the Endangered Species Act (ESA) have been identified in the project area (USFWS 2007). No State-listed endangered species are found within the project area (ADF&G 2007b).

Four bird species—the olive-sided flycatcher, the blackpoll warbler, the Townsend’s warbler, and the greycheeked thrush—are each listed as an Alaska Species of Special Concern and may be found in the project area. The four Species of Special Concern are all neotropical migrants that appear to nest in low densities in the project area, but no local abundance or trend information is available. The primary conservation concern for these species is habitat loss in both nesting and wintering areas due to logging, fire suppression, and road building. Pesticide contamination and increased predation as a result of habitat fragmentation are also concerns (KABATA 2006c).

Olive-sided flycatchers and blackpoll warblers are regularly recorded in breeding-bird surveys on both sides of Knik Arm, while gray-cheeked thrush and Townsend’s warblers are less commonly observed (KABATA 2006c). The species all nest in mixed deciduous/coniferous forests typical of the project area. Although they should be displaced by vegetation clearing for construction, the project would not impact these species of concern because there is ample similar habitat for nesting, and because clearing activities would be avoided between May 1 and July 15 (USFWS agency letter in Appendix C). Because no State-listed endangered species are found in the project area, the project would not adversely affect State-listed endangered species.

#### *4.2.5 Geology and Soils*

Soil deposits and landforms in the project area have been affected by glacial advances. The most recent glacial advance in the area was the Naptowne Glaciation of approximately 47,500 to 10,000 years ago (USDA 1995). The Little Susitna River and associated tributaries have cut through these deposits and distributed fluvial sediments throughout the project area; in addition, wind and volcanic activity have added sediments to the region.

Surficial geology in the project area shows evidence of the landscape’s glacial origin, with moraine and outwash features throughout the region. As a result, much of the area’s near-surface sediments are reworked soils comprising silt loams, gravels, and sands. Near-surface soils are interspersed with peat bogs in lower lying areas. Underlying native soils consist largely of glacial till, which is typically very dense and poorly graded, or coarse, gravelly sand.

Prior to construction, geotechnical exploration and analysis may be conducted to evaluate the amount of excavation and fill required. Typically, outwash sand and gravels are well suited for embankment materials needed for construction, while poorly-drained and organic-rich overburden, loess, and bog soils are not, often requiring removal and replacement with more suitable material. Moraine deposits that may be locally available for borrow material are often moisture sensitive and difficult to compact in wet conditions.

A geotechnical reconnaissance of the project area, conducted in August and September 2007, identified areas on each alignment that would likely require removal and replacement with fill material. This information, when combined with conceptual engineering, has been used to estimate the cubic yards (CY) of poor or highly compressible soils (and therefore, likely to require removal and replacement by fill) associated with each alignment. Table 4-4 presents these

initial calculations. Appendix H lists additional information and figures regarding compressible soil locations.

**Table 4-4: Unsuitable soils identified by project alternative**

<b>Project alternative</b>	<b>Poor or highly compressible soils (in million CY)</b>
Mac West – Willow	1.15
Mac West – Houston – Houston North	1.73
Mac West – Houston – Houston South	1.19
Mac West – Big Lake	0.66
Mac East – Willow	1.05
Mac East – Houston – Houston North	1.62
Mac East – Houston – Houston South	1.08
Mac East – Big Lake	0.56

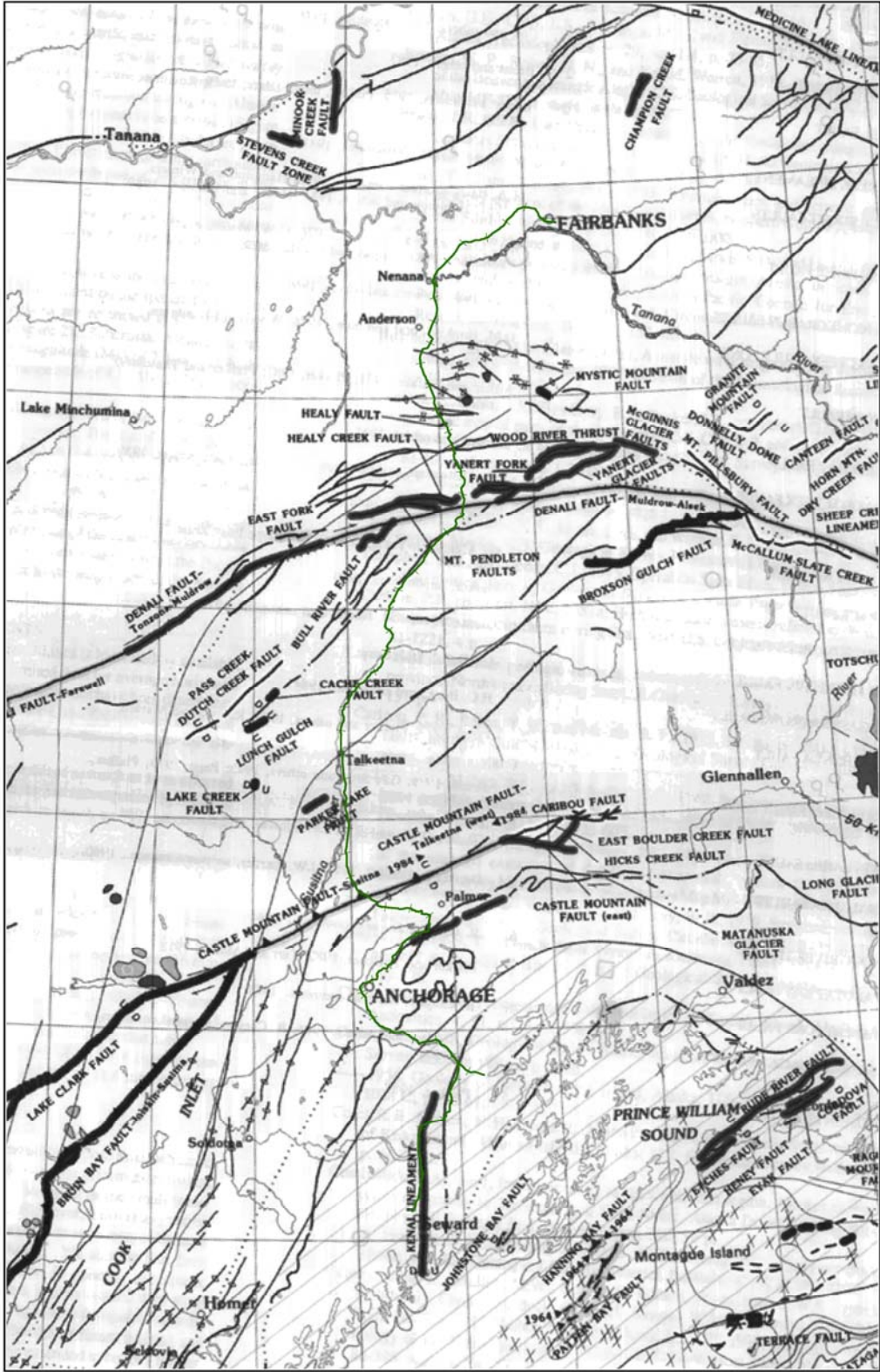
Source: TNH-Hanson 2007.

As shown in Table 4-4, the Mac West – Big Lake and Mac East – Big Lake alignments have the least unsuitable material for project construction, while the Mac East – Houston – Houston North and Mac West – Houston – Houston North alignments have the greatest amounts of unsuitable material. This is likely due to the Big Lake segment of the project being located on a glacial moraine, which is typically highly compacted and stable material. The Houston North segment occupies soils that are predominantly soft, with compressible soils ranging from 3 feet to more than 20 feet deep.

### **Seismic Hazards**

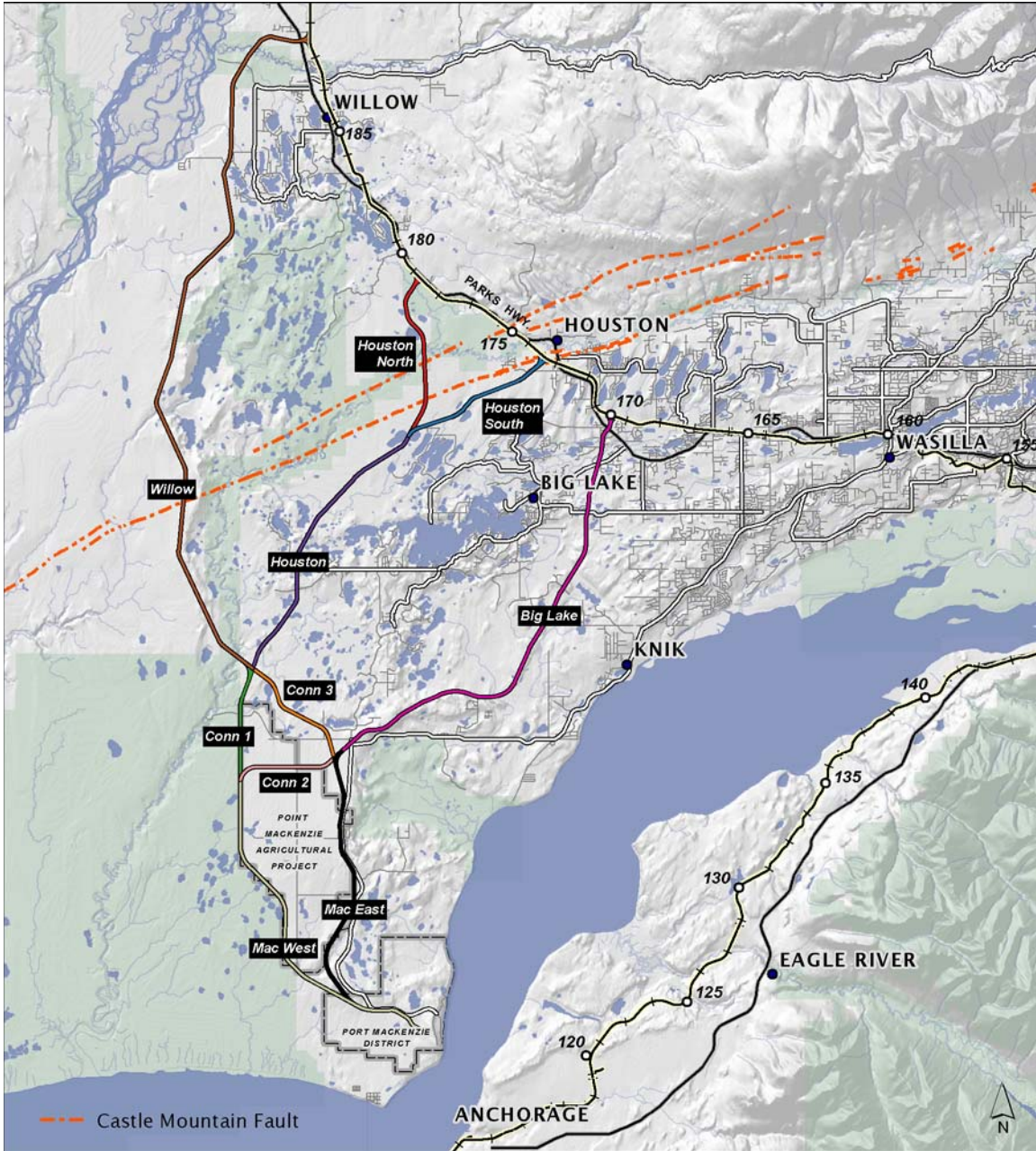
The ARRC track has historically been very resilient to the effects of seismic activity across the state (Cliff Fuglestad, pers. comm.). Figure 4.7 shows the major faults crossed by the ARRC system-wide, from Seward to Fairbanks. Although the track crosses several major fault systems, it has held up substantially well in past seismic events. Structures such as bridges (wood bridges in particular) and culverts seem to be most affected by seismic waves and their ancillary effects such as compression and compaction of soils, as well as upward movement of sediments within stream channels (USGS 1970). There is a high potential for seismic activity within the project area. Potential ancillary effects of a substantial seismic event include soil settlements, compaction, landspreading and landsliding, liquefaction, subsidence, ground cracking, and tsunamis (USGS 1970). All of these effects were observed in the March 27, 1964 earthquake.

The Castle Mountain Fault is the closest crustal fault in the region (USGS 2002) and trends approximately southwest-northeast across the project area (Figure 4.8). This fault has produced light to moderate 5.7 and 4.6 magnitude earthquakes in 1983 and 1996, respectively. The fault is projected to be capable of a 7.3 magnitude earthquake based on analysis of historical events by USGS. Seismic hazards associated with the fault include potential liquefaction and surface rupture. Recent research and field study by Dr. Peter Haeussler, USGS, suggested that discontinuities in terrain features and vegetation distribution may be related to surface rupture along the Castle Mountain Fault trace. Dr. Haeussler described strike slip displacement on the order of 5 feet of lateral movement in the Houston, Alaska vicinity over a 2,700 year time span.



Source: Neotectonic map of Alaska in Geology of Alaska, Geology of North America, in Decade of North American Geology: Boulder, Geological Society of America, v. G-1, plate 12

**Figure 4.7 Major Faults Along Railbelt**



**Figure 4.8 Castle Mountain Fault**

In addition to seismic events associated with the Castle Mountain Fault, earthquakes associated with the Alaska Subduction Zone (Aleutian Megathrust and deeper Benioff Zone quakes) could cause moderate intensity and long duration seismic vibrations in the study area.

According to an overview report describing damage to the ARRC track between Anchorage and Seward following the 1964 earthquake, 47 miles of the main line and 5 miles of side track were rendered unsafe for service due to severe subsidence and failure of the sub-grade (Fuglestad 1983). Much of this distress was related to subsidence (drop in the railroad embankment) from seismically induced compaction and/or spreading associated with vibration, poorly consolidated granular soil, and shallow water table. The extensive subsidence that occurred during the 1964 earthquake was associated with the rebound effect of the subduction fault that the earthquake

originated from (USGS 1970). Localized compaction of soils also contributed to this effect (USGS 1970). The Castle Mountain fault is a strike slip fault; any subsidence that would occur as a result of seismic activity originating from this fault would likely be attributed solely to the settling and compaction of substrate material.

Local geology, topography, and substrate composition determine what kind of secondary effects may result from seismic activity. The greatest damage to infrastructure as a result of the 1964 earthquake was observed in areas with thick unconsolidated sediments, shallow water table, and proximity to topographic depressions, relative to the area. The areas closest to the seismic energy release were observed to suffer the most damage (USGS 1970). Areas underlain by wet, alluvial, non-cohesive sediment deposits seemed to be the sites of the greatest concentration of damage. More damage was probably avoided due to the presence of frozen soils in late March (Cliff Fuglestad, pers. comm.).

Landspreading, the spreading of unconsolidated granular sediments, resulted in expansion within the substrate sediments and occurred on flat areas such as fans and deltas. This phenomenon is analogous to landslides in relatively flat areas. Damage associated with landspreading was most notable in areas with topographical depressions, as the substrate spread laterally to fill in these areas, carrying along embankment with them. Landspreading occurs due to liquefaction, although not exclusively (USGS 1970).

Liquefaction occurs when individual particles of substrate material compress as a result of sudden impact by seismic vibrations (Holtz and Kovacs 1981). As soil particles compress, the water pressure increases within the pores of the substrate at such a rate that the water has insufficient time to dissipate, causing the soil to lose all strength. This leads to a failure in soil stability and cause the soil mass to flow (Holtz and Kovacs 1981). The potential for liquefaction decreases as the soil water content decreases and the density of substrate particles increases; therefore, well-drained, dense soils have greater stability when subjected to seismic waves. As a result, proper compaction and drainage of railroad embankment can improve its resistance to liquefaction. However, with sustained vibrations associated with high stress, long lasting seismic waves, the likelihood that substrate material would be subject to settlement and liquefaction increases, regardless of how dense the material is.

Different substrate compositions liquefy at different capacities. Observations of the effect of the 1964 earthquake on bridge pilings conclude that the superficial substrate did not completely liquefy, and that liquefaction was present throughout multiple layers of sediment, extending deep into the substrate (USGS 1970). Ground cracking was also observed along the banks of streams. Uplift of the stream beds and valley floors indicated liquefaction at depths that created lateral movement of underlying material toward low-lying areas. In these low areas, the liquefied substrate raised streambeds and moved the more stable surface sediments toward these lower areas. This caused ground cracking and ruptures that separated ARRC track in certain areas where cracking was perpendicular to the embankment. Cracks also formed parallel to the embankments where settling occurred in the substrate material under the embankment fill. Severe ground cracking occurred in low-lying, active alluvial areas that were associated with a shallow water table (USGS 1970).

All of the alternatives have the potential to be impacted by geologic and seismic hazards identified in the study area. A map of the alignment alternatives in relation to the seismic faults within the study area are presented in Figure 4.8. Given the proximity of the alternatives to each other, it is likely that any large seismic event would have an effect on all of the alignment alternatives. Alignments with more structures such as bridges and culverts are expected to be more vulnerable to damage during a major seismic event. In addition, routes of shorter length, with less track to repair, are expected to be less susceptible to long term track outages following a

major seismic event. Table 4-5 presents a comparison of the project alternatives by the total length and number of major bridge structures. As is shown in the table, alternatives that include the Willow and Big Lake segments would be the most susceptible to outages due to their length and number of bridges. It should also be noted that the location of the water crossing on the Houston South siding would allow for a more efficient repair and diversion of train traffic over the existing mainline bridge at this location. Therefore, the two shortest alternatives, Mac East or West – Houston – Houston South, would be less susceptible to outages than longer alignments with more major bridge structures. Numerous culverts are associated with each alignment alternative to provide for uninterrupted water flow in small drainages and wetland areas. The length of track is directly proportional to the number of culverts on each alignment.

**Table 4-5: Proposed Structures**

<b>Project Alternative</b>	<b>Length of Track (Miles)</b>	<b>Major Water Crossings</b>	<b>Grade Separated Crossings</b>
Mac West – Willow	44.8	2	1
Mac West – Houston – Houston North	35.1	1	0
Mac West – Houston – Houston South	34.5	1	0
Mac West – Big Lake	35.8	0	2
Mac East – Willow	45	2	1
Mac East – Houston – Houston North	35.3	1	0
Mac East – Houston – Houston South	34.7	1	0
Mac East – Big Lake	31.8	0	2

New railroad structures and embankments are designed in accordance with the standards and guidelines recommended by AREMA. These guidelines include stringent seismic considerations, not in place at the time of the 1964 earthquake, which would be expected to partially mitigate many of the impacts to the track associated with that event; however, despite modern design practices, embankments for all alternatives would be subject to potential ground ruptures on the order of 5 feet from the Castle Mountain Fault. These types of surface rupturing events could displace and distort the tracks and result in full or partial closure until repairs could be completed. Much smaller displacements are anticipated on lesser faults or splays off of the primary fault. These smaller displacements would have less potential for disruption of the rail lines given their construction on a raised surface of ballast and sub-ballast. The rail and ballast system would tend to float or bridge and accommodate small magnitude settlements and ground movements. Since the results of geotechnical investigation and detailed seismic design would help ensure that the embankment is constructed properly, the effects of fault rupture and ground shaking during a nearby major earthquake are expected to be moderate. Potential interruption of rail operations and damage to structures that can be repaired could occur, but catastrophic structural failure is not anticipated. The effects of ground shaking during smaller earthquakes, as well as the effects of other earthquake-related hazards such as liquefaction and subsidence, are expected to be minor through the study area, as they would have localized effects that could potentially cause short-term disruption of activity.

Smaller magnitude fault displacements may be wholly or partially mitigated by the basic resiliency of the rail-ballast system. Large magnitude fault displacements remain likely in the

future and could damage structures within the study area. There are no practical means for constructing a rail embankment that can withstand multiple feet of lateral displacement of the kind possible in the study area.

Although Alaska is one of the most seismically active regions in the nation, there has never been a derailment caused by an earthquake. ARRC has specific protocol in place to avoid and minimize any potentially hazardous situations that may result from damage associated with earthquakes and the ancillary effects of seismic waves.

To minimize the potential for disruption to train operations from earthquakes, ARRC currently implements the following procedures:

- Restrict train speed or halt train movement (depending on severity of quake) as quickly as possible once ground shaking is experienced or an earthquake is reported with an epicenter near the track;
- Inspect tracks and adjacent areas within a specified radius of the epicenter with a focus on the identified fault crossings and areas underlain by young settlement-sensitive or liquefaction susceptible soils; and
- Repair any areas of deflected or distressed track and restore/re-level any areas of differential settlement or disturbed ballast.

## **4.3 Human Environment**

### *4.3.1 Land Use and Land Use Plans*

The project would be located from the Big Lake/Houston/Willow area to Point MacKenzie area of the MSB (Figure 1.1). The only incorporated city in the project area is the City of Houston. The Willow, Big Lake, Knik-Fairview, and Point MacKenzie community councils are the community councils in the area.

#### *4.3.1.1 Land Ownership*

In the Point MacKenzie area, land is owned by government, as well as private and institutional land holders (Figure 4.9). Government land is owned primarily by the State and MSB, with only a few parcels owned by the Federal government. The Federal government parcels include a post office near Willow Lake and several parcels on Flat Lake near Big Lake. Institutions holding land in the project area include the University of Alaska and The Trust. Land owned by these institutions is used to generate revenue for their on-going operations. Institutions often own large tracts of land which can reduce the number of landowners to negotiate with during the ROW acquisition phase. Large tracts of land within the project area (seen in Figure 4.9 as grey) are recorded as “NO DATA” in the MSB GIS parcel data. ADNR Division of Mining, Land and Water (DMLW) confirmed that these lands were in fact State lands, and as such, were included with State lands in all analyses (ADNR DMLW meeting notes in Appendix D).

Private land can be owned by an individual or a corporation including an Alaska Native corporation. A Native corporation administers the land and financial resources awarded under the Alaska Native Claims Settlement Act. Village corporations own the surface rights to their lands while regional corporations own the surface and subsurface rights to their land and the subsurface rights to village corporations’ lands. Native corporation land is often used for subsistence purposes or developed/ sold to generate revenue for the corporation. Native corporation land is also often held in large tracts. Native corporation-owned lands within the project area consist of



parcels owned by the Cook Inlet Region, Inc. (CIRI), a regional Native Corporation, and the Knikatu, Inc, a village Native Corporation.

Native corporation land is different from a Native allotment. A Native allotment refers to the land given to authorized, individual Indians, Aleuts, and Eskimos in Alaska under the Native Allotment Act of 1906. No Native allotments are expected to be impacted by any alternative.

The rest of the land in the area is privately held. Table 4-6 summarizes land ownership by alternative.

**Table 4-6: Land Impacted by Ownership Type**

Alternative	Ownership Type (in acres) *					
	Private	Borough	State	Native Corporation	The Trust	University
Mac West – Willow	238	359	445	6	15	16
Mac West – Houston – Houston North	206	181	251	12	143	59
Mac West – Houston – Houston South	233	189	126	85	158	59
Mac West – Big Lake	396	246	93	83	16	23
Mac East – Willow	183	410	375	81	22	16
Mac East – Houston – Houston North	150	232	181	87	151	59
Mac East – Houston –Houston South	177	240	55	160	165	59
Mac East – Big Lake	307	284	29	107	24	23

\* Acreage based on a 200-foot ROW for each alternative.

#### 4.3.1.2 Land Use and Zoning

The MSB does not have a Borough-wide zoning code but it regulates land use through special land use districts, residential land use districts, and other mechanisms. Figure 4.10 shows land use<sup>d</sup>. The special land use districts in the project area include:

- Nancy Lake Recreation Area Special Land Use District
- Port MacKenzie Special Land Use District
- City of Houston

All alternatives begin in the Port MacKenzie Special Land Use District. Rail transportation corridors are a permitted use under the district regulations. The Nancy Lake and City of Houston Special Land Use District regulations do not address transportation corridors. They govern

<sup>d</sup> Land use data was not available for parcels in the project area so MSB Tax Assessor codes (GIS data dated June 19, 2007) were used as a proxy. The building activity codes were grouped together into general land use categories. A tax assessor code was not available for every parcel. The tax assessor codes are subject to change by the MSB.

**Figure 4.9  
LAND OWNERSHIP**



**LEGEND**

- Native Allotment
- General Land Ownership
  - Borough
  - Mental Health
  - Native Corporation
  - Private
  - Public University
  - State

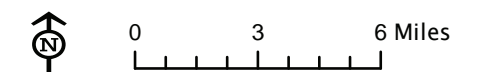
Preliminary Alternatives\*

- Mac East
- Houston
- Mac West
- Houston North
- Conn 1
- Houston South
- Conn 2
- Willow
- Conn 3
- Big Lake

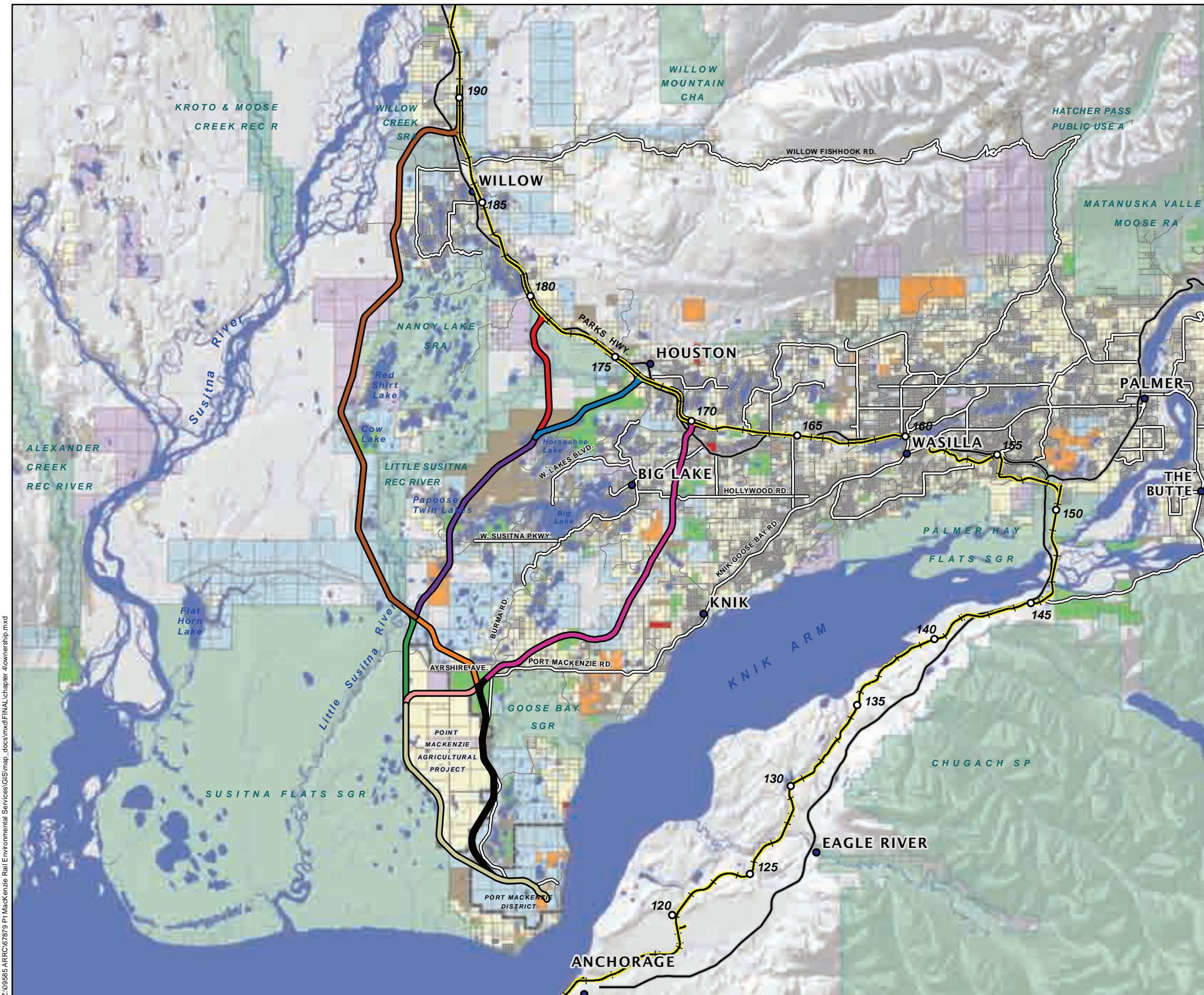
- ARRC Milepost
- ARRC Track
- Highway
- Medium Rd.
- Minor Rd.
- Park or Refuge

\*These lines generally represent corridors which are subject to further refinement.

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: December 21, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.



Z:\09585 ARRC\67879 P1\Mackenzie Rail\Environmental Services\GIS\map\_docs\mx\of\FINAL\chapter 4\ownership.mxd

*(This page intentionally left blank.)*

**Figure 4.10  
LAND USE**



**LEGEND**

Land Use

- Agricultural
- Commercial/Industrial
- Institutional
- Private/Residential

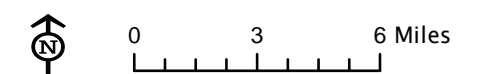
Preliminary Alternatives\*

- Mac East
- Houston
- Mac West
- Houston North
- Conn 1
- Houston South
- Conn 2
- Willow
- Conn 3
- Big Lake

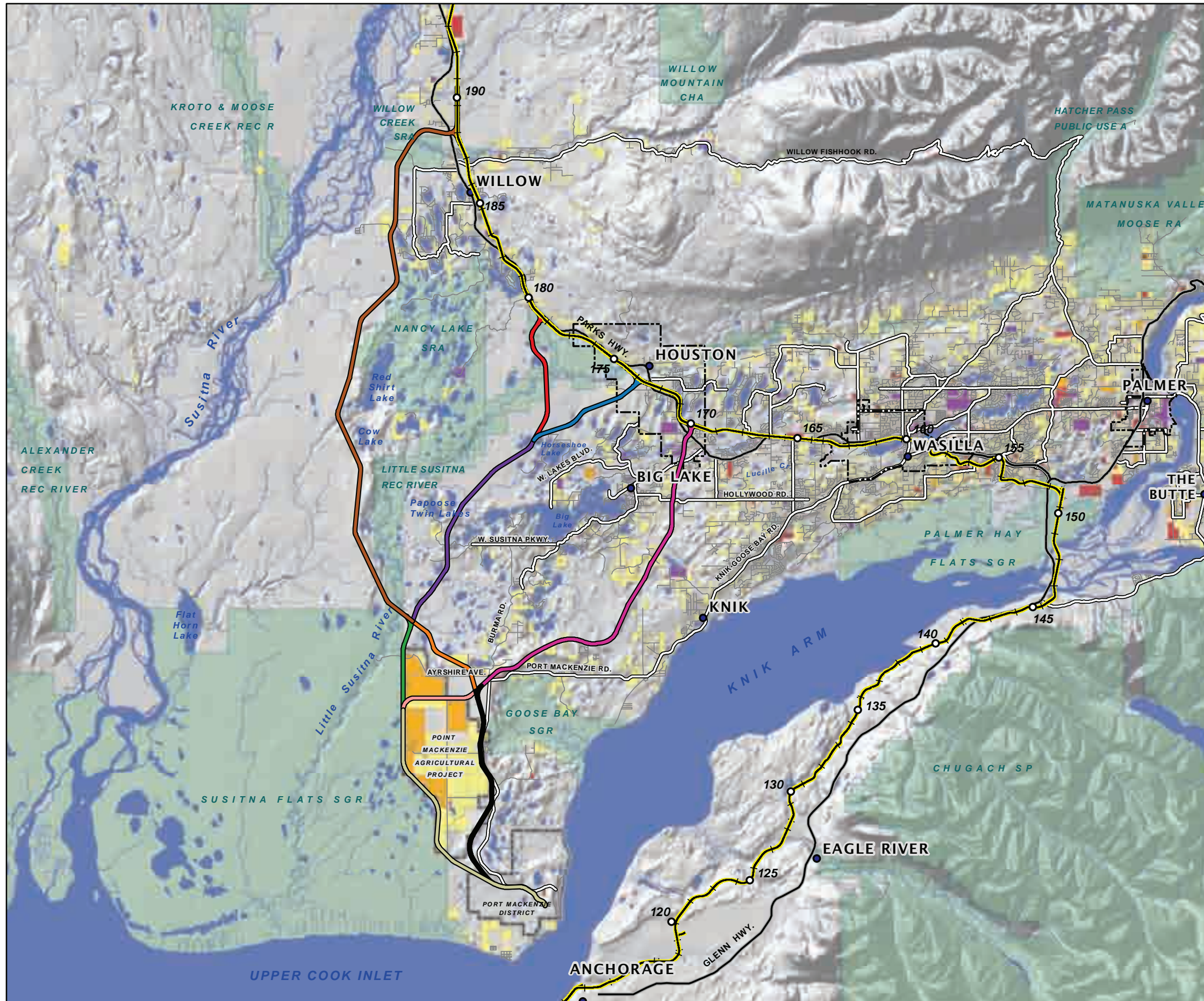
- ARRC Track
- ARRC Milepost
- Highway
- Medium Rd.
- Minor Rd.
- City Boundary
- Park or Refuge

\*These lines generally represent corridors which are subject to further refinement.

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: November 5, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.



*(This page intentionally left blank.)*

minimum lot sizes, setbacks and other private development requirements that could influence the process for acquiring land from parcels in these areas.

The intent of residential land use districts in the area is to create future development that is compatible with the residential nature of the existing development. In general, transportation needs are not addressed in the regulations. Further consultation with the MSB would be required to determine how a rail corridor can be designed to be an acceptable land use. Chapter 17.52 of the MSB Code, “Residential land use district,” for more information on the regulations governing these districts. The residential land use districts in the project area are:

- Kelly Lake Area Residential Land Use District
- Lynne Lake Area Residential Land Use District
- Nancy Lake Northwest Shore Residential Land Use District
- Dawn Lake Estates No. 1 Subdivision Residential Land Use District
- Nancy Lake Northeast Shore Residential Land Use District

As mentioned in Section 2.0, the ARRC has tried to avoid private land when developing the corridors and refining the alternatives to reduce the potential impacts of the project to residences (e.g., from noise). In addition, avoiding developed parcels was intended to help reduce the ROW costs of the project because undeveloped land is usually less expensive than developed land. Table 4-7 shows the number of parcels by development status that lie within 100 feet of either side of the centerline (200 feet total width) of each alternative.

**Table 4-7: Parcels by Development Status within 200 feet of the Alternative Centerline**

Alternative	Government		Institutional		Native Corporation		Other/Private	
	Undeveloped	Developed	Undeveloped	Developed	Undeveloped	Developed	Undeveloped	Developed
Mac West – Willow	59	0	2	0	1	0	27	15
Mac West – Houston – Houston North	44	0	16	0	2	0	5	13
Mac West – Houston – Houston South	37	0	17	0	6	0	9	13
Mac West – Big Lake	33	3	4	0	6	0	32	32
Mac East – Willow	58	0	2	0	5	0	31	7
Mac East – Houston – Houston North	43	0	16	0	6	0	9	5
Mac East – Houston – Houston South	36	0	17	0	10	0	13	5
Mac East – Big Lake	35	2	4	0	7	0	34	24

A large percentage of the land in the project area has not been developed. For the developed parcels, the current land uses are diverse, but the dominant land use based on number of parcels is residential. Other land uses include commercial, industrial, institutional, and agricultural. Table 4-8 summarizes the acreage of each type of land use for the parcels (or portions of parcels) that lie within 100 feet of either side of the centerline (200 feet total width) of each alternative.

A large amount of the government-owned land in the area is designated as state game refuge or state recreation area. According to MSB GIS data, other government land uses in the area include schools, communication facilities, transportation facilities, post offices, and other government facilities. Most of the land held by institutions is undeveloped.

The proposed rail extension would not provide access to adjacent properties as no depots or flag stops are included in the project. Therefore, additional urban/suburban development is not expected to occur as a result of this project. Growth would occur as a result of the general development pressures that already exist in the MSB, regardless of whether this project is constructed.

**Table 4-8: Summary of Land Use**

<b>Alternative</b>	<b>Residential</b>	<b>Commercial</b>	<b>Government</b>	<b>Agricultural<sup>a</sup></b>	<b>Parks<sup>b</sup></b>	<b>Unknown</b>	<b>Other</b>
Mac West – Willow	30.5	0	0	138.7	174.7	955.7	0
Mac West – Houston – Houston North	33.2	2.9	0	138.7	169.9	707.9	0.9
Mac West – Houston – Houston South	24.3	0	0	138.7	98.4	713.2	0
Mac West – Big Lake	98	0	4.9	161.3	60.2	611.2	21.6
Mac East – Willow	6.2	0	0	56.2	79.5	1048.9	0
Mac East – Houston – Houston North	8.9	2.9	0	56.2	74.7	801.1	0.9
Mac East – Houston –Houston South	0	0	0	56.2	3.2	806.4	0
Mac East – Big Lake	73.7	0	4.9	56.2	0	654.7	21.6

a. MSB parcel land use information was supplemented with the Point MacKenzie Agricultural Project boundary. Parcels within the boundary were considered agricultural land use even though many were coded as residential.

b. Park impacts were derived from using the ADNR GIS dataset on parks, refuges, and recreation areas.

#### 4.3.1.3 Agricultural Land

The MSB has a long farming history, and the MSB is important for agriculture in Alaska. Many of the original agricultural areas near Palmer and Wasilla have been converted to other uses due

to development pressure. The Point MacKenzie Agricultural Project is primarily used for dairy farming. The future viability of these dairy farms is uncertain with the impending closure of the Matanuska-Maid Dairy. It is unknown if farmers would find a use for their products or if they would have to switch to a different type of farming.

The State of Alaska's Point Mackenzie Agricultural Project is the largest contiguous agricultural area in Alaska, totaling over 15,000 acres. As a result, retaining the commercial viability of this area is important to the Alaska DOA (Steve Trickett, pers. comm.). In addition to this state-designated area, there are other parcels used for agricultural purposes in the area (Figure 4.10). Identification of agricultural land uses was done by querying MSB parcel land use codes. Also, parcels located within the Point MacKenzie Agricultural Project were considered as "agricultural" land use, even though many were coded as "residential." It is possible other lands are currently being used for agricultural purposes but are not coded as such within the MSB GIS parcel dataset. Based on MSB parcel data, none of the alternatives impacts lands known to be in current use as agricultural land outside the Point MacKenzie Agricultural Project.

Given the dis-contiguous railroad specific easements existing on some agricultural parcels, it would seem that the establishment of a contiguous railroad ROW along the edge of the agricultural area would be consistent with existing land covenants.

**Mac West – Willow**

This alternative runs along the boundary between the Point MacKenzie Agricultural Project and the Susitna Flats SGR. It was assumed that the ROW would be acquired from the agricultural parcels and the Susitna Flats SGR although the final ROW requirements have yet to be determined. The opportunity for a land swap between the agricultural areas and the Susitna Flats SGR should be explored further. It is assumed that land from 12 agricultural parcels would be acquired. On seven parcels, the land would be acquired from the edge of the parcel which is less likely to severely impact agricultural activities. On the remaining 5 parcels, the alternative crosses the parcel and divides it into two pieces. The western portion of the parcel would be inaccessible without crossing the railroad tracks. Further research would be necessary to determine if this would have an impact on agricultural activities.

While the acquisition of land may negatively impact an individual parcel, it is unlikely that it would impact the agricultural viability of the area. The State Farm Conservation Plan for each affected parcel may need to be revised. The purpose of a State Farm Conservation Plan is to ensure proper site-specific soil and water conservation measures are used to preserve the land's suitability for agriculture. Having an approved State Farm Conservation Plan is one of the conditions placed on agricultural parcels when they were sold by the State (11 Alaska Administrative Code [AAC] 67.177 Farm conservation plan).

**Mac West – Houston – Houston North**

This alternative has the same impacts to agricultural land as the Mac West – Willow alternative.

**Mac West – Houston – Houston South**

This alternative has the same impacts to agricultural land as the Mac West – Willow alternative.

**Mac West – Big Lake**

This alternative bisects the northern half of the Point MacKenzie Agricultural Project. This alternative would require acquiring property from 10 agricultural parcels. It would leave a portion of six parcels inaccessible without crossing the railroad tracks. In addition, the Mac West segment of this alternative would act as a barrier between two parcels under the same ownership. It is assumed that these parcels are jointly operated for agricultural purposes. Additional consultation would be required to determine the impact this would have on farming activities.



It is assumed that the alternative would go to the south of the Point MacKenzie Correctional Farm and would have no negative impacts to the farm. The State Farm Conservation Plan for each affected parcel may need to be revised.

#### **Mac East – Willow**

This alternative follows the eastern edge of the Point MacKenzie Agricultural Project. It requires acquiring land from six agricultural parcels. It separates one parcel owned by the University of Alaska system from the rest of the agricultural area.

#### **Mac East – Houston – Houston North**

This alternative has the same impacts to agricultural land as the Mac East – Willow alternative.

#### **Mac East – Houston –Houston South**

This alternative has the same impacts to agricultural land as the Mac East – Willow alternative.

#### **Mac East – Big Lake**

This alternative would impact the same number of parcels as the other three alternatives that use the Mac East segment.

#### *4.3.1.4 Regional Plans*

Consistency with local and regional plans is important to ensure that future development helps implement the government's vision of the area and is being done in a coordinated manner. There are many local and regional plans applicable to the project area. Below is a brief discussion of the relevant plans. Consistency with the MSB's coastal zone management plan is discussed under coastal consistency in Section 4.3.7.

#### **Long Range Transportation Plan**

The MSB LRTP recommends the development of a rail connection between the ARRC main line and Port MacKenzie. The alignment included in the LRTP generally follows the Mac West – Willow alternative (MSB 2007a). This route was identified as the proposed route in the 2003 Rail Corridor Study (MSB 2003a). That study was commissioned by the MSB and emphasized engineering criteria in the alternative selection process. This project is a continuation of that work and considers environmental factors as well as engineering criteria in the alternative development process.

#### **MSB Comprehensive Plan**

The proposed project was adopted by resolution into the MSB Comprehensive Plan (MSB 2005). The comprehensive plan provides general goals and policy recommendations for future development in the borough. The plan relies on community-council based plans to identify local goals and objectives as well as how to implement the Borough's goals and objectives in that area. The alternatives cross multiple local community planning areas. Each local plan is briefly discussed below.

- Big Lake Plan. According to the Big Lake Comprehensive Plan (MSB 1996), the community wants to remain a primarily residential and recreational community within a regional context. The plan acknowledges the need for a rail corridor to Port MacKenzie provided it is compatible with the existing residential and recreational character of the area.
- Knik-Fairview Plan. The Knik Fairview Comprehensive Plan (MSB 1997) aims to maintain the area's existing rural, low density residential development pattern. Local residents value the area's privacy, recreational opportunities and clean environment.

Growth is acceptable provided it does not negatively impact the area's natural resources, environment, or quality of life. In addition, protecting trail resources is an important element to the plan. Further consultation would be required to determine if the proposed project is consistent with the plan.

- Houston Plan. The Houston Comprehensive Plan (MSB 2003b) wants to maintain the area's rural character and quality of life. One goal of the plan is to broaden and strengthen the economy. The plan appears to support a rail corridor if it is associated with additional economic development opportunities.
- Willow Plan. The project team was unable to obtain a copy of the existing 1970 Willow comprehensive plan. The community of Willow is currently working with the MSB to update its comprehensive plan. Additional consultation would be required to determine if the proposed project is consistent with the updated plan.

### **Asset Management Plan**

The MSB Asset Management Plan for parks, recreation and open space (MSB 2001) provides guidance for the MSB's recreation and leisure time resources to ensure that the future demand for parks, recreation and open space can be met. This plan does not include or exclude the possibility of a rail corridor in the project area.

### **Economic Development Strategy**

A goal of the Matanuska-Susitna Comprehensive Economic Development Strategy (MSB 2006) is to support development of a rail spur to Port MacKenzie. The proposed project would achieve this goal and is therefore consistent with the plan.

### **MSB Trails Plans**

The MSB developed an overarching Recreational Trails Plan (MSB 2000) that recognized the importance of trails to the region and established goals, objectives, and policies for establishing legal trail easements, maintaining existing trails, establishing new trails, and minimizing conflicts between trail users. One of the overall goals of the plan is to provide trail recreation opportunities for visitors and residents alike in conjunction with the desires of its populace.

All of the rail alternatives under consideration would cross MSB-recognized trails. The ARRC is working with MSB trail planners and recreational user groups to identify trails where grade-separated crossings are desired. Dialog between these groups during the design of the project would ensure that degradation of trails at crossing sites and trail user conflicts are minimized and the project remains consistent with the MSB trails plan.

### **Willow Trails Plan**

The Willow Trails Plan (WACO 2006) promotes the preservation and development of trails in the Willow area. The plan indicates that local opposition of the project is likely if the proposed alignment interferes with trails or "mushing habitat" preservation. The proposed project would maintain trail continuity as per Section 4.3.2.3, which addresses officially-recognized trails.

### **Willow Sub-Basin Plan**

The State's Willow Sub-Basin Area Plan (ADNR 1982) establishes the policy of ADNR for State lands within the area. Within this plan, the area is divided into different management units. The intent of the management units vary, with development being encouraged in some units while the preservation of the natural environment is emphasized in others. None of the management areas explicitly excludes transportation corridors. This plan is in the process of being updated as is expected to be available in 2009.

## **Fish Creek Management Plan**

The MSB and ADNR currently are updating the Fish Creek Management Plan (MSB 2007b). The draft plan proposes land uses for agricultural, rural recreational and residential homes, trails, and preservation of wildlife habitat and watershed resources. The plan includes discussion of reservation for a rail corridor based on the 2003 rail corridor study (MSB 2003a).

### *4.3.2 Parks and Open Space*

Open space landscapes dominate the project area and provide areas for recreational opportunities enjoyed by Alaskans and visitors. Minimizing impacts from this rail project to designated parks, refuges, recreation areas, and trails is repeatedly identified as a primary concern by the public and may be a requirement of some federal funding agencies under Section 4(f) of the Department of Transportation Act. The sections below identify the myriad of parks, refuges, recreation areas, and trails located within the project area and how they interact with the alternative assignments.

#### *4.3.2.1 Parks and Open Space in the Project Area*

The designated parks, refuges, and recreation areas in the project area (Figure 4.11) include:

- Willow Creek SRA
- Nancy Lake SRA
- Little Susitna Recreation River
- Little Susitna Public Use Facility
- Susitna Flats SGR
- Goose Bay SGR
- Fish Creek Park
- Big Lake North, State Recreation Site
- Big Lake South, State Recreation Site
- Rocky Lake State Recreation Site

Most of these are owned and managed by the State of Alaska, including the ADF&G and the ADNR, Division of Parks and Outdoor Recreation (DPOR). Appendix C lists regulatory agency correspondence and Appendix D for agency regulatory meeting minutes, both from the fall of 2007.

#### **Willow Creek State SRA**

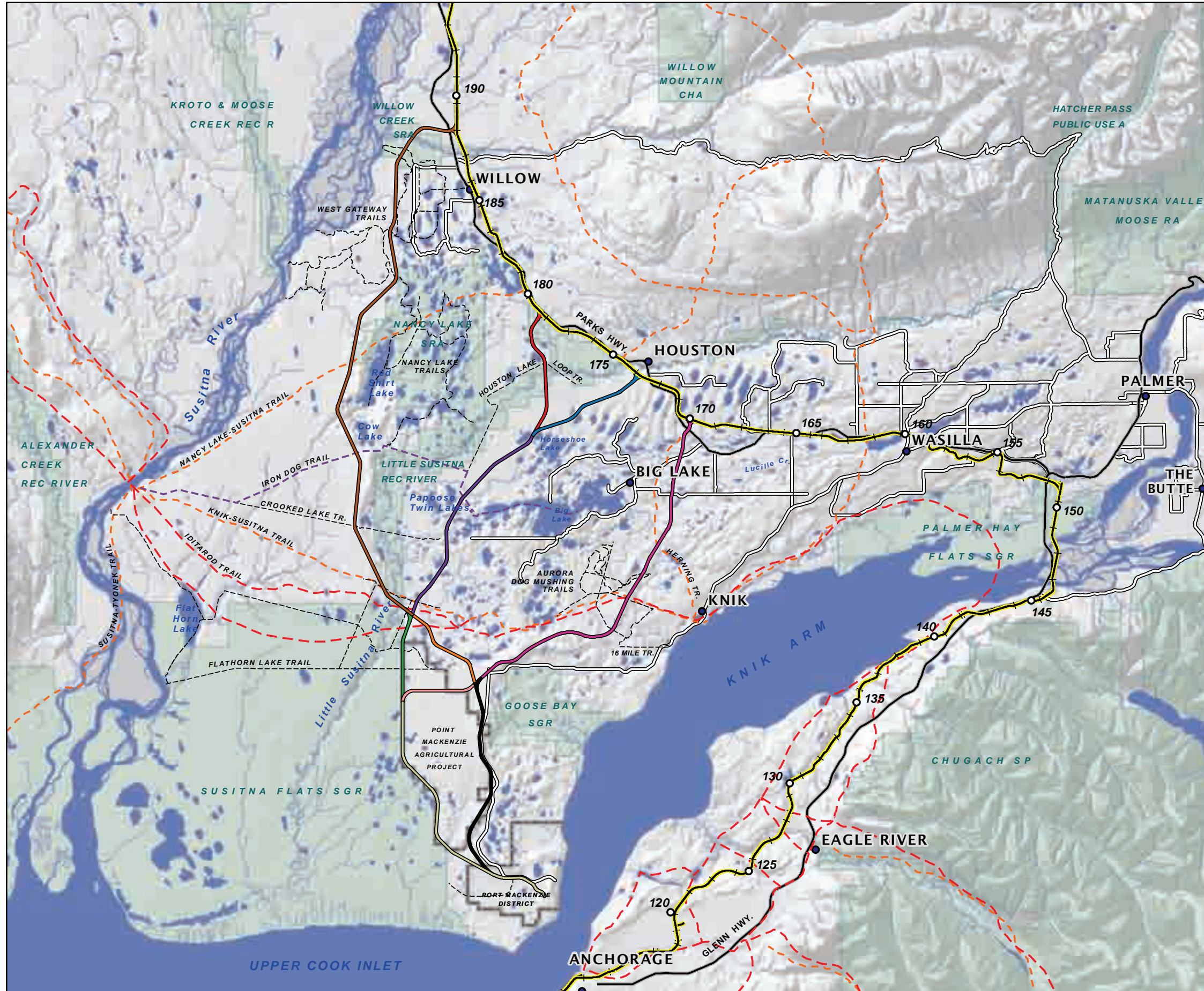
The Willow Creek SRA was legislatively designated in 1987. It is approximately 3,000 acres in size and receives approximately 40,000 visits each year. The Willow Creek SRA is used primarily during the summer for fishing, camping, floating/boating, wildlife viewing, and hunting. Winter use is much lower than summer use and focuses primarily on the trail use.

The Willow Creek SRA does not have a management plan. Historically, land use permits for development on state land have been authorized on a temporary basis, but these permits do not convey a permanent interest in the land.

#### **Nancy Lake SRA**

The Nancy Lake SRA was legislatively designated in 1966 and was one of the first State recreation areas in the system. The 1983 Nancy Lake SRA Master Plan (agency letter in Appendix C) identifies the recreation area purpose: “provide a diversity of outdoor recreation activities appropriate to the area’s resource character and regional setting.” It contains over 130 lakes with approximately 25 miles of foot trails and 15 miles of canoe trails (agency letter in

**Figure 4.11 PARKS, REFUGES, AND RECOGNIZED TRAILS**

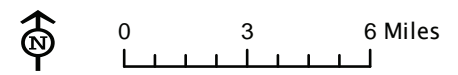


**LEGEND**

Preliminary Alternatives*	Trails
— Mac East	- - - RS2477
— Mac West	- - - Iditarod
— Conn 1	- - - Iron Dog
— Conn 2	- - - Multi-use
— Conn 3	■ Park or Refuge
— Houston	○ ARRC Milepost
— Houston North	— ARRC Track
— Houston South	— Highway
— Willow	— Medium Rd.
— Big Lake	

\*These lines generally represent corridors which are subject to further refinement.

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: December 12, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

*(This page intentionally left blank.)*

Appendix C). The MSB has created a Special Land Use District along the park boundaries to further protect the area's recreational values (agency letter in Appendix C). Nancy Lake SRA is used year round for a variety of activities including canoeing, fishing, hiking, camping, skiing, snowmachining, and dog mushing. Several state-owned cabins are available for public use.

There is a management plan for the Nancy Lake SRA, but the project team has been unable to obtain a copy. Development in this area may have similar restrictions as the Willow Creek SRA.

### **Little Susitna State Recreation River**

The Little Susitna State Recreation River is approximately 18,200 acres in size. It is one of six designated state recreation rivers in Alaska. The purpose of a recreation river is to maintain and enhance the land and water for recreation purposes (agency letter in Appendix C). Its primary uses (by order of use, highest first) are floating, fishing, camping, wildlife viewing, and hunting. It is a heavily used river corridor for recreational float trips, with approximately between 2,000 to 3,000 float trips occurring each year (agency letter in Appendix C).

The 1991 Susitna Basin Recreation Rivers Management Plan includes the Little Susitna Recreation River (ADNR 1991). The plan allows for the development of "motorized transportation" when/if it is consistent with the purpose of establishing and maintaining the recreational river and is found to be consistent with the management plan. If the proposed project is found to be consistent with the management plan, there are additional guidelines and regulations within the plan that have to be met and would likely lengthen the permitting process.

### **Little Susitna Public Use Facility**

The Little Susitna Public Use Facility, located within the Susitna Flats SGR, is owned by ADF&G but operated by the DPOR. It was established in 1989 and is approximately 720 acres in size. The facility includes a boat launch, a trailer parking area, and more than 40 camping sites. It is the only developed public access to the Little Susitna River south of the Parks Highway, making it a popular site for boating, fishing, hunting, and other outdoor recreation activities.

### **Susitna Flats SGR**

The Susitna Flats SGR is located between the Beluga River and Point MacKenzie. It was established by the Alaska Legislature in 1976 and is approximately 300,800 acres in size. The refuge was created to protect fish and wildlife habitats and population, particularly waterfowl nesting, feeding, and migration areas; moose calving areas; spring and fall bear feeding areas; and salmon spawning and rearing habitats. The Susitna Flats SGR also provides public use of fish and wildlife and their habitats, particularly waterfowl, moose, and bear hunting; wildlife viewing and photography; and outdoor recreation. The refuge sees large spring and fall concentrations of migrating waterfowl and shorebirds. According to the Susitna Flats SGR Management Plan, this refuge is one of the most popular recreational hunting and fishing areas in Alaska (ADF&G 1988).

The 1988 Susitna Flats SGR Management Plan does not explicitly exclude the development of a rail corridor (ADF&G 1988). The development of a rail line would require a Special Area Permit. To obtain the permit, the ARRC would have to demonstrate that the proposed project is consistent with the goals of the plan. ADF&G has indicated that the proposed project should avoid crossing into or through the Susitna Flats SGR (agency letter in Appendix C).

### **Goose Bay SGR**

The Goose Bay SGR was established in 1975. It features a wetlands embayment drained by Goose Creek (ADF&G 2007c). According to the Division of Wildlife Conservation, the refuge is an important spring and fall resting and feeding area for migrating waterfowl. The inland portion

of the refuge is known to be a moose calving area (ADF&G 2007c). Other mammals that can be found in the refuge include beavers, muskrat, mink, bears, red fox, and lynx.

The Goose Bay SGR does not have management plan. A rail corridor would require a Special Area Permit. The ADF&G has stated that the proposed project should avoid crossing into or through the Goose Bay SGR (agency letter in Appendix C).

#### **Fish Creek Park**

Fish Creek Park is a 3-acre day-use park operated by the MSB that provides access to Fish Creek. It is located near South Big Lake Road at Fish Creek. Facilities include a salmon observation deck, a picnic area, and a fitness court. No impacts to this facility are anticipated from any alternative.

#### **Big Lake North State Recreation Site**

Big Lake North State Recreation Site is located approximately 60 miles north of Anchorage and provides fishing opportunities and views of the Alaska Range. This 19 acre site is located on Big Lake and has 63 overnight parking spaces, walk-in tent sites, a picnic area, a boat launch, shelters, water, and outhouses. No impacts to this facility are anticipated from any alternative.

#### **Big Lake South State Recreation Site**

A mile down Big Lake Road from Big Lake North SRA is Big Lake South State Recreation Site. This 22-acre site offers a day-use and camping area with fire pits, outhouses, water, parking, and a boat ramp. Like the North SRA, the South site provides fishing opportunities. No impacts to this facility are anticipated from any alternative.

#### **Rocky Lake State Recreation Site**

Rocky Lake State Recreation Site is located at Mile 3.5 of Big Lake Road. This 49-acre site features camping and fishing and has 10 campsites, day use areas, a boat launch ramp, outhouses, water, and parking. No impacts to this facility are anticipated from any alternative.

#### **Section 6(f) Resources**

The LWCF Act of 1965, as amended, was established to provide funds to develop outdoor recreation resources. Section 6(f)(3) of the act requires lands that have been developed using LWCF monies not be converted to a land use other than public outdoor recreation, unless lands of equal value or usefulness are exchanged for the converted lands. The Nancy Lake SRA and the Big Lake North and South State Recreation Sites received LWCF Act monies.

#### *4.3.2.2 Use of Park and Open Space Lands by the Alternatives*

##### **Mac West – Willow**

The Mac West –Willow Alternative bisects the Willow Creek SRA. The alignment crosses an undeveloped area and would change the “feel of the area” resulting in what DPOR states is a negative impact (agency letter in Appendix C).

The DPOR states Nancy Lake SRA would be affected through negative impacts to neighboring recreational lands. Nancy Lake SRA has more than 30 private inholdings (agency letter in Appendix C). An inholding is privately owned land inside the boundaries of a designated park or other land unit. They are the result of the land being privately owned prior to the designation of the park. The Willow segment would encroach on the western-most edge of Nancy Lake SRA.

This alternative runs along the eastern edge of the Susitna Flats SGR and Little Susitna Public Use Facility. According to DPOR, the proposed rail line would affect users arriving at the

facility's "front door." This alternative would cross the Little Susitna Recreational River at approximately River Mile 33, which is approximately one-quarter mile from seven developed riverside campsites (agency letter in Appendix C). DPOR indicated the close proximity is likely to reduce the quality of the recreation experience. It would potentially displace a north-south trail that is used by the public to access hunting areas in the refuge (agency letter in Appendix C). DPOR indicated this alternative would have negative impacts on the Little Susitna Recreation River, as additional bridge crossings would detract from the wilderness setting.

**Mac West – Houston – Houston North**

The Mac West – Houston – Houston North Alternative runs along the eastern edge of the Susitna Flats SGR and Little Susitna Public Use Facility disrupting the wilderness setting of the entrance area. It crosses the Little Susitna Recreation River twice so river users would likely experience a diminished wilderness experience as a result of additional bridge crossings. This alternative appears to cross the Nancy Lake Creek Junction public use site, which is a popular area for fishing and camping (ADNR 1991).

**Mac West – Houston – Houston South**

The Mac West – Houston – Houston South Alternative runs along the eastern edge of the Susitna Flats SGR and the Little Susitna Public Use Facility. It does not cross the Little Susitna Recreation River.

**Mac West – Big Lake**

The Mac West – Big Lake Alternative runs along the eastern edge of the Susitna Flats SGR and the Little Susitna Public Use Facility. At its nearest point, it is approximately 0.3 mile away from the Goose Bay SGR. No impacts to the Goose Bay SGR are anticipated.

**Mac East – Willow**

The Mac East – Willow Alternative would have the same impacts to the Willow Creek, SRA, Nancy Lake SRA, and Little Susitna Recreation River as the Mac West – Willow alternative. This alternative is approximately 0.3 miles away from the Goose Bay SGR. No impacts to the Goose Bay SGR are anticipated.

**Mac East – Houston – Houston North**

According to DPOR, the Mac East – Houston – North Alternative would impact the Little Susitna Recreation River and may detract from the wilderness setting. No impacts to other parks and refuges are anticipated.

**Mac East – Houston – Houston South**

The Mac East –Houston – South Alternative is not expected to have any impacts to parks or refuges.

**Mac East – Big Lake**

At its nearest point, the Mac East – Big Lake Alternative is approximately 0.3 miles from the Goose Bay SGR. No impacts to the Goose Bay SGR are anticipated. No impacts to other parks and refuges are anticipated.

*4.3.2.3 Trails*

In the MSB, trails are an important recreational and transportation resource and are abundant throughout the project area. Recreationally, trails serve as training and competition areas for snowmachining, dog mushing, skiing, and skijoring. They also serve as the primary means of



accessing many of the cabins and other recreational properties throughout the area. For the purpose of this project, the ARRC and MSB defined an officially-recognized trail as a trail located on a formally dedicated easement or on public land on which the MSB or the State has indicated to the project team that there is intent to dedicate an easement. The project team evaluated officially-recognized trails in this report; these include:

- West Gateway Trail System
- Nancy Lake – Susitna Trail
- Nancy Lake Trail System
- Iron Dog Trail
- Crooked Lake Trail
- Flat Horn Lake Trails
- Knik-Susitna Trail
- Houston Lake Loop Trail
- Aurora Dog Mushing Trails
- Iditarod National Historic Trail (INHT)
- Herning Trail
- Houston-Willow Creek Trail

All crossings of the official trails are intended to be grade-separated. Table 4.9 lists a summary of trail crossings.

A majority of the official trails in the project area are trails created under Revised Statute 2477, or RS 2477 trails (refer to Section 1.4 for description of RS2477 trails). Although many of the RS 2477 trails in Alaska have not been surveyed, most of them in the state have been validated with a 100-foot-wide easement (meeting notes in Appendix C).

The project team worked closely with ADNR and MSB trail planners to identify official trails that could potentially be impacted by project alternatives. This includes the myriad of trails identified in the MSB Recreational Trails Plan (2000) and subsequent amendments (2004, 2006 and 2007). It is important to note that the MSB trails plan is a “work in progress” in that the plan provides for annual additions and subtractions to the list of trails in the plan (MSB 2007d). Inclusion of a trail in the trails plan does not guarantee that the trail is available for public use or that there are no private property or other limitations on use of the trail. The placement of a trail or trail system in the plan is the first step of having a trail recognized by the MSB so that it can be evaluated for easement acquisition if it meets MSB qualifications (MSB 2007d). The ARRC would continue to work with ADNR and MSB trail planners to identify trails that may have been missed during preliminary evaluation..

Figure 4.11 contains the officially-recognized trails identified through project team research and extensive consultation with ADNR, MSB, and the public. Due to the evolving nature of the MSB trails plan, this figure is to serve as a “snapshot” in time of official trails and further refinement may be necessary during the NEPA process to identify and resolve potential trail conflicts with project alternatives as trails are added or removed from the MSB trails plan.

One trail system not fully represented on Figure 4.11 is the Figure Eight Lake and Pt. MacKenzie and Susitna River Loop Trails (as identified in the 2004 trails plan amendment). The beginning of the trail system can be seen on Figure 4.11, with the trailhead located within the Port MacKenzie District. These are winter trails that follow old seismic testing lines and the vast wetlands that lie between Point MacKenzie and the Susitna River and are almost entirely located within the Susitna Flats SGR. These trails are not impacted by any of the project alternatives. The location of this trail system was not available in a digital format.

Much consideration has been given to providing trail egress across the proposed rail line in one of two forms, at-grade or grade-separated. Although an at-grade crossing represents the most cost effective means to provide crossing of recreational traffic, little design information presently exists for the majority of vehicles anticipated to be utilizing the crossings (snow machines and dog sleds). Further, concern exists regarding the reaction of a dog sled team when exposed to the advance train whistle at a crossing. It was therefore established by the project team that all trails which would cross the tracks be grade-separated to the extent possible.

There are three potential ways to grade-separate the trail crossings: (1) culvert, (2) bridge for the trail over the railroad tracks, and (3) bridge for the railroad over the trail. A large culvert is the most practicable of the three options. Culverts may be problematic to sled dogs because of a lack of snow within the culvert and the reluctance of sled dogs to enter the structure (agency letter in Appendix C). However, culverts seem to be preferred by snowmachine users. Large culvert trail crossings should be engineered to prevent water from collecting in the culvert. A trail bridge over the railroad is more expensive but it is easier to maintain and provide snow cover. The most expensive and least practicable option is bridging the trail over the railroad due to the necessary vehicular clearance required. The type of crossing for each location has not yet been determined.

Unofficial trails for the sake of this project are those trails which have no legal ROW instrument or recognition in State or Borough trails plans. Though considerable effort has been made to document the number of unofficial trails in the project area, state regulations make the identification difficult. Present policy by State agencies provide for anyone to develop their own trail up to 5 feet wide on state land without permit or other notice. To attempt to provide grade-separated access for trails under these guidelines is not practicable. To provide at-grade crossings given the types of vehicles presently employed is likely not safe. Therefore, the connectivity of all unofficial trails crossed by any of the proposed alignments would not be maintained. However, several of these unofficial trails which have been identified are actually frozen stream channels used only in the winter. In some cases, these trails may remain passable where the ARRC utilizes larger structures to cross the waterbodies.

Several trails in the project area are part of the MSB trails SnowTRAC Grooming Pool and receive state funds from snowmachine registration fees to maintain and groom snowmachine trails (agency letter in Appendix C). Through this program, a network of groomed snowmachine trails is developing in the area. As a result, there is the potential for expanding the winter recreation and tourism industry in this region. According to the DPOR, reducing the number and quality of snowmachine trails would negatively impact this industry.

In addition to being a trail, the INHT is an important historical resource. Please see Section 4.3.3.1 for more information about the Iditarod trail.

Despite having grade-separated crossings, the two Willow alternatives may have negative impacts to trails. An alternative with the Willow segment may create a barrier between Willow Creek and Nancy Lake, making east-west traffic difficult (agency letter in Appendix C). The West Gateway trail system links Nancy Lake and Willow Creek SRA. According to the DPOR, this system is an important part in making the area attractive for winter recreation. There are important trail corridors between Red Shirt Lake and Susitna Flats SGR. The DPOR feels the Willow alternatives would “be a major detriment to recreational values in the area and would adversely affect the quality of life for many area residents” (agency letter in Appendix C).

Table 4-8 summarizes crossings of the officially-recognized trails by the project alternatives. The alignments containing the Houston segments have the fewest crossings (six or eight), and the Mac West – Willow Alternative has the most (13). All cross the INHT. A trail may be crossed in multiple locations in a close proximity, which in some cases the crossings would be condensed to just one crossing instead of multiple crossings.

**Table 4-9: Summary of Officially-Recognized Trail Crossings**

<b>Alternative</b>	<b>Trail Crossings</b>	<b>West Gateway Trail System</b>	<b>Nancy Lake – Susitna Trail</b>	<b>Nancy Lake Trail System</b>	<b>Iron Dog Trail</b>	<b>Crooked Lake Trail</b>	<b>Flathorn Lake Trails</b>	<b>Knik-Susitna Trail</b>	<b>Houston Lake Loop Trail</b>	<b>Aurora Dog Mushing Trails</b>	<b>Iditarod National Historic Trail</b>	<b>Herning Trail</b>
Mac West – Willow	13	X	X		X	X	X	X			X	
Mac West – Houston – Houston North	8				X		X	X	X		X	
Mac West – Houston – Houston South	7				X		X	X	X		X	
Mac West – Big Lake	8									X	X	X
Mac East – Willow	11	X	X		X	X	X	X			X	
Mac East – Houston – Houston North	6				X		X	X	X		X	
Mac East – Houston – Houston South	5				X		X	X	X		X	
Mac East – Big Lake	8									X	X	X

Note: An officially-recognized trail is defined as a trail located on a formally dedicated easement or on public land on which the MSB or the State has indicated to the project team that there is intent to dedicate an easement.

### 4.3.3 Archaeological and Historical Properties

The project area contains numerous cultural resources, including archaeological sites, historic structures, and traditional cultural properties. There are more than 100 recorded cultural sites in the project area. However, based on records in the AHRs database on file at the Alaska Office of History and Archaeology (OHA), the project area has not been thoroughly surveyed, and it is likely that numerous additional prehistoric and historic sites exist that have not yet been identified.

Areas of high probability for archaeological sites have been identified based on similarities to recorded sites (e.g., similar terrain, topography, and distance to water). The identification of high probability areas provides a basis for prioritizing further field survey and investigation. A detailed

summary of the cultural resource probability modeling is in Appendix I; a brief summary of the methodology used for identifying high probability areas follows:

Staff from the Cultural Resources Division of the MSB submitted topographic maps identifying areas with high probability for containing cultural sites within the project area, a region heavily used and populated by the Dena'ina people. Most of the information on Dena'ina land use patterns was based mainly on one man's oral history: Shem Pete related numerous areas used by his family both in the recent and historic past (Kari and Fall 2003). It is obvious from his recollections and from oral history handed down to him, that the region has the potential to hold a greater number of villages and subsistence areas than the recollections of one person.

The probability map submitted to the Alaska Rail Road by MSB staff included areas likely to yield important data on archaeological sites. Field surveys of areas identified as high probability for containing archaeological sites (as highlighted on the map) would include emphasis on the following features:

- the confluence of a river with a lake;
- the confluence of two rivers;
- bluffs above major waterways;
- promontories that may have provided good look-outs for prehistoric camps;
- promontories near or adjacent to swamps where water fowl might have congregated; and
- bluffs surrounding inland lakes and streams

The above information and suggested survey areas included in the probability mapping did not include post 20<sup>th</sup> Century historical data on the built environment.

Preliminary consultation with the SHPO indicates further cultural resource surveys and field investigation of areas may be necessary. These efforts would be conducted, in consultation with SHPO, Tribal governments and other interested parties, pursuant to Section 106 of the National Historic Preservation Act (36 CFR 800).

#### *4.3.3.1 Iditarod Trail*

The INHT runs through the project area and its east-west orientation make crossing the trail unavoidable. The location of the crossing would differ with the alternative alignments under consideration. The Iditarod Trail was designated as the Iditarod National Historic Trail in 1978 by Congress. The Iditarod Trail was first constructed by the Alaska Road Commission about 1910-1911, now known as the "Primary Route." Hundreds of miles of routes and trails, and associated historic sites, branch from this trail, all of which make up the INHT System. The INHT has been designated a nationally commemorated route by Congress and the entire Trail has been considered as "potentially eligible for inclusion into the National Register of Historic Places" (BLM 1986). Currently, only certain sites or districts associated with INHT are on the National Register. Portions of the INHT are used annually for the Iditarod Trail Sled Dog Race, Iron Dog snowmachine race, and Iditasport races (skiing, winter mountain biking, etc.). The INHT is managed cooperatively by Federal, State, and local government and private entities according to the trail's Comprehensive Management Plan (BLM 1986). BLM coordinates the cooperative management and serves as the primary point of contact for matters relating to the INHT.

According to the Comprehensive Management Plan, on non-Federal land, Section 4(f) would apply to INHT segments and sites determined eligible for nomination to the National Register. Within the Comprehensive Management Plan, sections of the INHT are identified as "significant" and given a rating of "recommended priority management," "recommended secondary management," or "recommended minimum management." It would have to be determined

whether Section 4(f) applies to the impacted segment of the INHT. Later analysis should determine the ratings of segments in the project area and the recommended management approach to each segment, including Section 4(f) applicability. Impacts to the INHT would be addressed under Section 106 of the NHPA, in coordination with management agencies and interested consulting parties, to mitigate adverse effects the rail project may have on the INHT.

The project team has discussed potential impacts to the INHT with BLM and ADNR (meeting notes in Appendix D). The BLM recommends constructing a memorandum of agreement (MOA) between all affected agencies to reach agreement on mitigation to help ensure that the INHT resources in question are adequately protected (agency letters in Appendix C).

#### 4.3.4 Socioeconomics

##### **Demographics**

The project area is completely within the MSB limits. The MSB covers more than 24,000 square miles and had an estimated population of 77,174 in 2006 (Alaska Department of Commerce, Community and Economic Development [ADCCED] 2007). The MSB is the fastest growing area in the State.

To determine the demographics of the area, information from the 2000 Census was collected and analyzed. For the purposes of this analysis, the project area contains all census blocks or block groups in the area. For each alternative, the analysis used all census blocks (or block groups) crossed by the centerline of the alternative (Table 4-10).

Approximately 4,301 people live in the project area. The majority lives in the Willow, Houston, and Big Lake areas (Figure 4.12). The rest of the project area is more sparsely populated. There are approximately 1,595 households in the project area.

The percentage of the population in the area that classifies itself racially as “White Alone” (87.8%) is greater than the state as a whole (69.3%) and is approximately equal to the MSB as a whole (87.6%). The largest minority population in the area is American Indian and Alaska Native. The mean median household income of the project area is \$40,162. It is less than both the state as a whole (\$51,571) and the MSB as a whole (\$51,221). At a census block group level, approximately 1,030 individuals are considered to be in poverty. The U.S. Bureau of the Census uses a set of income thresholds that vary by family size and composition to detect who is poor. If the total income for a family or unrelated individuals falls below the relevant poverty threshold, then the family or household of unrelated individuals is classified as being “below the poverty level.” For information on environmental justice, please see Section 4.3.10. A summary of population, race, and poverty data for each alignment is provided in Table 4.9. The proposed project is not expected to generate population growth. It may cause a small shift if, due to increased Port activity, workers move nearer the Port.

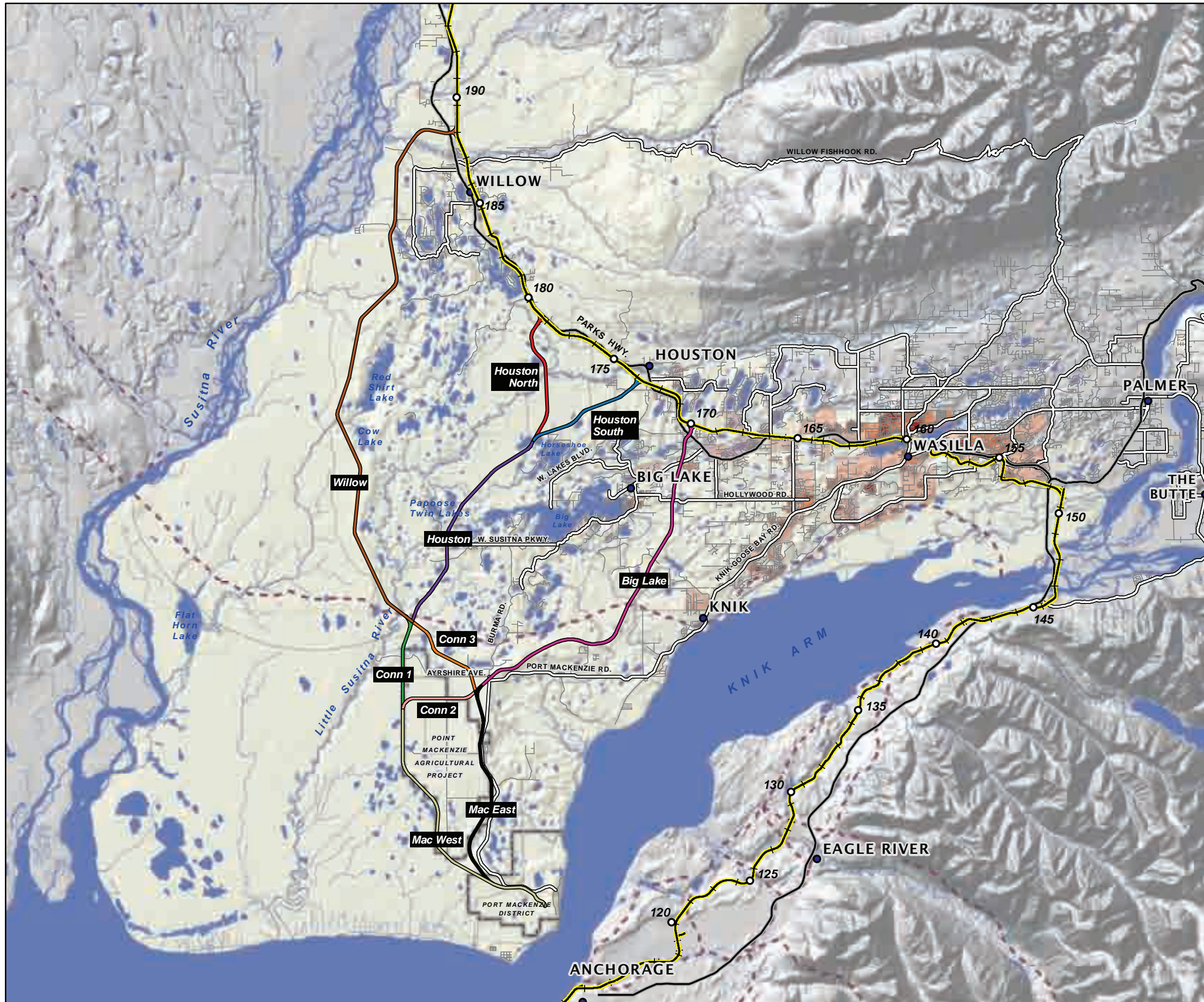
##### **Housing**

As of the 2000 Census, there were approximately 3,670 housing units<sup>e</sup> in the area that were permanent dwellings, seasonal/recreational dwellings, or vacant. Given the development in the MSB since 2000, this number is likely to be higher. Of those housing units, approximately 1,898 (51.7%) of them were for seasonal or recreational use. Table 4-11 provides a summary of housing units by alternative. Each alternative impacts parcels with structures on them. It is possible that some households may be relocated as the result of the proposed project. Because the project area

---

<sup>2</sup> A Housing Unit is different than a Household. The U.S. Census Bureau defines a housing unit as a house, an apartment, a mobile home, a group of rooms, or a single room that is intended for occupancy as separate living quarters. A household includes all people who occupy a housing unit.

**Figure 4.12**  
**PROJECT AREA POPULATION DENSITY**

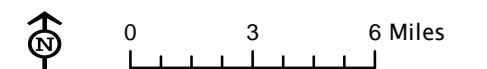


**LEGEND**

Preliminary Alternatives*	Population/Square Mile
— Mac East	0 - 150
— Mac West	151 - 500
— Conn 1	501 - 999
— Conn 2	Greater than 1,000
— Conn 3	○ ARRC Milepost
— Houston	—+— ARRC Track
— Houston North	— Highway
— Houston South	— Medium Rd.
— Willow	— Minor Rd.
— Big Lake	- - - Iditarod Trail
	□ Park or Refuge

\*These lines generally represent corridors which are subject to further refinement.

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: December 12, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc.,  
 MSB GIS, TNH- Hanson, USGS.

*(This page intentionally left blank.)*

is not fully developed, it is highly likely that any household that needs to be relocated should be able to obtain replacement housing near their existing residence.

**Table 4-10: Summary of Population, Race and Poverty by Alternative**

Alternative	Population	Households	Minority (%)	Median Household Income (\$)	Poverty (%) <sup>1</sup>
Mac West – Willow	445	119	17.5	39,176.33	18.7
Mac West – Houston – Houston North	368	100	19.3	40,156.25	15.1
Mac West – Houston – Houston South	499	160	18.4	40,156.25	15.1
Mac West – Big Lake	726	243	19.1	40,156.25	15.1
Mac East – Willow	461	125	16.7	39,176.33	18.7
Mac East – Houston – Houston North	384	106	20.1	40,156.25	15.1
Mac East – Houston – Houston South	515	166	19.0	40,156.25	15.1
Mac East – Big Lake	738	247	16.8	40,156.25	15.1

Source: U.S. Bureau of the Census 2000

<sup>1</sup> Poverty statistics are reported at the block group level. The percentage of individuals in poverty is based on the population of the impacted block groups.

**Table 4-11: Summary of Housing Units<sup>2</sup> by Alternative**

Alternative	Housing Units	Vacant Housing Units for Seasonal, Recreational or Occasional Use	Number of Developed Parcels Impacted
Total in Project Area for Comparison (Note: this not sum of numbers below)	3,670	1,898	See below
Mac West – Willow	370	231	15
Mac West – Houston – Houston North	354	215	13
Mac West – Houston – Houston South	403	208	13
Mac West – Big Lake	362	81	35
Mac East – Willow	384	239	7
Mac East – Houston – Houston North	368	223	5
Mac East – Houston –Houston South	417	216	5
Mac East – Big Lake	374	89	26

Source: U.S. Bureau of the Census 2000, MSB



The two alternatives that use the Big Lake segment may have an impact on neighborhood cohesion, because there are residential subdivisions on each side of the proposed alignment. The subdivisions are not connected by road, however, and have undeveloped land or very low density development between them. In addition, this segment crosses several developed residential parcels east of Kenlar Road between Big Lake Road and the Parks Highway. The Big Lake Road would likely be grade-separated. The other alternatives are not expected to have an impact on neighborhood cohesion.

### **Economics**

One of ARRC's enabling statutes is to foster and promote long-term economic growth and development of the state's land and natural resources. The project would expand and complement the regional transportation network, and would represent a milestone in the continued development of the port. As indicated in Section 1.1, the project would support Port MacKenzie's continuing development as a bulk material resources export and import facility and as a multi-modal facility; provide transportation via rail to accommodate the continuing development of minerals and other natural resources; and provide an alternative mode for transporting materials and equipment for large construction projects. The proposed project would temporarily increase construction jobs.

In addition to the direct benefits offered by the project, there would likely be potential secondary benefits to the local and regional area as well. One benefit would be an increase in employment and support for the tax base for communities benefiting from natural resource development and rail freight activity. Another benefit is the opportunity for future development of rail passenger service to the western MSB.

#### *4.3.5 Air Quality*

Air quality monitoring for criteria pollutants was conducted in the MSB in 2007 for 61 days; during that time, 60 days received a rating of "good" and 1 day received a rating of "moderate" (USEPA 2007). There are no non-attainment areas in the MSB and therefore in the project area (USEPA 2007). Air quality is generally reported to be good.

Each of the alternatives is similar in length and would result in a similar increase in train traffic of two trains a day. The anticipated effect on air emissions is not expected to be significant. Construction-related impacts are expected to be temporary and minor and can be mitigated with use of appropriate BMPs.

#### *4.3.6 Noise*

The current noise environment in the project area ranges from undeveloped areas with minimal human-generated noise, to rural areas with minimal or occasional human noise impacts, to residential and moderately urban areas with higher levels of road traffic noise, particularly adjacent to the Parks Highway. Noises common to all areas include all-terrain vehicles and snowmachines and air traffic from small private and commercial aircraft, commercial jets using Ted Stevens Anchorage International Airport, and U.S. Air Force aircraft using Elmendorf Air Force Base. Aviators use the Big Lake Airport to practice instrument approaches because it is one of the few airports in the Anchorage vicinity that has a VOR (very high frequency omnidirectional range) navigation aid. In addition, numerous lakes in the project area are lined with recreational cabins and rural homes. Noise sources including motorized boat, personal watercraft, float plane, and snowmachine traffic is common on these lakes, although noise or motorized use restrictions have been enacted around some lakes (e.g., Red Shirt Lake, lakes in parks).

Project area residents and recreational cabin owners expressed concern about increased noise in rural, residential, and urban areas. Concerns included the sound of trains running along the rails and the noise of train whistles at road crossings. General train noise can be minimized by constructing the rail line with continuously welded rail and concrete ties in areas close to homes, cabins, and businesses.

A preliminary screening analysis was conducted to identify possible parcels with structures within 500 feet of the rail centerline for each alternative alignment and the existing rail in the project area (1,000 feet total). Parcels falling within 500 feet of the existing rail line were removed from the analysis so that parcels (with building structures) already impacted by existing main line noise were not counted as newly-impacted parcels. The 1,000 foot screening distance was chosen to represent a conservative estimate of nearby parcels that had appraised building values, as these parcels may reflect the presence of sensitive receptors (residents). Table 4-12 depicts the number of parcels with appraised building values that fall within 500 feet of the rail line for each alternative.

**Table 4-12: Number of Parcels with Appraised Building Values within 500 Feet of Proposed Alignments**

<b>Alternative</b>	<b>Number of Parcels with Appraised Building Values</b>
Mac West – Willow	14
Mac West – Houston – Houston North	10
Mac West – Houston – Houston South	9
Mac West – Big Lake	57
Mac East – Willow	15
Mac East – Houston – Houston North	12
Mac East – Houston – Houston South	11
Mac East – Big Lake	56

The ARRC is required by Federal Railroad Administration (FRA) regulations and ARRC rules to sound a train horn when approaching at-grade road crossings (ARRC Train Whistle Noise Reduction Fact Sheet 2007). Table 4-13 identifies the number of new at-grade crossings where locomotive horn noise may occur for each alternative alignment.

**Table 4-13: Number of New At-Grade Crossings Where Locomotive Horn Noise Would be Heard**

<b>Alternative</b>	<b>Number of New At-Grade Crossings</b>
Mac West – Willow	4
Mac West – Houston – Houston North	4
Mac West – Houston – Houston South	5
Mac West – Big Lake	9
Mac East – Willow	6
Mac East – Houston – Houston North	6
Mac East – Houston – Houston South	7
Mac East – Big Lake	9

#### 4.3.7 Coastal Consistency

The entire project area is located within the coastal zone as identified by the MSB Coastal Zone Management District and the Alaska Coastal Zone District boundaries (ADNR 2006c). As such, all alternative assignments would include construction in Alaska's coastal zone and would be subject to consistency review under the Alaska Coastal Management Program (ACMP), the MSB Coastal Management Plan (CMP), and the CMP's associated Point MacKenzie AMSA Plan (as amended in 2006). The enforceable policies of the ACMP are implemented at the district level and the Point MacKenzie AMSA is intended to supplement the MSB CMP. The ADNR Division of Coastal and Ocean Management (DCOM), formerly housed in the Office of Project Management and Permitting (OPMP), must perform a formal review of the project and make its finding of consistency before permits would be issued. For this reason, the project would be developed to be consistent with the MSB CMP requirements and policies.

The 2006 AMSA Amendment designated the Point MacKenzie area as a Major Energy Facilities Area, and as such is deemed suitable for energy facilities and supporting infrastructure. The purposes for defining the area as a Major Energy Facility Area are as follows:

1. To facilitate development of the port and associated infrastructure
2. To facilitate development of the uplands for industrial, energy-related uses, commercial uses, and transportation corridors
3. To protect other important uses and values of the area, and minimize conflicts with port development
4. To plan for future development of the port district and wise use of its coastal resources

Rail access to the Port is one of the key issues addressed in the AMSA. The plan states "the development of a railroad connection to the Alaska Railroad system is also crucial to full utilization of the port facility. The utility of the Port to support energy-related facilities and operations, including the storage, treatment, processing, or transport or transfer of energy-related products, depends on well-developed access. Improvements in access modes to the Port would increase development potential." Also, Point MacKenzie "is currently not served by rail; the nearest segment of the railroad lies about 22 miles to the northeast near Houston. A rail connection would be required to make the shipment of coal and other bulk commodities, such as gravel and wood chips, possible and would also support the use of the facility as a general cargo port."

Relevant AMSA goals and objectives with regard to access are as follows:

- Goal 1 To support the development of, or improvement to existing, intermodal surface transportation systems that serve the Port, including but not limited to road, marine, railroad, and pipeline modes.
- Goal 2 To promote a cost-effective, convenient, well-integrated transportation system that provides save, convenient, and environmentally sound access that links Point MacKenzie with the local community and region.
- Objective C Support development projects that improve road, rail, and marine access to Point MacKenzie and the Port.

As infrastructure is developed, the AMSA notes several ways to maintain consistency with the CMP:

- Construction and design of transportation and utility facilities should be accomplished in a manner that minimizes adverse impacts to important habitat.
- Increased runoff and sedimentation should be minimized or mitigated.

- Coordinated extension and construction of utility lines and corridors is necessary to avoid waste of land and capital, interference with other development needs, creation of unnecessary visual pollution, and degradation of important scenic and recreation values.
- Material sites (sand and gravel) essential to road, railroad, airport, and utility development should be identified and reserved.
- Ensure measures would be incorporated during construction of roads, railroads, and upland facilities to maintain adequate drainage, properly placed culverts, and to avoid siltation and other adverse effects on water quality.

4.3.8 Energy

Other than incidental crossings, the project is not anticipated to affect any petroleum pipelines, electric transmission lines, or other utilities in the area, nor is it anticipated to affect recyclable commodities.

A comparison of energy consumption by alternative is found in Table 4-14. These numbers are based on the amount of energy required to power one train traveling in one direction along each alternative from Port MacKenzie to railroad MP 188.9, near Rogers Creek north of Willow.

**Table 4-14: Comparison of Energy Consumed by One Train Traveling in One Direction**

Alternative	Train <sup>1</sup> Energy Consumed <sup>2</sup> (horsepower-hours)
Mac West – Willow	8,070
Mac West – Houston – Houston North	7,300
Mac West– Houston – Houston South	8,140
Mac West – Big Lake	9,770
Mac East – Willow	8,660
Mac East – Houston – Houston North	7,900
Mac East – Houston–Houston South	8,730
Mac East – Big Lake	9,100

- a. Design train is 100 cars, each weighing 100 tons, traveling northbound.
- b. Alternatives include energy expended along the existing mainline between the respective alignment's junction and MP 188.9 north of Willow.

Source: TNH-Hanson

Since the maximum grade is presently 0.50% for each alternative, the energy consumption is not largely effected by grade resistance. The primary driver of train energy consumption is the length of each alternative. Thus the alternative combinations that involve the Houston North segment consume the least train energy, and the combinations that involve the Big Lake segment consume the most.

There is the potential that some goods currently moved by trucks should be transmitted by rail, resulting in a decrease in energy consumption. It is likely some trains coming from the Interior to the Port of Anchorage, Whittier, or Seward would choose Port MacKenzie instead. Overall train energy use would be reduced because the travel distance to Port MacKenzie is less.

Construction of the project would require an increase in energy consumption; however, impacts to energy due to construction are anticipated to be minor and temporary.

#### 4.3.9 Public Input

The project team conducted public involvement activities between September and December 2007. The Public Involvement Activities Summary (Volume 4) provides a comprehensive look at the process used to gather information and the concerns raised by the public, agencies, and the local Federally-recognized tribe, Knik Tribal Council.

The project team received 361 comments through December 2007. In general, public comments supported the Port MacKenzie Rail Extension project, but often opposed locations close to residential areas. Comments covered a broad range of topics including:

- Agricultural lands
- Archaeological sites
- Local and regional economies
- Noise
- Recreation
- Residential property
- Safety
- Seismic activity
- Trails
- Wetland impacts
- Wild land fire
- Wildlife habitat

Comments regarding specific corridors included:

##### **Willow**

###### Safety concerns

- Number of trail crossings needed

###### Recreational concerns

- Impacts to recreational use
- Preservation of multi-use trails for snowmachiners, dog mushers, cross-country skiers, and snowshoers
- Impacts to the Willow Creek SRA
- Impacts to the Nancy Lake SRA
- Impacts to the West Gateway Trail System

###### Environmental Impacts

- Noise and vibration
- Wildlife
  - Moose on the back side of Red Shirt Lake
- Vegetation
- Stream and river crossings
  - Impacts to the Little Susitna River
  - Bridge over Willow Creek
- Historic and cultural sites in the Red Shirt Lake area
- Suitable soils to provide a stable sub-base for construction

###### Socio-economics

- Impacts to full-time residents
- Potential to open up remote property for private land sales
- Impacts to local tourism and ecotourism businesses
- Impacts to community values and lifestyle

**Houston**

Economics

- Potential benefits to business
- Opportunity for an industrial park

Environmental

- Noise
- Impacts to wildlife
  - Calving moose
  - Nesting Sandhill cranes

Socio-Economic

- Impacts to private property

**Big Lake**

Economics

- Potential benefits to business

Socio-Economic

- Impacts to private property
- Congestion

**Port MacKenzie West and East**

Socio-Economic

- Impacts to farms
  - Bisecting productive fields
- Impacts to historic properties

The project team considered the comments above in evaluating the project corridors and, in some cases the corridors were refined to avoid or minimize potential impacts. In several instances, comments based on the project maps brought attention to certain sensitive environmental areas, and alignments were modified to avoid or minimize potential impacts to some of these areas. Public feedback also identified areas of potentially poor soil conditions, which was used to help guide geotechnical investigations. Comments and discussions regarding the location and use of trails highlighted the importance of recreational use in the area.

*4.3.10 Environmental Justice*

Executive Order 12898 requires federal agencies to incorporate consideration of environmental justice into the NEPA evaluation process. Consideration of environmental justice means addressing the potential for disproportionately high and adverse human health or environmental effects on minority populations and low-income populations. If impacts are appreciably greater than the adverse effect that would be suffered by the non-minority population or non-low-income population, they are considered environmental justice impacts.

For this study, the project team analyzed demographic data from the 2000 Census to identify minority and low-income populations within the project area. Race data for the project area is available to the block level and is shown in Figure 4.13 (Section 4.3.3 above). Data for population below the poverty line is only available to the block group level and is shown in Figure 4.14. Additional information on the demographics of the project area is presented in Section 4.3.4 Socioeconomics. Areas to note include:

- Race data indicates that all connector segments (Conn 1, Conn 2, Conn 3) pass through a census block composed of 40-66 percent non-white population.
- The northern portion of the Willow segment crosses a census block group where 19-26 percent of the population is living below the poverty level.

Public participation efforts focused on communicating with potentially affected residents and property owners. No environmental justice issues were raised during public involvement activities for this study.

The analysis indicates that minority and/or low-income populations in the project area would not be disproportionately affected when compared to non-minority and/or non-low-income populations.

## **4.4 Safety**

### *4.4.1 Crossings – roads and trails*

Placing a new track in an area where a track previously did not exist creates new opportunities for trespass and user conflict. The ARRC has an aggressive education campaign regarding safety. Rail safety would be enhanced by reducing or eliminating the interactions between people, automobiles, and trains.

### **Road Crossings**

The proposed rail extension would create both at-grade and grade separated crossings. At-grade crossings with roads having AADT of more than 500 would be equipped with flashing lights and gates. At-grade crossings with roads that have AADT of less than 500 would be marked with crossbuck signs. Each crossing and its associated signalization or signage is discussed in Section 4.1 and listed in Appendix F.

Currently, Big Lake Road is a two-lane minor arterial with traffic volumes exceeding 4,500 AADT. The MSB LRTP indicates that Big Lake Road would be upgraded to a four-lane road. With this scheduled improvement, it is anticipated that Big Lake Road would have sufficient traffic volumes to warrant a grade-separated crossing. Refer to Volume 3 for a conceptual drawing of the grade separation at Big Lake Road.

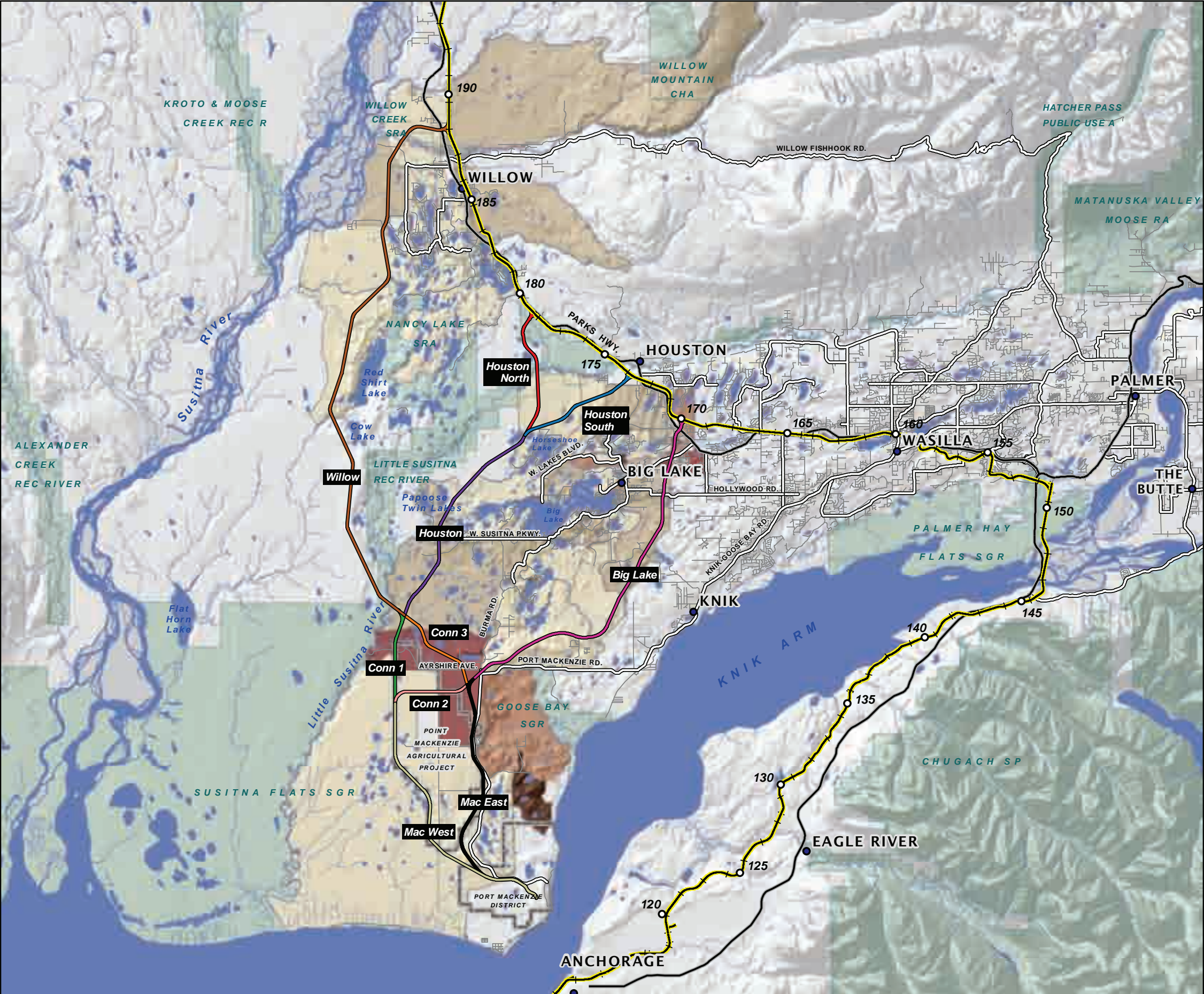
Conceptual drawings for the grade separations at the Parks Highway, for the Willow segment, and for the Big Lake segment are in Volume 3. The Big Lake segment crosses the Parks Highway very close to the existing intersection with Hawk Lane, which is the access road to Houston Middle and High Schools. This project would relocate Hawk Lane to a new intersection with the Parks Highway west of the railroad grade separation, as shown in Volume 3. The embankment near this intersection would be constructed with 4:1 slopes to the extent possible with the intent of optimizing visibility by limiting the need for guardrail for the southbound Parks Highway traffic. Guardrail would be provided only for the bridge itself and the immediate approach to the bridge.

With the anticipated grade-separated crossings for the Parks Highway and Big Lake Road and appropriate signals or signs at remaining roads, the proposed rail extension is not expected to impact safety.

### **Trail Crossings**

All alternatives would require multiple trail crossings. All officially recognized trails would remain connected via grade-separated trail crossings. There are three different options for creating a grade-separated trail crossing. Two options include constructing the railroad over the trail on either a multi-plate arch pipe culvert or on a short bridge as depicted in Figure 2.4. A third option is to construct a trail bridge over the railroad. At this time, the type of crossing to be used at each location has not been decided. The crossing type would depend on the trail location and its use. Each crossing could include fences to deter trail users from entering the ARRC ROW.

**Figure 4.13 PERCENT NON-WHITE POPULATION BY CENSUS BLOCK**

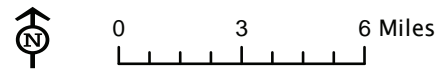


**LEGEND**

- |                                  |                           |
|----------------------------------|---------------------------|
| <b>Preliminary Alternatives*</b> | <b>Percent Non- white</b> |
| — Mac East                       | 0-5%                      |
| — Mac West                       | 6-21%                     |
| — Conn 1                         | 22-39%                    |
| — Conn 2                         | 40-66%                    |
| — Conn 3                         | 67-100%                   |
| — Houston                        | ○ ARRC Milepost           |
| — Houston North                  | — ARRC Track              |
| — Houston South                  | — Highway                 |
| — Willow                         | — Medium Rd.              |
| — Big Lake                       | — Minor Rd.               |
|                                  | ■ Park or Refuge          |

\*These lines generally represent corridors which are subject to further refinement.

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: November 27, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.



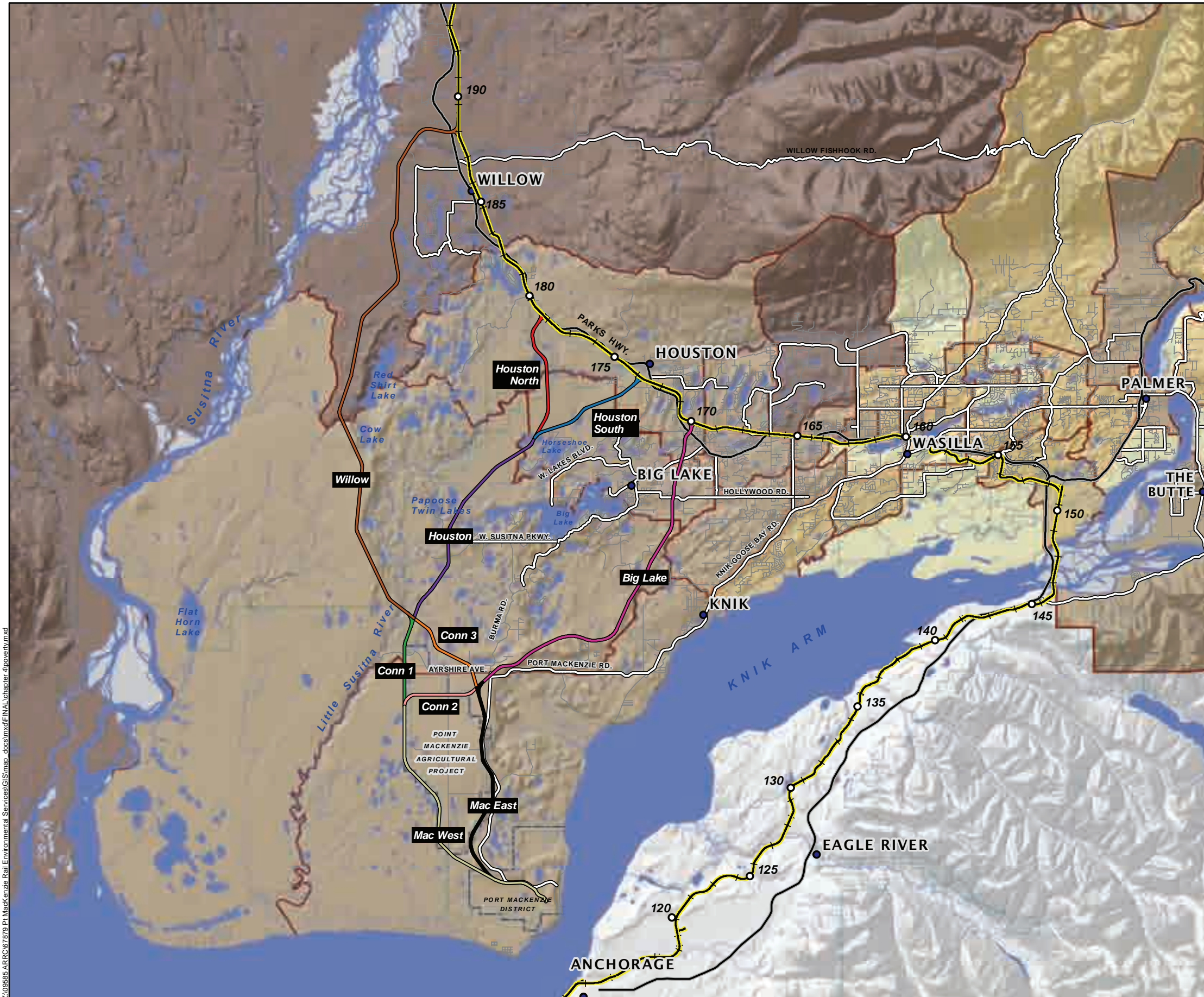
*(This page intentionally left blank.)*

**Figure 4.14 PERCENT BELOW POVERTY LEVEL BY CENSUS BLOCK GROUP**



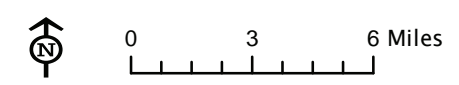
**LEGEND**

- |                                  |                                    |
|----------------------------------|------------------------------------|
| <b>Preliminary Alternatives*</b> | <b>Percent Below Poverty Level</b> |
| — Mac East                       | 0-5%                               |
| — Mac West                       | 6-10%                              |
| — Conn 1                         | 11-18%                             |
| — Conn 2                         | 19-26%                             |
| — Conn 3                         | ○ ARRC Milepost                    |
| — Houston                        | — ARRC Track                       |
| — Houston North                  | — Highway                          |
| — Houston South                  | — Medium Rd.                       |
| — Willow                         | — Minor Rd.                        |
| — Big Lake                       |                                    |



\*These lines generally represent corridors which are subject to further refinement.

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: November 27, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

Z:\05855 ARRC\67879 Pt Mackenzie Rail Environment\GIS\map\_docs\mxd\FINAL\chapter\_4\poverty.mxd

*(This page intentionally left blank.)*

Safety considerations for unofficial/informal trails include installing signs and diverting the trail to a grade-separated crossing. Decisions on the specific measures to be incorporated at each location for these trails would not be made until after an alternative is selected. All trail safety measures would be coordinated with the appropriate organizations including the MSB, local trail user groups, and landowners. With these measures incorporated into the proposed project, no safety impact related to trail crossings is anticipated.

#### 4.4.2 Hazardous Materials and Petroleum Products

It is currently not known if any shippers would require the transport of hazardous materials along the Port MacKenzie rail extension, although such customers are possible. The ARRC currently transports all classes of hazardous materials along its main route. Petroleum products are also shipped throughout the ARRC system. Locomotive engines carry upwards of 4,000 gallons of diesel fuel each, which could be detrimental to the environment in the event of a derailment and spill. In addition, construction and maintenance equipment have the potential for petroleum product and antifreeze leaks. The ARRC maintains an Oil Spill Contingency Plan and has an Emergency Response process in place for immediate containment and cleanup of hazardous materials along its existing track. This plan is regularly updated and would be amended to include the Port MacKenzie rail extension.

Hazardous material regulations, established by the federal DOT (49 CFR 100-185), are enforced by the FRA, along with general rail safety regulations (49 CFR 200-399). Hazardous materials are divided into nine hazard classes. Because the ARRC, as a transporter, does not assume ownership of the materials it transports (and therefore responsibility for its proper identification and packing), identification of hazardous materials along the ARRC system is limited to identification of hazard class (shippers are responsible to properly document and verify contents). The ARRC handles hazardous material in accordance with federal DOT regulations. Materials are handled and transported in a variety of containers appropriate to the material's hazard class. Shipments of hazardous materials enter or exit the ARRC system at several locations, including ocean ports, petroleum refineries, flat car yards, and industry tracks (ARRC 2006).

#### **Emergency Response Capabilities**

The ARRC maintains and periodically updates their oil spill contingency plan, which outlines chain-of-command and emergency response procedures for operations along the ARRC line throughout Alaska. Separate response plans are developed for stationary operations, such as fuel terminals and depots, as well as for passenger rail line operations. The oil spill contingency plan would be updated to include any of the alternative alignments in the project area.

#### **Known Contaminated Sites in the Project Area**

Generally, contaminated sites are located in populated areas, although not exclusively. The alternative alignments that pass through developed areas incur an associated increase in the likelihood of encountering a known contaminated site.

A record search for contaminated sites, performed in December 2007, inventoried properties within 600 feet both sides of the project centerline, and compared the property information to databases held and maintained by the ADEC and the federal government. Searched ADEC databases include inventories of registered leaking underground storage tanks and contaminated sites. Federal databases searched include the emergency response notification system, Resource Conservation and Recovery Act (as amended, 42 USC §§6901 *et seq*) generators and facilities lists, Comprehensive Environmental Response, Compensation and Liability Information System (maintained by the USEPA) lists, and National Priorities Lists. The records search was conducted in conformance with ASTM E 1527-05. In accordance with ASTM E 1527-05, a qualified

environmental professional reviewed the records search and evaluated potential Recognized Environmental Conditions (REC) associated with each alternative.

One site was identified in the records search as a potential REC: an ADEC-registered contaminated site located at MP 56 of the Parks Highway. The site (ADEC Reckey number 1997220115301) has been conditionally closed, but states that petroleum-contaminated soil persists within the ARRC ROW. The site is located on the Parks Highway, which is outside the 600-foot buffer of the Houston South segment; however, the source of impacted soil is noted as a 1972 trail derailment. Construction of the rail siding along the Houston South segment, therefore, may expose contaminated soil. No RECs were identified within the 600-foot buffer for other alternative alignments.

If contamination is encountered during construction, the ARRC would address it in accordance with applicable state and federal regulations.

#### **4.5 Possible Actions Leading to Indirect and Cumulative Effects**

The project team identified projects or events (actions) that

- may occur as a result of the construction of the Port MacKenzie rail extension that could cause indirect effects on the environment
- are reasonably foreseeable future actions unrelated to the Port MacKenzie rail extension that could result in cumulative effects to the environment

These actions are discussed in this section.

##### *4.5.1 Actions with Possible Indirect Effects*

#### **Port MacKenzie Facility Expansion**

As the rail line is extended to Port MacKenzie, the Port may have to develop additional rail facilities to connect with the terminal reserve operated by ARRC. These rail facilities should include buildings, facilities, roads, industrial spurs, sidings, loading/unloading tracks, and other ancillary facilities throughout the upland port district. Generally, it is presumed that these facilities would be developed as the port continues to grow. These facilities would be particular to the specific traffic needs and are expected to be generally consistent with Port master planning documents.

#### **Changes in Port Access Traffic**

Construction of the rail line would result in a shift in the traffic to the port from truck to rail. While some goods would still move through the port by truck, other goods would move by rail. This shift may result in a decrease in truck traffic on roads leading to the port and on the Parks Highway. This decrease in traffic may lead to increased safety on these roads.

##### *4.5.2 Actions with Potential Cumulative Effects*

The project team identified projects or events (actions) that are unrelated to the Port MacKenzie rail extension project that the STB could consider as cumulative impacts. Some actions may not be applicable, depending upon which alignment is chosen.

#### **Knik Arm Bridge**

KABATA currently is finalizing an Environmental Impact Statement evaluating the impacts of a road bridge across Knik Arm between Anchorage and Point MacKenzie (KABATA 2006a). The

bridge would be located north of Port MacKenzie and rail, port, and bridge facilities would not conflict with each other (ADOT&PF and KABATA meeting minutes in Appendix D). KABATA is moving forward with a public-private partnership to conduct final design for the bridge and initiate construction (KABATA 2007)). The Knik Arm Bridge project and the Port MacKenzie Rail Extension project are unrelated.

### **Cook Inlet Ferry**

The MSB has prepared an Environmental Assessment to evaluate the potential impacts of using a ferry to cross Knik Arm between Point MacKenzie and Anchorage (FTA and MSB 2006). The ferry is scheduled to be operational in late 2009 but a ferry landing site on the Anchorage side of Knik Arm has not been agreed upon at this time. The terminal building on the MSB side has been constructed, but the ferry landing dock facilities have not been constructed on the MSB side nor the Anchorage side.

### **Point MacKenzie Road Upgrades**

The MSB and ADOT&PF are currently working on upgrades to Point MacKenzie Road, from the intersection of Burma Road to the Port MacKenzie Dock. The project includes straightening curves in the road, widening portions of the road, and paving.

### **West Matanuska-Susitna Access Project**

The MSB is investigating the potential to build a road and bridge to access lands on the west side of the Little Susitna River (MSB 2007a). This project would be the first road access to this portion of the MSB and would provide access to the Fish Creek Management area (see below). On the east side of the river, the road would connect to South Big Lake Road, Ayrshire Road, North Big Lake Road or possibly an upgraded Burma Road. The MSB has initiated preparation of an EIS for this project.

### **Fish Creek Management Area**

The MSB is in the draft planning stages for the Fish Creek area on the west side of the Little Susitna River (MSB 2007b). Property in the Fish Creek Management area currently is owned by the MSB and the State. Alternative plans for this 45,000-acre area include selling 10-20 acre parcels for agricultural development and residential settlement; managing the land for timber resources, recreation, and wildlife habitat until more is known about the future potential for agriculture; and maintaining public ownership of the property and managing for forestry, recreation, and wildlife habitat.

### **Construction of a New Prison near Point MacKenzie**

The MSB has identified land near Point MacKenzie for a new medium-security prison (MSB 2007c). The prison property would be approximately 160 acres and the prison building would be 600,000 to 800,000 square feet, with room for 1,200 to 2,251 prisoners.

### **Additional Development in the Project Area**

The MSB is the fastest growing population region in Alaska (MSB 2007a), increasing by 77% between 1990 and 2004 (KABATA 2007), and growth is predicted to continue into the future. Undeveloped private lands in the project area are likely to continue to be developed for residential or recreational use, particularly around area lakes. Population growth in the Point MacKenzie area is likely to increase when the Knik Arm Bridge is constructed (KABATA 2006b). Based on population projections prepared for the Knik Arm Crossing project (KABATA 2006b), the population in the Point MacKenzie area is anticipated to be 32,800 in 2030 if the Knik Arm Bridge is not constructed and 60,700 by the same year if the bridge is constructed, a difference of 85%.

### **Industrial Development in Houston**

The City of Houston has expressed interest in developing an industrial area adjacent to the existing ARRC main line near the proposed Houston South connection. Currently the industrial area does not have a formal development plan in place. If this site is developed in the future, it could be equally served from the existing main line and therefore is not contingent upon the completion of this project.

## 5.0 Alternatives Study

One purpose of Port MacKenzie Rail Extension Preliminary Environmental and Alternatives Report is to use quantitative measures to compare the strengths and weaknesses of the Port MacKenzie Rail Extension project alternatives. For planning and policy analyses, a *Goeller scorecard*, called a matrix in this report, is useful as it utilizes a disaggregate method to display and compare the strengths and weaknesses of the alternatives (Patton and Sawicki 1993). This method describes the impacts for each alternative in “natural” units, such as physical units, monetary terms, or other quantifiable terms, and then allows decision-makers to assign a score by which to compare the alternative alignments. Each column of the matrix represents one impact and each row represents an alternative alignment. The result is a final analysis summary matrix used to present the various impacts.

### 5.1 Criteria Identification

The matrix evaluation criteria used in this analysis were derived from STB criteria (49 CFR 1105.7) and environmental and engineering criteria specific to the project area. Criteria were selected based on (1) availability of a quantifiable measure and (2) differences in measurements for alternatives that allowed for comparison. Quantifiable criteria that resulted in similar results for all alternatives were removed from the analysis as they would not be useful in differentiating the alternatives. The criteria used in the matrix to compare alternatives are described in Table 5-1. A weighting of the various criteria was avoided to ensure that each criterion was considered equally.

The ARRC is seeking to acquire a 200-foot ROW along the entire length of the proposed alignment. The ARRC would build the rail and all other associated infrastructure within this ROW. Based on this specification, a 200-foot wide buffer along the proposed alignments was used to calculate the aerial impacts to the various evaluation criterions using the GIS.

Each criterion was evaluated separately. Alternatives were assigned a plus (+), neutral (0), or minus (-) for each criterion. This was determined by assigning the least impacting alternative(s) a plus and the most impacting alternative(s) were assigned a minus. Alternatives falling in the middle were then assigned a neutral value. The numbers of pluses, neutrals and minuses were totaled for each alternative, and the number of minuses was subtracted from the number of pluses to compare the overall strengths and weaknesses of the alternatives. The number of neutral criteria was not calculated into the final comparison number. Matrix measurements and evaluations are presented in Table 5-2.

The environmental and engineering opportunities and constraints were compiled by alternative into the final matrix (Table 5-2). The cells of each criterion column contain two results; the actual unit value of each category as well as the overall scoring result. The final score of each alternative is represented in the ‘total’ column. Construction costs of each alternative were not a part of the final score, although they are included in the matrix as a separate column.



**Table 5-1: Description of criteria used in the matrix**

<b>Matrix Criterion</b>	<b>Description</b>
Poorly or highly compressible soils (cubic yards)	Poor soils are often located in wetlands, may include hydrology considerations, and can complicate constructability. Geotechnical fieldwork, including subsurface probing that evaluated all soil types, including peat which is highly compressible, was conducted for the proposed routes. Based on an assumed 80-foot construction width, the approximate amounts (cubic yards) of poor or highly compressible soils to be excavated were calculated by alternative and compared.
Number of new road crossings	Crossing roads, with either a grade separated bridge crossing or an at-grade crossing add cost to the project. At-grade crossings also may impact traffic patterns on existing roads, increase noise levels at development near road intersections, and affect local socio-economics. Additionally, grade crossings require driver attentiveness and ongoing maintenance. As a measure of each of these potential impacts, the number of new road crossings was tallied for each alternative and compared.
Land availability (acres/mile)	The project area is comprised of privately owned parcels; local and state owned designated parcels (parks, refuges, and other public uses); and large tracts of undeveloped or undesignated parcels owned by government (State of Alaska and MSB), Native Corporations, The Trust (Alaska Mental Health Trust Authority), and University of Alaska. This category includes only these large tracts of undeveloped and undesignated land as they represent this category and are land types that are the most compatible for locating a rail line and minimize impacts to more developed areas and people. Because the lengths of each alternative varies, the acreages of undeveloped lands within a 200-foot-wide ROW were calculated for each alternative and divided by the length of that alternative. More acres per mile in this category were considered positive.
Number of developed parcels	Among the privately owned parcels in the project area, many are currently undeveloped. To evaluate the greater impact to human development, the MSB GIS parcel database was queried for parcels containing an appraised building value. The number of parcels crossed by each alternative that contained structures was counted. A higher number of developed parcels along a route represent impacts to more densely populated areas, individual property owners, and community cohesion.
Designated land use (acres)	Designated lands include state and MSB lands that are designated as refuges, recreation areas, and parks; state or private lands established for agricultural uses; and private lands designated as residential areas. MSB and state GIS land use databases were examined and the acres of designated lands within each 200-foot-wide ROW were estimated and compared.

Train energy (horsepower-hours)      The movement of trains is resisted by forces related to distance, grades, and curvature. The amount of energy needed for a standardized train to overcome these forces was calculated for each alternative and compared.

Matrix Criterion	Description
Wetlands (acres)	The NWI dataset was examined. Wetland types within the project area are highly diverse and have varying levels of functionality and overall value. For this analysis, all wetland types were combined. Wetland impacts were avoided and minimized to the greatest extent possible. The total wetland impacts were calculated for each alternative and compared.
Number of mapped anadromous fish stream crossings	Crossing anadromous streams could impact salmon populations, habitat, and/or sportfishing. State regulations require that culverts and bridges with in-water support structures be designed to allow for fish passage. The ADF&G catalog of anadromous fish streams was used to tally and compare the number of stream crossings by each alternative
High potential for archaeological sites (acres)	Areas of cultural and historic significance were avoided where practicable. An archaeological probability map provided by MSB cultural resource specialists was used to calculate the acres of potential impact to areas with highest probability of having cultural resources along each alternative.
Fragmentation of designated refuge and recreation areas (Yes/No)	State lands designated as recreation areas and refuges represent areas of high quality wildlife habitat and areas of significant recreational opportunity for fishing, boating, hunting and other outdoor activities. Crossing or encroaching on the boundary of these areas is seen as fragmentation. An alternative that encroached on the boundary of one or more recreation area or refuge was given a 'yes' for fragmentation, while alternatives that had no impact on these areas were given a 'no'. In the scoring, alternatives with no encroachment were assigned a plus (+), alternatives that bisected one or more of these areas were assigned a minus (-), and alternatives that encroached a border but did not bisect the property were assigned as neutral (0).
Construction costs	Preliminary total construction cost estimates, including ROW acquisition, were compiled and used to compare each of the alternatives. Construction costs were not included as part of the overall comparison of strengths and weaknesses.

*(This page intentionally left blank.)*

Table 5-2: Final matrix evaluation

Proposed Routes	Criterion										+ / 0 / -	Total	Cost Estimate (millions)	
	1 Poor or highly compressible soils (cubic yards in 10 <sup>6</sup> yds <sup>3</sup> )	2 New road crossings (#)	3 Land availability (acres/mile)	4 Developed parcels (#)	5 Designated land use (acres)	6 Train energy (horsepower-hours)	7 Wetlands (acres)	8 Mapped anadromous fish streams (#)	9 High potential for archaeological sites (acres)	10 Fragmentation of designated refuges and recreation areas (yes/no)				
Mac West - Willow	1.15 0	5 +	15.8 0	15 0	440 -	8,100 +	200 0	5 0	585 -	YES -	2/5/3	-1	\$285	-
Mac West - Houston North	1.73 -	4 +	14.6 0	13 +	440 -	8,600 0	350 -	7 -	225 +	YES -	3/2/5	-2	\$220	0
Mac West - Houston South	1.19 0	5 +	16.3 0	13 +	415 -	9,500 0	280 -	6 0	280 +	NO 0	3/5/2	+1	\$200	+
Mac West - Big Lake	0.66 +	11 -	11.3 -	35 -	320 -	11,600 -	220 0	7 -	530 -	NO 0	1/2/7	-6	\$240	0
Mac East - Willow	1.05 0	7 0	18.9 0	7 +	150 +	8,700 0	90 +	4 +	580 -	YES -	4/4/2	+2	\$280	-
Mac East - Houston North	1.62 -	6 0	18.7 0	5 +	145 +	9,100 0	240 0	6 0	225 +	YES -	3/5/2	+1	\$220	0
Mac East - Houston South	1.08 0	7 0	20.4 +	5 +	130 +	10,100 0	175 +	5 0	280 +	NO +	6/4/0	+6	\$200	+
Mac East - Big Lake	0.56 +	11 -	15.4 0	26 -	120 +	10,900 -	160 +	7 -	540 -	NO +	4/1/5	-1	\$220	0

*(This page intentionally left blank.)*

## 5.2 Topics Not Covered or Removed from Matrix

Several topics were either not covered or removed from the evaluation matrix. As previously mentioned, some topics were not applicable to the project, some were difficult to quantify, and others were so similar once quantified that they did not contribute to the comparison. The criteria not included in the matrix are described in Table 5-3.

**Table 5-3: Criteria not included in the evaluation matrix**

<b>Criterion removed</b>	<b>Description</b>
Air Quality	There are no non-attainment areas in the MSB and therefore in the project area (USEPA 2007). Air quality is generally reported to be good. Each of the alternatives is a similar length and would result in similar air emissions from train traffic. None of the alternatives are likely to increase air emissions to a level of non-attainment.
Noise	Because most train noise is generated at crossings, alternatives that require more new at-grade crossings may have a greater noise impact. Noise was not included as a stand-alone criterion in the matrix as it seems to duplicate the “number of new road crossings” criterion.
Safety	Safety is an issue for all alternatives, and is generally incorporated in the “number of new road crossings” criterion.. While some alternatives would have more at-grade road crossings than others, all alternatives cross recreational trails that may pose a safety concern. All alternatives would have equal potential impacts from the frequency or probability of a release to the areas from hazardous materials. Hazardous spills contingency plans would be modified by ARRC to include the new alignment. Because safety concerns are essentially equal among each of the alternatives, this issue was not a discriminating factor for comparing the alternatives.
Endangered Species	There are no threatened or endangered species present within the project area.
Coastal Zone	All alternatives would include construction in Alaska’s coastal zone and would be subject to consistency review under the ACMP and the MSB CMP including the Point MacKenzie AMSA Plan (as amended in 2006). Therefore, coastal zone impacts were not a discriminating factor for comparing the alternatives.
Operations and Maintenance	Operations and maintenance requirements would be similar for all alternatives and are not a discriminating factor for comparing the alternatives.
Public Comment	Public comment was not included in the matrix evaluation because it is difficult to measure public comment in quantifiable terms. However, public comment was important for developing the alternatives and for understanding issues specific to the project area. Public comments were considered and helped to refine the proposed corridors and influenced preliminary design elements.

<b>Criterion removed</b>	<b>Description</b>
--------------------------	--------------------

---

Socio-economics	The potential socio-economic impacts of the project are an important consideration. This criterion, however, is complex and is difficult to quantify and compare the impacts between the alternatives in a matrix format. U.S. Bureau of the Census data from 2000 were analyzed to evaluate project area demographics, housing, and the economy.
Habitat Fragmentation	Most of the project area provides quality habitat for a wide range of wildlife species because of the undeveloped to moderately developed nature of the project area. For this reason it was difficult to quantify wildlife habitat fragmentation and impact. This criterion was included in the fragmentation of a park or refuge category.
Trail Crossings	The project area has a multitude of trails, both officially recognized through easements and unofficial 'social' trails. Extra effort was taken to identify trail locations and to maintain trail connectivity. The number of trail crossings for each alternative was originally included in the analysis, but was dropped because the numbers of officially recognized trails were similar for all alternatives and the numbers of social trails are not easily quantifiable.

---

### 5.3 Strengths and Weaknesses

Based on the results of the matrix, the various alternatives were described by their strengths and weaknesses. This was done to provide an easy to comprehend list, without numbers, of the pros and cons of each alternative. Table 5-4.

**Table 5-4: Alternatives strengths and weaknesses**

<b>Route</b>	<b>Strengths</b>	<b>Weaknesses</b>
Mac West/ Willow	<ul style="list-style-type: none"> <li>• Requires fewer number of new road crossings</li> <li>• Requires smaller expenditure of train energy</li> </ul>	<ul style="list-style-type: none"> <li>• Crosses more incompatible land uses</li> <li>• Has higher probability of impacting archaeological and/or historical sites</li> <li>• Has greatest impacts to designated state refuges and recreation areas</li> </ul>
Mac West/ Houston North	<ul style="list-style-type: none"> <li>• Requires fewest number of new road crossings</li> <li>• Impacts fewer developed parcels</li> <li>• Has less probability of impacting archaeological and/or historical sites</li> </ul>	<ul style="list-style-type: none"> <li>• Crosses more incompatible land uses</li> <li>• Has less suitable soil conditions</li> <li>• Impacts more wetlands</li> <li>• Impacts a designated refuge and fragments a state recreation area</li> <li>• Crosses greater number of mapped anadromous streams</li> </ul>
Mac West/ Houston South	<ul style="list-style-type: none"> <li>• Requires fewer number of new road crossings</li> <li>• Impacts fewer developed parcels</li> <li>• Has less probability of impacting archaeological and/or historical sites</li> </ul>	<ul style="list-style-type: none"> <li>• Crosses more incompatible land uses</li> <li>• Impacts more wetlands</li> <li>• Requires moderate expenditure of train energy, but less than the two Big Lake alternatives</li> </ul>
Mac West/ Big Lake	<ul style="list-style-type: none"> <li>• Has more suitable soil conditions</li> </ul>	<ul style="list-style-type: none"> <li>• Involves greater number of new road crossings</li> <li>• Impacts more developed parcels</li> <li>• Crosses more incompatible land uses</li> <li>• Requires greater expenditure of train energy</li> <li>• Crosses greater number of mapped anadromous streams</li> <li>• Has higher probability of impacting archaeological and/or historical sites</li> </ul>
Mac East/ Willow	<ul style="list-style-type: none"> <li>• Impacts fewer developed parcels</li> <li>• Crosses more compatible land uses</li> <li>• Impact less wetlands</li> <li>• Crosses fewest mapped anadromous streams</li> </ul>	<ul style="list-style-type: none"> <li>• Has higher probability of impacting archaeological and/or historical sites</li> <li>• Fragments designated state recreation areas</li> </ul>



Route	Strengths	Weaknesses
Mac East/ Houston North	<ul style="list-style-type: none"> <li>• Crosses more compatible land uses</li> <li>• Impacts fewer developed parcels</li> <li>• Less probability of impacting archaeological and/or historical sites</li> </ul>	<ul style="list-style-type: none"> <li>• Has less suitable soil conditions</li> <li>• Fragments a designated state recreation area</li> <li>• Requires moderate expenditure of train energy, but less than the two Big Lake alternatives</li> </ul>
Mac East/ Houston South	<ul style="list-style-type: none"> <li>• Impacts fewer developed parcels</li> <li>• Crosses more compatible land uses</li> <li>• Impacts less wetlands</li> <li>• Has less probability of impacting archaeological and/or historical sites</li> <li>• Avoids designated state refuges and recreation areas</li> </ul>	<ul style="list-style-type: none"> <li>• Requires moderate expenditure of train energy, but less than the two Big Lake alternatives</li> <li>• Involves moderate number of new road crossings, but less than the two Big Lake alternatives</li> <li>• Has less suitable soil conditions than the Big Lake alternatives, but better than the Houston North alternatives</li> </ul>
Mac East/Big Lake	<ul style="list-style-type: none"> <li>• Avoids designated state refuges and state recreation areas</li> <li>• Crosses more compatible land uses</li> <li>• Has more suitable soils</li> <li>• Impacts less wetlands</li> </ul>	<ul style="list-style-type: none"> <li>• Impacts more developed parcels</li> <li>• Involves greater number of new road crossings</li> <li>• Requires greater expenditure of train energy</li> <li>• Crosses greater number of mapped anadromous streams</li> <li>• Has higher probability of impacting archaeological and/or historical sites.</li> </ul>

## 6.0 Cost Estimates

The cost estimates for the Port MacKenzie Rail Extension alternatives were prepared using the conceptual level designs that evolved through the alternatives analysis process. The basis for the ground surface elevations was 10-foot interval Digital Elevation Model (DEM) data obtained from the USGS. The design criteria for both railroad and roadway construction are as stated in Section 2.1. Table 6-1 details the cost estimates for each alignment.

### 6.1 Assumptions for Estimate Development

The concept designs and resulting cost estimates were based on the following basic assumptions:

- Typical sections for the primary elements of construction were prepared.
- Horizontal and vertical alignments were prepared for each alternative segment.
- All alignments and earthwork quantities were calculated using In-Roads software.
- A preliminary Hydrology and Hydraulic (H&H) analysis was done for the identified streams encountered.
- Concept level bridge designs were prepared for the crossing of Willow Creek and the Little Susitna River, based on actual field bridge site surveys, the preliminary H&H analysis and the conceptual track crossing profile.
- It was assumed that the smaller drainages would be crossed using appropriately sized culverts.
- Where culvert sizes to accommodate the design flow exceeded 72-inches, short span bridges were assumed.
- Where the stream was identified as an anadromous fish streams the culverts were arbitrarily upsized to either a 72 inch culvert or a short span bridge.
- The planed grade line was held approximately 7 feet above the surrounding terrain unless in a cut to provide for well drained roadbed.
- Grade breaks and vertical curves were spaced such that a train would be passing through only one crest or sag vertical curve at time.
- All unit prices are in 2007 dollars.

### 6.2 Estimate Items

Construction quantities and probable unit costs were developed as discussed below.

#### 6.2.1 Civil

The Civil category of the estimate is comprised of four items.

Clearing and grubbing quantities were estimated on a “per acre” basis for each alternative. The acreage was estimated from the concept designs and calculated as the construction foot print area covering the entire area that would be disturbed by grading operations, either cut or fill. The unit price developed for this estimate is a blend of pricing from recent ARRC projects and ADOT&PF projects.

Aggregate base course is the layer of crushed gravel normally placed immediately below the pavement section in a roadway surface. For this project, aggregate base would be used, depending on alignment, when roads are crossed at grade as part of the construction of the road approach to the new crossing. Aggregate base would also be used if the Parks Highway is to be crossed as part of the reconstruction of the highway pavement section. The quantity has been estimated for a

6-inch layer placed the full width of the roadway base. The unit price developed for this estimate is a blend of pricing from recent ADOT&PF projects.

Asphalt concrete is the material to be used to provide the new driving surface in restoring pavements on the Parks Highway or other paved roadways or pathways. The unit price developed for this estimate is a blend of pricing from recent ADOT&PF projects.

Culvert pipe would be required for all alternatives. Because the hydrology and hydraulics analysis done to date is based on the DEM information rather than field measurements, the assumption was made that four pipe sizes would be used for the estimate. These are 24-inch, 36-inch, 48-inch and 72-inch round corrugated metal pipe. Unit prices for each, on a Linear Foot basis are based on information provided by ARRC and ADOT&PF.

### 6.2.2 Earthwork

The Earthwork category of the estimate includes three volumetric items.

Unclassified Excavation quantities were estimated by using In-Roads software to impose a template through the existing ground DEM, controlled by the design horizontal and vertical alignments. The software then calculated the amount of excavation associated with each alignment. Where areas of compressible soils had been defined by the geotechnical field reconnaissance described in Section 4, the excavation includes removal of all compressible soils within a width of 80 feet. The unit price developed for this estimate is a blend of pricing from recent ARRC projects and ADOT&PF projects.

Borrow quantities were calculated by subtracting the values for useable excavation from the total fill quantities. The total fill quantities were estimated using In-Roads software and are defined as the amount of material to construct embankment from the bottom of excavation as determined in the Unclassified Excavation calculation to the planned subgrade elevation. The useable excavation was calculated as the Unclassified Excavation quantities minus values for clearing and grubbing and compressible soils, both of which are assumed to be unsuitable for embankment. No adjustments have been made for shrink or swell factors. The unit price developed for this estimate is a blend of pricing from recent ARRC projects and ADOT&PF projects.

Subballast is the layer of granular material in the railroad structural section between subgrade and ballast. For this project the subballast would be one-foot thick covering the top of the subgrade. All subballast material is considered to be borrow material, screened at the source to achieve a specified gradation. No adjustments have been made for shrink or swell factors. The unit price developed for this estimate is a blend of pricing from recent ARRC projects.

### 6.2.3 Track

The Track category summarizes four items that relate to the completed track structure and associated grade crossings.

Track, concrete ties, Ballasted, 141 lb CWR – this item is an aggregation of crushed rock ballast, pre-cast and pre-stressed concrete ties, tie hardware and 141 lb/yd mainline continuously welded rail. This combined unit is typically bid on a per foot of ballasted track in place basis. The unit prices included for this item are based on information provided by ARRC.

Railroad turnouts, No. 15 – 141 lb, are the switch units that are an integral part of the track work and would be used at each siding and/or other track junction. The unit prices included for this item are based on information provided by ARRC.

For grade crossing – crossbucks, the cost of each installation includes standard advance warning signs, the crossbuck signs on each approach to the crossing, and the pre-cast concrete grade crossing panels and related site construction. There are a varying number of new grade crossings in each alternative. Traffic volumes are generally low on these roadways, often not more than 100 to 200 per day such that more sophisticated levels of crossing protection do not appear to be warranted. Prior to construction, each crossing location would be evaluated by a diagnostic team and the most appropriate level of crossing protection selected. The unit prices included for this item are based on information provided by ARRC and ADOT&PF.

For grade crossing – signals/gates, the cost of each installation includes standard advance warning signs and a combination unit at the crossing that includes both the cross-arm gates and flashers, both of which are activated by an approaching train. The cost also includes the related signal equipment, pre-cast concrete grade crossing panels, and related site construction. These installations are typically used where there is a higher volume of vehicular and/or rail traffic. Prior to construction, each crossing location would be evaluated by a diagnostic team and the most appropriate level of crossing protection selected. The unit prices included for this item are based on information provided by ARRC and ADOT&PF.

#### 6.2.4 Structures

The Structures category of the estimate is comprised of four items that relate to the completed track structure and associated grade crossings.

River bridges occur in one form or another on the Willow and Houston alignments. There are no bridge structures expected over waterbodies on the Big Lake alignment. The Willow alignment crosses the Little Susitna River and Willow Creek. The Houston North alignment crosses the Little Susitna River and the Houston South alignment includes a Little Susitna River crossing on the siding that is part of the junction with the existing mainline track. Each of the bridges was laid out using concept level track profile elevations and field collected bridge site survey data. Each of the river bridges are, conceptually, standard ARRC deck girder structures with the exception of the Houston South structure which is a through girder, matching the existing mainline structure at the same location. Bridge costs were developed on a “Per Each” basis for the estimate and as they are very similar in layout they are estimated at the same “Per Each” price. The “Per Each” price has been developed using a weighted average of bid prices for comparable bridge structures in Alaska and in the Pacific Northwest.

Highway grade separation structures are anticipated at the Parks Highway for both the Willow and Big Lake alignments. The Big Lake alignment also has a grade separation with Big Lake Road. On the Willow alignment, the tracks are far enough below the Parks Highway grade in a through cut deep enough that there is sufficient clearance between the top of rail elevation and the bottom of girder elevation that the highway grade would remain essentially as it is today. The highway bridge is expected to be a conventional 3-span pre-stressed concrete girder highway bridge. On the Big Lake alignment at Big Lake Road the tracks are above Big Lake Road and provide the desired clearance between roadway surface and bottom of structure. The structure would be a standard 3-span deck girder railroad bridge. The Big Lake alignment meets the Parks Highway below highway grade however, it would be necessary to raise the highway approximately 9 feet to provide adequate clearance. This would require reconstruction of about 4,000 feet of the Parks Highway and relocation of the Hawk Lane/Parks Highway intersection. This intersection is the primary access to Houston Middle and High Schools. Relocation of the intersection also includes relocation of approximately 1,000 feet of Hawk Lane. The quantities of materials associated with the roadway construction are included in the grading, aggregate and paving quantities discussed above. The Parks Highway bridge would be a conventional 3-span

pre-stressed concrete girder highway bridge. The unit prices for the highway bridges are based on information provided by ADOT&PF bridge section.

Drainage Structures would be used at several locations where smaller drainages and waterbodies are either anadromous fish streams requiring a pipe larger than 72-inch diameter and/or where design flows suggest a pipe larger than 72-inch diameter. The drainage structures could possibly be designed as multi-plate culverts, precast concrete arches, or one or more spans of 28-foot standard ARRC deck girder bridges. The number of drainage structures varies from alignment to alignment. The “Per Each” price has been developed using a weighted average of bid prices for comparable bridge structures in Alaska and in the Pacific Northwest.

Trail grade separations would be used at a number of locations for each alignment. These trails are used by dog mushers and by snowmachiners. For those locations where the rail grade is above the trail a multi-plate culvert or short ARRC standard deck girder bridge could be used. Where a combination of terrain and design profile makes it convenient for the trail to cross over the tracks, a trail bridge may be used. The unit price included in the estimate is a “Per Each” price and is based on recent experience in Alaska and in the Pacific Northwest.

### 6.2.5 *Right-of-Way*

Right-of way acquisition is a rough order of magnitude estimate per alignment segment of the cost to purchase and deliver ROW. This includes estimates of the cost to purchase land and improvements and the estimated costs of appraisals, title reports, acquisition and relocation staff and administration, and relocation benefits costs. Land and improvement values were based on 2006 MSB assessed values, adjusted to approximate 2007 fair market values. All 2006 improvements on impacted parcels were considered to be full acquisitions. Some of these improvements are likely to not be acquired, however, this tends to balance the circumstance that improvements entered onto the MSB tax rolls since 2006 were not known or considered. Where no MSB assessed value data was available, values were estimated based on surrounding values. No estimated costs were developed for ARRC-owned property.

It would be necessary for the ROW estimates to be adjusted as a result of market, salary, and operational cost changes over time. At this time it is reasonable to assume a 5% yearly increase of the rough order of magnitude ROW estimate. Major changes in the State or local economy, such as approvals of new major mineral extraction activities, greatly improved infrastructure, and/or residential and commercial development would have an unpredictable affect on the market and could make the forecast of a 5% annual increase in costs inaccurate.

### 6.2.6 *Additives*

Mobilization – This standard bid item covers the cost incurred by the contractor for a project office, communications facilities, relocating key staff and bringing construction equipment to the job site and placing the equipment in service. In this instance, due to the magnitude of the project, Mobilization is estimated at 8% of the total of all other items. This percentage is based on an averaging of information provided by ARRC, ADOT&PF, and the project team’s experience.

Engineering and construction management includes such activities as surveying and mapping, geotechnical investigations, detailed design of project elements, production of plans, specifications and estimates for soliciting construction bids, permitting, assistance during bidding, assistance during construction and may include full “Owners Representative”, materials testing and on-site inspection services. The 15% of other items for engineering and construction management included herein is based on an averaging of information provided by ARRC, ADOT&PF, and the project team’s experience.

Contingency – this is a cost included at the early stages of project development to provide a budget number for the multitude of smaller project elements that cannot be fully identified at the early stages of project development. It is a reserve amount included to cover “unknowns”. The 25% of all other costs, at this stage of any project, is a normal amount to be included as contingency. This is based on the practice of ARRC, ADOT&PF and the experience of the project development team.

*(This page intentionally left blank.)*

Table 6-1 Conceptual Opinion of Probable Cost

Categories	Alignments							
	Mac West - Willow	Mac West – Houston – Houston North	Mac West – Houston – Houston South	Mac West – Big Lake	Mac East - Willow	Mac East – Houston – Houston North	Mac East – Houston – Houston South	Mac East – Big Lake
Length (miles)	44.8	35.1	34.5	35.8	45.0	35.3	34.7	31.8
Civil	\$4,130,900	\$3,318,000	\$3,016,000	\$5,176,300	\$4,238,900	\$3,366,000	\$3,124,000	\$4,854,300
Earthwork	\$88,065,000	\$71,707,000	\$62,058,000	\$73,170,000	\$88,140,000	\$71,782,000	\$62,158,000	\$66,986,000
Track	\$69,200,000	\$53,750,000	\$53,600,000	\$56,950,000	\$70,000,000	\$54,550,000	\$54,400,000	\$51,700,000
Structures	\$20,600,000	\$11,100,000	\$9,800,000	\$12,100,000	\$18,000,000	\$8,500,000	\$7,200,000	\$12,100,000
Right-of-Way	\$3,890,000	\$5,360,000	\$3,330,000	\$6,790,000	\$3,030,000	\$4,500,000	\$2,470,000	\$5,120,000
Subtotal	\$185,885,900	\$145,235,000	\$131,804,000	\$154,186,300	\$183,408,900	\$142,698,000	\$129,352,000	\$140,760,300
Mobilization and demobilization (8% of subtotal)	\$14,871,000	\$11,619,000	\$10,545,000	\$12,335,000	\$14,673,000	\$11,416,000	\$10,349,000	\$11,261,000
Engineering and Construction Management (15% of subtotal)	\$27,883,000	\$21,786,000	\$19,771,000	\$23,128,000	\$27,512,000	\$21,405,000	\$19,403,000	\$21,115,000
TOTAL (With no contingency)	\$228,639,900	\$178,640,000	\$162,120,000	\$189,649,300	\$225,593,900	\$175,519,000	\$159,104,000	\$173,136,300
25% Contingency	\$58,000,000	\$45,000,000	\$41,000,000	\$48,000,000	\$57,000,000	\$44,000,000	\$40,000,000	\$44,000,000
<b>TOTAL WITH CONTINGENCY</b>	<b>\$286,639,900</b>	<b>\$223,640,000</b>	<b>\$203,120,000</b>	<b>\$237,649,300</b>	<b>\$282,593,900</b>	<b>\$219,519,000</b>	<b>\$199,104,000</b>	<b>\$217,136,300</b>



*(This page intentionally left blank.)*

## 7.0 Permits

The purpose of this section is to provide preliminary identification of the major Federal, State, and local permits and clearances that would be required to carry this project into construction phase.

Permitting for this project would be coordinated through a Reimbursable Service Agreement with ADNR OPMP Large Projects. ARRC and MSB initiated the permitting process with agencies by having a project introduction meeting on September 18, 2007 and subsequent one-on-one or small group meetings in October and November 2007. Preliminary letters received from agencies are included in Appendix C and minutes from agency meetings are included in Appendix D. In addition, the ARRC has developed preliminary voluntary mitigation measures to streamline the permitting process (Appendix E).

### 7.1 Implications for Permitting

The USACE, USEPA, U.S. Coast Guard (USCG), ADEC, ADNR, and MSB would require permits or approvals to construct any of the Port MacKenzie Rail Extension project preliminary alignments. This project would require agency coordination to obtain the following permits and clearances:

- Coastal Consistency Determination – Coastal Project Questionnaire and Coastal Project Consistency Evaluation; ADNR DCOM, formerly OPMP; Note: As of December 2007, the ADNR branch of the OPMP that administered the coastal consistency determination is now housed in the new DCOM.
- Section 404/10 Permit; USACE
- Title 41 Fish Habitat Permit; ADNR OHMP
- Flood Plain Development Permit; MSB
- Section 401 Certification (Certificate of Reasonable Assurance); ADEC
- National Pollutant Discharge Elimination System (NPDES) general permit for storm water discharges from construction activities; USEPA

Depending on the alternative selected, the following permits may also be required:

- Section 9 Bridge Permit; USCG
- Land Use Permit; ADNR DMLW
- Temporary Water Use Permit; ADNR DMLW
- Special Area Permit; ADF&G
- Special Use Permit; ADNR DPOR (State Parks)

#### Coastal Consistency Review

Consistency review of the project under the ACMP helps to ensure that all aspects of a project within identified coastal boundaries are considered during a review and approval process. Permissible land and water uses within the coastal zone are identified in the statewide standards of the ACMP, the approved district coastal management plans and AMSA (Port Mackenzie District).

A coastal project questionnaire would need to be prepared. ADNR DCOM issues a Coastal Consistency Determination to certify compliance with these standards. The coastal consistency determination process generally runs concurrently with the USACE 404 permit process. The ADNR OPMP must perform a formal review of the project and make a finding of consistency before major resource agency permits are issued.

### **Section 404 Permit**

Under the CWA, a Section 404 Permit from the USACE would be required for the discharge of dredged or fill material into waters of the United States, including wetlands.

### **Section 10 Permit**

Under the Rivers and Harbors Act of 1899, a Section 10 permit would be required for work in, on, and over navigable waters of the U.S., including fill and structures placed below mean high water. Consultation with the USACE and other regulatory agencies is necessary to address as many concerns as possible in advance of application submittal. Once a complete package is accepted, the USACE would prepare a Public Notice (PN). The PN would give the public and agencies 30 days to comment on the project.

### **Title 41 Fish Habitat Permit**

Alaska Statute 41.14.840 (Fishway Act) and Alaska Statute 41.14.870 (Anadromous Fish Act) require that an individual or government agency notify and obtain authorization (Title 41 Fish Habitat Permit) from the ADNOR OHMP for activities within or across a stream used by fish if the OHMP determines that such uses or activities could represent an impediment to the efficient passage of fish. For example, culvert installation; water withdrawals; stream realignment or diversion; dams; low-water crossings; and construction, placement, deposition, or removal of any material or structure below ordinary high water all require approval from the OHMP. All activities within or across a specified anadromous waterbody and all in-stream activities affecting a specified anadromous waterbody require approval from the OHMP.

### **Flood Plain Development Permit**

A Flood Plain Development Permit is required before any development within a Federally Designated Flood Hazard Area. A Flood Plain Development Permit (issued by MSB) must include both the MSB Flood Hazard Development Permit and the Elevation Certificate. An Alaska registered Architect or Engineer must certify the Development Permit Application and either a Registered Engineer or Surveyor must complete the elevation certificate.

**401 Certification** – Pursuant to Section 401 of the CWA the State of Alaska certifies that the project complies with State water quality standards. This is commonly known as the 401 Certification. This review typically results in conditions placed on either or both the Section 404 permit and Coastal Consistency Determination. The 401 Certification is initiated by the USACE as part of the 404 permitting process. The ADEC issues the certification.

### **NPDES General Permit**

The CWA prohibits anybody from discharging "pollutants" through a "point source" into a "water of the United States" unless they have an NPDES permit. As authorized by the CWA, the NPDES permit contains limits on what can be discharged, monitoring and reporting requirements, and other provisions to ensure that the discharge does not hurt water quality or people's health. In essence, the permit translates general requirements of the Clean Water Act into specific provisions tailored to the operations of each person discharging pollutants.

Alaska falls under Region 10, where the USEPA is the permitting authority; a Construction General Permit (CGP) outlines a set of provisions construction operators must follow to comply with the requirements of the NPDES stormwater regulations. The NPDES Stormwater program requires operators of construction sites one acre or larger to obtain authorization under the CGP and develop and implement a stormwater pollution prevention plan.

### **Section 9 Bridge Permit**

Under Section 9 of the Rivers and Harbors Act of 1899, a USCG Bridge Permit would be required to construct any bridge or causeway over any navigable river or navigable water of the U.S. The need for one or more Section 9 permit(s) would depend on the selected alternative.

### **Land Use Permit**

Land use permits are authorizations issued to use state land, on a temporary basis, for a variety of purposes. The permits range in duration from one to five years. They are intended for temporary, non-permanent uses such as floating lodges, log storage, scientific research, guide camps, equipment storage and commercial recreation uses. Land use permits are also issued for most activities in navigable waters because the state owns land below the ordinary high water (OHW) line (i.e., bridge crossings of navigable waters where pier placement occurs below OHW).

### **Temporary Water Use Permit**

Issued by ADNR DMLW, a temporary water use authorization may be needed if the amount of water to be used is a significant amount, the use continues for less than five consecutive years, and the water to be used is not appropriated. This authorization does not establish a water right but would avoid conflicts with fisheries and existing water right holders.

### **Special Area Permit**

This permit should be used for any disturbance-producing or habitat altering activity that would occur in a designated state game refuge, critical habitat area, or game sanctuary.

### **Special Use Permit**

Special Use Permits are issued by ADNR State Parks for a variety of activities and uses occurring within a state recreation area or state park. This permit may be necessary for alternatives affecting a state recreation area.

## 8.0 References

- Alaska Climate Research Center. 2007. <http://climate.gi.alaska.edu/Climate/Location/CookInlet/Matanuska.html>
- Alaska Department of Commerce, Community, and Economic Development (ADCCED). 2007. Matanuska-Susitna Borough Information Summary. <http://www.commerce.state.ak.us/dca/commdb/CIS.cfm>
- Alaska Department of Fish and Game (ADF&G). 1988. *Susitna Flats State Game Refuge Management Plan*: Anchorage.
- . 2007a. *Catalog of waters important to anadromous fish -Southcentral Region*. Alaska Department of Fish and Game, Anchorage, Alaska. [http://www.sf.adfg.state.ak.us/SARR/FishDistrib/PDFs/sfdoc\\_SCN\\_2007\\_final.pdf](http://www.sf.adfg.state.ak.us/SARR/FishDistrib/PDFs/sfdoc_SCN_2007_final.pdf)
- . 2007b. Division of Wildlife Conservation Web site. State of Alaska Endangered Species List. <http://www.wildlife.alaska.gov/index.cfm?adfg=endangered.list>
- . 2007c. Goose Bay State Game Refuge Web site. <http://www.wc.adfg.state.ak.us/index.cfm?adfg=refuge.goosebay> (accessed October 8, 2007).
- Alaska Department of Natural Resources (ADNR) and Matanuska-Susitna Borough (MSB). 1987. *Fish Creek Management Plan, Amended*. Prepared by Alaska Department of Natural Resources, Division of Land and Water Management in cooperation with Matanuska-Susitna Borough: Anchorage.
- Alaska Department of Natural Resources (ADNR). 1982. *Willow Sub-Basin Area Plan*. Prepared by Alaska Department of Natural Resources, Division of Research and Development, Land & Resource Planning Section. Adopted by the Matanuska-Susitna Borough: Anchorage.
- . 1991. *Susitna Basin Recreation Rivers Management Plan*. Prepared by ADNR, Division of Land, Land and Resource Section, and ADF&G, in cooperation with Mat-Su Borough, and with assistance from National Park Service.
- . 2006a. Revised Matanuska-Susitna Borough Coastal Management Plan. *Volume II: Point MacKenzie AMSA*.
- . 2006b. Revised Matanuska-Susitna Borough Coastal Management Plan. *Volume I: Matanuska-Susitna Borough Coastal Management Plan*
- . 2006c. Revised Matanuska-Susitna Borough Coastal Management Plan. *Volume III: Maps*.
- . 2007. RS2477 Project Web site. Division of Mining, Land and Water. <http://www.dnr.state.ak.us/mlw/trails/rs2477/>
- Alaska Department of Transportation & Public Facilities (ADOT&PF). 1995. *Moose-Vehicle Accidents on Alaska's Rural Highways*.
- . 2006. *Mat-Su Valley Traffic Map*. Prepared by ADOT&PF Division of Program Management, Statewide GIS/Mapping Section. <http://www.dot.state.ak.us/stwdplng/mapping/adt.shtml>
- Alaska Railroad Corporation (ARRC). 2006. *Technical Manual P2, Hazardous Traffic Analysis*. Alaska Rail Corporation: Anchorage.

- Andersen, Reidar, Bjornar Wiseth, Paul H. Pedersen and Vemund Jaren. 1991. *Alces* 27:79-84.
- Becker, Earl F., and Carl A. Grauvogel 1991. Relationship of Reduced Train Speed on Moose-Train Collisions in Alaska. *Alces* 27:161-68.
- Bureau of Land Management (BLM). 1986. *Iditarod National Historic Trail: Seward to Nome Route: A Comprehensive Management Plan*. 2 vols.: Anchorage.
- . 2000. *Environmental Assessment #99-206, Alaska Railroad Company Track Realignment Project – Military Segment*. Prepared by Dames & Moore, Inc. May 2000.
- Collazzi, E.J., M.A. Maurer, and S.J. Carrick. 1988. *Reconnaissance of surface-water resources in the Houston area, Alaska, 1985-86*. Alaska Department of Natural Resources, Division of Geological and Geophysical Surveys.
- Federal Transit Administration and Mat-Su Borough. 2006. *Final Cook Inlet Ferry NEPA Supplemental Environmental Analysis*. Prepared by HDR Alaska: Anchorage.
- Fuglestad, T.C. 1983. *The Alaskan Railroad Between Anchorage and Fairbanks: Route Description and Engineering Problems*. Fourth International Conference on Permafrost: Fairbanks.
- Holtz, R.D., and Kovacs, William D. 1981. *An Introduction to Geotechnical Engineering*, Prentice Hall Civil Engineering and Engineering Mechanics Series.
- Jaren, Vemund, Reider Anderson, Morten Ulleberg, Paul Harald Pedersen and Bjørnar Wiseth. 1991. Moose-Train Collisions: The Effects of Vegetation Removal with a Cost-Benefit Analysis. *Alces* 27:93-9.
- Jokela, J.B., Munter, J.A. and Evans, J.G. 1990. *Ground-water resources of the Palmer-Big Lake area, Alaska: a conceptual model*. Alaska Department of Natural Resources, Alaska Division of Geological and Geophysical Surveys.
- Knik Arm Bridge and Toll Authority (KABATA). 2005. *Knik Arm Crossing Groundwater Resource Evaluation*. Prepared by GeoEngineers at the request of HDR Alaska, Inc.: Anchorage.
- . 2006a. *Draft Environmental Impact Statement and Draft 4(f) Evaluation*. Prepared by HDR Alaska, Inc.: Anchorage. [http://www.knikarmbridge.com/project\\_docs.html](http://www.knikarmbridge.com/project_docs.html)
- . 2006b. *Knik Arm Crossing Land Use and Transportation Forecasting: DRAFT. Appendix A: Draft Land Use and Affected Environment Technical Report* Prepared by HDR Alaska, Inc.: Anchorage.
- . 2006c. *Knik Arm Crossing Draft Terrestrial Mammals Technical Report*. Prepared by HDR Alaska, Inc.: Anchorage.
- . 2007. <http://www.knikarmbridge.com/index.html>
- Lamke, Robert D., Timothy P. Brabets, and Jacqueline A. McIntire. 1995. *Hydrologic Unit Codes Map for the State of Alaska*.
- Matanuska-Susitna Borough (MSB) and Sustainable Environments, LLC (SEL). 2007a. *Su-Knik Environmental Bank, South-Central Alaska: Umbrella Mitigation Bank Instrument: Palmer*.
- . 2007b. *Su-Knik Environmental Bank, South-Central Alaska: Umbrella Mitigation Bank Instrument. Appendix G: Big Lake South Individual Bank Plan: Palmer*.

- Matanuska-Susitna Borough (MSB). 1996. *Big Lake Comprehensive Plan*. Prepared by Big Lake Citizens' Planning Advisory Committee and the Matanuska-Susitna Planning Department: Palmer.
- . 1997. *Knik-Fairview Comprehensive Plan: Transportation*. Mat-Su Borough: Wasilla.
- . 1997. *Long Range Transportation Plan*: Palmer.
- . 2000. *Matanuska-Susitna Borough Recreational Trails Plan*.
- . 2001. *Matanuska-Susitna Borough Asset Management Plan: Parks, Recreation & Open Space Plan*. Prepared by Land Design North: Anchorage.
- . 2003a. *Matanuska-Susitna Borough Rail Corridor Study*. Prepared by Tryck Nyman Hayes, Inc.: Anchorage.
- . 2003b. *City of Houston Comprehensive Plan*: Palmer.
- . 2005. *Comprehensive Plan (2005 Update)*. Palmer.
- . 2006. *Mat-Su Comprehensive Economic Development Strategy*. Prepared by Robert Wells and Dave Hansen: Palmer.
- . 2007a. *Long Range Transportation Plan Draft Final*. Prepared by HDR Alaska, Inc.: Anchorage.
- . 2007b. *Fish Creek Management Plan: Draft Alternatives*. Prepared by Agnew: Beck Consulting, LLC and Jade North Consulting: Anchorage.
- . 2007c. Mat-Su Borough Prison project Web site.  
<http://www.matsugov.us/administration/projects/prison>
- . 2007d. Assembly Ordinance No. 07-061. *Amendment to the Matanuska-Susitna Borough Recreational Trails Plan Component of the Borough's Comprehensive Plan*.
- Modafferi, Ronald D. 1991. Train Moose-Kill in Alaska: Characteristics and Relationship with Snowpack Depth and Moose Distribution in Lower Susitna Valley. *Alces* 27:193-207.
- Patton, Carl V. and David S. Sawicki. 1993. *Basic Methods of Policy Analysis and Planning* (2<sup>nd</sup> ed.). Prentice Hall: New Jersey.
- U.S. Bureau of the Census. U.S. Census 2000. <http://www.census.gov/main/www/cen2000.html>
- U.S. Department of Agriculture (USDA). 1995. *Soil Survey of Matanuska-Susitna Valley Area, Alaska*. Natural Resources Conservation Service.
- . 1998. *Soil Survey of Matanuska-Susitna Valley Area, Alaska*. Natural Resources Conservation Service.
- . 2007. Alaska SNOTEL Snow Water Equivalent Averages: 1971-2000. Natural Resources Conservation Service.  
<http://www3.wcc.nrcs.usda.gov/nwcc/sweavg.jsp?state=AK>
- U.S. Environmental Protection Agency (USEPA). 2007. Air Quality Index Report, Alaska.  
<http://www.epa.gov/air/data/monaqi.html?st~AK~Alaska>
- U.S. Fish and Wildlife Service (USFWS). 2007. Threatened and endangered species in Alaska. Environmental Conservation Online System.  
[http://ecos.fws.gov/tess\\_public/StateListing.do?state=AK&status=listed](http://ecos.fws.gov/tess_public/StateListing.do?state=AK&status=listed)

- U.S. Geological Survey (USGS). 1970. *Effects of the earthquake of March 27, 1964, on the Alaska Railroad*. Prepared by D.S. McCulloch and M.G. Bonilla. U.S. Geological Survey Professional Paper 545-D, p. D1-D161: Washington.
- . 2002. “USGS Study Shows that Anchorage Area Fault is Capable of a Large Quake.” Prepared by Peter J. Haeussler and Catherine Puckett.  
<http://www.scienceblog.com/community/older/archives/E/usgs308.html>
- . 2005a. Water Year, South-Central Region, Little Susitna River near Palmer, Alaska.  
<http://pubs.usgs.gov/wdr/2005/wdr-ak-05-1/regions/southcentral/gs/spQ15290000.pdf>
- . 2005b. Water Year, South-Central Region, Willow Creek near Willow, Alaska.  
<http://pubs.usgs.gov/wdr/2005/wdr-ak-05-1/regions/southcentral/gs/spQ15294005.pdf>
- Willow Area Community Organization (WACO). 2006. *Willow Area Trail Plan*. Prepared by Willow Trails Committee: Willow.



## ACRONYMS & ABBREVIATIONS

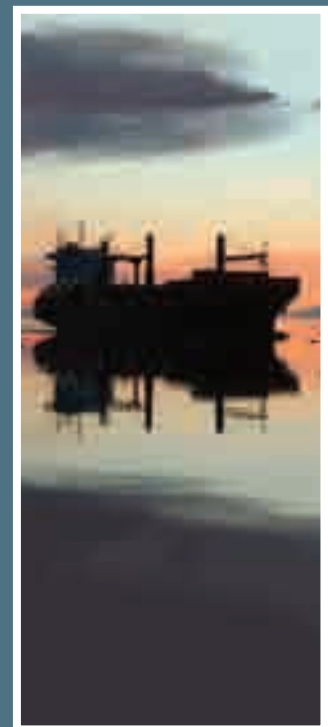
AAC	Alaska Administrative Code
AADT	Annual Average Daily Traffic
ACMP	Alaska Coastal Management Program
ADCCED	Alaska Department of Commerce, Community and Economic Development
ADEC	Alaska Department of Environmental Conservation
ADF&G	Alaska Department of Fish and Game
ADNR	Alaska Department of Natural Resources
ADOC	Alaska Department of Corrections
ADOT&PF	Alaska Department of Transportation and Public Facilities
AHRS	Alaska Heritage Resource Survey
AMSA	Area Which Merits Special Attention
AREMA	American Railway Engineering and Maintenance-of-Way Association
ARRC	Alaska Railroad Corporation
BLM	Bureau of Land Management
BMP	Best Management Practice
CFR	Code of Federal Regulations
CGP	Construction General Permit
CIRI	Cook Inlet Region, Inc.
CMP	Coastal Management Plan
CWA	Clean Water Act
CY	cubic yard
DEM	Digital Elevation Model
DMLW	Division of Mining, Land and Water (ADNR)
DOA	Division of Agriculture (ADNR)
DCOM	Division of Coastal and Ocean Management (ADNR)
DPOR	Division of Parks and Outdoor Recreation (ADNR)
EFH	Essential Fish Habitat
ESA	Endangered Species Act
FEMA	Federal Emergency Management Agency
FRA	Federal Railroad Administration
°F	degree Fahrenheit
GIS	Geographic Information System
GMU	Game Management Unit
H&H	hydrology and hydraulic
INHT	Iditarod National Historic Trail
KABATA	Knik Arm Bridge and Toll Authority
lb	pound
LWCF	Land and Water Conservation Fund
L RTP	Long Range Transportation Plan
MBTA	Migratory Bird Treaty Act
MSB	Matanuska-Susitna Borough
MP	Milepost
mph	miles per hour
NEPA	National Environmental Policy Act
NHPA	National Historic Preservation Act

NOAA Fisheries	National Oceanic and Atmospheric Administration National Marine Fisheries Service
NPDES	National Pollutant Discharge Elimination System
NRCS	Natural Resources Conservation Service
NWI	National Wetland Inventory
OHMP	Office of Habitat Management & Permitting
OHW	Ordinary High Water
OPMP	Office of Project Management and Permitting
REC	Recognized Environmental Conditions
ROW	right-of-way
RS	Revised Statute
SGR	State Game Refuge
SHPO	State Historic Preservation Office(r)
SRA	State Recreation Area
STB	Surface Transportation Board
SWPPP	stormwater pollution prevention plans
USACE	U.S. Army Corps of Engineers
USCG	U.S. Coast Guard
USDA	U.S. Department of Agriculture
USDOT	U.S. Department of Transportation
USEPA	U.S. Environmental Protection Agency
USFWS	U.S. Fish and Wildlife Service
USGS	U.S. Geological Survey
yd	yard



# Appendices

*Port Mackenzie Rail Extension Project — Preliminary Environmental and Alternatives Report*



Prepared for the Matanuska-Susitna Borough and the Alaska Railroad Corporation

## **Appendix A: Relevant Past MSB Assembly Resolutions and Actions**

MSB Assembly Resolution Serial No. 07-139, Adopted December 11, 2007

*A resolution of the Matanuska-Susitna Borough Assembly regarding additional important social, economic, and environmental issues for consideration during the development of the Surface Transportation Board's (STB) Environmental Impact Statement of the Alaska Railroad Extension to Port MacKenzie.*

MSB Ordinance No. 05-011 (AM), Adopted February 1, 2005

*An ordinance of the Matanuska-Susitna Borough Assembly amending MSB 15.24.030, comprehensive plan and purposes, specifically to include the June 2003 Matanuska-Susitna Borough rail corridor study.*

MSB Planning Commission Action Memorandum No. 92-109, dated April 21, 1992

*The Port MacKenzie Access Corridor Study and the Planning Commission recommend combinations of Routes 4 and 5 [a route to Houston] as the rail route to the Port, as based on the Point MacKenzie Transportation Corridor Study conducted for the Matanuska-Susitna Borough by PND, Inc in March 1992.*

Sponsored by: Assemblymember Bettine  
Amended: 12/11/07  
Adopted: 12/11/07

**MATANUSKA-SUSITNA BOROUGH  
RESOLUTION SERIAL NO. 07-139 (AM)**

A RESOLUTION OF THE MATANUSKA-SUSITNA BOROUGH ASSEMBLY REGARDING ADDITIONAL IMPORTANT SOCIAL, ECONOMIC, AND ENVIRONMENTAL ISSUES FOR CONSIDERATION DURING THE DEVELOPMENT OF THE SURFACE TRANSPORTATION BOARD'S (STB) ENVIRONMENTAL IMPACT STATEMENT OF THE ALASKA RAILROAD EXTENSION TO PORT MACKENZIE.

---

WHEREAS, the Alaska Railroad and the Matanuska-Susitna Borough are working cooperatively to study and construct a rail extension from the existing Alaska Railroad mainline to Port MacKenzie; and

WHEREAS, the work has been completed on the preliminary engineering and environmental analysis of the proposed rail corridors; and

WHEREAS, preliminary meetings and open houses have taken place to inform the public of the proposed rail line extension and to obtain comments; and

WHEREAS, the Assembly of the Matanuska-Susitna Borough has had limited time to consider the preliminary public and agency comments received to date as well as the results of the preliminary engineering and environmental analysis; and

WHEREAS, the Assembly of the Matanuska-Susitna Borough has conducted a work session on the proposed project to become more fully informed of the engineering and environmental issues related to the proposed project.


NOW, THEREFORE, BE IT RESOLVED, that the Matanuska-Susitna Borough believes the results of the preliminary reports and agency

comments are incomplete and should be studied and reviewed during the Surface Transportation Board (STB) process to a fuller extent; and

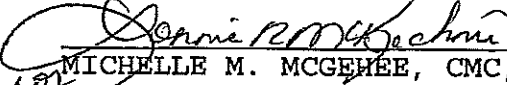
BE IT FURTHER RESOLVED, that the Matanuska-Susitna Borough Assembly requests that the following social, economic, engineering, and environmental issues be more fully explored during the STB's environmental impact statement process. These issues being:

- The potential effects of the Castle Mountain fault, and other faults, on the proposed rail line.
- The potential effects of the rail line on drainage, particularly the drainage associated with the Big Lake watershed and Little Susitna River.
- The potential effects of future population growth, through the year 2025, on the proposed corridors and rail use along the proposed corridors for the communities of Willow, Houston, Knik-Goose Bay, Point MacKenzie, Big Lake Community Council area, and the Borough as a whole.
- The potential effects of future road and trail construction, through the year 2025, on the proposed corridors.
- The potential effects of noise and vibration generated by rail use along the proposed corridors on existing and future development.
- The potential effects of rail line use, along any of the proposed corridors, on future community development, with special attention to residential housing, commercial development, and other economic opportunities.
- The potential effects of the rail line on agriculture, recreation, and fish and wildlife along the proposed corridors.
- Preparation of future population estimates for both 20 year and 40 year time horizons and review of related impacts.

ADOPTED by the Matanuska-Susitna Borough Assembly this 11 day  
of December, 2007.

  
CURTIS D. MENARD, Borough Mayor

ATTEST:

  
MICHELLE M. MCGEEHEE, CMC, Borough Clerk  
for  
(SEAL)

PASSED UNANIMOUSLY: Woods, Houston, Church, Kvalheim, Bettine, and  
Wells



CODE ORDINANCE

By: Borough Manager  
Introduced: 01/04/05  
Public Hearing: 01/18/05  
Public Hearing Continued to 02/01/05: 01/18/05  
Public Hearing: 02/01/05  
Amended: 02/01/05  
Adopted: 02/01/05

**MATANUSKA-SUSITNA BOROUGH  
ORDINANCE SERIAL NO. 05-011 (AM)**

AN ORDINANCE OF THE MATANUSKA-SUSITNA BOROUGH ASSEMBLY AMENDING MSB 15.24.030, COMPREHENSIVE PLAN AND PURPOSES, SPECIFICALLY TO INCLUDE THE JUNE 2003 MATANUSKA-SUSITNA BOROUGH RAIL CORRIDOR STUDY.

---

WHEREAS, the Transportation Advisory Board Resolution Serial No. 04-08, adopted November 17, 2004, recommends the inclusion of the June 2003 Matanuska-Susitna Borough Rail Corridor Study in the overall Matanuska-Susitna Borough Comprehensive Plan; and

WHEREAS, the Planning Commission Resolution Serial No. 04-06 adopted December 6, 2004, recommends the inclusion of the June 2003 Matanuska-Susitna Borough Rail Corridor Study in the overall Matanuska-Susitna Borough Comprehensive Plan.

BE IT ENACTED:

Section 1. Classification. Section 2 and 4 of this ordinance are non-code. Section 3 of this ordinance is of a general and permanent nature and shall become a part of the Borough code.

Section 2. Amendment of the Matanuska-Susitna Borough Rail Corridor Study, June 2003. At the February 1, 2005, the Assembly amended map C9, sheet 11 of the June 2003 Matanuska-Susitna Borough Rail Corridor Study, to move Corridor 3 one-quarter mile to the east.

Section 3. Amendment of section. MSB 15.24.030 is hereby amended to read as follows:

**(G) The Matanuska-Susitna Borough Rail Corridor Study, June 2003 has been adopted by the commission and assembly (adopted by the assembly as amended) as part of the overall Matanuska-Susitna Borough Comprehensive Plan.**

Section 3. Effective date. This ordinance shall take effect upon adoption by the Matanuska-Susitna Borough Assembly.

ADOPTED by the Matanuska-Susitna Borough Assembly this 1 day of February, 2005.

**/S/**

\_\_\_\_\_  
TIMOTHY L. ANDERSON, Borough Mayor

ATTEST:

**/S/**

\_\_\_\_\_  
MICHELLE M. MCGEHEE, CMC, Borough Clerk

(SEAL)

PASSED UNANIMOUSLY: Woods, Allen, Colberg, Kvalheim, Simpson,  
Colver, and Vehrs

BOROUGH ASSEMBLY DOCUMENT  
CONTROL & AM/IM FORM

APPROVED AS AMENDED

\* (See amendment  
of the Borough  
7/15/92)

MATANUSKA-SUSITNA BOROUGH  
350 EAST DAHLIA AVENUE  
PALMER, ALASKA 99573-1000  
NO. AM 92-109

FOR AGENDA OF: April 21, 1992

SUBJECT: Port MacKenzie Transportation Corridor Study

ATTACHMENTS: Final Draft Transportation Corridor Study (partial copy)  
Planning Commission Resolution Serial No. 92-14 and Map

ROUTE TO:	DEPT/COMMITTEE/INDIVIDUAL	INITIALS	REMARKS
1	(Please review & return to originator) George Scanor	<i>GS</i>	
2	Public Works Director	<i>Bl...</i>	
3	Finance Director	<i>FD</i>	
	Attorney		
4	Manager	<i>MS</i>	
	Mayor	<i>GM</i>	
2A	Planning Director	<i>PD</i>	

SUMMARY STATEMENT:

The Matanuska-Susitna Borough contracted with Peratrovich, Nottingham & Drage to conduct a Transportation Corridor Study from Port MacKenzie to the existing Parks Highway and the Alaska Railroad. Drafts of the Study have been completed, and public meetings have been held at Houston, Big Lake, Cottonwood, Willow, and in Anchorage. Additionally, the routes they have recommended have been presented to the Matanuska-Susitna Borough Planning Commission for their review and recommendation. These comments in the form of Planning Commission Resolution Serial No. 92-14 are included.

The consultants received the previous corridor studies as well as considered new corridors based upon topography, etc. Based on their analysis of previous work and new corridors, the consultants recommend that corridor #5 be selected as a railroad corridor and that corridors #3, #8, and #10 be selected as long, mid, and short term road corridors.

The Borough Planning Commission received the consultants report and modified the railroad corridor to include corridor #4. The road corridors were accepted by the Commission as recommended. The Department of Public Works concurs with the Planning Commission's recommendation.

(Continued)

RECOMMENDED ACTION:

Approve corridors #4 and #5 as rail corridors and corridors #3, #8, and #10 as long term, mid term, and short term respectively, road corridors connecting the Alaska Railroad mainline and the Parks Highway with the Point MacKenzie Port.

Approved:

*Donald L. Moore*  
Donald L. Moore  
Borough Manager

\*Approved corridor #4a as the rail corridor and #3 as long term, #8 as mid term and #10 as short term road corridors connecting the Alaska Railroad mainline and the  
Page 1 of 2 Parks Highway with the Point MacKenzie Port.  
No. AM 92-109  
PWNTRANS 6

AM 92-109  
Port MacKenzie Transportation Corridor Study  
Continued

It is necessary to identify corridors now for several reasons. The next phase of port selection required detailed topographic, soils, land with ownership analysis; it is too expensive to obtain this information for more than the number of corridors presently recommended. Secondly, land users and owners need to know which corridors will be recommended so that they may plan their actions accordingly. For example, in order for the Planning Department to move forward with land disposal in the area, it must know where the corridors are so that it does not inadvertently dispose of the wrong property. In addition, in order to obtain State and Federal funding, corridors with detailed information must be available. Finally, should the port feasibility study be completed with a favorable recommendation it will be necessary to move forward with the transportation links as quickly as possible.

The corridors shown are strictly conceptual at this time. An exact route could shift as much as several hundred feet from where currently shown due to engineering design necessities or other currently unknown influences. Final engineering criteria will not be known until the actual design is underway.

The next required step in the design process will be aerial photogrammetry that will ultimately be used to establish contours for the final design process.

BOROUGH ASSEMBLY DOCUMENT  
CONTROL & AM/IM FORM

*Presented*  
*4/21/92*  
*K. Kohl*  
Matanuska-Susitna Borough  
350 East Dahlia Avenue  
Palmer, Alaska 99645-6488

For Agenda of: April 21, 1992

No. IM 92-014

SUBJECT: Port MacKenzie Railroad Route Photography

ATTACHMENT(S): Fiscal Note: Yes \_\_\_ No X

Route to:	Dept/Committee/Individual	Initials	Remarks
1	: Originator G. Strother Ext.810	<i>MS</i>	
2	: Public Works Director	<i>P-20</i>	
	: Clerk		
	: Assistant to the Manager		
	: Attorney		
	: Finance Director		
	: Planning Director		
	: Assessor		
cc	: Mayor	<i>MS</i>	

SUMMARY STATEMENT: The Port MacKenzie Access Corridor Study and the Planning Commission recommend combinations of Routes 4 & 5 as the rail route to the Port. To help speed any design, staff recommended to the Port Commission that controlled aerial photography be taken during the first two weeks of May (after snow melt, prior to leaves on the trees). The photography control panels survey and the design topographic maps can be done later. The cost for this first work is estimated at over \$20,000. The surveyor's contract for setting the panels is estimated at over \$15,000 and proposals are due Wednesday, April 22, 1992 at 2:00 pm.

RECOMMENDED ACTION: Due to the critical window of opportunity for aerial photography, and not to delay any port railroad design until Fall, 1992 or Spring, 1993 photography, the Manager intends to award these contracts so as not to delay any design work.

APPROVED BY:

*D. Leonard Moore*  
Donald L. Moore, Borough Manager

Page 1 of 1

Number: IM 92-014

form 100

# POINT MACKENZIE TRANSPORTATION CORRIDOR STUDY



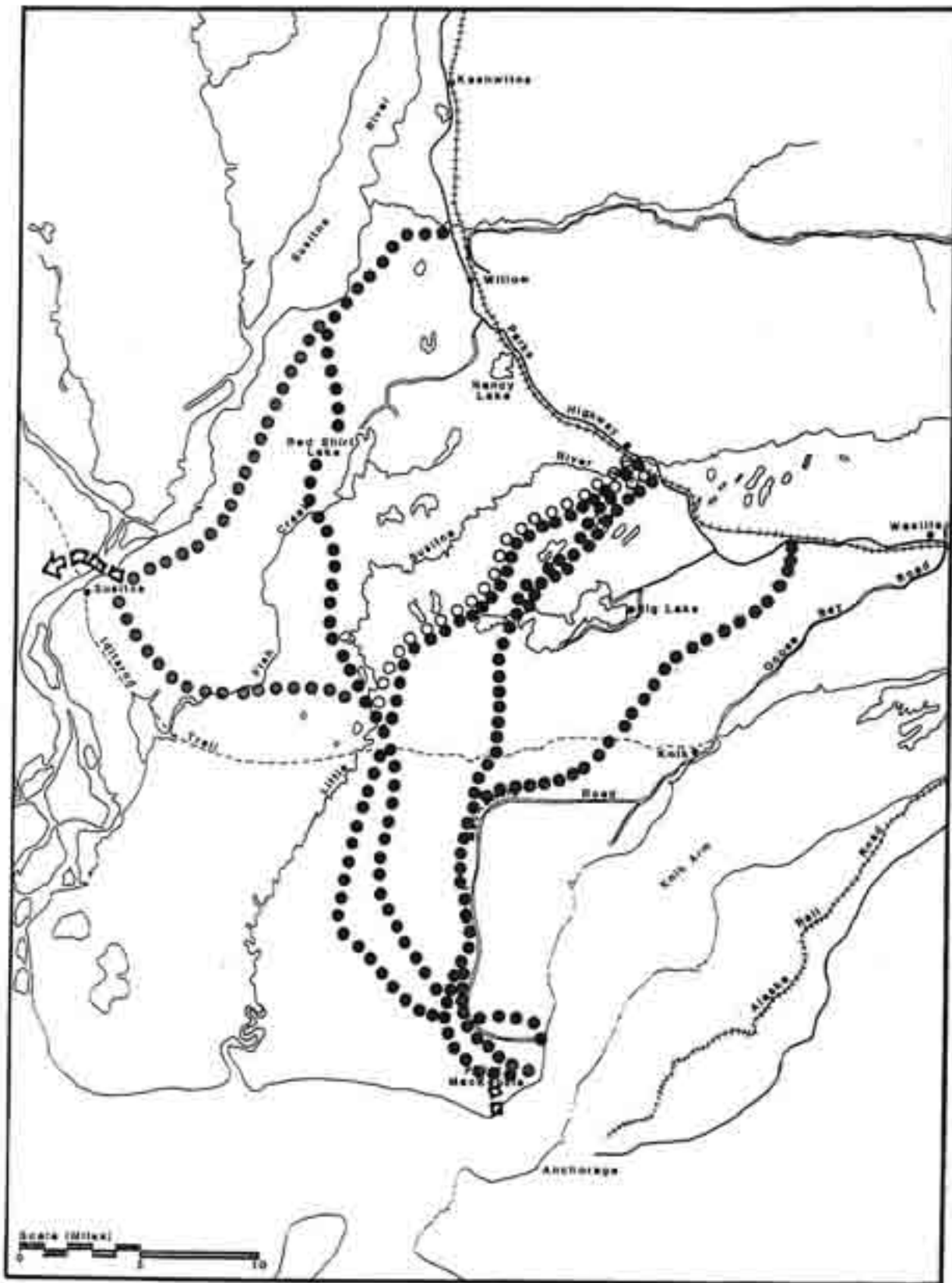
Prepared for:

*Matanuska-Susitna Borough*  
350 East Dahlia  
Palmer, Alaska 99645

Prepared by:

*Peratovich, Nottingham & Drage, Inc.*  
1506 West 36th Avenue  
Anchorage, Alaska 99503

March 2, 1992

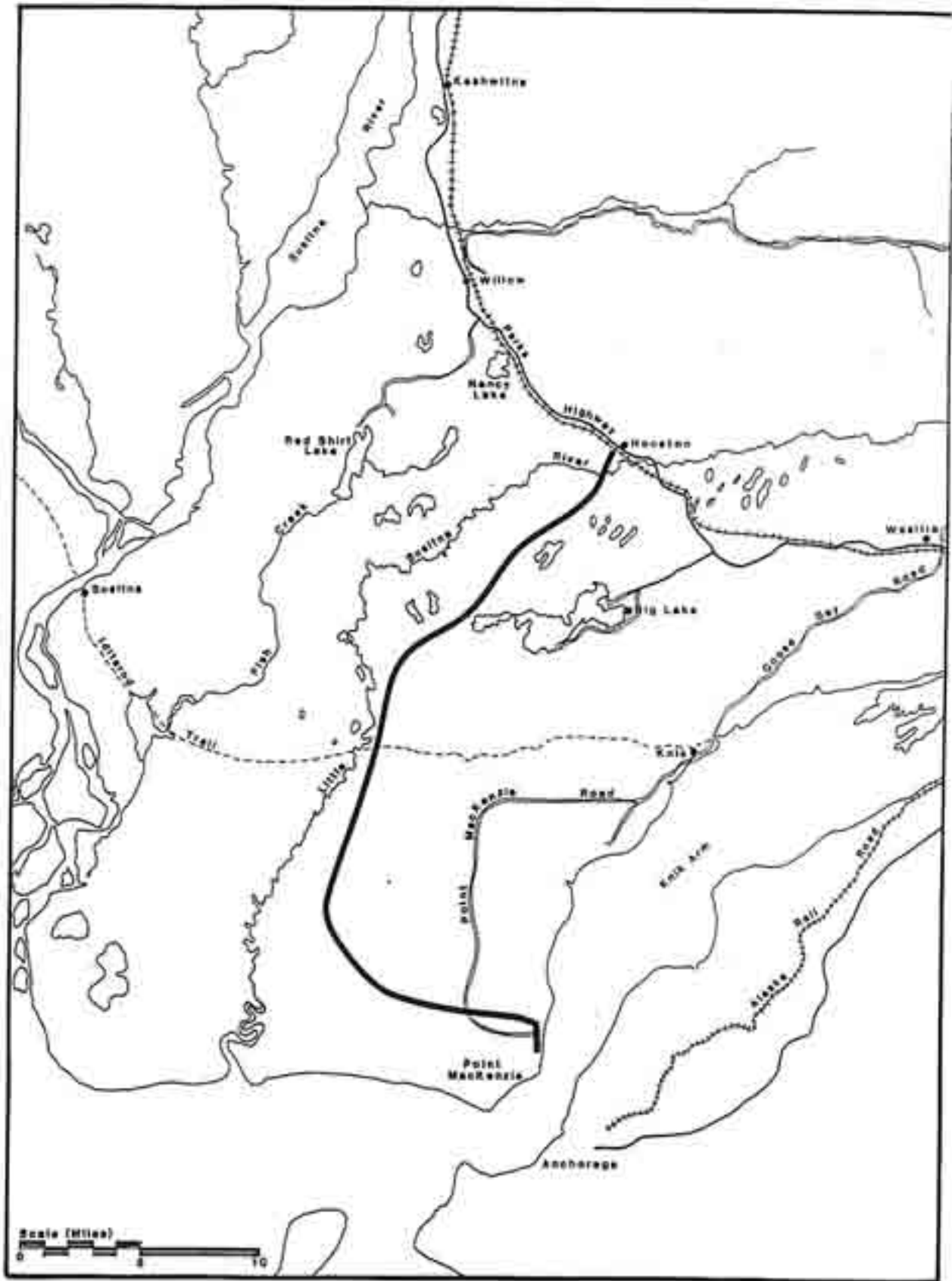


**RAILROAD ROUTES:**

- CNRMSE "HOUSTON R.O.W." (72); ALSO HOUSTON COMPREHENSIVE TRANSPORTATION PLAN (21)
- BORNHOFF "1" (82)
- BORNHOFF "2" (82)
- BORNHOFF "2A" (82); ALSO "CHUTHA" PORTION MSH COMPREHENSIVE TRANSPORTATION PLAN (34)
- BORNHOFF "3B" (82)
- MSH "POINT MACKENZIE STUDY" (79)
- YSI "HOUSTON CORRIDOR" (80)

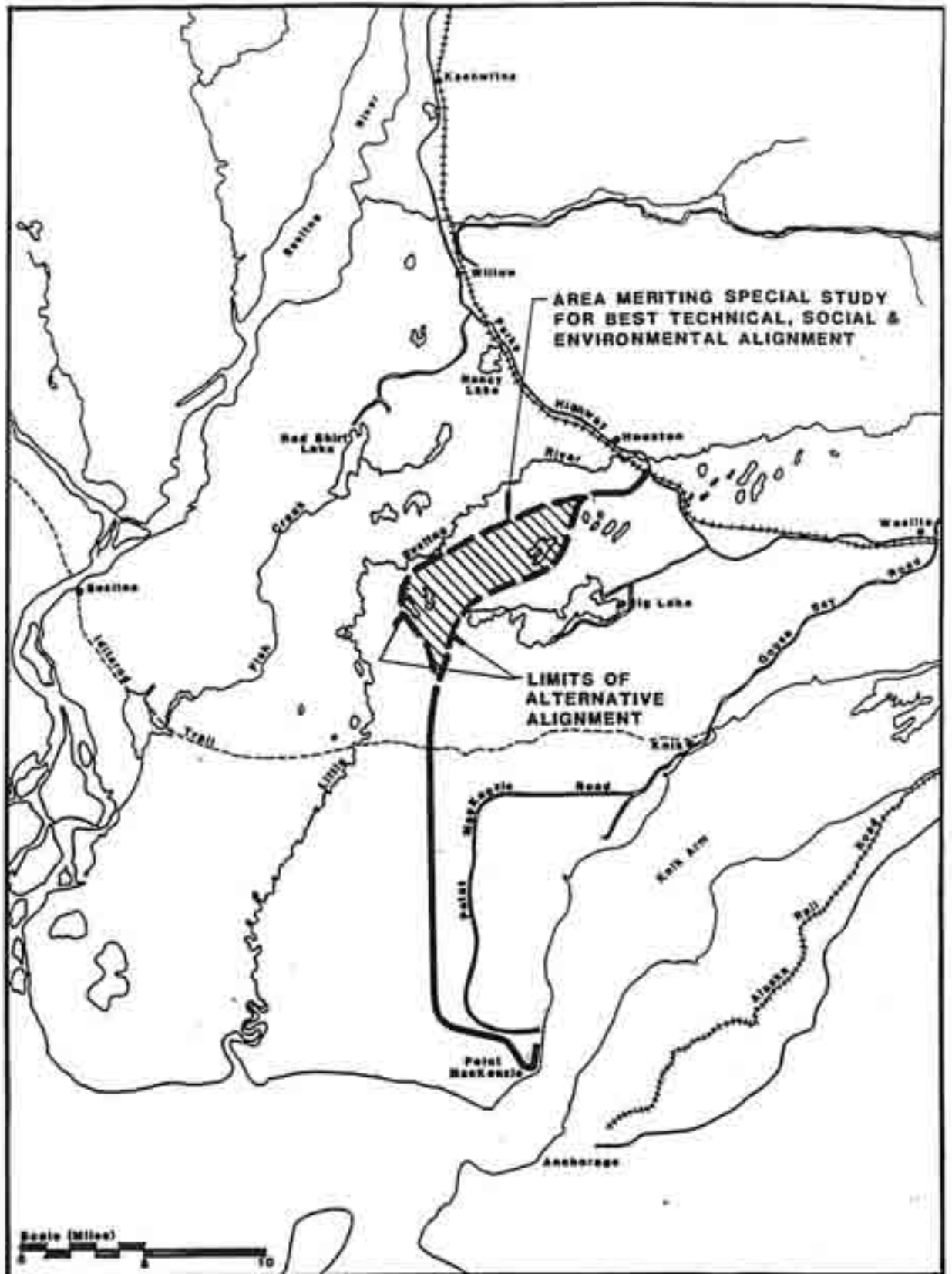
**PREVIOUS RAILROAD  
ROUTE STUDIES**

**FIGURE 13**



**1992 TRANSPORTATION CORRIDOR 4  
(36.0 MILES)**





**1992 TRANSPORTATION CORRIDOR 5  
(31.5 MILES ±)**

**FIGURE B-5**

*(This page intentionally left blank.)*

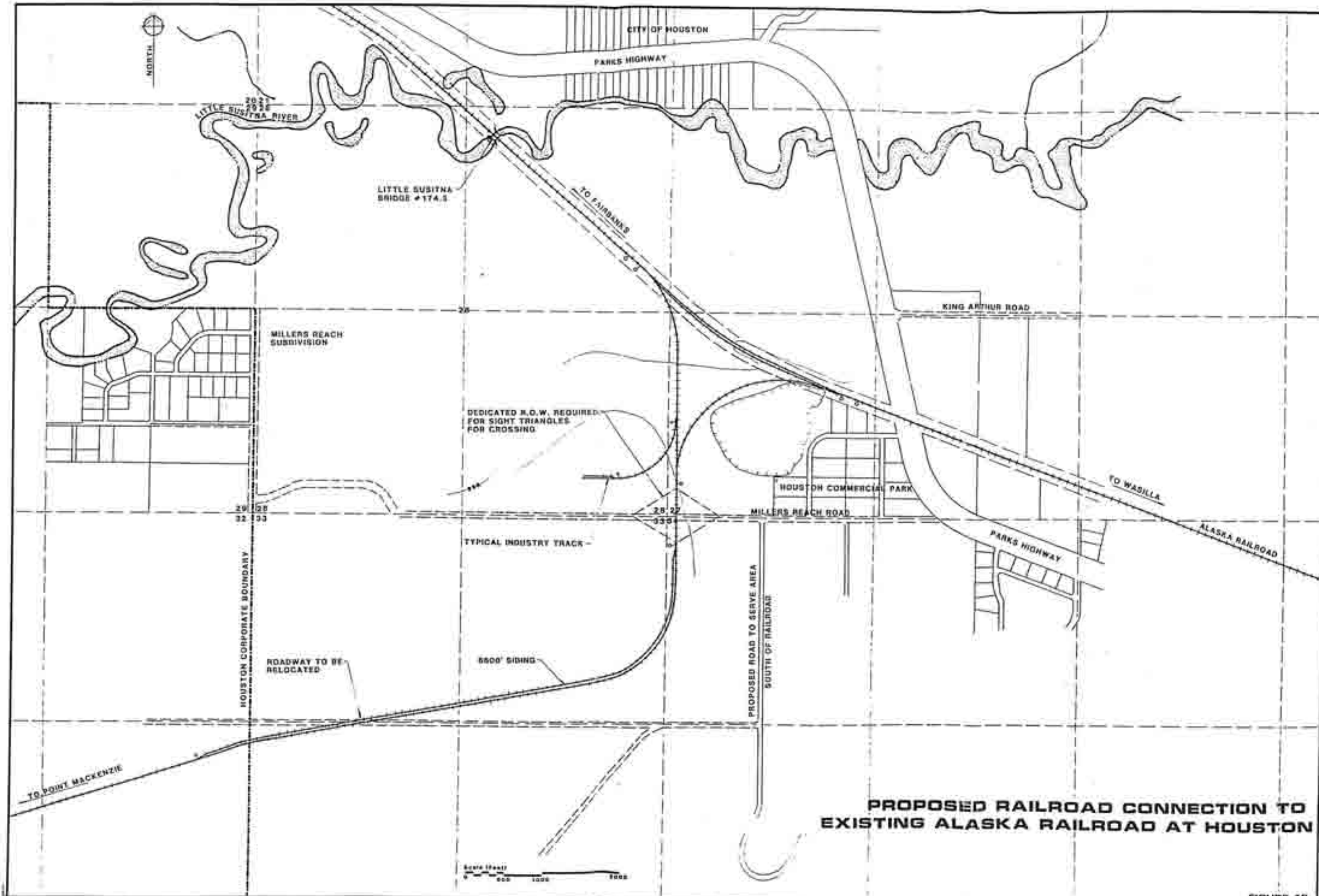


FIGURE 19

## Appendix B: NWI Wetland Categories in the Project Area

The table below lists all of the wetland types occurring within and beyond the Port MacKenzie Rail Extension project area. The project team classified uplands and wetlands within the project area into four categories based on their estimated importance within the local ecosystem for use in the constraints analysis (see Preliminary Environmental and Alternatives Report Section 2.2, Wetlands). These categories were based on the wetland type depicted on the National Wetland Inventory (NWI) maps (that were available digitally) and the general wetland functions these wetland types typically perform.

Category	Wetland Type and NWI Code	Suitability for Construction	
Category 1	<u>Uplands</u> U	High	
Category 2	<u>Forested Wetlands</u>		
		PFO1Cb	
	PF01A	PFO4/1A	
	PF04B	PFO4/1B	
	PFO1/4A	PFO4/SS1A	
	PFO1/4B	PFO4/SS1B	
	PFO1/SS1A	PFO4/SS1C	
	PFO1/SS1B	PFO4/SS4B	
	PFO1/SS1C	PFO4A	Moderately High
	PFO1/SS4B	PFO4B	
	PFO1A	PFO4C	
	PFO1B	PFO5C	
	PFO1C	PFO4/SS1F	
		PFO5F	
		PFO5Fb	
		<u>Scrub/Shrub Wetlands</u>	
		PSS1/4A	
		PSS1/4B	PSS1Fb
		PSS1/4C	PSS1S
		PSS1/FO1A	PSS4F
	PSS1/FO1B	PSS5Sb	
	PSS1/FO4B	PSS4/1A	
	PSS1/FOA	PSS4/1B	
	PSS1A	PSS4/1C	
	PSS1B	PSS4B	
	PSS1C	PSS5B	
	PSS1C	PSS5Cb	
	PSS1E		
	PSS1F		

<u>Excavated, Diked, Partially Drained, or Ditched Vegetated Wetlands</u>	PAB4Hx
PEM1/SS1Ad	PSS1/4Bd
PEM1Bd	PSS1/4Cd
PEM1/SS1Ah	PSS1/EM1Cx
PEM1/SS1Fh	PSS1/USAh
PEM1/SS1Ch	PSS1/USCx
PEM1/SS1Cx	PSS1Ch
PEM1/UBFh	PSS1Cx
PEM1/UBFx	PSS1Fx
PEM1/UBHh	PUB/AB3Hh
PEM1/UBHx	PUB/EM1Fx
PEM1/USCx	PUBFh
PEM1Ax	PUBFx
PEM1Cd	PUBHh
PEM1Ch	PUBHx
PEM1Cx	PUSCx
PEM1Fh	
PEM1Fx	
PAB3Hx	

---

Emergent Wetlands

	PEM1/FO4B	PEM5B	
	PEM1/ML1C	PEM5C	
	PEM1/PSS1C	PEMFb	
	PEM1/PSSC	PFO1/EM1A	
	PEM1/SS14B	PFO1/EM1C	
	PEM1/SS1A	PFO4/EM1B	
	PEM1/SS1B	PFO4/EM1C	
	PEM1/SS1C	PFO5/EM1C	
	PEM1/SS1Cb	PSS1/EM1A	
	PEM1/SS4A	PSS1/EM1B	
	PEM1/SS4B	PSS1/EM1C	
Category 3	PEM1/SS4C	PSS1/EM1Cb	Moderately Low
	PEM1/SS5C	PSS1/EM5B	
	PEM1/USA	PSS1/EM5C	
	PEM1A	PSS4/EM1B	
	PEM1B	PSS4/EM1C	
	PEM1C	PSS1/EM1E	
	PEM1Cb	PSS4/EM1F	
	PEM1E	PEM1/SS1F	
	PEM1F	PEM1/SS1Fb	
	PEM1Fb	PEM1/SS5F	
	PEM1H	PSS1/EM1F	
		PSS1/EM1Fb	

---

Category 4	<u>Ponds</u>	PUBHb	Low
	PUBKx	PSS1/UBF	
	PUB/EM1F	PSS1/UBH	
	PUB/EM1H	PSS5/UBHb	
	PUB/EM1Hb	PAB4/UBH	
	PUB1H	PEM1/UBF	
	PUBF	PEM1/UBH	
	PUBFb	PEM5/UBH	
	PUBH		
	<u>Vegetated Ponds</u>	PEM1/AB3H	
	PAB3/EM1H	PEM1/AB4H	
	PAB3Fb	PEM1Hb	
	PAB3H	PFO5Hh	
	PAB4H		
PAB4Hb			
PAB5H			
PEM1/AB3F			
<u>Estuarine Marshes</u>			
E2EM1/SS1P	E2EM1P		
E2EM1/USM	E2FO5/EM1P		
E2EM1/USN	E2SBN		
E2EM1/USP	E2SS1P		
E2EM1F	E2SS5/EM1P		
E2EM1N			
<u>Estuarine Mudflats</u>			
E1AB1L	E2USP		
E1UBL	E2US/EM1N		
E2AB1N	E2US/EM1P		
E2USN			
<u>Lake or Reservoir Basins</u>	L2AB4/UBH		
L1AB3H	L2AB4H		
L1UBH	L2UBH		
L1UBHh	L2US/UBH		
L2AB3/EM1H	L2US/Ubh		
L2AB3/UBH			
L2AB3H			
L2AB3Hh			
<u>Coastal Swamps and Marshes</u>	PSS1/5R		
PFO4/EM1F	PSS1/EM1R		
	PSS1R		

PFO1/SS1R	PSS5/1R
PFO1R PFO1S	PUS/EM1R
PEM1/SS1R	PUBV
PEM1/UBV	PUBZ
PEM1N	PEM1V
PEM1R	
PEM1S	
PEM1T	
<u>Wetlands Adjacent to a Creek/Stream/Pond</u>	PUS/SS1A
	PUS/SS1C
PSS1/USR	PUS5
PSS1/USC	PUSA
PSS1/USA	PUSC
PUS/EM1A	
PUS/EM1C	
<u>Excavated Ponds in Gravel Pits</u>	
L1UBHx	
<u>Rivers or Stream Channels</u>	R3FLC
R1UBV	R2UBHx
R1US/UB	R3SBC
R1USR	R3UB/US
R2UBH	R3UBH
R2USC	R3US/UB
R3FLA	R3USA
	R3USC

---

## APPENDIX C: PRELIMINARY LETTERS RECEIVED FROM REGULATORY AGENCIES

### C.1 Agencies Involved

A wide variety of regulatory agencies will have responsibility for environmental review, permitting, and approval of the Port MacKenzie Rail Extension Project. The primary purpose for agency involvement during this initial stage was to provide information about the purpose and need of the project and to work with agencies early in the process to determine areas of concern or potential conflicts with the proposed corridors. Meeting with the agencies also provided notice that the project partners will be submitting an application for a rail extension project to the STB. Between September and November 2007, MSB and the ARRC hosted one large group meeting where all agencies were invited to attend, as well as ten smaller, one-on-one agency meetings.

Agencies contacted included:

- Alaska Department of Natural Resources
  - State Historic Preservation Office
  - Office of Project Management & Permitting
  - Office of Habitat Management & Permitting
  - Division of Mining Land & Water
  - Division of Parks & Outdoor Recreation
- Alaska Department of Fish and Game
- National Oceanic & Atmospheric Administration, National Marine Fisheries Service
- US Bureau of Land Management
- US Environmental Protection Agency
- US Army Corps of Engineers
- US Fish and Wildlife Service

### C.2 Agency Involvement Activity

#### C.2.1 Large Meeting

One large group agency meeting was held to introduce the project and to discuss concerns relating to the agencies.

**September 18, 2007, Large Project Meeting:** The purpose of this meeting was to provide agencies with an introduction to the proposed project and to introduce the Port MacKenzie Rail Extension Project Team. This meeting was coordinated with the assistance of the Alaska Department of Natural Resources, Office of Project Management & Permitting, Large Project Permitting group. The project team provided an overview of the project history, area, purpose, and potential benefits. Topics discussed included agency participation and comments, the STB process and the environmental constraints analysis used to develop corridors, refine alignments and the alignments under consideration.



### C.2.2 One-on-one and Small Group Meetings

The project team held ten one-on-one and small group meetings with specific agencies in October and November 2007 (Table 2.1). These meetings were held to obtain comments on items of particular interest to individual agencies or discuss resources in common among several agencies.

**Table C-1 One-on-One Agency Meetings**

<b>Agency</b>	<b>Date (2007)</b>	<b>Purpose</b>
US Army Corps of Engineers	October 2	Discuss potential wetlands impacts and mitigation strategies
Alaska Department of Fish and Game	October 3	Discuss potential impacts to parks and refuges, fish, fish habitat, and mitigation strategies
Alaska Department of Natural Resources, Division of Parks and Recreation	October 5	Discuss potential impacts to state parks and mitigation strategies
Alaska Department of Natural Resources, Division of Mining, Land, and Water	October 10	Begin a preliminary discussion on potential impacts to State lands, ROW acquisition procedures, and identify additional information needed
Alaska Department of Natural Resources, Office of Habitat Management and Permitting	October 10	Begin a preliminary discussion on potential impacts to fish populations and stream crossings, habitat, and mitigation strategies
Bureau of Land Management and Alaska Department of Natural Resources (Trails Meeting)	October 17	Identify and locate existing trails and their legal status. The latter half of the meeting focused specifically on the Iditarod National Historic Trail
Alaska Department of Natural Resources, Division of Parks and Outdoor Recreation, Office of History and Archaeology.	October 23	Follow up on letter from SHPO, dated September 26, 2007. The main comments to discuss included the Iditarod National Historic Trail; the Port MacKenzie Agricultural Area; location of the 1915 Matanuska Farm Station; and potential indirect effects
Alaska Department of Natural Resources, Division of Parks and Outdoor Recreation, OMHP, Division of Mining, Land & Water	November 9	Follow up with DNR agencies related to public use of undesignated state lands, trails, fisheries impacts, and park and recreation area impacts
US Army Corps Of Engineers	November 21	

### C.2.3. Agency Comments

Ten comment letters were received from agencies (list). In addition, an R.S. 2477 Rights-of-Way Fact Sheet (September 2001) was included with the Alaska Department of Natural Resources Division of Mining, Land & Water letter. The letters are located in Appendix A: Agency Comments. Following is a brief summary of the letters.

#### **September 26, 2007 Alaska Department of Natural Resources/Division of Parks and Outdoor Recreation/ Office of History and Archaeology**

The Office of History and Archaeology provided recommendation for additional archaeological surveys on selected alternatives and expressed concern regarding alternatives intersecting with the Iditarod National Historic Trail. There was also a recommendation to evaluate possible National

Register eligibility of the Port MacKenzie Agricultural Area, and to consider both potential direct and indirect effects to historic properties.

**October 17, 2007 US Bureau of Land Management Anchorage Field Office Iditarod National Historic Trail**

The BLM expressed the desire to work with other agencies in the development of a Memorandum of Agreement to deal with the Iditarod National Historic Trail. The Memorandum of Agreement that was developed for the Takotna, Alaska Airport project was cited as an example.

**October 19, 2007 US Department of Interior Fish and Wildlife Service**

Comments from the USFWS included concerns related to habitat fragmentation, cumulative impacts, and compensatory mitigation. Comments also referenced the wintering, denning, breeding, feeding, and migration corridors for both fish and wildlife. Other comments included recommendations to fully assess the potential for fuel and hazardous material spills and avoidance measures as part of the alternative design. The USFWS also identified the bald eagles as a species of concern. It was recommended that each alternative corridor be surveyed to identify nests and to work with the agency to develop buffer zones near active and inactive nests.

**October 23, 2007 US Environmental Protection Agency**

Comments from the USEPA focused on two recommendations: 1) establish a detailed project baseline and 2) design a project that avoids impact to the maximum extent possible. The baseline information should be sufficient in scope and analysis to be included into the NEPA document and be used to support the Clean Water Act section 404 permitting review. According to USEPA, there is a need for ground-truthing, detailed mapping, and preliminary analysis of the environment. Cumulative impacts should also be included.

**October 31, 2007 Department of Fish and Game, Division of Sport Fish**

Comments provided included recommendations for minimizing the impacts to coastal resources including wetlands, rivers, streams, lakes, and State Game Refuges, and suggestions for mitigation. For example, the Division suggested using railway bridges rather than culverts, especially for streams containing anadromous fishes. Other mitigation measures included using native plant species to revegetate areas disturbed with construction. This would protect habitat from invasive species. In addition, the Division also suggested the ARRC participate in regional planning efforts pertaining to green infrastructure and develop contingency plans to address potential spills for the selected corridor.

**November 14, 2007 Department of Natural Resources, Division of Parks and Outdoor Recreation**

The Division of Parks and Outdoor Recreation provided a letter outlining their preferred alternative and discussed their least favorable option. The division recommended the Houston South-Houston-Connector 3 – Mac East alternative be chosen as the preferred alternative, because from their perspective it minimizes impacts to the major recreational/public use areas, eliminates additional bridges over the Little Susitna River and Willow Creek, and would align the rail adjacent to the existing road corridors.

The Willow Corridor, from the standpoint of outdoor recreation is identified as the least favorable option to the Division for the following reasons:

- Willow Creek State Recreation Area would be significantly impacted
- Nancy Lake State Recreation Area would be affected through negative impacts on neighboring recreational lands
- Little Susitna State Recreational River would be significantly impacted

- Little Susitna Public Use Facility would be significantly impacted
- Regional Trail impacts
- State Trail Grooming Pool program
- Historical/Cultural impacts
- Barrier issue
- Contiguous Public Land Block
- Habitat protection

**November 21, 2007 Department of the Army, US Army Engineer District, Alaska, Regulatory Division**

The letter from the Regulatory Division outlined guidance relating to information and documentation that may be required to satisfy the regulatory requirements of the agency. Three areas were highlighted in the letter:

1. Consultation with the National Marine Fisheries Service to discuss and gather comments on impacts and recommendations regarding Essential Fish Habitat.
2. Consultation with the State Historic Preservation Officer to determine eligibility and/or determination of effect on historic properties.
3. NEPA requirements to review the project under the Environmental Protection Agency's 404 (b) (1) Guidelines. The guidelines require the applicant to show that all the appropriate and practicable steps to minimize potential impacts of the discharge on the aquatic ecosystem have been considered, and that the proposed alternatives represents the least environmentally damaging alternative.

**November 26, 2007 Department of Natural Resources, Office of Habitat Management and Permitting OHMP**

DNR-OHMP provided the following preliminary scoping comment regarding information needs, routing and design considerations and route preferences. In terms of additional information needed, OHMP recommended comprehensive stream sampling to determine the presence or absence of fish, as well as hydrology studies to map the wetland areas. The design considerations focused on utilizing bridges to minimize impacts to the fish and fish passages, and designing culverts using stream simulation methodology. Routing considerations recommended avoiding wetlands, fish-bearing streams and anadromous water bodies.

**November 28, 2007 Matanuska-Susitna Borough Cultural Resources Division Planning and Land Use Department**

Comments from the MSB Cultural Resources Division included the preference for the Houston North, Houston South, Conn 1 and Mac West corridors. These corridors appeared to be the least likely to impact historic, proto-historic and prehistoric sites based on preliminary information. The Division also recommended archaeological surveys in consultation and participation with the Knik Tribal Council, and on-the-ground, walk-over surveys to be conducted by an archaeologist.

**December 12, 2007 Department of Natural Resources, Division of Mining, Land, and Water**

Comments from DNR-DMLW were general in nature describing project impacts on state lands and applicable statutes pertaining to acquiring ROW on state lands. The Southcentral Regional Office (SCRO) noted the requirement of using the public process to make the decision and give public notice to convey an interest in state land to ARRC. Any land approved for a railroad corridor will be subject to existing ADL authorizations for roads, trails, utility, or other access easement purposes. The SCRO will also reserve additional ADL authorizations along existing roads, trails, utility, or access routes if improvements are determined as representing a local, regional, or statewide significance. The SCRO

referenced several area plans in effect within the project area that required project consistency: the Susitna Area Plan, Willow Sub-Basin Area Plan, Fish Creek Management Plan, and Susitna Basin Recreational Rivers Management Plan. The letter also mentioned requirements for procuring materials in the construction phase, land use permits needed for man camps and staging areas, and coordination review by the Alaska Coastal Management Program.

# STATE OF ALASKA

SARAH PALIN, GOVERNOR

## DEPARTMENT OF NATURAL RESOURCES

DIVISION OF PARKS AND OUTDOOR RECREATION

OFFICE OF HISTORY AND ARCHAEOLOGY

550 W. 7TH AVENUE, SUITE 1310  
ANCHORAGE, ALASKA 99501-3566  
PHONE: (907) 269-8721  
FAX: (907) 269-8908

September 26, 2007

File No.: 3130-2R ARRC

SUBJECT: Port Mackenzie Rail Extension, Matanuska-Susitna Borough

Brian Lindamood  
Alaska Railroad Corporation  
P. O. Box 107500  
327 Ship Creek Avenue  
Anchorage, AK 99501

Dear Mr. Lindamood,

The Alaska State Historic Preservation Office has reviewed the information on the Port Mackenzie Rail Extension Project that you presented during the agency scoping meeting on September 18, 2007. We have the following comments:

1. As mentioned in your presentation, the project area contains numerous cultural resources. Only a fraction of the project area has been archaeologically surveyed however and it is likely that there are many additional, currently unreported prehistoric and historic sites. Regardless of which alternative is selected, we will likely be recommending additional archaeological survey.
2. All of the alternatives intersect the Iditarod National Historic Trail. The trail was designated by Congress in 1978 for its significance as a historic transportation route. Effects to the trail resulting from the rail extension will need to be addressed.
3. The Matanuska Farm Station was established in 1915 in what is now the Port Mackenzie Agricultural Area. This agricultural landscape will need to be evaluated for eligibility for the National Register of Historic Places.
4. In defining the area of potential effect and identifying historic properties, be sure consider both potential direct and indirect effects to historic properties. Indirect effects may include increased development or changes in setting as a result of the project.

We look forward to continued consultation with you regarding this project. Please contact Stefanie Ludwig at 269-8720 if you have any questions or if we can be of further assistance.

Sincerely,



Judith E. Bittner  
State Historic Preservation Officer

JEB:slf

Cc: Don Perrin, DNR/OPMP



# United States Department of the Interior

FISH AND WILDLIFE SERVICE  
605 West 4<sup>th</sup> Ave. G-61  
Anchorage, Alaska 99501



IN REPLY REFER TO:

OCT 19 2007

AFWFO

Mr. Brian A. Lindamood  
Alaska Railroad Corporation  
P.O. Box 107500  
Anchorage, Alaska 99501-7500

Re: Scoping Comments on the Proposed Port Mackenzie Rail Extension

Dear Mr. Lindamood:

The U.S. Fish and Wildlife Service (Service) is providing early comments on issues and impacts associated with the proposed Port Mackenzie railroad extension project. We attended the interagency meeting on September 18, 2007, at which you described the status of the proposed project, and the process for submittal of an application to the Surface Transportation Board. Agency input was requested by mid-October, so that information can be considered in your evaluation of alternatives.

Our comments and recommendations emphasize three main areas of concern to the Service that need to be considered both in your decision making process and in the Surface Transportation Board's preparation of environmental documents for the project: (1) habitat fragmentation, (2) cumulative impacts, and (3) compensatory mitigation. We are also providing some specific guidelines and legislative references that should be incorporated into the project proposal. Overall, the relative value of fish and wildlife resources in the project area will need to be identified, quantified, and compared for each viable alternative. The proposed railroad expansion's potential impact on those resources and options to avoid, minimize, and compensate for those impacts will need to be analyzed for each alternative.

### **Minimize Habitat Fragmentation and Limit the Project's Overall Footprint**

One of the most significant impacts of the proposed project is the fragmentation of high value, previously undisturbed habitat that will result from development of the railroad extension. Additionally, alternatives that are farther from existing developments and bisect undeveloped areas will result in ancillary development into remote areas. This will cause larger impacts on fish and wildlife. All three alternatives under study will cut

through and affect wintering, denning, breeding, feeding, and migration corridors for a wide variety of fish and wildlife species. Relocation or reduction of local populations of some species will result; wildlife/vehicle collisions will occur.

Choosing the shortest route for the railroad extension, with the smallest footprint of associated infrastructure, will minimize fragmentation of sensitive habitats and decrease the area of habitat adversely affected. The eastern alternative is the shortest route, is closest to existing developments, and would cross the fewest waterways, including anadromous streams, but could still affect several, high value wetland complexes. The western alternative would cross the largest number of anadromous streams, including key tributaries to the Little Susitna and Susitna Rivers, as well as some high value wetland complexes. Additionally, the western alternative is adjacent to the Susitna Flats State Game Refuge, a notable wildlife area.

### **Address Cumulative Impacts**

Cumulative impacts associated with this project are the most significant and difficult issue that must be addressed. This project will result in increased industrial infrastructure and expanded industrial, commercial, and residential development throughout the Matanuska-Susitna Borough. The scope of cumulative impacts assessed will need to include: 1) current and future uses of the transportation corridor (e.g., number of trips per day and passengers and cargo served; number of railroad cars in each trip, length of time it will take for a given train to cross a specific area, etc.) 2) current and future use levels and locations of connector corridors (including associated roads, utilities, and secondary development within the corridors), 3) current and future activities for the community closest to the intersection of the rail spur with the existing rail line, and 4) where commercial and industrial activities can be expected to develop as a result of creation of the rail spur. Additional extractive projects in interior Alaska can also be expected as this project will bring transportation to shipping much closer to such projects. Expanding port facilities in Knik Arm are other cumulative impacts that need to be factored into the analysis. All of these impacts can be addressed through comprehensive land use planning, including consideration of Green Infrastructure (<http://www.greeninfrastructure.net/>), as described below.

### **Needed: Process for Compensatory Mitigation and Watershed Planning**

Project impacts to Service trust resources, including anadromous fish, migratory birds, and wetlands, should be avoided or minimized to the greatest extent possible, primarily through consideration and selection of alternative route corridors and specific design features (e.g., bridges rather than culverts for major stream crossings). An analysis and detailed measures for avoiding/minimizing impacts will need to be included in the project's environmental documents. Where impacts cannot be avoided or minimized, compensatory mitigation options will need to be addressed. The cost of compensatory mitigation associated with rail development in relatively pristine areas compared to rail development in previously disturbed areas will likely be much greater. Compensation for

unavoidable habitat losses could require use of a process similar to the Anchorage Debit/Credit Methodology, or mitigation banking.

We recommend that the environmental documents include discussion of mitigating cumulative impacts through some type of interagency, cooperative land-use or watershed planning effort in the Matanuska-Susitna Borough. Borough planners, federal and state agency representatives, and several special interest groups attended a Green Infrastructure course in the Mat-Su Borough last spring and the Borough is requiring a Green Infrastructure component in all future community comprehensive plans. Green Infrastructure involves planning to identify and maintain an interconnected network of lands and waters that supports native species, maintains natural ecological processes, sustains air and water resources, and contributes to the health and quality of life for communities, people and wildlife, while at the same time providing access and services also essential to a vital economy.

### **Other Resource Issues and Avoidance/Minimization Procedures to Consider**

Project Effects on Freshwater Aquatic Habitats – The project's environmental analysis should identify and quantify potential direct and indirect impacts on all freshwater aquatic habitats, including wetland complexes, rivers, streams, and lakes. Many of the aquatic resources (anadromous and freshwater resident fish) in the proposed corridors have not been thoroughly surveyed and quantified; baseline maps of all streams and anadromous systems will need to be produced and aquatic population data will be needed. Field studies for areas where aquatic resources have not been quantified will be needed.

In addition to identifying all anadromous and resident fish streams, routing to avoid or minimize crossings of streams, river mouths, lakes, ponds, and wetlands will decrease adverse impacts on fish and other aquatic organisms. By properly siting and designing bridges and large arch culverts for unavoidable stream crossings, fish passage problems can generally be avoided. Wetland fills should be minimized where possible. Upland routes should be maximized to avoid unnecessary impacts on water bodies and their fish and wildlife uses. At the same time, analyses of habitat values will show where consideration should be given to those uplands with more valuable wildlife habitats than some wetlands.

Potential for Spills of Fuels and Hazardous Materials – The environmental documents should fully assess the potential for fuel and hazardous material spills along each alternative corridor, and within the connector and port areas. Because of the many important wetland complexes and freshwater and adjacent marine water bodies along each of the proposed routes and at the port, identifying the effects of small and large spills throughout each study area will be necessary. Essential spill avoidance measures will need to be part of the project design for all alternatives. Additionally, possible impacts on fish and wildlife resources, cleanup methodologies, and mitigation measures for spills will need to be fully addressed for each alternative.



Project Effects on Migratory Birds and Upland and Wetland Bird Habitats – Loss of nesting habitats and “take” of migratory birds must both be considered in project planning. Habitat studies will need to address how to minimize loss of high value nesting habitat, particularly for species of concern. Preventing impacts is much less expensive than reacting to them once they occur. Migratory birds, including songbirds, waterfowl, shorebirds, and raptors, are protected under the Migratory Bird Treaty Act (16 U.S.C. 703-712). Federal regulations prohibit unauthorized “take” of migratory birds. “Take” includes by any means or in any manner, any attempt at hunting, pursuing, wounding, killing, possessing or transporting any migratory bird, nest, egg, or part thereof. The MBTA does not distinguish between intentional or unintentional take. Destruction of active bird nests, eggs, or nestlings that can result from spring and summer vegetation clearing, grubbing, and other site preparation/construction activities can violate the MBTA.

Each of the corridors will affect high value upland and wetland habitats used by migrating, nesting, and feeding migratory birds. Regardless of which alternative is proposed, construction activities and clearing associated with the rail spur, ancillary roads, and other infrastructure will need to avoid sensitive nesting areas from May 1 until July 15 to prevent disturbing nesting migratory birds. See the attached guidelines for clearing activities in southcentral Alaska for compliance with the MBTA.

Bald Eagles – Bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668-668c) (BGEPA) and the Migratory Bird Treaty Act of 1918 (MBTA). The Alaska Railroad and their contractors are responsible for ensuring that construction does not disturb bald eagles. During the nesting period (March 1 through August 31), bald eagles are sensitive to noise and obtrusive human activities in the vicinity of nest sites. Prolonged activities can result in disturbance, forcing eagle pairs to abandon nests, eggs, and young, or may cause eaglets to prematurely leave the nest before they are capable of sustained flight. Nest trees must be protected by vegetative buffers and preserved throughout the year, but particularly during the nesting season.

Numerous bald eagles nest along the fish-bearing waters throughout the proposed corridors. We recommend that each alternative corridor be surveyed to determine the presence of nests and eagles, so that these can be avoided. If nests are located, the Service should be notified to discuss buffer zones and other measures to protect nesting eagles. Guidelines are in place regarding recommended buffer zones and allowable activities near active and inactive nests.

We look forward to working with you as planning for this project moves forward. Thank you for the opportunity to provide comments and recommendations. If you have any questions regarding these recommendations, please contact project biologist Phil Brna at 271-2440 or by email at [phil\\_brna@fws.gov](mailto:phil_brna@fws.gov).

Sincerely,



Ann G. Rappoport  
Field Supervisor

Attachment

Attachment and Letter Cc'd to Recipients

Cc: M. Fink, ADF&G  
M. Bethe, ADNR  
D. Perrin, ADNR  
D. Limpinsel, NMFS  
M. Lacroix, EPA  
S. Joy, CE  
J. Duffy, MSB



## **ADVISORY: Recommended Time Periods for Avoiding Vegetation Clearing in Alaska in order to Protect Migratory Birds**

### **General Information:**

Under the Migratory Bird Treaty Act (MBTA) (16 U.S.C. 703), it is illegal for anyone to "take" migratory birds, their eggs, feathers or nests. "Take" includes by any means or in any manner, any attempt at hunting, pursuing, wounding, killing, possessing or transporting any migratory bird, nest, egg, or part thereof. Take and possession under MBTA can be authorized through regulations, such as hunting regulations, or permits, e.g., salvage, research, depredation, or falconry. The MBTA does not distinguish between intentional and unintentional take. In Alaska, all native birds except grouse and ptarmigan (protected by the State of Alaska) are protected under the MBTA.

Destruction of active bird nests, eggs, or nestlings that can result from spring and summer vegetation clearing, grubbing, and other site preparation and construction activities would violate the MBTA. The following timing guidelines are not regulations, but are intended as recommendations to help you comply with the MBTA. Some species and their nests have additional protections under other federal laws, including those listed under the Threatened and Endangered Species Act (ESA), and bald and golden eagles (protected under the Bald and Golden Eagle Protection Act or BGEPA). Please contact the U.S. Fish and Wildlife Service to ensure compliance with ESA and BGEPA if these species may be present in your project area.

**These Timing Guidelines are current for 2007.**

### **Directions:**

1. Apply timing window guidelines to your project planning, unless project-specific review results in unique guidelines from the USFWS for your project.
2. If you encounter an active nest *at any time*, including before or after the local timing window, leave it in place and protected until young hatch and depart. "Active" is indicated by intact eggs, live chicks, or presence of adult on nest. Timing guidelines should considerably reduce the risk of inadvertent nest destruction, but final compliance with the law is your responsibility: do not destroy eggs, chicks, or adults of wild bird species.
3. If you have any questions regarding the MBTA and the timing guidelines, including projects that may occur in "boundary areas" between regions described on the matrix, contact your local Fish and Wildlife Field Office for assistance:

Anchorage (907) 271-2888  
Fairbanks (907) 456-0203

Kenai (907) 262-9863  
Juneau (907) 780-1160



**Recommended Time Periods for Avoiding Vegetation Clearing**

HABITAT TYPE →	Forest or woodland <sup>1</sup> <i>(i.e., trees present)</i>	Shrub or Open <i>(i.e., shrub cover or marsh, pond, tundra, gravel, or other treeless/shrubless ground habitat)</i>	Seabird colonies <i>(including cliff and burrow colonies)</i>	Raptor and raven cliffs
REGION ↓				
Southeast	April 15 – July 15	May 1 – July 15 <sup>2</sup>	May 1 – September 15 <sup>3</sup>	April 10 – August 10
Kodiak Archipelago			April 15 – September 7 <sup>3</sup>	
Southcentral <i>(Lake Illiamna to Copper River Delta; north to Talkeetna)</i>	May 1 – July 15 <sup>2</sup>			
Bristol Bay/AK Peninsula <i>(north to Lake Illiamna)</i>	April 10 – July 15	May 1 – July 15 <sup>2,4</sup>	May 10 – September 15	
Interior <i>(north of Talkeetna to south slope Brooks Range; west to treeline)</i>	May 1 – July 15 <sup>2</sup>		May 1 – July 20 <sup>3</sup>	April 15 – August 1
Aleutian Islands		April 25 – July 15	May 1 – September 15 <sup>3</sup>	April 1 – August 1
Yukon-Kuskokwim Delta <i>(east to treeline)</i>		May 5 – July 25 <sup>2,4</sup>	May 20 – September 15	April 15 – August 15
Seward Peninsula		May 20 – July 20 <sup>4</sup>		
Northern <i>(includes northern foothills of Brooks Range)</i>		June 1 – July 31 <sup>2</sup>		
Pribilof and Bering Sea Islands		June 1 – July 15	May 25 – September 1	

USFWS May 2007

<sup>1</sup> Owl species may begin to nest two or more months earlier than other forest birds, and are fairly common breeders in forested areas of Alaska. You may wish to survey for nesting owls (or other early spring tree-cavity nesters) prior to tree-cutting. It is your responsibility to protect active nests from destruction.

<sup>2</sup> Canada geese and swan habitat: begin April 20

<sup>3</sup> Storm petrel burrow habitat: April 1 – October 15

<sup>4</sup> Black scoter habitat: through August 10

<sup>5</sup> Seabird colonies in Interior refer to terns and gulls

## Alcantra, Rosetta M.

---

**From:** Robertson, Donna  
**Sent:** Thursday, October 18, 2007 8:45 AM  
**To:** Robbins, Leslie; Schick, Jon  
**Subject:** FW: Developing a Mitigation Agreement on Iditarod Nat.Historic Trail Xing for Pt. MacK Spur

**Attachments:** Takotna MOA 6-5-06-1-3b.doc



Takotna MOA  
6-5-06-1-3b.doc

fyi

Donna Robertson  
907.644.2127

-----Original Message-----

From: Brian Lindamood [mailto:LindamoodB@akrr.com]  
Sent: Thursday, October 18, 2007 7:53 AM  
To: Sasha Forland; Barbara Hotchkin; Robertson, Donna; Anderson, Kirsten J.; Brad.Sworts@matsugov.us; fseagerboss@matsugov.us  
Cc: Clark Hopp; Eileen Reilly; KKusskeFloyd@mayerbrown.com; engrperk@msn.com  
Subject: FW: Developing a Mitigation Agreement on Iditarod Nat.Historic Trail Xing for Pt. MacK Spur

All-

Mr. Keeler forwarded me this MOA with SHPO regarding the relocation of the Iditarod trail for an airport from 2006. I would expect this to be similar to what we will be expected for our project. Please review and make comments soon as I would like to have this at least in draft form by our meeting with SHPO next week.

Thanks,

Brian

-----Original Message-----

From: <Kevin\_Keeler@ak.blm.gov> [mailto:Kevin\_Keeler@ak.blm.gov]  
Sent: Wednesday, October 17, 2007 5:25 PM  
To: <lindamoodb@akrr.com>  
Cc: <donna.robertson@hdrinc.com>, <Fseagerboss@matsugov.us>, <judy.bittner@alaska.gov>, <benjamin.hagedorn@alaska.gov>, <Bruce.Paulsen@matsugov.us>  
Subject: Developing a Mitigation Agreement on Iditarod Nat. Historic Trail Xing for Pt. MacK Spur

Brian:

I've had some more thoughts on BLM's involvement in review of the Pt. MacK project.

I've attached a MOA between the major players for a DOTPF project to build a new Takotna Airport (DOTPF, BLM, SHPO, HDR I believe, and FAA). I understand the SHPO has used similar MOA's to document mitigation agreements and consensus between involved parties (and may routinely require such agreements). In the case of the Takotna Airport, the MOA was completed in advance of NEPA work, and then was rolled into and referenced in the NEPA document.

Therefore, I would like to see, and recommend the use of a similar process, with the outcome of an Mitigation Agreement between all affected agencies, including BLM, for the AK RR Pt. MacKenzie project. Such an agreement

would help ensure that the trail resources in question are adequately protected, and enhance the understanding of all involved parties. kk

(See attached file: Takotna MOA 6-5-06-1-3b.doc)

Kevin Keeler  
Iditarod National Historic Trail Administrator  
BLM Anchorage Field Office  
6881 Abbott Loop Rd.  
Anchorage, AK 99507

Phone: (907) 267-1207  
Fax: (907) 267-1267  
Email: kevin\_keeler@ak.blm.gov

<http://www.blm.gov/ak/iditarod>

---

"This is a dream I've been dreaming since I was a little boy."

Lance Mackey, March 13, 2007

On becoming the first person ever to win the 1,100 mile Iditarod Trail Sled Dog Race and the 1,000 mile Yukon Quest Sled Dog Race in the same year.

---



UNITED STATES ENVIRONMENTAL PROTECTION AGENCY  
REGION 10  
1200 Sixth Avenue, Suite 900  
Seattle, Washington 98101-3140

23 OCT 2007

Reply To: ETPA-083

Mr. Brian Lindamood  
Alaska Railroad Corporation  
P.O. Box 107500  
Anchorage, AK 99510-7500

RE: Port MacKenzie Rail Extension Pre-Application Comments

Dear Mr. Lindamood,

The U.S. Environmental Protection Agency (EPA) has reviewed the materials distributed by the Alaska Railroad Corporation (ARRC) and the Matanuska-Susitna Borough (MSB) regarding your joint effort to plan, engineer, design, and construct a new rail line to connect Port MacKenzie to the existing rail system. The new line would be thirty to forty-five miles in length, and would tie into the existing rail system somewhere between the communities of Meadow Lakes and Willow.

We understand that the ARRC and MSB are currently preparing an application to the federal Surface Transportation Board (STB) for a license to construct and operate the new rail line. The STB, in turn, will initiate and conduct an environmental review of the proposed project in accordance with the requirements of the National Environmental Policy Act (NEPA). Materials submitted by the ARRC and MSB will help to inform the NEPA review. Preparatory activities include the solicitation of public and agency comment, environmental baseline work, preliminary engineering, and an alternatives analysis.

Three principal corridors are being evaluated for this new rail line. They are referred to in project documents as Mac West, Central, and Mac East. Each of the corridors contains sections where alternative alignments are possible. In general, the following comments are not corridor-specific and are relevant for the project regardless of which corridor might ultimately be selected.

At this early stage our feedback consists of two primary messages for the ARRC and MSB. These are recommendations to: 1.) establish a detailed project baseline, and 2.) design a project that truly avoids impacts to the maximum extent practicable. Each of these issues is addressed in more detail below.

#### Baseline Assessments

To allow for the full disclosure and assessment of project impacts for each of the corridors and alignments, it is important that environmental baseline information be sufficient in scope and analysis for incorporation into the NEPA document. In this context, the studies should allow for a functional assessment of the affected environment.

As the NEPA document will also be used to support the Clean Water Act section 404 permitting review, the baseline information need to be sufficiently detailed to establish compliance with EPA's 404(b)(1) guidelines (40 CFR Part 230). These guidelines allow only the least environmentally damaging practicable alternative to be permitted.

The section 404 permitting review is a discrete and yet expansive evaluation of potential impacts to waters of the U.S., including wetlands. There are four major categories of impacts to the aquatic ecosystem that are evaluated. These include impacts to: 1.) the physical and chemical characteristics of the system, such as to substrate, water quality, flow patterns and normal fluctuations; 2.) the biological characteristics of the system, including to fish, aquatic organisms in the food web, and other wildlife associated with the ecosystem; 3.) special aquatic sites, including refuges, wetlands, and riffle and pool complexes; and lastly, 4.) human use characteristics, including water supplies, fisheries, water-related recreation, aesthetics and parks.

The 2003 Rail Corridor Study prepared for the MSB contains valuable information, but as a reconnaissance evaluation does not in itself contain sufficient detail to quantify and evaluate the project impacts. The materials prepared to date, including the recent constraint mapping, do not contain enough detail for project permitting. There is a definite need for ground-truthing, detailed mapping, and preliminary analysis of the environment. Much of this information is, of course, also necessary to support the design and engineering of the project.

The 2003 study focused to a large degree on the potential impacts of construction of a rail corridor. In addition to the direct impacts from construction and operation of the line, the NEPA document should also fully analyze all indirect and cumulative impacts from the proposed project, as well as from connected projects. The connected actions/direct, indirect and cumulative impacts analysis should give full consideration of all public and private projects that are connected or related to the proposed project.

This analysis should include projects that are connected to the rail line extension because the rail line would make them economic or allow them to expand operations. Indirect effects include induced growth and reasonably foreseeable future development within the corridor. In addition, the analysis of cumulative impacts needs to include past, present, and reasonably foreseeable future independent projects that are effecting the same environment.

The sophistication of induced growth analysis has increased in recent years. As a large project in the fastest growing area of the state, this rail line has tremendous potential to affect the patterns of regional development. The MSB has stated that economic analysis has already been conducted which shows this project would be an economic driver and make a number of other projects feasible. If this is indeed the case, the analysis of induced growth will be an important component of the NEPA documentation.

Some of the information necessary to establish the project baseline already exists. Much of the physical data, however, such as information on hydrology, water quality, soils, and vegetation within each corridor, will have to be collected. Information such as hydrographs for ungaged streams and wildlife movement corridors will have to be calculated or generated. Assessments about future development will have to be made, such as potential impacts to marine fish and intertidal habitats from expansion of the port itself.

#### Impact Avoidance

In addition to informing the NEPA process and the permitting review, a detailed project baseline will allow the rail line to be designed and engineered so as to avoid impacts to sensitive resources. In this context, we wish to emphasize that the project sponsors have a very real obligation to avoid impacts to aquatic resources to the maximum extent practicable.



To again reference the 404(b) (1) guidelines, it is clear that a project does not comply with the guidelines if: 1.) there is a practicable alternative to the proposed discharge; 2.) the proposed discharge will result in significant degradation of the aquatic ecosystem; or 3.) the proposed discharge does not include all appropriate and practicable measures to minimize potential harm to the aquatic ecosystem. In addition, a project will be considered non-compliant with the guidelines if there is not sufficient information to make a reasonable judgment as to whether the proposed discharge will comply.

The guidelines establish a high bar for the avoidance and minimization of project impacts, and require the evaluation of alternatives as well as the incorporation of measures to minimize harm.

There are two important points to be made concerning this project and evaluations that will be made about whether alternatives and measures are "practicable." As defined in 40 CFR 230.3, the term "practicable" means available and capable of being done after taking into consideration cost, existing technology, and logistics in light of overall project purposes. The first is the obvious fact that this is a new rail line, as opposed to a repair or realignment of an existing line. As such, there are none of the operational constraints on design and construction which exist when working on existing lines.

This new line will be extensive in length and will tie into the existing rail system at a single point. This means that issues such as curve radii and track grade may be addressed over a larger area of the alignment than is possible when working with an existing line. Perhaps most importantly, there is a lack of existing infrastructure along substantial portions of each of the prospective alignments. This is particularly true for the western alignment, where land ownership is also less complex.

All of these facts create a situation where the ARRC has flexibility in how the rail line is designed and constructed. Given this flexibility, the expectation is that many of the project impacts will be avoidable.

The second point relates to project cost. It is not uncommon for feasible engineering solutions to be rendered less than practicable on the basis of cost. The rejection of alternatives or measures solely on the basis of increased cost always requires justification, however, and in this case such justification will be closely evaluated. As presented by the project sponsors, this new rail line will be a "legacy" project, an economic driver that will make many other projects possible. Economic analysis has already been done to demonstrate this fact. If correct, this project will have tremendous economic value not only for the MSB, but also for the state.

Given the potential economic return on this capital investment, construction costs will be less of an issue than they are for many projects. The expectation is that most alternatives and measures that are feasible from an engineering standpoint will be considered practicable and that the rail line will be designed and constructed so as to avoid many of the project impacts.

From the standpoint of aquatic resources, this means that stream crossings should not impede fish passage or impair the hydrologic functioning of the water body and its floodplain or riparian habitat. Road crossings should also maintain the connectivity of wetlands adjacent to stream channels and accommodate sheet flow within such wetlands. Bridges and elevated portions of the line are best able to achieve this.

According to the 2003 Corridor Study, construction within the western corridor would involve crossing approximately one thousand feet (1,000') of the Little Susitna River's floodplain, and three thousand eight hundred feet (3,800') of Willow Creek's floodplain. The study states: "The engineering of the floodplain crossing would need to take the 100-year flood events into consideration so that the rail bed

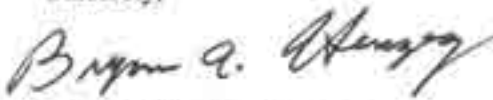
would not adversely alter flood flows and impact adjacent properties and public safety." We concur with this statement, and would add the issue of impact to public trust resources.

Stream channels and their floodplains are not the only aquatic resources within the potential corridors. Wetlands also represent a valuable resource. The Corridor Study indicated that development of the western corridor could result in the loss of up to two hundred and ninety-four (294) acres of wetlands. That study was evaluating a wider right-of-way, but the potential for loss remains substantial.

For the reasons presented above, the EPA believes that many of the potential project impacts to aquatic resources are avoidable. The rail alignment can be altered to avoid wetlands and the line elevated where it passes through them. Bridges can span streams and floodplains, allowing for navigation and the transport of flood debris. Existing drainage patterns and water quality can be protected. Economic development can occur in a responsible manner that is protective of the rich natural heritage enjoyed by residents of the Matanuska-Susitna Borough.

We appreciate the opportunity to provide comments at this stage in the project and look forward to working with you and the MSB as the project moves forward. If you have questions regarding this document, please do not hesitate to contact Matthew LaCroix at 907-271-1480, or by email at [lacroix.matthew@epa.gov](mailto:lacroix.matthew@epa.gov).

Sincerely,

*for*   
Michael Szerlog, Manager  
Aquatic Resources Unit

cc: D. Perrin, DNR/OPMP  
M. Bethe, DNR/OHMP  
P. Brna, USFWS  
N. Brudie, DNR/OPMP  
K. Klein, ADF&G  
D. Limpinsel, NMFS  
J. Bittner, DNR/OHA  
L. Phillips, COE  
B. Sworts, MSB

# STATE OF ALASKA

## DEPARTMENT OF FISH AND GAME

*Division of Sport Fish*

**SARAH PALIN, GOVERNOR**

333 Raspberry Road  
Anchorage, AK 99518-1599  
PHONE: (907) 267-2342  
FAX: (907) 267-2464

October 31, 2007

Brian Lindamood  
Alaska Railroad Corporation  
P.O. Box 107500  
Anchorage, AK 99510-7500

Re: Port MacKenzie Rail Extension Pre-Application Comments

The Alaska Department of Fish and Game (ADF&G) has reviewed the preliminary information regarding the proposed Port MacKenzie Rail Extension project pursuant to the Alaska Coastal Management Program (ACMP) (AS 46.40), Special Areas Permitting (5 AAC 95), and the Fish and Wildlife Coordination Act (16 U.S.C. 662).

The Alaska Railroad Corporation (ARRC) and the Matanuska-Susitna Borough (MSB) have jointly proposed to design and construct a 30- to 45-mile rail line from Port Mackenzie to the existing rail system at a point between Meadow Lakes and north of Willow. The anticipated timeline is as follows: 2007-2009, completion of the requirements of the National Environmental Protection Act (NEPA); 2008-2009, final project design; 2009-2011, construction; and 2011-2012, operation. Three major routes have been proposed, but none has thus far been selected. The proposed routes are identified in Attachment 1: Project Area Alternatives. Factors influencing the final route selection include presence of water bodies and anadromous fish streams, wildlife habitat, cultural sites, native allotments, parks and refuges, wetlands, soils, land use and ownership, and feasibility of acquisition of Rights of Way (ROW) by ARRC.

After review of the project alternatives, ADF&G has identified several important considerations. Per 11 AAC 112.300, the selected route should avoid adverse impacts to coastal resources including wetlands, rivers, streams, lakes, and State Game Refuges. Additionally, facilities and improvements associated with ARRC should avoid impacts to offshore areas, estuaries, and tideflats where such impacts could negatively affect water flow and natural drainage patterns or competing uses such as commercial, recreational, or subsistence uses. Where adverse impacts cannot be avoided, measures must be taken to minimize and mitigate all adverse impacts. The ADF&G is mandated to, "manage, protect, maintain, improve, and extend the fish, game, and aquatic plant resources of the state..." In order to avoid impacts and promote healthy fish and wildlife populations, ADF&G offers the following comments for consideration during project development:

---

*"... shall manage, protect, maintain, improve, and extend the fish, game and aquatic plant resources of the state in the interest of the economy and general well-being of the state"*




- The selected route should avoid crossing into or through Susitna Flats and Goose Bay State Game Refuges.
- The selected route should minimize the number of stream and wetland crossings.
- Avoid crossing large streams such as Willow Creek, the Little Susitna River, and Fish Creek whenever possible.
- Impacts to rivers and streams should be minimized through use of railway bridges rather than culverts, particularly for streams containing anadromous fishes.
- Bridges should span 100-year floodplains in order to maintain natural water flow and drainage patterns of streams, rivers, and wetlands. All abutments and other infrastructure should be built outside of the floodplain whenever possible. Bridges spanning floodplains will help to maintain riparian vegetation, streambank integrity, and wildlife corridors.
- Public access should be maintained to, from, and along coastal waters, traditional access routes, National Historic Trails, and existing easements (including those along section lines).
- To reduce the likelihood of invasive weed expansion, all soil disturbance due to construction in areas of previously-undisturbed vegetation adjacent to or associated with the rail line should be revegetated with native species within one growing season of the disturbance activity, except where doing so would increase risk of wildfire.
- The construction of a rail line in previously undisturbed areas will result in increased habitat fragmentation. Habitat connectivity should be maintained to the greatest extent possible. The Mac West route and the Willow connection have the greatest potential for fragmenting previously undisturbed habitat. The Mac East route and Big Lake connection is the shortest route, crosses the fewest waterways, and will result in the least fragmentation of previously-undisturbed habitat.
- All three proposed routes will cross areas frequently used by moose, potentially reducing travel between habitat patches, and increasing moose-railcar collisions. A baseline field study should be conducted to identify important seasonal moose concentration areas, movement corridors and habitat resources. Once identified, the impacts of the railroad on these areas must be avoided and minimized. Effective wildlife crossings and conventional road crossings should be optimized to facilitate wildlife movement across the track and to reduce wildlife-railcar collisions. Moose overpasses, elevated sections of track, and extended lengths of bridges across rivers should all be considered and constructed where appropriate.
- Important moose habitat, movement corridors, and effective buffer zones around corridors should be integrated along with green infrastructure, rivers and floodplains, wetlands, recreation areas, and other natural resources into a region-wide land-use plan in order to identify, prioritize, and limit human activities that negatively impact the ecological functionality of the landscape. ARRC should participate in regional planning efforts in coordination with borough planners, federal and state agency representatives, special interest groups, and the public. Regional land use planning should be addressed during assessment of the railway's cumulative impacts.
- An analysis of impacts to fish, wildlife, habitat, and aquatic resources must be conducted and should include a detailed assessment of cumulative effects of rail construction as well as associated developments. The associated developments should include roads, utilities, material sources, secondary development, and industry that can be expected to develop as a

result of creation of the rail line. Where current accurate baseline data is lacking, studies to identify the existing resources and potential impacts are needed. In particular, wetlands need to be accurately mapped, hydrology, including flood data, in-stream flow data, and water quality information is needed for potentially affected streams and water bodies.

- Negative impacts to fish, wildlife, habitat, and aquatic resources should be avoided. Where impacts to public trust resources cannot be avoided, they should be minimized and mitigated. A comprehensive approach to identifying effective methods to minimize and mitigate for unavoidable impacts is needed. Mitigation plan development should be conducted in coordination with borough planners, federal and state agency representatives, special interest groups, and the public.
- Potential impacts of a spill of oil, gas, or other hazardous material should be identified along each alternate route. A plan for minimizing the possibility of spills as well as contingency plan to address spills is needed for the selected alternative.

This concludes our pre-application comments on the Port MacKenzie Rail Extension project. These comments represent our review at the pre-application stage; more specific information and recommendations will be forthcoming. We look forward to working with you and other project collaborators on this project. If you or your staff has any questions about the department's comments, or need additional information, please give me a call at 267-2812.

Thank you for the opportunity to comment on this project.

  
for Kimberly Klein  
Habitat Biologist

cc via email.

Dave Rutz, ADF&G  
Tony Kavalok, ADF&G  
John Hechtel, ADF&G  
Jim Fall, ADF&G  
Tom Rothe, ADF&G  
Jeff Fox, ADF&G  
Cecil Rich, ADF&G  
Tom Brookover, ADF&G  
Tom Cappiello, ADF&G  
Jason Mouw, ADF&G  
Mike Bethe, DNR  
Phil Brna, FWS  
Doug Limpinsel, NOAA  
Matthew LaCroix, EPA

# STATE OF ALASKA

## DEPARTMENT OF NATURAL RESOURCES

### DIVISION OF PARKS AND OUTDOOR RECREATION

SARAH PALIN, GOVERNOR

MAT-SU / COPPER BASIN AREA

HC 32 Box 6706

Wasilla, Alaska 99654

phone: 907/ 745-3975

fax: 907/ 745-0938

webpage: <http://www.alaskastateparks.org>

November 14, 2007

Brian Lindamood  
Alaska Railroad Corporation  
PO Box 10700  
Anchorage, AK 99510-7500

Re: Port MacKenzie Railroad Corridor Alternative Comments

Dear Mr. Lindamood,

I have reviewed the Port MacKenzie Railroad Corridor proposal and the various alternatives that are being considered. The Alaska Division of Parks and Outdoor Recreation is responsible for more than just managing the Alaska State Park system, as we are also charged with promoting and enhancing outdoor recreation outside state parks. One example of this is through our promotion of trails for motorized and non-motorized users with funding grants or expertise in designing or managing trails, or by establishing easements. As such, we offer the following comments regarding the proposed rail extension from both a State Park as well as a general outdoor recreation perspective.

**We recommend that the Houston South – Houston – Connector 3 – Mac East alternative be chosen as the preferred alternative.**

This route minimizes impacts to the major recreational/public use areas, eliminates additional bridges over the Little Susitna River and Willow Creek, and focuses the rail line adjacent to existing road corridors.

Although we understand that the Willow Corridor is favored as a route from a design perspective, and because it may have better soils and fewer crossings of private lands, we object to this alternative as this area provides outstanding outdoor recreational opportunities that would be significantly impacted with a railroad bisecting the area. The limited private property is what makes this area so valuable in terms of recreation -- and one of the reasons that many residents live along this corridor.

**From an outdoor recreation perspective, a rail line through the Willow Corridor is the least favorable option presented.**

The following provides additional detail to illustrate our significant concerns with the Willow Corridor:

**1 Willow Creek State Recreation Area would be significantly impacted.** (DNR: Division of Parks and Outdoor Recreation)

The park encompasses almost all of Willow Creek from the Parks Highway to its confluence with the Susitna River. The Willow Corridor would pass through the heart of the undeveloped portion of the park, requiring either a major (one mile long) cut and fill across the river valley or an extensive overhead trestle. Either method would constitute a major feature that would ultimately change and dominate the ambiance of the park. Willow Creek is used predominately by fishers, with peak use occurring during the king salmon season, although it hosts all five salmon species. Silver salmon is the second biggest fishing attraction through late summer, with rainbow trout fishing third. Use is concentrated along the lower creek section between the Parks Highway and the confluence with the Susitna River. "Fishing tubes" are very popular on the creek. The park receives less use in the winter, with almost 100% being winter trail use.

The historic Lucky Shot Trail was a major transportation corridor from the Susitna River to the Lucky Shot Mine near Hatcher Pass, and passes through the park. This trail is still heavily used during the winter months as a major groomed winter trail. A historic trappers cabin remains at one of the proposed rail alignments across Willow Creek.

There is also a high potential for impacting prehistoric cultural resources within the Willow Creek SRA. The following reported archaeological sites are located within the park south of Willow Creek. All sites contain cache and house pits (cultural depressions) likely associated with late prehistoric Dena'ina culture:

- TYO-014: between 10 and 15 cultural depressions;
- TYO-041: at least 10 cultural depressions (two of them double celled);
- TYO-060: at least 12 cultural depressions;
- TYO-061: over 100 cultural depressions.

Based on the maps provided, two of these sites (TYO-014 and TYO-060) will be directly affected by the Willow Corridor. While the other reported sites are outside of the railroad footprint, they may be indirectly affected by staging activities associated with this project or by resulting increased development or other activity within the Willow Creek SRA. In addition, there could be other archaeological sites in the area that are currently unreported.

Additional information:

- a. Method Established: Legislatively Designated
- b. Date of Establishment: 1987
- c. Acreage: 3,000 acres
- d. Visitation:

Visitation Type	FY2007	FY 2006	FY 2005	FY 2004
Day Use	22,483	18,387	20,048	10,973
Camping	10,966	15,445	11,792	10,013

Willow Ck Floats (estimated)*	8,550	9,000	9,000	8,000
TOTAL	42,000	42,832	40,840	28,986

\*Approx 20-50 rafts per day, except 200+/day during king salmon season, 2 ave/raft

*NOTE: This visitor data is not statistically valid, numbers are approximate and should only be used to identify trends over time, and not taken literally.*

- e. Primary Recreation Types (by order of use, highest first): fishing, camping, floating/boating, winter trails, wildlife viewing, hunting
- f. Commercial Use: Guided and unguided float trips and fishing along Willow Creek and the Susitna River
- g. Historical Significance: Historic Lucky Shot Trail, trappers cabin, numerous cultural sites

2 **Nancy Lake State Recreation Area would be affected through negative impacts on neighboring recreational lands.** (DNR: Division of Parks and Outdoor Recreation)

The park was legislatively established in 1966 as one of the first state parks in the system due to its close proximity to both Anchorage and the growing Mat-Su Valley areas. The area still possessed its natural qualities, unlike the more heavily developed Big Lake area to the south. The 1983 Nancy Lake State Recreation Area Master Plan identifies the purpose of the park to "...provide a diversity of outdoor recreation activities appropriate to the area's resource character and regional setting". The same year, the Mat-Su Borough also created a Special Land Use District along the park boundaries to further protect the area's recreational values through its zoning laws. The park contains over 130 lakes, with about 25 miles of terrestrial trails, and 15 miles of canoe trails through its maze of lakes. Although the Willow Corridor will not directly pass through any portion of the park, it does skirt along the southwest corner within one mile of Red Shirt Lake, a heavily used lake for recreational fishing, boating, and winter trail use. Direct impacts to the park will be increased noise from nearby trains, and restricted winter trail access to the west and south.

Additional information:

- a. Method Established: Legislatively Designated
- b. Date of Establishment: 1966
- c. Acreage: 22,615 acres
- d. Visitation:

Visitation Type	FY2007	FY 2006	FY 2005	FY 2004
TOTAL	39,548	43,708	43,927	43,552

*NOTE: This visitor data is not statistically valid, numbers are approximate and should only be used to identify trends over time, and not taken literally.*

- e. Primary Recreation Types (by order of use, highest first): fishing, camping, canoeing, trails (canoe, hiking, winter trails), wildlife viewing
- f. Commercial Use: Canoe rental concession
- g. Historical Significance: Fishing camp established along the outlet stream of Red Shirt Lake and historic trail to Susitna Landing
- h. Special Concerns: Over 30 private inholding parcels exist within the park, with 88 parcels bordering on Red Shirt Lake alone. Most inholders enjoy living in/by the park



for its natural, quiet qualities. Residents along Red Shirt and Nancy Lake have organized homeowners associations.

3 **Little Susitna State Recreational River would be significantly impacted.** (DNR: Division of Mining, Land and Water)

Although the Little Susitna River is not managed by the Division of Parks, it is a high-use river corridor managed for the primary purpose of recreational float trips. Fishing, hunting, camping, boating and paddling are the primary uses. The river hosts all five species of salmon, and receives the heaviest sportfish use of all the Mat-Su Valley rivers. It provides a very popular float from the Parks Highway Bridge (River Mile 69.8) since there are two takeouts: Skeetna Lake at River Mile 54.6 (ties into the Nancy Lake Canoe Trail system); and Little Susitna Public Use Facility at River Mile 28.5 on the river. Additional bridge crossings along this corridor will detract from the wild qualities of this popular multi-day float trip. We strongly discourage any routes that will cross the river to maintain the current recreational integrity of this important river corridor.

Additional information:

- a. Acreage: 18,218 acres
- b. Visitation: Estimated annual use is 2000-3000 floats per year.
- c. Primary Recreation Types (by order of use, highest first): floating, fishing, camping, wildlife viewing, hunting
- d. Commercial Use: Guided and unguided float trips and fishing along the Little Susitna River

4 **Little Susitna Public Use Facility (LSPUF) would be significantly impacted.** (ADF&G: Division of Sportfish)

Owned by ADF&G, this facility is operated by the Division of Parks and Outdoor Recreation through a cooperative agreement. For this reason we feel qualified to comment on impacts to this facility. The LSPUF lies within the Susitna Flats State Game Refuge. It provides the only developed public access to the Little Susitna River south of the Parks Highway (approximately 70 river miles). It is a very popular destination for fishers, hunters, and other recreationists. Connector 1 will flank the LSPUF's east boundary which will affect users arriving at the "front door" of the facility, and displace a north-south trail that is used by the public to access hunting areas in the refuge. The Willow Corridor would cross the river at approximately River Mile 33, only one-quarter mile from seven developed riverside campsites maintained as part of the LSPUF. This will have an impact on the recreational experience that these remote sites offer.

Additional information:

- a. Date of Establishment: 1989
- b. Acreage: 720 acres
- c. Visitation:

Visitation	FY2007	FY 2006	FY 2005	FY 2004
TOTAL	30,340	22,503	18,908	22,770

*NOTE: This visitor data is not statistically valid, numbers are approximate and should only be used to identify trends over time, and not taken literally.*

- d. Primary Recreation Types (by order of use, highest first): fishing, camping, boating, hunting access, winter trail use, hunting, general
- e. Commercial Use: Guided and unguided fishing along the Little Susitna River

### **Regional Trail Impacts**

Both Nancy Lake and Willow Creek State Recreation Areas are linked by a myriad of winter trails (West Gateway trail system) that are an extremely important part of the region's attractiveness as a hub for winter recreation.

Between Red Shirt Lake and the Susitna Flats State Game Refuge are critical trail corridors, including the historic Iditarod Trail. These trails are used for routine recreation, competitive training and actual races. There are many sanctioned races on these trails, including dog mushing, snowmachining, and ski-joring. Additionally, these trails are critical winter transportational corridors to cabins, camps and lodges throughout the Susitna River Valley. Many of these corridor origination points are located in state park units.

No route completely eliminates trail impacts, but our preferred option keeps these impacts to a minimum. Since the area trail clubs will be providing specific comments regarding regional trail impacts, we will not elaborate further here other than impacts to Division of Parks programs.

### **State Trail Grooming Pool Program**

Trails throughout the proposed rail corridor are also part of the Mat-Su trails SnowTRAC Grooming Pool, and receives state funding from snowmachine registration fees to maintain and groom snowmachine trails in the winter. This program has been very successful, and the Division of Parks now administers grooming grants for well over 100 miles of trails between Big Lake and Denali State Park far to the north. The program has grown every year, with an objective to develop winter trail corridors throughout Southcentral Alaska, possibly connecting to the Denali and the Fairbanks North Star Boroughs. Such a network has great potential to create new economic opportunities for small businesses during a traditionally quiet part of the year. Part of the mission for the Division of Parks is to promote recreation in Alaska, and support the tourism industry. Winter recreation tourism has become an important part of the greater Willow area economy and steps should be taken to foster this endeavor, not weaken it.

### **Historical/Cultural Impacts**

Regional comments regarding cultural impacts were covered under a separate letter by the Division of Park's Office of History and Archaeology.

### **Barrier Issue**

The Willow Rail Corridor would effectively create 15 miles of a fence-like barrier between Willow Creek and Nancy Lake that will make cross-country travel east and west far more restrictive. A few strategically placed trail crossings are not sufficient to adequately resolve the barrier issue – even if they were at-grade crossings with elevated rail. Sub-grade (culvert) crossings are problematic due to pooling water, lack of snow, and the innate reluctance of animals (dog teams, wildlife) to enter such structures. Note that there are likely many more minor, non-dedicated trails, that traverse this country than what is indicated on most maps.

**Contiguous Public Land Block**

The area comprised by the combination of Nancy Lake State Recreation Area, the Little Susitna Recreational River, and the Susitna Flats State Game Refuge comprises a total of over 342,000 acres of lands reserved for public use. The South Houston – Houston – Connector 3 – Mac East route will completely avoid significant impacts to this block.

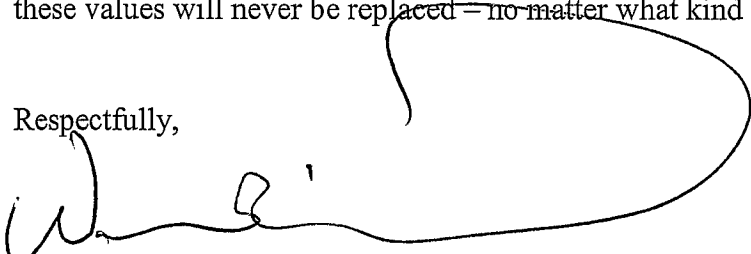
**Habitat Protection**

The Willow Corridor poses inherent risks to sensitive habitat that is very important for Alaskans in terms of recreational pursuit and for subsistence purposes (hunting, fishing, etc.). Each anadromous stream crossing is a new point source of contamination in the event of accidental discharges of hazardous materials. Eliminating the additional crossings of Willow Creek and the Little Susitna Rivers should be a high priority for this project.

**Conclusion**

Rail development through the Willow Corridor would be a major detriment to recreational values in that area, and will adversely affect the quality of life for many area residents. During the 2004 *Statewide Comprehensive Outdoor Recreation Plan* public survey, 98% of the respondents indicated that parks and outdoor recreation are important or very important to them. Once lost, these values will never be replaced – no matter what kind of mitigation ensues.

Respectfully,



Wayne Biessel, Mat-Su Area Park Superintendent

- Cc: James King, DNR/DPOR Director
- Michael Bethe, DNR - Habitat
- Sam Means, DNR - Mining, Land and Water
- Don Perrin, DNR – Permitting
- Judy Bittner, DNR/SHPO
- Dave Rutz, ADF&G
- Mary Anderson, Mat-Su Area State Parks Citizens Advisory Board
- Willow Area Community Organization
- Dave Hanson, Mat-Su Borough



# KNIK TRIBAL COUNCIL

KNIK, THE OLDEST VILLAGE IN COOK INLET

November 20, 2007

Mr. Brian A. Lindamood, P.E.  
Alaska Railroad Corporation  
P. O. Box 107500  
Anchorage, AK 99510-7500

Dear Mr. Lindamood:

The purpose of this letter is to respond to your presentation and request at a meeting of the Knik Tribal Council Cultural and Historical Preservation Committee. The presentation you provided to the committee regarding the impact of the extension of the Alaska Railroad Corporation (ARRC) from Cantwell to Port MacKenzie was very informative, especially from the Knik Tribal Council's desire to protect the cultural integrity of our Dena'ina heritage. The map provided by Fran Seager Boss noted areas of actual and possible culturally significant sites along the three corridor options proposed by the Railroad and the Matanuska-Susitna Borough where additional survey would be recommended.

As Chair of the Cultural and Historic Preservation Committee and Vice President of the Council, I reviewed the map with members of the governing board of Knik Tribal Council. We appreciate the opportunity to provide the following comments.

The Knik Tribal Council and the Cultural and Historic Preservation Committee support the Houston North route as noted by a red line that becomes purple past Big Lake, then the Conn 1 (green line) to the Mac West (spruce green line) and concluding at Point MacKenzie. The rationale for supporting this proposed corridor is that there are fewer documented sites. Likewise, we do not support the Willow Route as the preliminary information indicates that there is a high probability of cultural and historic sites in this area. The Willow area is also documented in Shem Pete's writing as a very important subsistence resource area to the Dena'ina Athabaskan, especially around the Red Shirt Lake area.

The preservation of areas and sites of cultural significance to federally recognized Knik Tribe is of paramount concern, especially as the MSB continues to grow and expand into undeveloped areas. Within the past five years, the Knik Tribal Council has made significant strides to establish itself and to be recognized as an important sector of the community. The Cultural and Historic Preservation Committee has been directed to identify, document and preserve our cultural heritage. To meet our mission and our objectives it is imperative that we continue as a consulting party, during the rail development and to insist that if any cultural site is discovered during the course of this project (surveying and construction) that Knik Tribal Council be notified. In addition, we are very interested in accompanying the 3<sup>rd</sup> party contractor and/or their sub-contractor during the investigation and evaluation of cultural sites potentially affected by the proposed project. Towards that end, we request that in addition to funding for archaeological and historic surveys, that funds be allocated for field verification and documentation support by Knik Tribal Cultural and Historic Preservation Committee staff under the Section 106 process. Please contact the Knik Tribal Council office at 373-7991 or Debra Call at 330-8016.

Thank you for providing us the opportunity to respond and we look forward to working with you in the future.

Sincerely,



Debra Call, Vice President and  
Chair of the Cultural and Historical Preservation Committee



REPLY TO  
ATTENTION OF:

DEPARTMENT OF THE ARMY  
U.S. ARMY ENGINEER DISTRICT, ALASKA  
REGULATORY DIVISION  
P.O. BOX 6898  
ELMENDORF AFB, ALASKA 99506-0898

**NOV 21 2007**

Regulatory Division  
POA-2007-1586-4

Mr. Jon Schick  
HDR Alaska, Inc.  
2525 C Street, Suite 305  
Anchorage, Alaska 99503-2632

Dear Mr. Schick:

This letter is in response to the October 2, 2007, scoping meeting on the Port Mackenzie Rail Extension project. Thank you for providing the Corps of Engineers the opportunity to provide comments early in the process.

We have reviewed the information provided, including maps illustrating the alternative locations of the proposed rail extension. We realize that a project at the scoping level is less detailed than a project that is ready for permit evaluation. Our scoping comments at this time are limited and may not provide you with all of the information necessary. In order to expedite the process we have included some additional guidance concerning information and documentation that may be required for us to satisfy our regulatory responsibilities.

Section 404 of the Clean Water Act requires that a DA permit be obtained for the placement or discharge of dredged and/or fill material into waters of the U.S., including jurisdictional wetlands. The Corps defines wetlands as those areas that are inundated or saturated by surface or groundwater at a frequency and duration sufficient to support, and under normal circumstances do support, a prevalence of vegetation typically adapted for life in saturated soil conditions. Several acres of wetlands habitat are present along and within the vicinity of the potential extension routes. In order to evaluate impacts of the project a delineation of the proposed routes is required. Note that prior to permitting, the results of your delineation must be approved by the Corps. The delineation should include the following:

- Field data sheets
- Photographs of sample sites
- A map or drawing that shows locations and/or GPS coordinates of sample points within the project area in relation to jurisdictional areas
- Aerial photos of the area
- A written summary of the delineation, summarizing the data sheets, with information that describes existing conditions, the footprint of the project as proposed, and how the proposed project affects aquatic resources within the footprint corridor.

The proposed extension routes include several crossings of anadromous fish stream and rivers. The Magnuson-Stevens Fishery Conservation and Management Act requires consultation with the National Marine Fisheries Service (NMFS) on all actions that may adversely impact Essential Fish Habitat (EFH); therefore, consultation with the NMFS must be conducted to gather comments and recommendations concerning EFH.

The latest published version of the Alaska Heritage Resources Survey has been consulted for the presence or absence of historic properties and determined that several sites are located within the vicinity of the proposed routes. If any properties are determined to lie within the project area and/or would be impacted by the project, a determination of eligibility and, if needed, a determination of effect will be required in consultation with the State Historic Preservation Officer.

Our responsibilities under National Environmental Policy Act require us to review your project under the Environmental Protection Agency's 404 (b)(1) Guidelines. Under the Guidelines, the applicant must show that all appropriate and practicable steps to minimize potential impacts of the discharge on the aquatic ecosystem have been considered, and that the current proposal represents the least environmentally damaging practicable alternative. The applicant must summarize the steps that they have taken to avoid, minimize and/or mitigate the unavoidable impacts of their proposed project. The burden of proof to demonstrate compliance with the Guidelines rests with the applicant; where insufficient information is provided to determine compliance, the Guidelines require that no permit be issued.

You may contact me at (907) 753-2819, toll free from within Alaska at (800) 478-2712, by email at [serena.e.sweet@usace.army.mil](mailto:serena.e.sweet@usace.army.mil) or by mail at the address above, ATTN: CEPOA-RD, if you have questions. For additional information about our Regulatory Program, visit our web site at [www.poa.usace.army.mil/reg](http://www.poa.usace.army.mil/reg).

Sincerely,



Serena Sweet  
Project Manager

# STATE OF ALASKA

## DEPARTMENT OF NATURAL RESOURCES

### *Office of Habitat Management and Permitting*

**SARAH PALIN, GOVERNOR**

1800 GLENN HIGHWAY, SUITE 12  
PALMER, ALASKA 99645-6736

PHONE: (907) 745-7363

FAX: (907) 745-7369

### **FISH HABITAT CASE NUMBER FH-07-IV-0428**

November 26, 2007

Mr. Brian Lindamood  
Alaska Railroad Corporation  
P.O. Box 107500  
Anchorage AK 99510-7500

Re: Port MacKenzie Rail Extension Pre-Application Scoping Comments

Dear Mr. Lindamood;

Pursuant to AS 41.14.870 and AS 41.14.840, the Department of Natural Resources, Office of Habitat Management and Permitting (OHMP) has reviewed the proposed routes presented by the Alaska Railroad Corporation (ARRC) and the Matanuska-Susitna Borough for the Port MacKenzie Rail Extension Project. The purpose of this project is to provide rail access from the main ARRC track to the marine port near Point MacKenzie. The ARRC has asked that OHMP review the alternative routes and submit scoping comments based on our statutory permitting authority.

The three proposed routes divert south from the existing rail line near Willow, Houston (north and south alternatives) and Big Lake (see attached map). The rail line would then intersect, via three possible connector segments with two alternative routes (Mac-East and Mac-West) continuing southward to the Port Mackenzie area.

All of the potential routes for this project traverse a large geographic area and have the potential for negatively impacting a wide range of sensitive habitat areas. All work associated with this project that could potentially impact anadromous streams (AS 41.14.870) or could potentially block the free passage of fish (AS 41.14.840) requires a Fish Habitat Permit from the OHMP prior to commencement of any construction.

All comments contained herein are submitted as scoping comments and should be viewed as preliminary in nature. The OHMP offers the following comments:

#### **Information Needs**

- Comprehensive stream sampling to determine/confirm anadromy and the presence or absence of fish will be required. Fish usage patterns may have changed since the area was initially surveyed, and many smaller streams have yet to be sampled.
- Fragmentation of aquatic habitat is a concern. Hydrologic studies will be required to map wetland areas associated with fish bearing drainage systems. This project has the potential to isolate the free flow of water through these wetland areas, thus impacting fish-bearing waters.

*“Develop, Conserve, and Enhance Natural Resources for Present and Future Alaskans.”*

## Routing and Design Considerations

- To the maximum extent practicable, the route should be sited to avoid wetlands, fish-bearing streams and anadromous water bodies. Any preferred route should minimize the number of stream crossings, particularly over systems that produce significant numbers of salmon such as the Little Susitna River, Willow Creek, and streams in the Nancy Lake and Big Lake watersheds.
- The use of bridges to span floodplain areas is the preferred method of providing for the long-term free passage of fish on anadromous systems. Bridge abutments should be located outside the floodplain and above the ordinary high water mark (OHW) to minimize potential impacts to riparian vegetation and streambank integrity.
- Culverts should be designed using stream simulation methodology. The culvert design width at the OHW should be greater than or equal to 125-percent of the width of the stream at the OHW stage. The culvert grade should approximate the surrounding slope of the stream channel ( $\pm 1\%$ ). Culverts should be buried to approximately 40-percent of their diameter with substrate material that will remain dynamically stable at all expected flood discharge rates. Other design criteria will apply as well. We can meet later to flesh out the necessary design criteria for fish passage if you have any questions.
- It shall be the responsibility of the ARRC to ensure the free passage of fish throughout the lifetime of each stream crossing. Beavers are common along the various alternative routes. Culvert designs should account for long-term maintenance for fish passage and be of sufficient size (diameter) to discourage blockages associated with beaver dam construction.

## Route Preferences

OHMP prefers a route that would minimize potential impacts to wetland areas associated with fish bearing waterways, minimizes the total number of actual stream crossings and avoids crossings of important salmon producing systems such as the Little Susitna River, Willow Creek, and streams in the Nancy Lake and Big Lake watersheds whenever possible. These criteria appear to be met best with the following route:

1. Houston South
2. Houston
3. Connector 3
4. Mac East

## Route Discussion

This conclusion is based on our initial examination of existing data and aerial imagery and should be viewed as preliminary. Also note that we recognize that selection of final routing should be based on other considerations as well. Influencing factors should include fragmentation of wildlife habitat, the presence of cultural sites, native allotments, parks and refuges and historic land use patterns. Land ownership and the successful acquisition of Rights-of-Way will also significantly affect the final route selection.

Based on our analysis of existing materials, we believe that the Willow route will result in more fragmentation of fish and wildlife habitat, particularly in undeveloped areas, than the other alternatives. Crossings over Willow Creek and the Little Susitna River would be necessary.



Because of the extended length of this route, the potential impacts to wetland areas associated with these drainages could be significant. It is the view of OHMP that this route (Willow) is the least preferable of all of the alternatives.

Thank you for the opportunity to comment on your project. If you have any questions, please feel free to contact me at the above address and telephone number or by e-mail at [mike\\_bethe@dnr.state.ak.us](mailto:mike_bethe@dnr.state.ak.us).

Sincerely,



Michael L. Bethe  
Habitat Biologist  
Area Manager  
Palmer Mat-Su Area

Attachment: ARRC Alternative Routes Map

-kab/mlb

Distribution: S. Joy, COE  
D. Rutz, ADF&G  
J. Hewitt, COE  
M. Fink, ADF&G  
K. Klein, ADF&G  
D. Perrin, OPMP



**MATANUSKA-SUSITNA BOROUGH**  
CULTURAL RESOURCES DIVISION  
PLANNING AND LAND USE DEPARTMENT  
350 East Dahlia Avenue - Palmer, Alaska 99645-6488  
Fax (907)745-9876 - Phone (907)745-9851

November 28, 2007

Mr. Brian A. Lindamood P. E.  
Alaska Railroad Corporation  
P.O. Box 107500  
Anchorage, Alaska 99510-7500

**Subject:** Preferred Railroad Alternates

Dear Mr. Lindmood:

We appreciate the opportunity to comment on the proposed alternate railroad lines that connect Port MacKenzie with the existing railroad that traverses from Anchorage north through the Susitna Valley to Fairbanks. The Cultural Resources Division of the Matanuska-Susitna Borough has had a chance to review the various proposed railroad lines. A map was prepared and submitted to HDR indicating current sites and areas where cultural resources may be most likely found. The lower Little Susitna Valley was a region traditionally frequented by the Dena'ina people. It was part of an area where the greatest density of Dena'ina population lived during the historic and proto-historic periods. It is therefore an area likely to encompass numerous sites in the form of native villages; hunting camps; traditional locations where raw materials were collected for practical and artistic items; traditional fishing locations; and areas likely to have been significant for religious purposes.

This letter supports the railroad transportation corridors selected by the Knik Tribal Council's Historic Preservation Cultural Committee (HPCC). Our recommendation is derived from a joint meeting with members of the Knik Tribal Council's HPCC, from known sites plotted on the map submitted by our offices; and from topographic maps examined by our staff.

The preferred lines include the:

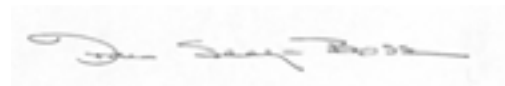
- Houston North Line (indicated in red) that connects with
- Houston South Line (indicated in Blue west of Big Lake) connecting with
- Conn 1 Line (indicated in green) and the
- Mac West Line (spruce green) that connects with the port.

The above lines appear to be the routes least likely to impact

historic, proto-historic and prehistoric sites. It is strongly urged however, that whichever transportation corridors are selected (including the ones indicated above), that an archaeologist, carry out archaeological surveys in consultation and participation with the Tribal Council's HPCC. It is further recommended that archaeologist(s) conduct on-the-ground, walk-over surveys with shovel testing. Coverage should include areas indicated on the map submitted by this office as sensitive to archaeological sites; and areas not covered on our map that the HPCC deem important. It is our understanding that HDR's Geographic Information Systems (GIS) is creating a final map of areas sensitive to cultural resources based on the draft map submitted by this office. We hope the map will be available soon and that we may have an opportunity to review it prior its final distribution.

Thank you again for the opportunity to comment on the proposed railroad routes connecting the port to the main north-south line from Fairbanks to Anchorage. If you have any further questions please do not hesitate to contact our offices at (907) 745-9859. Thank you.

Sincerely,

A handwritten signature in dark ink, appearing to read "Fran Seager-Boss", is written over a light-colored rectangular background.

Fran Seager-Boss  
Cultural Resources Specialist

# STATE OF ALASKA

SARAH PALIN, GOVERNOR

## DEPARTMENT OF NATURAL RESOURCES

DIVISION OF MINING, LAND AND WATER  
SOUTHCENTRAL REGION LAND OFFICE

550 W. 7TH AVE., SUITE 900C  
ANCHORAGE, ALASKA 99501-3577

December 11, 2007

Brian Lindamood, P.E.  
P.O. Box 107500  
Alaska Railroad Corporation  
Anchorage, AK 99510

Dear Mr. Lindamood:

Subject: Port Mackenzie Rail Extension

The Southcentral Regional Office of the Division of Mining, Land and Water offers the following comments on your study of several possible routes for a railroad from Port Mackenzie to either Willow, Houston or Big Lake. These comments are of a general nature involving impacts to state land managed by this office. Should a specific alignment be chosen, we will modify our comments to address specific land management issues in more detail.

- **The land title interest needed by ARRC.** ARRC requests a fee simple interest in a 200 foot wide corridor to build and operate a railroad. SCRO authority to grant the appropriate interest in state land resides in AS 42.40.360 and .370. SCRO will also use the public process required by AS 38.05.035 and .945 to make the decision and give public notice to convey an interest in land to ARRC.
- **Roads, Trails and Utilities.** Pursuant to 11 AAC 51.015, the Southcentral Regional Office will ensure that any area of DMLW managed land approved for railroad corridor uses will be subject to existing ADL authorizations for roads, trails, utility, or other access easement purposes. The Southcentral Regional Office will also reserve additional ADL authorizations along existing roads, trails, utility, or access routes if the Southcentral Regional Office determines that these improvements represent interests of local, regional, or statewide significance. Any rail corridor area conveyance or authorization granted by the Division will stipulate the preservation of legislatively imposed public access routes described in AS 19.10.010 (section line easements), AS 19.30.400 (RS 2477 routes) and AS 38.05.127 (navigable or public waterways.)  
If considered necessary for project development, railroad corridor officials may petition the Division and/or the local Platting Authority for formal vacation of existing easements or rights of way on a case-by-case basis in accordance with

# STATE OF ALASKA

SARAH PALIN, GOVERNOR

## DEPARTMENT OF NATURAL RESOURCES

DIVISION OF MINING, LAND AND WATER  
SOUTHCENTRAL REGION LAND OFFICE

550 W. 7TH AVE., SUITE 900C  
ANCHORAGE, ALASKA 99501-3577

established regulations and procedures. Any such petition will be subject to review and comment by the Southcentral Regional Office in addition to other agencies, interested entities, and members of the public.

- **Susitna Area Plan** This plan provides general land management guidelines that must be followed when considering major transportation projects.

<http://www.dnr.state.ak.us/mlw/planning/areaplans/susitna/index.cfm>

- **Willow Sub-Basin Area Plan.** Susitna Area Plan Revisions Affecting Willow Sub-Basin and Susitna Plans. For the past 25 years the use of state land in the area from Wasilla to the Talkeetna Y has been guided by the Willow Sub-Basin Area Plan (WSAP) and the South Parks Highway portion of the Susitna Area Plan (SAP), which the State Department of Natural Resources (DNR) adopted in 1982 and 1985 respectively. The amount of land owned by the state has decreased markedly during that period and the state is no longer the principal land owner in this area. Instead, the Borough, Mental Health Trust, University of Alaska, and CIRI are principal owners. Very little state land remains in areas near major population areas and that which remains consists of remnant parcels of generally small size. Our area plans have a design life of about 15-20 years and the WSAP and SAP need updating so that DNR decision making is based on more current information and therefore more pertinent.

In general, this plan provides the basis for state management of surface resources and land use, with decisions by the various DNR divisions (principally Agriculture, Forestry, Mining/Land/Water) to be based on the plan designations and management intent requirements identified in the area plan.

<http://www.dnr.state.ak.us/mlw/planning/areaplans/willow/index.cfm>

- **Fish Creek Management Plan.** Same comments as above.

[http://www.dnr.state.ak.us/mlw/planning/mgtplans/fish\\_ck/index.htm](http://www.dnr.state.ak.us/mlw/planning/mgtplans/fish_ck/index.htm)

- **Susitna Basin Recreational Rivers Management Plan** In general, this management plan provides more specific guidelines for minimizing impacts to the Little Susitna River related to major transportation projects. Options to mitigate impacts to recreational use of the Little Susitna River corridor can be developed when a specific route is known. Maintaining access along the banks of the Little Susitna River will be a major consideration.

<http://www.dnr.state.ak.us/mlw/planning/mgtplans/susitna/index.htm>

*"Develop, Conserve and Enhance Natural Resources for Present and Future Alaskans"*

# STATE OF ALASKA

SARAH PALIN, GOVERNOR

## DEPARTMENT OF NATURAL RESOURCES

DIVISION OF MINING, LAND AND WATER  
SOUTHCENTRAL REGION LAND OFFICE

550 W. 7TH AVE., SUITE 800C  
ANCHORAGE, ALASKA 99501-3577

- **Material Sales.** Material resources (sand, gravel, rock, peat) located outside of an approved conveyance or easement would be sold to ARRC. Under AS 38.05.110-38.05.120 and the regulations implementing these statutes allow these materials to be made available. Public Notice will be required consistent with AS 38.05.945.
- **Land Use Permits.** Man Camps, Staging Areas – Often large projects require areas adjacent to the project site to stage man, equipment and machines. Temporary locations used to facilitate the development of the ARRC project may be permitted under AS 38.05.850. Land Use Permits are the most commonly used tool for this activity. Review will be conducted for any request to use state land.
- **Alaska Coastal Management Program.** Any authorization requested to take place within the Coastal Zone may be subject to additional coordination and review by the Alaska Coastal Management Program.

Close cooperation between the ARRC and the Southcentral Regional Office is recommended as the rail route is more narrowly defined in order to facilitate the identification and protection of third party interests over the life of the project.

Sincerely,



Robert S. Means  
Natural Resource Manager

## **Appendix D: Minutes of Preliminary Regulatory Agency Meetings**

Meeting Minutes  
Sign-in Sheets



# Meeting Notes

Subject: DNR Large Projects Agency Meeting	
Client: Alaska Railroad Corporation (ARRC)	
Project: Port MacKenzie Rail Extension	Project No: 09585-67879
Meeting Date: September 18, 2007, 9:00am-10:30am	Meeting Location: 550 West 7 <sup>th</sup> Avenue, 11 <sup>th</sup> Floor Conference Room
Notes by: HDR Alaska – LR	

## Attendees:

Don Perrin, Alaska Department of Natural Resources (DNR), Office of Project Management & Permitting, Large Project Permitting  
Doug Limpinsel, National Oceanic & Atmospheric Administration, National Marine Fisheries Service  
Phil Byrna, U.S Fish & Wildlife Service  
Mary Lynn Nation, U.S. Fish & Wildlife Service  
Kimberly Klein, Alaska Department of Fish and Game  
Matt LaCroix, U.S. Environmental Protection Agency  
Nina Brodie, Alaska DNR, Office of Habitat Management & Permitting (OHMP)  
Stephanie Ludwig, Alaska DNR, Division of Parks & Outdoor Recreation  
Sam Means, Alaska DNR, DMLW, Land Use-ROW  
Clark Hopp, Alaska Railroad Corporation (ARRC)  
Brian Lindamood, ARRC  
Barb Hotchkin, ARRC  
Sasha Forland, ARRC  
Tom Brooks, ARRC  
Donna Robertson, HDR Alaska, Inc.  
Leslie Robbins, HDR Alaska, Inc.  
Mike Pochop, TNH-Hanson  
Dave McCourtney, TNH-Hanson  
Via Phone:  
Mike Bethe, Alaska DNR, OHMP, Title 41  
Brad Sworts, Matanuska-Susitna Borough

## Agenda

The meeting began with everyone introducing themselves.

Brian Lindamood went through a PowerPoint presentation, discussing the following topics:

1. Project History
2. Project Area
3. Project Purpose
4. Project Benefits
5. Project Participants
6. Funding
7. Surface Transportation Board (STB) NEPA Process
8. What's Happening Now
9. Anticipated STB Schedule
10. Next Steps
11. Opportunities and Constraints – Proposed corridors/ alternatives & environmental considerations



## Topics Discussed

### Agency Comment & Participation

- The ARRC will make every effort to coordinate with agencies on an individual basis. The primary point of contact will be Brian Lindamood, and he will make every effort to accommodate agency schedules. All resolutions should be distributed to the group to ensure that all agencies can identify conflicts.
- All agencies will be consulted when ranking the corridors with respect to each other.
- In early December, the ARRC will meet with all concerned agencies to inform them of the findings made by the project team.
- Don Perrin and the other presented agency members agreed that they would be able to provide agency input ('bigger picture' comments) by October 15.
- Agency members will be tied up in Pebble meetings the last week in November so the ARRC needs to be cognizant of that when scheduling a follow-up agency meeting. The group decided on the first week in December for an agency follow-up meeting.
- The agencies wanted an electronic copy of the PowerPoint to look at the GIS overlays. Brian was going to burn a CD after the meeting for Don, or alternatively, it could be placed on an ftp site.
- Don Perrin asked if a summary of the upcoming public workshops will be made available. Yes, comments will be tracked at the workshops using various methods, including a court reporter and comment sheets.
- Agencies are concerned with habitat fragmentation. They would generally recommend going through a more developed area than less developed.

### Alignments

- Exchanging a few small portions of refuge land for agricultural land will provide a corridor for our western alignment near port MacKenzie.
- It may not be possible to avoid the recreational area near Willow for the western alignment.
- Alignment options in developed areas may be more desirable as this will prevent habitat fragmentation.
- The corridors listed are for transportation in general. Utilities, pipelines, and other things may appear within the ROW established by the ARRC. Placing a roadway within the ROW is currently not being advocated by the ARRC; however, it could happen. Locating highways near railroad limits development along the road, and causes accessibility problems for businesses.

### Cultural Resources

- Stephanie asked if there are any known cultural resources studies for the area. MSB is working on a probability study of the sites. We feel we have a pretty good grasp of the known sites. Stephanie said overall there are not a whole lot of cultural surveys done for the area and she would likely recommend additional surveys.

### Environmental document and contractor

- Brian mentioned that STB will make a determination to either do the environmental document in-house or hire a third-party contractor. STB will likely use a third-party contractor.

- Phil Byrna asked when the STB makes a determination on what type of environmental document they will pursue. Brian said the STB will determine whether it'll be an EA or EIS after the application has been submitted.

#### Matrix/ Scoring Criteria – Costs

- During the presentation, Brian had discussed the scoring system and how it will be composed of STB's 18 analysis points, plus cost (high-level) and feasibility. The analysis will also include cost and constructability. Mary Lynn Nation asked about the matrix scoring criteria. She asked how does cost factor into the scoring. Brian said it will be factored in and also addressed in the document. As for costs, Brian said they'd have a better idea summer 2008.
- Phil Byrna said he is interested in looking at the scoring/ranking matrix early on and would be amenable to giving input on it.

#### Purpose and Need

- The MSB will market the Port for interior construction and overall transportation of goods. As for Agrium, it's not 'if' but 'when' it happens.

#### Refuges and Agricultural Lands

- Brian talked about the possibility of land swapping some of the agricultural lands for some of the refuge land. Nina Brodie commented that the boundaries for the refuges are legislative, and would require an act of legislation to change.

#### Right-of-Way (ROW)

- Doug Limpensel asked where the ARRC sees the least resistance right now, as far as ROW. Brian said the MSB. As for the public, we'll have a better idea after the public workshops being held October 1-5.
- There was a short discussion regarding whether the corridor includes rail or rail and highway, as other past studies have considered a joint corridor. This effort is looking at a rail-only corridor. Within the 200 foot corridor, there will need to be some access roads, etc for ARRC infrastructure. Phil Byrna said what is considered a good rail-only corridor may not be a good highway-rail corridor.

#### Trails

- Nina Brodie asked if RS2477 trails were included in the trails layer.

#### Other available data

- Wetlands: For analysis and corridor development purposes, wetlands were divided into three categories based on the amount of water present. The slide show from today's meeting did not show the category with the lowest water content. Phil Byrna suggested talking to Mike Gracz (907-260-5449) of the Kenai Watershed Forum. Apparently Mike has available data in the project area beyond the NWI mapping. Phil said that the NWI mapping is about 60% accurate. He also said, according to Mike's work, that the forested wetlands may be more important than thought.
- Additional Studies: Matt LaCroix asked how much environmental analysis was done for the 2003 study. It was a 'high level' study. Preliminary ground probes are occurring right now. Fieldwork is slated for next summer.
- Mary Lynn Nation suggested obtaining the latest Bald Eagle data for the area.

### Utilities, existing ROW, and easements

- Land for railroad ROW has not yet been acquired. Obtaining this land from the MSB should be straightforward.
- Matt LaCroix commented that the existing ROW, easements for pipelines, and transmissions are missing on the overlay. He said he's assuming these utility crossings will be analyzed. Yes, easements/ROW for existing pipelines and roads will be taken into consideration.

The meeting adjourned at 10:45am.

### Re-Cap of Action Items

- Place public workshops summaries on the project website so the agencies have an idea of what the public's concerns are.
- Schedule follow-up agency meeting for first week in December
- Phil Byrna is interested in looking at the matrix before the scoring occurs.
- Obtain additional wetlands data (Kenai Watershed Forum) and Bald Eagle data (Mary Lynn Nation).
- Make sure trails GIS layer includes RS2477 trails.

###



# Meeting Notes

Subject: U.S. Army Corps of Engineers Meeting	
Client: Alaska Railroad Corporation (ARRC)	
Project: Port MacKenzie Rail Extension	Project No: 09585-67879
Meeting Date: October 2, 2007, 10:30am-12pm	Meeting Location: HDR office, 2525 C St. Ste. 305
Notes by: HDR Alaska – JS	

## Attendees:

Brian Lindamood, ARRC  
Barb Hotchkin, ARRC  
Donna Robertson, HDR Alaska, Inc.  
Jon Schick, HDR Alaska, Inc.  
Jeff Schively, HDR Alaska, Inc.  
Skip Joy, USACE  
Serena Sweet, USACE

## Agenda

The purpose of the meeting was to discuss potential wetlands impacts and mitigation strategies.

Brian Lindamood began by presenting the project background, purpose and need, STB process and the anticipated project timeline.

## Topics Discussed

### Port MacKenzie Expansion Project

- Skip said that he'd be looking at this project and tying it in with the Port and KABATA projects when considering issuing permits. Skip had some serious issues with the Port Expansion project and it sounded like flaws in that project could hold up obtaining permits for this project.
- Brian noted that it is not the ARRC's intent to have dual purpose within their corridor. It would be used only for a rail line.

### Permitting Issues

- Skip noted that the Corps would only permit the alternative that is the least environmentally damaging. If the preferred alternative is not the least environmentally damaging, mitigation would be required to bring it to the least impacting alternative.
- The NEPA process, from the Corps standpoint, is just disclosure of the impacts and the Corps doesn't exactly follow a NEPA process. To obtain permits, the Corps could require a greater detailed analysis of wetland impacts than is required under NEPA.
- Brain asked if the practicability or constructability of alternative would play a role in the Corps' permitting of an alternative, as opposed to just approving the least environmentally damaging alternative. Skip said that yes, they consider the practicability, but they may suggest a different alternative than the one most practicable to build.
- Barb asked if a permitting level of data was required now to assist the Corps is formulating a stance on a preferred alternative. Skip said that no, general information on each alternative (i.e., general stream crossing locations, which streams to cross, etc.) is good. Skip also noted that ARRC would

need to provide specifics on each alternative, not just ARRC's preferred alternative. The ARRC needs to look into each alternative with the same level of detail.

### Wetland Maps and Ranking

- Donna explained how wetlands were categorized and ranked by their functional value and factored into the constraints analysis. Skip agreed that this was a good start and at this stage of the project, with the limiting NWI dataset, there isn't much more that we could do. Skip said he trusts the NRCS soils data better than the NWI data for depicting wetlands.

### The Corps' Role/ What ARRC needs from the Corps

- Skip noted that he would not be handling this project; Serena Sweet would be.
- Brian requested a formal letter from the Corps expressing any preliminary concerns they have about project alternatives. Statement of which alternative is better would help the application process, as far as providing the STB with as much information and agency input as possible.
- Skip noted that it is probably too early for the Corps to weigh in on which alternative is preferable.
- Skip said that they would likely issue their letter regarding the project after the official scoping process occurred for the project EIS. Skip did note that during the EIS process, the Corps does communicate with the applicant. There would be opportunities then for impact/mitigation discussions.

### Mitigation Discussion

- It was noted by the Corps that mitigation could be used to bring the preferred alternative up to the least environmentally damaging.
- Brain noted that he was not hoping to do nearly the extensive amount of fieldwork as was done for the Northern Rail project. ARRC planning on using existing wetlands data from other projects (KABATA, MSB?) to satisfy Corps' needs.
- The Corps noted that compensatory action would be the last thing to do. They encourage avoidance and minimization of impacts first.
- The Corps identified that mitigation could take many forms. Wetlands delineation and mapping of the project area, or a data gathering component, could serve as mitigation. The thought it that data gathered as mitigation for this project could benefit future development projects in the area and assist the Corps in the regulatory process.
- The Corps noted that, for economical purposes, the Railroad is already avoiding and minimizing wetlands impacts and that these measure should be fully disclosed in the environmental document in order to give credit to the Railroad.

### Mat-Su Borough Wetland Banks

- It was noted that one of the alternatives goes right through the MSB wetlands bank. The Corps noted that it was the Borough that designated them as wetlands banks and the Corps isn't entirely in agreement about those parcels. It was noted that some wetlands banks contain uplands, and that by avoiding a wetlands bank more wetlands could be impacted than if the alignment went through the bank.
- Skip said he'd talk to Jack Hewitt about the wetlands banks and the acceptableness of impacting them.

### Transmittal of Project Materials:

- Serena said that project materials on a ftp site was fine.

### Re-Cap of Action Items

- Providing Serena Sweet with project materials via a project ftp site. (this was done)
- The Corps would issue a letter with an official stance on a preferred alternative and offer mitigation strategies that could suffice. This letter may not come as soon as ARRC desires.

- Another meeting would be scheduled (in approx. 1 month?) to invite all resource agencies to discuss project specifics.



# Meeting Notes

Subject: Alaska Dept. of Fish and Game Meeting	
Client: Alaska Railroad Corporation (ARRC)	
Project: Port MacKenzie Rail Extension	Project No: 09585-67879
Meeting Date: October 3, 2007, 10:00- 11:30am	Meeting Location: HDR office, 2525 C St. Ste. 305
Notes by: HDR Alaska – JS	

## Attendees:

Brian Lindamood, ARRC  
Barb Hotchkin, ARRC  
Donna Robertson, HDR Alaska, Inc.  
Jon Schick, HDR Alaska, Inc.  
Mark Fink, ADF&G  
Tammy Massie, ADF&G

## Agenda

The purpose of the meeting was to discuss potential impacts to parks and refuges, fish, fish habitat, as well as mitigation strategies.

Brian Lindamood began by presenting the project background, purpose and need, STB process and the anticipated project timeline.

## Topics Discussed

### Using KABATA data and agency comments pertaining to this project

- Mark Fink noted there was a lot of information generated for the Knik Arm Crossing EIS. This project should take advantage of that data. Also, to keep from producing redundant comments, Mark suggested looking at ADF&G/DNR comments provided for the Knik Arm Crossing EIS as they pertain to the Pt. Mac Rail project, as many of the comments would be similar. In particular, comments given on the topic of wildlife crossings.

### ARRC's requesting of input from ADF&G

- Brian asked for guidance on fish and fish and wildlife habitat issues. Topics on what impacts ADF&G considers most important, how to assess the impacts, and any mitigation ideas would be helpful. It is ARRC's intention to provide the STB at application time a clear sense of the anticipated breadth of impacts as well as ideas on mitigation.
- Mark Fink expressed concern that ADF&G would either be asked for too much involvement or too little involvement on this project. He wanted to participate as a cooperating agency with the STB, providing input throughout the project and not just be handed a finished product by the STB.
- It was understood that this project would not involve as many agency meetings as KABATA and that schedule would be more or less determined by the STB. But, this project would entail several resource specific meetings with agencies.

## Moose

- ADF&G were asked to provide input on their moose habitat data and clarify or rank which habitat type was most critical. ADF&G expressed concerns about the data itself, as far as whether it really was compiled in 2005 or if it's data from years ago. They did not seem to have the highest confidence in the data's usefulness. It was noted that winter habitat was the most important type.
- ADF&G asked if the Railroad was planning any field work.
- ADF&G were most concerned with creating barriers to wildlife movement/migration and the long-term impacts. Brian sees the railroad less as a barrier to movement (moose can cross it fine) as a problem with increasing moose stikes/kills.
- ADF&G will likely ask for more data on moose movement and numbers as a part of this project. They would benefit from data on seasonal moose movements. This could be done by conducting radio-collar work. There was concern with the fast pace of the project and that there wouldn't be enough time to collect quality data. Of course, ADF&G noted that a study of this type would require their involvement, due to darting/handling large animals, and they don't yet know if they would even have the personnel to conduct such a study.
- Mark sees similarities in this project and the Northern Rail project. In fact, he felt this project would likely require closer scrutiny of data and anticipated impacts because of the many land uses and population numbers in the project area.
- There was discussion on the agreement that is in the works for N. Rail mitigation that provides ADF&G/UAF significant funding to conduct moose analysis. Brian asked if this agreement could serve as mitigation for this project, as it would be applicable data.
- ADF&G would talk to the Palmer office about their availability to conduct field work and acceptable mitigation strategies and if that was desired. They would decide if it could be done by a 3<sup>rd</sup> party contractor, or if ADF&G would need to be involved. Also, identification of how soon they could get started.
- Brian mentioned that funding would be much more readily available if a study was considered mitigation and not part of the EIS process. Also, in light of the fast pace of this project and limited time to conduct field work, it was noted by ARRC that the study would be best if conducted post construction for however many number of years was necessary (within reason).

#### Fish

- There was mention of a historic MOU/MOA between ADF&G and DNR relating to fish passages. Mark had some issue with that agreement. It sounded as if that memorandum was being revised. ADF&G would like to see the number of crossings reduced. Bridges were preferable to culverts. It was noted that a trestle/bridge structure works pretty well for fish passage and wildlife crossings.

#### Susitna Flats State Game Refuge

- Susitna Flats is co-managed by ADNR and ADF&G. DNR has purview over recreation lands and ADF&G has purview over refuge lands.
- ADF&G highly recommends the Refuge be avoided. They even had problems with an alignment along the edge of the Refuge. ADF&G stated that the SFSGR management plan does not encourage new roads, may even preclude it in the language.

#### Avoidance/Minimization Strategies

- ADF&G asked why ARRC could not build a rail corridor paralleling an existing road corridor. Brian explained that while it is a good idea environmentally, it isn't practicable from a safety and cost aspect.

#### Mitigation Strategies

- The notion of a land swap for mitigation seemed possible. It is unclear if the swap would be a 1:1, 3:1, etc. There would likely need to be a replacement of more refuge lands for the impacted refuge lands, i.e., replacement of lands of similar quality and similar levels of management/protection.
- Similar to an example on military lands where ARRC enhanced moose browse in some areas as mitigation to moose strikes, ARRC could agree to a similar program to mitigate impacts for this project.



- ADF&G noted that there would be no need for a MOA/MOU regarding the land swap, unless money exchanged hands for the funding of mitigation.
- ARRC also asked if they could double count mitigation, i.e., could mitigation for the Corps, for example, count towards mitigation for ADF&G. It sounded as if that was a possibility.



# Meeting Notes

Subject: Alaska Dept. Natural Resources – Parks and Outdoor Recreation	
Client: Alaska Railroad Corporation (ARRC)	
Project: Port MacKenzie Rail Extension	Project No: 09585-67879
Meeting Date: October 5, 2007, 10:30- 11:30am	Meeting Location: HDR office, 2525 C St. Ste. 305
Notes by: HDR Alaska – JS	

## Attendees:

Brian Lindamood, ARRC  
Sasha Forland, ARRC  
Donna Robertson, HDR Alaska, Inc.  
Jon Schick, HDR Alaska, Inc.  
Wayne Biessel, ADNR

## Agenda

The purpose of the meeting was to discuss potential impacts to state parks and refuges as well as mitigation strategies.

Brian Lindamood began by presenting the project background, purpose and need, STB process and the anticipated project timeline.

## Topics Discussed

### Impacting a State Recreation Area or Refuge

- Brian started by asking whether or not passing through a SRA is even an option? If avoidance of the recreation area is not feasible, the ARRC would like ideas on what could be done as well as examples of projects that have impacted state recreation areas and how DNR resolved them. Wayne noted that river corridors are important to DNR as well as to the public and that crossing one is a big deal from a resource perspective. He mentioned that cutting through a SRA is probably not an option.
- Wayne seemed most concerned with the Willow route and specifically the impacts to the Willow Creek State Recreation Area and the fact that this alternative would bisect this SRA. He noted that DNR – Parks would be aggressively opposed to crossing that recreation area.

### Potential Impacts

- Noise impacts would likely be an issue with this project. Wayne noted that the Red Shirt Lake Homeowners Assoc. (Nancy Lake SRA) is active and have voiced concerns about this project. It was suggested that some level of noise analysis be conducted for this project.
- Transporting coal (and the dust falling from the cars carrying it) can have an impact on anadromous streams. Brian mentioned that that concern can be solved by using ballast deck bridges to keep debris from falling through bridge.

## Potential Mitigation Strategies

- Brian talked about the different design techniques that can be used to decrease the amount of noise generated from a train. Usage of concrete ties and welded rails is much quieter. Also, bridges that have been designed with a concrete deck decrease noise.
- Trail impacts will be a big concern with this project. Brian mentioned the different trail crossing concepts (so far) that could be used to maintain access to trails. The 3 examples are all grade separated: (1) a culvert – which is not that good from a dog musher/snow perspective; (2) a bridge for the railroad – this is a costly procedure; and, (3) a bridge for the trail. It was mentioned that all legally-designated trails would have continuity maintained.
- The topic of a land swap for impacted park land was talked about. DNR could entertain that idea. DNR would not agree to swap less valuable uplands, for instance, for impacts to a valuable river corridor. It would be difficult to mitigate impacts to such an ecologically and recreationally important area as a river corridor.

## Public Comments

- Wayne asked about public comments regarding this project and their availability. Brian mentioned that the public comments would be made available both in a consolidated form in the alternatives evaluation as well as in their entirety in the public involvement report.

## ARRC's requesting of input from ADNR

- Brian asked for guidance (preferably within 30 days) on bridge design as it pertains to fish and boat passage. Topics on what impacts DNR considers most important, how to assess the impacts, and any mitigation ideas would be helpful. It is ARRC's intention to provide the STB at application time a clear sense of the anticipated breadth of impacts as well as ideas on mitigation.
- Wayne said that DNR Parks would consult on this project and try to get a letter out expressing their concerns prior to the upcoming large agency meeting in November.



# Meeting Notes

Subject: Alaska Dept. Natural Resources – Division of Mining, Land & Water	
Client: Alaska Railroad Corporation (ARRC)	
Project: Port MacKenzie Rail Extension	Project No: 09585-67879
Meeting Date: October 10, 2007, 1 – 2:30pm	Meeting Location: DNR office, Atwood Bldg. (downtown)
Notes by: HDR Alaska – JS	

## Attendees:

Brian Lindamood, ARRC  
Sasha Forland, ARRC  
Donna Robertson, HDR Alaska, Inc.  
Jon Schick, HDR Alaska, Inc.  
Carol Eaton, HDR Alaska, Inc.  
Tracy McDaniel, MSB  
Sam Means, ADNR-DMLW  
Clark Cox, ADNR-DMLW  
Justin Selvik, ADNR-DMLW

## Agenda

The purpose of the meeting was to begin a preliminary discussion on potential impacts to State lands, ROW acquisition procedures, and identifying additional information needed.

## Topics Discussed

### DNR-DMLW's role and responsibility

- (General Role) ADNR-DMLW is land manager for State lands and DNR-managed lands. They issue land use permits and easements on State lands. They also have somewhat of a role on agricultural parcels and some parks.
- (For this project) It is largely unknown whether or not the STB will include ADNR as a cooperating agency for this project. There is a possibility of ensuring this due to Senate Bill 31 (2004)...

### Granting easements on State lands

- Generally speaking, ADNR-DMLW doesn't find any 'show stopping' reasons for granting easements. However, should DNR-Parks&Rec say no, that would veto their ability to issue an easement.
- ADNR-DMLW rarely finds language within the various area planning documents that precludes development of a transportation corridor.
- ARRC noted that it is their policy to require fee simple acquisition (or something very close to) for land transfers. For the Northern Rail project, there was legislation passed with language that enabled ARRC to acquire state lands through fee simple acquisition. ADNR-DMLW said it would be useful to obtain an explanation of the nature of land title the ARRC is required to have.
- ADNR-DMLW noted that they only have authority of granting surface rights. The Constitution prohibits transferring of subsurface rights. There are areas where Mental Health of Alaska owns subsurface rights and DNR owns surface rights. In this instance, ARRC will need to work with Mental Health on a suitable arrangement.

### Types of activities on State lands/Trails

- ARRC asked about the myriad of uses occurring on State lands and their legal status. ADNR-DMLW pointed out that many uses (including an airstrip) do not require a permit. On State land it is legal to cut a 5'-wide trail without a permit.
- The ARRC asked about the deference paid by the State on non-permitted land activities. ADNR-DMLW noted that there is a MSB project underway to make many trails legally recognized and procuring easements.
- ARRC is willing to maintain access/connectivity to legally recognized trails. It was noted that many of the dog mushing trails are on undesignated trails. ADNR-DMLW should offer guidance on how ARRC should decide on which trails need to be recognized and which do not. ADNR-DMLW noted that significant weight will be paid to the Borough trails plan.
- ADNR-DMLW asked about resources dedicated to trails delineation/field work/trails inventory as part of the NEPA process. The ARRC will start by recognizing all legal trails, and decide where to go from there.
- ADNR-DMLW mentioned that they could selling materials (gravel, rock) for building the railroad should the ARRC need more than what's in the ROW.
- Iditarod trail – ADNR has 3 people working specifically on Iditarod easements. Cliff Larson is the ADNR-DMLW Iditarod easement manager.

### Land ownership map

- The grey area on the ownership maps (recorded as “NO DATA” in the MSB GIS dataset) was thought to be all State lands, according to ADNR-DMLW. That information does not exist in digital/GIS format. ADNR-DMLW didn't think they had that information in digital format.

### ROW research and acquisition process

- ADNR owns section line easements. All section lines have easements designated for a road and DOT has rights to that easement. ADNR ultimately owns the easement.
- For agricultural parcels, some are owned fee simple and some aren't. Each parcel's title report would need to be examined separately. The conveyances can be found on DNR's website.

### ARRC's requesting of input from ADNR-DMLW

- ARRC asked for a letter expressing concerns with this project. This letter could include perceived obstacles or any 'show stopping' information regarding each alternative.
- ADNR-DMLW will research each alternative and assess the ownership information (surface/subsurface) on State lands.

### Re-cap of Action Items

- HDR will provide ADNR-DMLW with the GIS shapefile of the proposed alternatives.
- ADNR-DMLW requested a RSA for billing some of their time to this project. Brian noted that an arrangement had been made with Don Perrin to manage one large RSA. Sam Means said he would then send Don a bill for DNR's work.



# Meeting Notes

Subject: Alaska Dept. Natural Resources – Office of Habitat Management and Permitting	
Client: Alaska Railroad Corporation (ARRC)	
Project: Port MacKenzie Rail Extension	Project No: 09585-67879
Meeting Date: October 10, 2007, 10:30- 11:30am	Meeting Location: HDR office, 2525 C St. Ste. 305
Notes by: HDR Alaska – JS	

## Attendees:

Brian Lindamood, ARRC  
Sasha Forland, ARRC  
Donna Robertson, HDR Alaska, Inc.  
Jon Schick, HDR Alaska, Inc.  
Mike Bethe, DNR-OHMP  
Tom Namtredt, DNR-OHMP

## Agenda

The purpose of the meeting was to begin a preliminary discussion on potential impacts to state parks and refuges, fish populations and habitat, and mitigation strategies.

Brian Lindamood began by presenting the project background, purpose and need, STB process and the anticipated project timeline. On November 9<sup>th</sup> there will be a large agency meeting.

## Topics Discussed

### Impacts to Fish

- DNR-OHMP spoke about a MOU with ADOT that addresses bridge structures for fish passage from an engineering perspective. They have some issues with that memorandum, but feel it is a reasonable place to start.
- For a project such as this one, DNR-OHMP will look at each proposed crossing on a case-by-case basis. They will examine each crossing individually.
- ARRC will conduct a close examination of the hydrology at each crossing and will provide a greater level of detail to DNR-OHMP on each proposed crossing before the large agency meeting on Nov. 9<sup>th</sup>.
- DNR-OHMP mentioned that cumulative impacts to fish and fish habitat should be examined for each route.

### Fish Data

- DNR-OHMP noted that baseline data doesn't exist on some (many?) of the crossings. ARRC would need to collect data on many of the streams where there are proposed crossings.
- DNR-OHMP requested good aerial photography of the proposed crossings.

### Potential Mitigation Strategies

- DNR-OHMP noted that if the bridge was designed correctly, there often times are minimal impacts to fish/fish passage.

- DNR-OHMP noted that their jurisdiction lies primarily bank to bank on the rivers and can sometimes extend to the floodplain.
- DNR-OHMP mentioned that sometimes mitigation could be accomplished by upgrading each proposed bridge structure.
- DNR-OHMP said they would restrict their input to only stream crossings.

#### ARRC's requesting of input from ADNR-OHMP

- Brian asked for guidance on what impacts DNR considers most important, how to assess the impacts, and any mitigation ideas would be helpful. It is the ARRC's intention to provide the STB at application time a clear sense of the anticipated breadth of impacts as well as ideas on mitigation.
- Additional input on the deficiencies of the MOU with DOT would be useful.
- DNR-OHMP would review the different alternatives and get back to ARRC on which one was preferable. As soon as they can issue it, a letter should be sent to ARRC regarding significant issues/concerns related to this project.



# Meeting Notes

Subject: State and Borough Trails and the Iditarod National Historic Trail	
Client: Alaska Railroad Corporation (ARRC)	
Project: Port MacKenzie Rail Extension	Project No: 09585-67879
Meeting Date: October 17, 2007, 10:00- 12:00pm	Meeting Location: DNR Lands office – Atwood Bldg.
Notes by: HDR Alaska – JS	

## Attendees:

Brian Lindamood, ARRC  
Donna Robertson, HDR Alaska, Inc.  
Jon Schick, HDR Alaska, Inc.  
Kirsten Anderson, HDR Alaska, Inc.  
Don Perrin, DNR  
Neil Shishido, DNR

Sam Means, DNR  
Ben Hagedorn, DNR  
Bruce Paulson, MSB  
Brad Swortz, MSB  
Fran Seeger-Boss, MSB

## Agenda

The purpose of the meeting was to begin a discussion on identifying and locating existing trails and determining their legal status. The latter half of the meeting focused specifically on the National Historic Iditarod Trail. Brian wanted to flesh out and prepare an official trails map to be brought to the Willow area trails meeting on Oct. 24<sup>th</sup> to show the public which trails are and are not officially dedicated, i.e., which ones will and will not have connectivity maintained by the ARRC.

## Topics Discussed

### Trails, Connectivity, & Conceptual Crossings

- It is ARRC's intention to maintain connectivity of legal trails only. They will work with the MSB and DNR to decide what to do with trails without an easement. It was understood that several trails that are not yet legally designated are recognized in the MSB Trails Plan and getting easements for those trails is underway. The MSB Trails Plan-identified trails will also have connectivity maintained. As for non-legal trails that cross the proposed alternatives, there remains the idea of either moving or removing them. But the official stance by the ARRC is that connectivity will not be maintained for these. DNR will lean heavily on the MSB for identifying which trails are to have easements implemented.
- ARRC is confident that almost all, if not all, trail crossings will be grade-separated. This primarily becomes a safety issue. With grade-separated crossings, it is easier for ARRC to build in deterrents to the public using the RR ROW. At a glance, the ARRC sees the number of crossings as "a handful."
- ARRC sees 3 potential options for trail crossings: 1. a culvert – inexpensive, snowmachiners seem to like them, but dog musher compatibility is questionable; 2. a bridge for the trail over the RR – about 10X more expensive, but easier to maintain/groom for snow conditions; and 3. spanning the RR over the trail – even more expensive.
- The ARRC envisions a mix of these types of crossings depending on the location and the predominant trail usage type.
- DNR feels that it is the project developers' role to engage the trail users' groups to discuss trail crossing arrangements. They, as the land managers, will not take on that responsibility.
- DNR agreed to provide ARRC/HDR with their list of user groups.



- There had been the suggestion from the public to dedicate a trail easement running parallel along the proposed RR ROW. The ARRC is not interested in that idea, primarily from a safety perspective. DNR agreed; they are mostly looking at maintaining east/west connectivity on trails.
- There is a possibility of shifting trail location of trails already with easements if avoidance by the RR is not feasible. DNR noted that the easement location changing process is just like the process of creating one; DNR would need to publish a public notice, but not required to hold a public meeting.
- RS2477 trails – DNR is the manager of these trails. These have a 100' easement. Most of them are not surveyed, but the vast majority of them do have easements.

#### Iron Dog Trail

- The Iron Dog trail has an easement of 50'.

#### Big Lake Trails

- The MSB is currently working towards obtaining easements of several Big Lake area trails. In general, the trails around Big Lake and Houston are on private property or Mental Health lands.

#### Iditarod Historic Trail

- This discussion focused primarily on the specific location of both the historic trail and the active race trail. Pinning down the location of the historic trail is difficult; agencies at this meeting all seemed to have their own idea of where the trail ran. Kevin Keeler noted that the section between the Knik Arm and the Susitna River has always been confusing.
- The ARRC suggested the idea of mitigation be a once-and-for-all survey of the historic trail to end the confusion.
- DNR noted that sometime in the 1970s someone surveyed the route. That then provided the location of a 400' easement.
- The historic route is often not used during the official Iditarod race due to unpredictable snow conditions. Also, Kevin Keeler noted that the dog mushers actually prefer starting in Willow to alleviate having to race through the developed area of Wasilla and Big Lake, where various trail crossings are common. The idea of combining the crossing location for both the active race trail and the historic trail may not be feasible. BLM would like to see both trails treated separately with this project. BLM also wanted to see the loops maintained.
- DNR has always been challenged with providing ample signage to show mushers where the legal trail is and to require them to stay on it. Most of the trail is remote in the area and there is no way to keep mushers on the easement, especially when snow conditions may not be suitable at that exact location.
- BLM noted that they would defer to SHPO on decisions affecting the historic trail. SHPO will take the lead on historic trails on state land, BLM will be happy just getting cc'd on all correspondences.
- BLM noted that it is permissible within the National Historic Trails Act to move the location of a historic trail.

#### Gas Line Trail

- This trail extends from Ayershire Rd. and receives significant usage in winter. The trail is mostly on a section line and is owned by ENSTAR. It is often used as an ice road in the winter by utility equipment to get over to Tyonek or other areas.

#### Trails map

- DNR-OHMP spoke about a MOU with ADOT that addresses bridge structures for fish passage from an engineering perspective. They have some issues with that memorandum, but feel it is a reasonable place to start.
- For a project such as this one, DNR-OHMP will look at each proposed crossing on a case-by-case basis. They will examine each crossing individually.

- ARRC will conduct a close examination of the hydrology at each crossing and will provide a greater level of detail to DNR-OHMP on each proposed crossing before the large agency meeting on Nov. 9<sup>th</sup>.
- DNR-OHMP mentioned that cumulative impacts to fish and fish habitat should be examined for each route.

#### Miscellaneous Topics

- DNR noted that they have in their possession an application from the MSB for a RR ROW. Brad Sworts noted that this was their initial application to the State for the RR. The original application calls for a 800' ROW. The actual ROW easement granted won't need to be this wide. It was determined that this application will remain on hold as the project progresses and may be eventually used when they get to that step.
- Ag lands – owners own agriculture rights only, state has other rights. The owner and the state must both agree to transfer land. Basically, a title by title search is necessary to determine exactly.
- It was noted that GIS data on existing trail easements of the state's is available on the ASGDC website



# Meeting Notes

Subject: SHPO – APE; Iditarod Trail; Agricultural Area; Indirect Effects	
Client: Alaska Railroad Corporation (ARRC)	
Project: Port MacKenzie Rail Extension	Project No: 09585-67879
Meeting Date: October 23, 2007, 10:00am – 11:00am	Meeting Location: DNR SHPO office – Atwood Bldg.
Notes by: HDR Alaska – LR	

## Attendees:

Brian Lindamood, ARRC  
Barb Hotchkin, ARRC  
Kirsten Anderson, HDR Alaska, Inc.  
Leslie Robbins, HDR Alaska, Inc.

Fran Seager-Boss, MSB  
Dave Gasek, DNR-SHPO  
Stephanie Ludwig, DNR-SHPO

## Agenda

The purpose of this meeting was to follow up on the comments provided to the ARRC by the Alaska Department of Natural Resources, Office of History and Archaeology in their letter, dated September 26, 2007. The main comments to discuss included the Area of Potential Effect (APE); the Iditarod National Historic Trail; the Port MacKenzie Agricultural Area and 1915 Matanuska Farm Station; and indirect effects.

## Topics Discussed

### APE

- The APE will be determined once the Surface Transportation Board (STB) comes on board and begins to lead the process.
- At some point, Stephanie said archaeological surveys will need to be done. Barb said that when the NEPA-STB process begins, they will focus on high probability areas for field surveys.

### Iditarod National Historic Trail

- SHPO recognizes that the trail cannot be avoided; it will have to be crossed.
- The exact trail location seems to be in discrepancy, based on surveys done by various agencies – DNR, BLM, etc. It seems like we're dealing with three components for trail location – the National Historical location, the actual race location, and the historical/scenic location.
- Apparently Jo Antonson at SHPO is determining the National Register eligibility for multiple Historic Trail crossings. She may have some additional information.
- Brian brought up the issue that the Iditarod Trail has been crossed by other transportation projects. Brian said the ARRC would be amenable to developing a memorandum of agreement (MOA) with the applicable agencies, similar to the MOA between FAA and SHPO for the Takotna Airport project. Interpretive panels were constructed as part of mitigation for the Takotna Airport project. ARRC would be amenable to putting up interpretive panels.
- Doug said it may be useful to look at other transportation corridor crossings in the country in which national historical trails were crossed.
- For grade-separated crossings, from a safety standpoint, the ARRC would prefer to go over the trail. Viewshed issues come into consideration with the grade-separated crossings.

### Port MacKenzie Agricultural Area

- There was some discussion regarding the 1915 Matanuska Farm Station. Apparently this project is outside of the project area and is completely different from the Point MacKenzie Agricultural Area near the Port, which apparently was established by the State in the late 1970s and 1980s.

- Doug said that the criteria would need to be applied to the Point MacKenzie Agricultural Area to determine its eligibility to be listed on the National Register. Apparently Janet Clemens (now with the Park Service) wrote a document that has information on the farming area. It would be good to get a copy of this.

#### Indirect effects

- There was some discussion regarding audible and visual impacts, and the possibility of changing the character and use of the area adjacent to the agricultural area. Barb mentioned that when it comes to rail noise, most of the noise is due to whistles at crossings. The idea is to keep the at-grade crossings limited, especially near the agricultural area. The ARRC also controls track noise by using continuously welded rail and concrete ties. The goal of the project is for the long haul movement of goods, not for spurring development (for purposes of identifying indirect effects).

#### Other

- Fran mentioned it might be a good idea to consult with the Trails Council in Knik. Joe Redington) formed the group, which is separate from the Iditarod Race group. This group may be disbanded.
- Judy mentioned that a company out of Duluth focuses on context sensitive design solutions. This is something that could be considered with the trail crossings.

Brian said he'd like to reconvene at the end of November with the applicable agencies (DNR, BLM, etc) to discuss the Iditarod Trail again.



# Meeting Notes

Subject: DNR All Departments Meeting – OHMP, DMLW, Parks&Rec	
Client: Alaska Railroad Corporation (ARRC)	
Project: Port MacKenzie Rail Extension	Project No: 09585-67879
Meeting Date: November 9, 2007, 9:00am – 11:00am	Meeting Location: DNR office – Atwood Bldg.
Notes by: HDR Alaska – JS	

## Attendees:

Brian Lindamood, ARRC  
Barb Hotchkin, ARRC  
Donna Robertson, HDR Alaska, Inc.  
Jon Schick, HDR Alaska, Inc.  
Don Perrin, ADNR

Mike Bethe, ADNR-OHMP  
Ken Bouwens, ADNR-OHMP  
Wayne Biesell, ADNR-Parks&OutdoorRec  
Sam Means, ADNR-DMLW

## Agenda

The purpose of this meeting was to follow up on the previous individual department meetings with ADNR DMLW, OHMP, and Parks & Outdoor Recreation. The main comments to discuss included the issuance of letters still being sought by ADNR-DMLW and ADNR-Parks&OR outlining their stance on significant resource issues and preference or opposition to any particular proposed route.

## Topics Discussed

### Lands (DMLW)

- Sam Means agreed that he would quickly organize DMLW's thoughts onto letterhead and issue to ARRC.
- There was discussion about the non-designated state lands in the project area, mainly the area west of Nancy Lakes. The public has some concerns about maintaining access to these areas. Sam pointed out that these state lands are not necessarily easy to dispose as they may be included in various plans. The Susitna Area Plan designates the area west of Nancy Lakes SRA as state public recreation lands and this classification allows the public to use the land and build trails without a permit. The classification may also limit the ability to easily convey ownership to the Railroad. It was pointed out that the plan could be amended to allow ROW acquisition.
- The ARRC asked about the Houston North alignment where it ties into the existing rail within the Railroad ROW and crosses a stream. Because it is already in Railroad ROW, no additional permits or special bridge design considerations would be needed.
- When acquiring ROW, the Railroad would be subject to the existing trail easements. DNR-DMLW will be providing a letter listing existing state trail easement as well as some that are recommended, but they may find more in the future to add to the list. It could be the case that there are too many section line easements that are being used as trails for the Railroad to create trail passages for them all. DNR may choose to retain a few of these section line easements, but they are not prepared to identify which ones yet.

### Coastal Management Plan and Floodplains

- The ARRC is still looking into the MSB Coastal Management Plan Enforceable Policies and the state-wide standard of 75' setback from water bodies. The Railroad is asking for clarification from DNR on how this policy applies to bridge building and whether or not they'd be able to place a pier within this distance.

- The problem with floodplains is seen as relating more to the interconnectivity of hydrologic functions than conveyance of floodplains. Cottonwood Creek was used by DNR as an example of disrupted hydrological functions due to roads and development. The Railroad pointed out that they've got good examples of bridges that do not restrict the lateral movement of overland flow due to equalization culverts.
- There was some discussion about fragmentation of aquatic habitats where proposed bridges cross anadromous streams. Title 41 (Alaska Statute 41.14.840 (Fishway Act)) requires that a structure not fragment a waterway used by fish.
- ARRC will have to plan bridges to withstand a 100-year event. OHMP is making it a standard requirement (except for DOT) that stream simulation use a 125% stream width modeling criteria.

#### Project Timeline

- Assuming that the STB begins the project in January, scoping could begin as early as March 2008. The Railroad will be advocating including ADNR as a cooperating agency. The Railroad will continue talking to agencies regarding significant issues.

#### Consultation with SHPO

- The Railroad cannot talk to SHPO about the APE; this is going to be hashed out by the STB. The discussion now revolves around that of historic landscapes.

#### OHMP letter discussion

- The Railroad is not in the position to eliminate any of the alternatives. There was discussion about the statement saying floodplains must be spanned by bridge structures/piers. Keeping bridge piers outside the floodplain is not practicable.
- All comments are noted as being preliminary, but accurate. Mike Bethe said that regarding fish passage and bridge structures, the MOU between DOT and ADF&G is a good place to start.
- Mike Bethe noted that ADF&G will have significant input on fish issues and all future meetings and correspondences should also include them.

#### Existing data vs. Gathering Additional Data

- OHMP doesn't think existing data is good enough to base specific decisions on. There would need to be much better wetlands data and fish sampling to make informed decisions. It is unlikely that the data requirements requested from the USACE would satisfy the data requirements for OHMP.
- OHMP may not require fish sampling on every little drainage. They will want a say in the fish sampling methodology.
- ARRC was inquiring into whether DNR-OHMP would ask for a project area-wide sampling measures or would they be more interested in doing finer sampling/data gathering on just one alternative-when there is a preferred selection. The letter coming from OHMP will mention that they are comfortable using existing data for narrowing down their preferred alternative and will then mention what additional data would be necessary on the alternatives that require more careful analysis.
- OHMP noted that detailed fish sampling could benefit the ARRC in that perhaps special bridge designs may not be needed on certain stream crossings if the results do not warrant it.

#### Parks & Outdoor Rec

- ARRC would like to see letter to be issued by DNR-Parks & OR explain their stance on each route. It should explain the practical realities of an alternative impacting a park or refuge. The ARRC needs to know DNR position on the proposed alternatives.
- Wayne Biesell will issue a letter by Nov. 15<sup>th</sup> to be provided to the MSB Assembly. He said it is premature to discuss mitigation before a preferred alternative is selected.

#### Other

- OHMP suggested that ADEC be involved in agency discussions. They have purview over water quality issues and often time ADEC and ADF&G comments overlap one another
- As it relates to ADEC, the Railroad has a spill response plan in place and it would be amended to include the Pt. Mac project area.



# Meeting Notes

Subject: USACE Agency meeting	
Client: Alaska Railroad Corporation (ARRC)	
Project: Port MacKenzie Rail Extension	Project No: 09585-67879
Meeting Date: November 21, 2007, 1:30 – 2:15 pm	Meeting Location: HDR office
Notes by: HDR Alaska – JS	

## Attendees:

Brian Lindamood, ARRC  
Sasha Forland, ARRC  
Donna Robertson, HDR Alaska, Inc.  
Jon Schick, HDR Alaska, Inc

Leroy Phillips, USACE  
Serena Sweet, USACE  
Mike Pochop, TNH-Hanson  
Brian Kovol, TNH-Hanson

## Agenda

The purpose of this meeting was to follow up on the October 02, 2007 initial meeting with the USACE. ARRC wanted to get early feedback on the USACE's concerns about the project, discuss wetlands and identification of wetlands for the National Environmental Policy Act (NEPA) process, and begin a discussion on acceptable potential mitigation measures.

## Topics Discussed

### General USACE Concerns

- Ms. Serena Sweet provided attendees a preliminary letter outlining the agency's broad view of the project. The letter details additional measures needed in delineating wetlands and assessing function in order to obtain a Dept of Army permit.
- In general, the USACE will be concerned with wetlands, anadromous streams, and fish habitat. In consulting with the SHPO, they will also be concerned with historic sites.

### Wetlands Data for NEPA

- USACE expressed concern about the lack of adequate baseline wetlands data. It was explained that at this stage of the project, the team has used existing data, primarily National Wetlands Inventory (NWI) data, for the project area.
- The ARRC inquired into whether or not NWI data is sufficient for a project-wide assessment of alternatives during the NEPA process or if data collection and reporting would be required for all alternatives. The USACE would like to see all alternatives carried through with the same level of baseline data.
- The USACE would not be satisfied with only NWI data. They did mention, however, that by supplementing the NWI data with soils data, aerial photography, and some ground truthing, the NWI data accuracy increases and that would suffice for the adequate baseline data for all alternatives.

### Wetlands Classification

- The USACE would like to see a HGM-type (hydrogeomorphic model approach) wetlands functional assessment for this project.
- The USACE suggested looking at the modified HGM ("HGM-light") procedures HDR used for the Chuitna Coal project for an example of the type of functional assessment appropriate for the NEPA process for this project.

### The STB and NEPA Process

- ARRC provided a brief summary of the hopeful schedule once all project materials are turned in to the STB. If all goes smoothly, the STB could begin their scoping process as early as March or April 2008. The ideal schedule would allow for only 1 field season in spring/summer of 2008.
- The USACE is required by law to be a cooperating agency during the NEPA process. It was agreed that the USACE needs should be tied into the scoping process, the sooner the better.
- It was the USACE intention to run the permitting process concurrent with the EIS.
- ARRC mentioned the Northern Rail Extension project and the MOU between the Fairbanks USACE office and the STB. The USACE said they could use this MOU as a model.

The meeting adjourned at 2:15pm.





# Meeting Notes

Subject: DOT&PF Agency meeting	
Client: Alaska Railroad Corporation (ARRC)	
Project: Port MacKenzie Rail Extension	Project No: 09585-67879
Meeting Date: December 5, 2007, 1:00 – 2:15 pm	Meeting Location: DOT&PF Central Region office
Notes by: HDR Alaska – JS	

## Attendees:

### ARRC

Brian Lindamood  
Tom Brooks

### HDR Alaska, Inc.

Donna Robertson  
Jon Schick  
Tom Brigham

### TNH-Hanson

Norm Gutcher

### KABATA

George Wuerch,  
Andrew Niemiec

### DOT&PF

Steve Horn  
Jennifer Witt

Gordon Keith  
Rob Campbell  
Kim Rice  
John Tolley  
Scott Karner

### MSB

Brad Sworts  
Joe Perkins

## Agenda

The purpose of this meeting was to introduce the Port MacKenzie Rail Extension project to Alaska Department of Transportation & Public Facilities (DOT&PF) and Knik Arm Bridge and Toll Authority (KABATA) staff. Joe Perkins and Brian Lindamood presented a PowerPoint presentation on the project background, environmental and engineering constraints mapping, and project scheduling.

## Topics Discussed

### General Project Information from Presentation

- Joe Perkins presented the project to the DOT&PF and KABATA representatives. The presentation touched on the history of the project, project team members and consultants, public involvement component, involvement of the Surface Transportation Board (STB), and project timeline.
- Joe noted that although it is not fully known yet, the STB more or less agreed this project would involve preparation of an EIS.
- All of the work done up until the delivery of the report to the STB is done in order to streamline the EIS process by getting them the necessary information early to successfully and efficiently prepare an EIS. It will provide them the breadth of concerns from the public and agencies. The environmental analysis that is being prepared for the STB is more or less the equivalent of an environmental assessment.

### *Discussions from the Q/A Session:*

### EIS Schedule

- DOT&PF staff asked about the 18 month EIS schedule and how that could happen in cooperation with the STB. Brian noted that they have requested a waiver with the STB to allow the ARRC to forego the 6 month waiting period the STB normally requires prior to starting an EIS. The Port Mac Team has hired an attorney in Washington D.C. who specializes in supporting Applicants during the NEPA process with the STB. This is standard practice during the third-party NEPA process which fosters communication between the STB and ARRC and helps maintain the project schedule.

- There was discussion of the STB's EIS process. The STB will choose a 3<sup>rd</sup> party consultant from a list of consulting firms. The 3<sup>rd</sup> party consultant will prepare the EIS at the direction of the STB. The ARRC will have limited contact with the 3<sup>rd</sup> party consultant during the EIS preparation.
- It is likely the largest potential hold-up in getting an EIS completed in 18 months is the level of public controversy. It was noted that this is why this pre-scoping work was being done—to limit the surprises and disclose as much information as possible to the public, regulatory agencies, and the STB.
- The STB does not have a 4(f) process, which could help in expediting the project. But, depending on funding for the project (e.g., Federal Railroad Administration [FRA]), 4(f) analysis may be required. The ARRC has approached the FRA about being a cooperating agency partly to ensure the section 4(f) analysis is completed as part of the NEPA process and 4(f) does not delay the project later.
- The ARRC is hoping to have all field work completed in one field season in order to adhere to the strict timeframe.

Is there a Preferred Route? Could it be a multi-purpose corridor?

- It was noted that the article in the Anchorage Daily News spoke of a preferred route, based on the matrix evaluation. To complicate the matter even more, the MSB is on record as endorsing the Willow route as presented from the 2003 reconnaissance study. Brian explained that there is no preferred alternative yet. Based on the objective evaluation of criteria within the matrix one route does in fact present less impacts. The EIS process and the STB could very well identify other routes or remove existing alternatives, so nothing is etched in stone.
- The MSB identified an 800-foot-wide multi-modal corridor for this project in the 2003 reconnaissance study. It has since been reduced to a 200-foot-wide corridor for rail only. A single purpose corridor is safer for rail and has the potential for fewer environmental impacts. From their standpoint and 20-year outlook, DOT&PF doesn't foresee the need to include an adjacent highway.

Project Funding?

- The project is being funded by a \$10 million appropriation by the State Legislature to study the rail extension and prepare an environmental document. The ARRC intends on getting the whole process completed, including the EIS, for under \$10 million.
- It is not yet known how the construction of this project will be funded.

Has there been coordination with the U.S. Army Corps of Engineers (USACE) to discuss wetland impacts?

- ARRC has been proactive with involving agencies. Discussions have already taken place with the USACE on potential wetland impacts. The MSB has also been proactive with the creation of wetland banks from which this project may draw from.

Joe and Brian concluded the presentation by asking DOT&PF to remain communicative with their plans for future transportation projects and how they may interact with or impact this rail project. Such projects could be the Wasilla Bypass project or Knik Arm Crossing.

The meeting adjourned at 2:00pm.



Port MacKenzie Rail Extension Project

Agency Meeting

550 West 7th Avenue, Suite 705

Tuesday, September 18, 2007

9:00 am

First Name	Last Name	Address	City	Zip	Email
Don	Perrin	Suite 705 550 W. 7th			don.perrin@alaska.gov
Doug	Limpinsel				Doug.Limpinsel@NOAA.GOV
Phil	Borna	605 W. 4th Ave - 61	Anch	99501	phil_borna@fws.gov
Mary Lynn	Nation	605 W. 4th Ave Rm 62	Anch	99501	mary-nation@fws.gov
MIKE	POCHOP	911 W. 8th Ave, Suite 300	ANC	99501	mpochop@hanson-inc.com
DAVE	McCOURTNEY	"	"	"	d.mccourtney@hanson-inc.com
Leslie	Robbins	HDR 2525 Ct St # 305	Anch	99503	Leslie.Robbins@hdr-inc.com
Donna	Robertson	"	"	"	Donna.Robertson@hdr-inc.com
Kimberly	Klein	333 Raspberry Rd.	ANCH	99508	Kim.klein@alaska.gov
Barbara	Hatchkin	AKRC	Anch	99501	hatchkin@akrr.com
Sasha	Forland	AKRC			forlands@akrr.com
Matthew	LaCroix	222 W. 7th Ave	Anch	99513	lacroix.matthew@epa.gov
Nina	Brudie	550 W 7th OPMP	"	99501	nina.brudie@alaska.gov
Stefanie	Ludwig	550 W 7th Suite 1310	"	99501	stefanie.ludwig@alaska.gov
CLARK	Hopp	AKRC	"		hoppc@akrr.com



Oct. 02 2007

Wetlands Meeting w/ USACE

<u>NAME</u>	<u>Company</u>	<u>Phone</u>	<u>E-mail</u>
Brian Lindamood	ARRC	265-3095	lindamoodb@arrc.ca
Sevna Sweet	COE	753-2819	Sevna.e.Sweet@ usace.army.mil
Donna Robertson	HDR	644-2127	donna.robertson@ hdrinc.com
Jon Schuck	HDR	644-2095	jon.schuck@hdrinc.com
Irvin T. Joy	COE	753-2144	irvin.t.joy@ usace.army.mil
Jeff Schively	HDR	644-2016	jeff.schively@ hdrinc.com
Barb Hotchkiss	ARRC	265-2373	hotchkiss@arrc.ca













## Port MacKenzie Rail Extension

Agency Meeting – ADNR, MSB, BLM: Trails Discussion  
October 17, 2007 Wednesday 10:00am – DNR office.

Name	Company	Phone	Email
Jon Schick	HDR	644 2015	jon.schick@hdrinc.com
Don Perrin	ADNR	265-3095	lindamath@adnr.com
Don Perrin	ADNR	269-7476	don.perrin@alaska.gov
Kevin Ardison	HDR	644 2077	kevin.ardison@hdrinc.com
Bruce Paulsen	MSB	745-9867	Bruce.Paulsen@matsegov.us
Neil Shishido	DNR	269-8564	neil.shishido@alaska.gov
Ben Hagedorn	DNR	334-2676	benjamin.hagedorn@alaska.gov
Sam Means	"	269-8548	Sam.means@alaska.gov
Donna Robertson	HDR	644-2127	donna.robertson@hdrinc.com
Kevin Keebler	BLM	267-1207	keebler@blm.gov
Fran Seager-Boss	MSB	745-9859	fseagerboss@matsegov.us
Brad Swalts	MSB		









## Port MacKenzie Rail Extension

Agency Meeting – Alaska DOT&PF

December 5, 2007 Wednesday 1:00pm – DOT&PF office

Name	Company	Phone	Email
Nancy Guelker	TWH-Hanson	213-0213	Nancy@twh-inc.com
George Wuerch	KARITA	269-6678	wuerch@alaska.net
Andrew Niemiec	KARITA	331-4490	Andrew.Niemiec@alaska.gov
Steve Korr	DOT/PF	269-0780	steve.korr@alaska.gov
Jennifer Witt	DOT/PF	269-0512	jennifer.witt@alaska.gov
George Keith	DOT/PF	269-0770	George.Keith@alaska.gov
Rob Campbell	DOT	269-0590	
Kiki Rice	DOT	269-0549	kiki.rice@alaska.gov
John Tolley	DOT	269-0520	JOHN.TOLLEY@ALASKA.GOV
Brad Swarts	MSB	746-7430	brad.swarts@matanuska.gov.us
Scott Kerner	DOT	269-0722	Scott.Kerner@alaska.gov
Tom Brooks	ARAC	265-2456	BROOKST@ARAC.COM
TOM BRIGHAM	HDR	644-2064	Tom.Bryham@hdrinc.com
Donna Robertson	HDR	644-2129	donna.robertson@hdrinc.com
Linda Moody	ARAC	265-3095	lindamod6@arac.com
Jon Schick	HDR	644-2095	jon.schick@hdrinc.com
Joe Perkins	MSB / Consultant		

## **Appendix E: Preliminary Voluntary Mitigation Measures**

The following features have been incorporated into the initial design of the proposed project in order to reduce the potential for adverse environmental effects during project construction, operations, and maintenance.

### **Natural/ Biological Resources**

The Alaska Railroad Corporation (ARRC) conducted a constraints analysis to guide alternative corridor development, and, to the extent possible, identified alternative alignment routes for consideration that would avoid or minimize impacts to the Susitna Flats and Goose Bay State game refuges, waterbodies, anadromous fish streams, high value wetland complexes (based on National Wetlands Inventory mapping), and MSB designated wetland banks.

Clearing of vegetation in preparation for construction would occur before or after the typical migratory bird nesting season as identified by the U.S. Fish and Wildlife Service (USFWS) (typically May 1 to July 15) to the extent possible to ensure compliance with the Migratory Bird Treaty Act. If clearing is required during the nesting season, a nest survey would be conducted at the USFWS would be consulted, as necessary, to identify additional compliance measures. This would also mitigate potential impacts to moose and many other mammals as it encompasses the most sensitive time when young are born.

During the bald eagle nesting season (typically March 1 through August 31), ARRC and its contractor(s) would ensure construction does not disturb bald eagles. Active nest trees would be protected by vegetative buffer zones in accordance with USFWS guidelines.

ARRC would construct an embankment wide enough to allow moose a place to retreat on one side when a train passes to reduce the potential for moose strikes.

ARRC would design and construct stream crossings that do not impede fish passage or impair the hydrologic functioning of the waterbody. Refer to additional mitigation measures below (Water Resources/Wetlands).

Construction in anadromous streams would be timed to minimize adverse effects to salmon during critical life stages. Timing windows would be incorporated into construction contract specifications for in-stream work.

During construction, temporary barricades, fencing, and/or flagging would be used in sensitive habitats to contain project-related impacts to the construction area. Staging areas would be located in previously disturbed sites to the extent practicable and not in sensitive habitat areas.

Areas disturbed during construction would be reseeded or replanted with native species within one growing season following construction to stabilize the banks and reduce the likelihood of invasive weed expansion.

### **Water Resources/ Wetlands**

ARRC would obtain all Federal permits, including the Clean Water Act Section 404 permit, required by the U.S. Army Corps of Engineers (USACE) for project related encroachment of jurisdictional waters of the U.S., including wetlands, prior to initiation of construction in these areas. Permit stipulations would be incorporated into the construction contract specifications.

ARRC would obtain coverage from EPA under the National Pollutant Discharge Elimination System General Permit for Storm Water Discharges from Construction Activities. A Stormwater Pollution Prevention Plan (SWPPP) would be prepared and implemented.

To minimize sedimentation into streams and waterways during construction, ARRC would employ best management practices as specified in the SWPPP.

To compensate for unavoidable impacts to jurisdictional wetlands, ARRC would implement compensatory mitigation negotiated as part of the USACE Section 404 permit for placement of fill in wetlands.

ARRC would use contaminant-free embankment and surface materials in construction.

ARRC would disturb the smallest area practicable around any streams and would conduct reseeded efforts to promote revegetation of disturbed areas as soon as practicable following project-related construction activities. Disturbed areas, except for the railroad embankment, would be reseeded with native vegetation to provide permanent stabilization and minimize the potential for erosion.

ARRC would design and construct culverts used for new stream crossings with a width greater than or equal to 125 percent of the width of the stream at the ordinary high water stage. The culvert grade would approximate the surrounding slope of the stream channel. Whenever possible, new culverts would be buried to approximately 40 percent of their diameter with substrate material that would remain stable at expected flood discharge rates.

ARRC would obtain necessary State permits and authorizations (e.g., Alaska Department of Natural Resources (ADNR) Fish Habitat Permit, ADF&G Special Use Permit, Coastal Zone Consistency determination). Permit stipulations would be incorporated into the construction contract specifications.

When project-related construction activities, such as culvert and bridgework, require work in streambeds, ARRC would conduct these activities, to the extent practicable, during low-flow conditions.

During construction, ARRC would prohibit project-related construction vehicles from driving in or crossing streams at other than established crossing points.

All stream crossing points would be returned to their preconstruction contours to the extent practicable and the banks would be reseeded or replanted with native species within one growing season following construction to stabilize the banks and reduce the likelihood of invasive weed expansion.

ARRC would coordinate with the local MSB Floodplain Administrator to ensure that new project-related stream and floodplain crossings are appropriately designed. For crossings within the mapped 100-year flood plain, ARRC would design drainage crossing structures to pass a 100-year flood. These crossings would comply with MSB floodplain management regulations and permit conditions, and would not raise the backwater surface elevation by more than 1 foot.

ARRC would design and construct the new rail line in such a way as to maintain natural water flow and drainage patterns to the extent practicable. This would include placing equalization culverts through the embankment as necessary to maintain existing drainage patterns, prevent impoundment of water or excessive drainage, and maintain the connectivity of floodplains and wetlands.



Depending on the alignment selected, ARRC would obtain a Section 9 Bridge Permit from the U.S. Coast Guard for construction of new rail bridges over navigable rivers (e.g., Willow Creek and Little Susitna River). Permit stipulations would be incorporated into the construction contract specifications.

During construction, ARRC would install appropriate BMPS within its parallel drainage ditches that are within 1,000 feet of perennial waters to provide stormwater retention and filtration. ARRC would maintain drainage ditches as necessary (e.g., by removing accumulated sediments to maintain storm water retention capacity and function).

## **Land Use**

### *General Land Use*

Land areas that are directly disturbed by project-related construction and are not owned by the ARRC (such as temporary access roads, haul roads, and crane pads) would be restored to their original condition, as may be reasonably practicable, upon completion of construction.

ARRC would require contractor(s) to dispose of waste generated during project-related construction activities in accordance with applicable Federal, State, and local regulations.

ARRC would continue to participate in regional land use planning efforts to coordinate with MSB planners and other participants.

### *Community Outreach*

Prior to initiation of construction activities related to this project, and for a period of one year following start-up of operations on the new rail line, ARRC would establish a Community Liaison to consult with affected communities, businesses, and agencies; develop cooperative solutions to local concerns; be available for public meetings; and conduct periodic public outreach. ARRC would provide the name and phone number of the Community Liaison to mayors and other appropriate local officials in each community through which the new rail line passes.

ARRC would continue its ongoing community outreach efforts by maintaining a website about the project throughout the period of construction of the new line.

### *Residential*

Project-related construction vehicles, equipment, and workers would not access work areas by crossing residential properties without the permission of the property owners.

In the unlikely event of any inadvertent damage, ARRC would work with affected landowners to appropriately redress any damage to each landowner's property caused by project-related construction activities.

### *Business and Industrial*

Project-related construction vehicles, equipment, and workers would not access work areas by crossing business or industrial areas, including parking areas or driveways, without advance notice to the business owner.

In business and industrial areas, project-related equipment and materials would be stored in established storage areas or on ARRC property. Parking of equipment or vehicles, or storage of materials along driveways or in parking lots, is prohibited unless agreed to by the property owner.

ARRC would work with affected businesses or industries to appropriately address project related construction activity issues affecting any business or industry.

To the extent practicable, ARRC would ensure that entrances and exits for businesses are not obstructed by project-related construction activities, except as required to move equipment.

### *Utility Corridors*

ARRC would make reasonable efforts to identify all utilities that are reasonably expected to be materially affected by the proposed construction within the right-of-way (ROW) or that cross the ROW. ARRC would consult with utility owners during design and construction so that utilities are protected during project-related construction activities. ARRC would notify the owner of each such utility identified prior to project-related construction activities and would coordinate with the owner to minimize damage to utilities.

ARRC would make reasonable efforts to minimize the utility disruptions by timing construction work and outages to low use time periods. ARRC would notify residents and other utility customers in advance of construction activities requiring temporary service interruptions.

### **Geology and Soils**

ARRC would limit ground disturbance to only the areas necessary for project-related construction activities.

During earthmoving activities, ARRC would remove topsoil and segregate it from subsoil. ARRC would also stockpile topsoil for later application during reclamation of disturbed areas along the ROW. ARRC would use appropriate erosion control measures to minimize the potential for erosion of stockpiles.

ARRC would restore disturbed areas as soon as practicable after construction ends along a particular stretch of rail line. The goal of restoration would be the rapid and permanent reestablishment of native ground cover on disturbed areas. If weather or season precludes the prompt reestablishment of vegetation, ARRC would use temporary erosion control measures (such as mulching or erosion control blankets) until reseeded can be completed.

Prior to initiating project-related construction activities, ARRC would consult with the local offices of the Natural Resource Conservation Service and the Palmer Plant Center to develop an appropriate plan for restoration and revegetation of disturbed areas (including appropriate seed mix specifications). This would apply to areas that cannot be revegetated using natural recruitment from the native seed sources in the stockpiled topsoil.

ARRC would, to the extent practicable, revegetate the bottom and sides of the drainage ditches using natural recruitment from the native seed sources in the stockpiled topsoil or a native seed mix.

### **Recreation/ Public Access**

ARRC conducted a constraints analysis and to the extent possible, identified alternative alignment routes for consideration that would avoid or minimize impacts to the Willow Creek State Recreation Area, the Nancy Lakes State Recreation Area, and the Little Susitna Recreation River.

Depending on the alignment selected, ARRC would coordinate with the U.S. Coast Guard to provide adequate clearances for navigation of recreational boats on navigable rivers (e.g., Willow Creek and Little Susitna River).

Depending on the alignment selected, during construction of the new railroad bridge crossing over navigable rivers, some short-term temporary restrictions of watercraft traffic could occur for safety purposes. ARRC would install warning devices to notify boaters of project-related bridge construction activities. Signs providing the name, address, and telephone of a contact person would be displayed on-site to assist motorists and waterway users in obtaining immediate responses to questions and concerns about project activities.

Public access would be maintained to and from legally authorized trails and MSB recognized trail easements. ARRC would provide separated grade crossing locations where the new rail line crosses these trails, although some trails may require some realignment to consolidate crossings. This would ensure the public can cross through the embankment to access public use areas. ARRC would work with trail user groups to design and construct these separated grade trail crossings.

ARRC would work with the Bureau of Land Management; ADNR Division of Mining, Land, and Water; the State Historic Preservation Officer (SHPO); The Iditarod Trail Sled Dog Race committee; and the non-profit organization of the Iditarod National Historic Trail to design and construct a separated grade crossing at the Iditarod National Historic Trail.

### **Transportation and Crossings**

ARRC would consult with appropriate State and local transportation agencies to determine the final design and other details of grade-crossing warning devices. Implementation of all grade-crossing warning devices on public roadways would be subject to the review and approval of the Alaska Department of Transportation & Public Facilities and/or the MSB Department of Transportation.

During construction of grade crossings, road users would be notified of temporary road closings and other construction-related activities, so alternate routes can be planned. When practicable, ARRC would provide for detours and associated signage, as appropriate, or maintain at least one open lane of traffic at all times to allow for the quick passage of emergency and other vehicles. Signs providing the name, address, and telephone number of a contact person would be displayed on-site to assist the public in obtaining immediate responses to questions and concerns about project activities.

To the extent practicable, ARRC would confine all project-related construction traffic to a temporary access road within the ROW or established public roads. Where traffic cannot be confined to temporary access roads or established public roads, ARRC would make necessary arrangements with landowners to gain access from private roadways. Any temporary access roads constructed outside the rail line ROW would be removed and restored upon completion of construction unless otherwise agreed to with the landowners.

ARRC would coordinate with MSB Port MacKenzie personnel regarding activities occurring within the port district and to ensure that track construction activities conducted by the ARRC and MSB are compatible.

### **Air Quality**

To minimize fugitive dust emissions created during project-related construction activities, ARRC would implement appropriate fugitive dust suppression controls, such as spraying water or other approved measures. ARRC would also operate water trucks on haul roads as necessary to reduce dust.

ARRC would work with its contractor(s) to make sure that construction equipment is properly maintained and that mufflers and other required pollution-control devices are in working condition in order to limit construction-related air emissions.

### **Noise and Vibration**

ARRC would work with its construction contractor(s) to minimize, to the extent practicable, construction-related noise disturbances near residential areas. Construction and maintenance vehicles would be in good working order with properly functioning mufflers to control noise.

ARRC would use continuously welded rail and rail lubricants, as appropriate, on the newly constructed line in order to reduce wheel/rail wayside noise.

### **Cultural/ Archaeological Resources**

ARRC would develop protocols to inform and prepare construction supervisors of the importance of protecting archaeological resources, graves, and other cultural resources and how to recognize and treat the resources.

Should construction activities unearth any cultural or archaeological resources, construction would be halted in the immediate area, and coordination and consultation would commence with the Knik Tribal Council, MSB Cultural Resources Division and the SHPO. The protocol and contingency plan would outline the appropriate methods of documentation and procedures.

### **Emergency Response**

At least one month prior to initiation of construction activities in the area, ARRC would provide the information described below regarding project-related construction of the new rail line, as well as any additional information, as appropriate, to fire departments and the Local Emergency Planning Commissions (LEPCs) for communities within the project area

- The schedule for construction throughout the project area, including the sequence of construction of public grade crossings and approximate schedule for these activities at each crossing.
- A number for ARRC's contact, who would be available to answer questions or attend meetings for the purpose of informing emergency-service providers about the project construction and operation.
- Revisions to this information, including changes in construction schedule, as appropriate.

Before the start of operations, ARRC would contact the LEPCs to provide them with information concerning the proposed operations to allow the LEPCs to incorporate the information into local response plans.

For each of the public grade crossings on the new and existing rail line, ARRC would provide and maintain permanent signs prominently displaying both a toll-free telephone number and a unique grade-crossing identification number in compliance with Federal Highway Regulations (23 CFR Part 655). The toll-free number would be answered 24 hours per day by ARRC personnel.

### **Hazardous Materials/ Potential Spills**

Prior to initiating any project-related construction activities, ARRC or its contractor(s) would develop a spill prevention plan for petroleum products or other hazardous materials during construction activities. The plan would include a requirement to conduct daily inspections of equipment for any fuel, lube oil, hydraulic, or antifreeze leaks. If leaks are found, ARRC would require the contractor(s) to immediately remove the equipment from service and repair or replace it.

Standard spill-prevention measures would be implemented during construction and spill clean-up equipment (e.g., oil-absorbent pads) would be available onsite.

ARRC would incorporate the new rail line into the existing ARRC Emergency Response process and would update its Oil Spill Contingency Plan to include the new rail line.

In accordance with ARRC's Oil Spill Contingency Plan, ARRC would make the required notifications to the appropriate Federal and State environmental agencies in the event of a reportable hazardous materials release. ARRC would work with the appropriate agencies such as the Alaska Department of Environmental Conservation, EPA and USFWS to respond to and remediate releases with the potential to affect sensitive habitats such as wetlands.

ARRC would continue its ongoing efforts with community officials to identify the public emergency response teams located in the project area and would provide, upon request, hazardous material training.

### **Seismic Considerations**

The project would be designed and take into account the region's potential for earthquake activity to mitigate potential damage to bridges and tracks. Separated grade crossings would be designed in accordance with the latest applicable seismic codes.

## **APPENDIX F: ROAD CROSSINGS HAZARD INDEX CALCULATIONS**

Although not used as an input to the evaluation matrix presented in Chapter 5, calculations were performed to determine the composite grade crossing hazard index for each alternative. The index values obtained intended for comparison of the alternatives.

### **F.1 Assumptions for Calculations**

The hazard index calculations were based on the following basic assumptions:

- Road ownership was determined by records from the Matanuska-Susitna Borough and a Federal Railroad Administration grade crossing inventory website (<http://safetydata.fra.dot.gov/OfficeofSafety/NewCrossing/Default.asp>).
- Annual Average Daily Traffic (AADT) was determined where possible from the ADOT&PF 2005 traffic map and the Matanuska-Susitna Borough 2006 traffic map.
- Where AADT was not available from agency traffic maps, TNH-Hanson, LLC estimated values, utilizing aerial photography to identify land usage and structures.
- Train traffic is initially estimated by ARRC to be 2 trains per day.
- The two trains per day shown for the crossings on the existing ARRC mainline are the increase in trains attributed to this project above those currently using those crossings.

### **F.2 Calculation Methodology**

A hazard index for each crossing was calculated using the New Hampshire Method as adopted by ADOT&PF, as the product of the roadway Annual Average Daily Traffic, trains per day, and a crossing protection factor. The crossing protection factor is defined as 1.00 for crossbuck signage, 0.90 for crossbucks with stop signs, 0.20 for crossbucks with automatic flashers, and 0.11 for crossbucks with automatic flashers and gates. Crossings identified by ARRC that would be grade separated are assigned a crossing protection factor of zero, appropriately resulting in a zero hazard index for that crossing.

The calculations at each crossing are presented in Table F-1 on the following page. Although these calculations were not used in the matrix evaluation in Chapter 5, they provide additional data regarding the relative traffic impacts of the various alternatives. The index values provide a comparison of the probability for train-vehicle collisions. Higher index values point to a higher probability of collisions.

*(This page intentionally left blank.)*

Table F-1: Hazard Index

Segment	Milepost	Road Name	Owner	Type of Crossing	Type of Protection	AADT	Date of Traffic Count	Data Source	Trains per Day (ADTT)	Mac West, Conn 1, Willow		Mac West, Conn 1, Houston, Houston North		Mac West, Conn 1, Houston, Houston South		Mac West, Conn 2, Big Lake		Mac East, Conn 3, Willow		Mac East, Conn 3, Houston, Houston North		Mac East, Conn 3, Houston, Houston South		Mac East, Big Lake	
										Crossing Protection Factor (P)	DOT/PF Hazard Index	Crossing Protection Factor (P)	DOT/PF Hazard Index	Crossing Protection Factor (P)	DOT/PF Hazard Index	Crossing Protection Factor (P)	DOT/PF Hazard Index	Crossing Protection Factor (P)	DOT/PF Hazard Index	Crossing Protection Factor (P)	DOT/PF Hazard Index	Crossing Protection Factor (P)	DOT/PF Hazard Index	Crossing Protection Factor (P)	DOT/PF Hazard Index
Mac West	MW 5.7	S. Guernsey Road	MSB	At-Grade	Crossbucks	<100	Estimated Traffic		2	1.00	200	1.00	200	1.00	200	1.00	200	NA	NA	NA	NA	NA	NA	NA	NA
Mac East	ME 4.9	Baker Farm Road	Private	At-Grade	Crossbucks	<100	Estimated Traffic		2	NA	NA	NA	NA	NA	NA	NA	NA	1	200	1.00	200	1.00	200	1.00	200
Mac East	ME 9.8	W. Holstein Avenue	MSB	At-Grade	Crossbucks	<100	Estimated Traffic		2	NA	NA	NA	NA	NA	NA	NA	NA	1	200	1.00	200	1.00	200	1.00	200
Conn 1	C1- 2.4	Little Su River Road	MSB	At-Grade	Crossbucks	148	2005	MSB	2	1.00	296	1.00	296	1.00	296	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA
Conn 2	C2- 2.3	S. Guernsey Road	MSB	At-Grade	Crossbucks	<100	Estimated Traffic		2	NA	NA	NA	NA	NA	NA	1.00	200	NA	NA	NA	NA	NA	NA	NA	NA
Conn 3	C3- 0.7	Ayrshire Avenue	MSB	At-Grade	Flashing Lights/Gates	555	2005	MSB	2	NA	NA	NA	NA	NA	NA	NA	NA	0.11	122	0.11	122	0.11	122	NA	NA
Conn 3	C3- 1.0	W. Carpenter Lake Road	MSB	At-Grade	Crossbucks	56	2005	MSB	2	NA	NA	NA	NA	NA	NA	NA	NA	1	112	1.00	112	1.00	112	NA	NA
Willow	W 19.8	W. Deshka Landing Road	MSB	At-Grade	Crossbucks	159	2005	MSB	2	1.00	318	NA	NA	NA	NA	NA	NA	1	318	NA	NA	NA	NA	NA	NA
Willow	W 23.4	Willow Creek Parkway	MSB	At-Grade	Crossbucks	380	2005	ADOT	2	1.00	760	NA	NA	NA	NA	NA	NA	1	760	NA	NA	NA	NA	NA	NA
Willow	W 25.9	Parks Highway	State of Alaska	Grade Separation	Hwy over RR	NA	NA	NA	NA	0.00	0	NA	NA	NA	NA	NA	NA	0	0	NA	NA	NA	NA	NA	NA
Houston	H 3.3	W. Susitna Parkway	MSB	At-Grade	Crossbucks	352	2007	MSB	2	NA	NA	1.00	704	1.00	704	NA	NA	NA	NA	1.00	704	1.00	704	NA	NA
Houston	H 4.0	W. Papoose Twins Road	MSB	At-Grade	Crossbucks	157	2005	MSB	2	NA	NA	1.00	314	1.00	314	NA	NA	NA	NA	1.00	314	1.00	314	NA	NA
Houston South	HS 5.0	W. Millers Reach Road	City of Houston	At-Grade	Crossbucks	<150	Estimated Traffic		2	NA	NA	NA	NA	1.00	300	NA	NA	NA	NA	NA	NA	1.00	300	NA	NA
Big Lake	B 0.8	S. Burma Road	MSB	At-Grade	Flashing Lights/Gates	611	2005	MSB	2	NA	NA	NA	NA	NA	NA	0.11	134	NA	NA	NA	NA	NA	NA	0.11	134
Big Lake	B 3.2	Homestead Road	MSB	At-Grade	Crossbucks	<100	Estimated Traffic		2	NA	NA	NA	NA	NA	NA	1.00	200	NA	NA	NA	NA	NA	NA	1.00	200
Big Lake	B 5.0	Homestead Road	MSB	At-Grade	Crossbucks	<100	Estimated Traffic		2	NA	NA	NA	NA	NA	NA	1.00	200	NA	NA	NA	NA	NA	NA	1.00	200
Big Lake	B 11.1	S. Larrys Lane	MSB	At-Grade	Crossbucks	<100	Estimated Traffic		2	NA	NA	NA	NA	NA	NA	1.00	200	NA	NA	NA	NA	NA	NA	1.00	200
Big Lake	B 14.0	W. Hollywood Road	State of Alaska	At-Grade	Flashing Lights/Gates	1426	2005	ADOT	2	NA	NA	NA	NA	NA	NA	0.11	314	NA	NA	NA	NA	NA	NA	0.11	314
Big Lake	B 15.8	W. Big Lake Road	State of Alaska	Grade Separation	RR over Hwy	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	0.00	0	NA	NA	NA	NA	NA	NA	0.00	0
Big Lake	B 16.4	W. Calonder Way	City of Houston	At-Grade	Crossbucks	<50	Estimated Traffic		2	NA	NA	NA	NA	NA	NA	1.00	100	NA	NA	NA	NA	NA	NA	1.00	100
Big Lake	B 16.7	W. Larae Road	City of Houston	At-Grade	Crossbucks	<100	Estimated Traffic		2	NA	NA	NA	NA	NA	NA	1.00	200	NA	NA	NA	NA	NA	NA	1.00	200
Big Lake	B 17.1	Parks Highway	State of Alaska	Grade Separation	Hwy over RR	NA	NA	NA	NA	NA	NA	NA	NA	NA	NA	0.00	0	NA	NA	NA	NA	NA	NA	0.00	0
Existing ML	171.3	Cheri Lake Drive	City of Houston	At-Grade	Flashing Lights/Gates	<200	Estimated Traffic		2	NA	NA	NA	NA	NA	NA	0.11	44	NA	NA	NA	NA	NA	NA	0.11	44
Existing ML	173.0	Parks Highway	State of Alaska	Grade Separation	RR over Hwy	NA	2006	ARRC	NA	NA	NA	NA	NA	NA	NA	0.00	0	NA	NA	NA	NA	NA	NA	0.00	0
Existing ML	180.0	N. Lynx Lake Road	MSB	At-Grade	Crossbucks/Stop Signs	<100	Estimated Traffic		2	NA	NA	0.90	180	0.90	180	0.90	180	NA	NA	0.90	180	0.90	180	0.90	180
Existing ML	180.8	W. Twitty Avenue / Nancy Lake Landing	MSB	At-Grade	Crossbucks/Stop Signs	<100	Estimated Traffic		2	NA	NA	0.90	180	0.90	180	0.90	180	NA	NA	0.90	180	0.90	180	0.90	180
Existing ML	182.6	Parks Highway	State of Alaska	Grade Separation	RR over Hwy	NA	2006	ARRC	NA	NA	NA	0.00	0	0.00	0	0.00	0	NA	NA	0.00	0	0.00	0	0.00	0
Existing ML	185.5	Willow Station Road	MSB	At-Grade	Flashing Lights/Gates	395	2005	ADOT	2	NA	NA	0.11	86.9	0.11	86.9	0.11	86.9	NA	NA	0.11	86.9	0.11	86.9	0.11	86.9
Existing ML	186.9	Willow Fishhook Road	State of Alaska	At-Grade	Flashing Lights/Gates	710	2005	ADOT	2	NA	NA	0.11	156.2	0.11	156.2	0.11	156.2	NA	NA	0.11	156.2	0.11	156.2	0.11	156.2
<b>HAZARD INDEX:</b>											<b>1574</b>		<b>2117</b>		<b>2417</b>		<b>2395</b>		<b>1712</b>		<b>2255</b>		<b>2555</b>		<b>2395</b>

Notes:

- Crossing data from FRA website -- <http://safetydata.fra.dot.gov/OfficeofSafety/NewCrossing/Default.asp>
- The State of Alaska and the MSB both own portions of Hollywood Road and Big Lake Road.
- The City of Houston and the MSB both own portions of Millers Reach Road
- Guernsey Road, Baker Farm Road, and Holsetin Avenue did not appear on MSB or ADOT traffic maps.
- Teeka Run Circle and Lynx Lake Road primitive/unimproved road. No traffic data is available.
- Millers Reach Road, Larrys Lane, Calonder Way, Larae Road, Cheri Lake Drive, and Twitty Ave/Nancy Lake Landing are minor roads. No traffic data is available.
- AADT numbers for Little Su River Road, Carpenter Lake Road, Deshka Landing Road, Papoose Twins Road, and Burma Road were estimated from closest available data points.



## **APPENDIX G: HYDRAULIC CONCEPTUAL DESIGN**

Although not used as a direct input to the evaluation matrix presented in Chapter 5, the hydraulic conceptual design described below was used to generate a portion of the conceptual construction costs for each of the alignment alternatives. The intent of this design was to ascertain an approximate construction cost for whatever hydraulic conveyance structures final design of the selected alternative might call for. Structure types and sizes described herein are preliminary and may be refined as the project design progresses. The general methodology for the hydraulic design process was broken down into five steps:

1. Identification of mapped stream and river crossings and delineation of their drainage areas
2. Calculation of design flows for mapped stream crossings
3. Sizing of major bridges for mapped streams and rivers
4. Sizing of culverts and drainage structures for mapped streams
5. Identification of un-mapped minor drainages and inclusion of additional quantities

### **G.1 Water Crossing Identification and Drainage Area Delineation**

Stream and river crossings to be sized for flow were identified from data obtained from the Matanuska-Susitna Borough's GIS Division based on tax parcel maps and orthoimagery. Once crossing locations were identified, their drainage areas were delineated using ESRI's ArcHydro program. After computing flow directions based on a USGS 2 arc-second (30 meter) digital elevation map, a flow accumulation grid was obtained for the project area. The same program was then used to delineate the drainage area of each crossing location based on the flow direction and accumulation patterns. Drainage divides located according to this method can be somewhat unreliable in some of the flat, poorly drained areas without a denser, highly accurate digital elevation model. Therefore, the computer-generated delineations were subsequently checked and refined using USGS digital topographic quadrangle maps. A map illustrating the twenty-nine crossing locations (listed in Table G-1) and drainage area delineations can be seen in Figure G-1, attached at the end of this appendix. The delineations used for this preliminary analysis are the most accurate possible without more extensive survey. The presence of lake inlets and outlets or stream courses located by future field investigations may warrant modification of the boundaries used here.

**Table G-1 Drainage Area Properties**

Milepost	Total Area (sq mi)	Storage Areas	Storage Area (sq mi)	Storage % Total Area	Annual Precip. (in)	Q-100 (cfs)	Q-100 Flow Sized Structure	Maximum Culvert Flow (cfs)
MW-5.0	4.08	Freshwater Pond Lake	0.03 0.36	9.5%	20	91.1	48-in Culvert	98.7
MW-11.3	0.91	Freshwater Pond Lake	0.02 0.06	9.2%	20	25.8	36-in Culvert	45.0
ME-4.4	1.66	Freshwater Pond Lake	0.02 0.36	23.0%	20	35.1	36-in Culvert	45.0
C1-2.6	5.96	Freshwater Pond Lake	0.03 0.22	4.1%	20	148.7	72-in Culvert	272.5
W-0.6 (Little Susitna River)	295.5	Freshwater Pond Lake	1.53 8.90	3.5%	28	5891.8	Bridge	NA
W-10.0	32.14	Freshwater Pond Lake	0.52 4.23	14.8%	20	474.3	Drainage Structure	NA
W-14.4	0.75	Freshwater Pond Lake	0.00 0.00	0.0%	20	37.8	36-in Culvert	45.0
W-16.7	5.4	Freshwater Pond Lake	0.10 0.70	14.8%	20	104.9	72-in Culvert	272.5
W-20.9	0.86	Freshwater Pond Lake	0.00 0.00	0.2%	20	40.6	36-in Culvert	45.0
W-24.0 (Willow Creek)	255.57	Freshwater Pond Lake	0.92 2.03	1.2%	27	5946.0	Bridge	NA
MP-189.0 (Existing Bridge)	12.54	Freshwater Pond Lake	0.07 0.00	0.6%	24	448.4	Bridge	NA
MP-190.3 (Existing Bridge)	5.43	Freshwater Pond Lake	0.03 0.04	1.3%	25	212.3	Bridge	NA
H-0,8	25.06	Freshwater Pond Lake	0.32 1.64	7.8%	20	440.9	Drainage Structure	NA
H-4.3	2.14	Freshwater Pond Lake	0.03 0.42	21.0%	20	44.4	36-in Culvert	45.0
H-6.3	17.35	Freshwater Pond Lake	0.07 1.69	10.2%	20	305.3	Drainage Structure	NA
H-9.6	2.81	Freshwater Pond Lake	0.01 0.22	8.3%	20	68.4	48-in Culvert	98.7
HN-3.2 (Little Susitna River)	202.54	Freshwater Pond Lake	0.44 2.02	1.2%	32	5787.4	Bridge	NA
HN-5.7	21.44	Freshwater Pond Lake	0.31 1.44	8.2%	20	382.4	Drainage Structure	NA
HS-1.0	0.77	Freshwater Pond Lake	0.00 0.00	0.0%	20	38.6	36-in Culvert	45.0
MP-174.3 (Existing Bridge)	160.41	Freshwater Pond Lake	0.08 0.39	0.3%	35	5798.3	Bridge	NA
B-6.4	13.38	Freshwater Pond Lake	0.09 1.34	10.7%	20	242.6	72-in Culvert	272.5
B-9.0	118.04	Freshwater Pond Lake	1.27 11.15	10.5%	20	1535.5	Drainage Structure	NA

(Table G-1 Cont.)

Milepost	Total Area (sq mi)	Storage Areas	Storage Area (sq mi)	Storage % Total Area	Mean Annual Precip. (in)	Q-100 (cfs)	Q-100 Flow Sized Structure	Maximum Culvert Flow (cfs)
B-14.3	0.23	Freshwater Pond Lake	0.03 0.00	11.6%	20	7.7	24-in Culvert	14.5
B-14.8	1.09	Freshwater Pond Lake	0.07 0.00	6.1%	20	38.6	36-in Culvert	45.0
B-15.1	0.56	Freshwater Pond Lake	0.00 0.00	0.0%	20	29.5	36-in Culvert	45.0
B-15.2	15.86	Freshwater Pond Lake	0.11 0.60	4.5%	20	334.4	Drainage Structure	NA
B-15.9	46.01	Freshwater Pond Lake	0.38 2.57	6.4%	20	767.6	Drainage Structure	NA
B-16.6	2.12	Freshwater Pond Lake	0.00 0.21	10.1%	20	51.7	48-in Culvert*	98.7
B-17.4	1.75	Freshwater Pond Lake	0.00 0.21	12.3%	20	42.1	36-in Culvert*	45.0

## NOTES:

From Mannings Equation the maximum flow (full pipe flow) for the corresponding sizes of corrugated metal pipes (Manning's N=.023) at 1% slope and a head to a depth of 1.5 X the culvert diameter (AREMA Manual for Railway Engineering 2006 Section 4.8.2) (calculated using FHWA's HY-8 culvert design software)

24-inch: 14.5 cfs

36-inch: 45 cfs

48-inch: 98.7 cfs

72-inch: 272.5 cfs

Pipe sizings were designed for the 100 year event (AREMA Manual for Railway Engineering 2006 Section 4.8.2) for each drainage area as calculated by the USGS regression equations published in USGS Water-Resources Investigation Report 03-4188

Total Drainage Areas were calculated using ArchHydro watershed delineation of USGS 2-arc second DEM's as well as USGS quad maps

Areas of lakes and ponds within each drainage area were taken from USFWS NWI quad maps

Drainage Structures will be determined during the final design process and multi-plate culverts; pre-cast arches; and single or multiple short span bridges

Anadromous streams, highlighted in blue, are currently sized for flow; however, sizing for fish passage will also need to be performed for final design.

\* - Conveyance structures within 1500' of trail crossings were sized up to drainage structures in Table G-2

## **G.2 Design Flow Calculation**

The design flow used to size hydraulic structures for mapped streams was calculated for the 100-year event, as recommended by the American Railway Engineering and Maintenance-of-Way Association (AREMA Manual for Railway Engineering 2006). For each drainage area, the 100-year flow was calculated by the USGS regression equations published in USGS Water-Resources Investigation Report 03-4188. The regression equations for the project area take into account total drainage area, the percent area of water storage within each drainage area, and the mean annual precipitation. The drainage areas were first computed from the GIS drainage delineations. The areas of lakes, ponds, and rivers within each drainage area were then computed by intersecting each GIS drainage area with digital USFWS NWI quadrangle maps. Finally, the mean annual precipitation for each drainage area was computed using digital annual precipitation data obtained from the USGS Water Resources Department. The input values and results of these calculations for the twenty-nine water crossings analyzed can be seen in Table G-1. Unmapped minor drainages, wetland equalization culverts, and additional culverts listed in subsequent tables have not yet been sized for flow. This will be completed during the final design process once an alignment has been selected.

## **G.3 Bridge Sizing**

Of the crossing locations analyzed, three were designated as major bridge crossings. These included the Willow alignment over Willow Creek (Willow), the Houston North alignment over the Little Susitna River (Little Susitna-Miller), and the Willow alignment over the Little Susitna River (Little Susitna-Berma). Three additional bridges are shown in Table G-1 which will be sized to match existing bridges on the ARRC Mainline. For the new bridge locations, a HEC-RAS 3.1.3 hydraulic model was created in order to calculate the minimum opening, or maximum contraction, that would result in no more than a 1-foot rise in the headwater upstream of the bridge for the 100-year event.

For each of these models, channel and overbank cross-sections were surveyed at the approximate bridge location, as well as at 50-, 100-, and 500-feet upstream and downstream. A typical cross section layout is shown in Figure G-2. The seven cross-sections for each bridge location were then entered into each model according to the schematic diagram below in Figure G-3. The HEC-RAS river stationing increases from downstream to upstream.

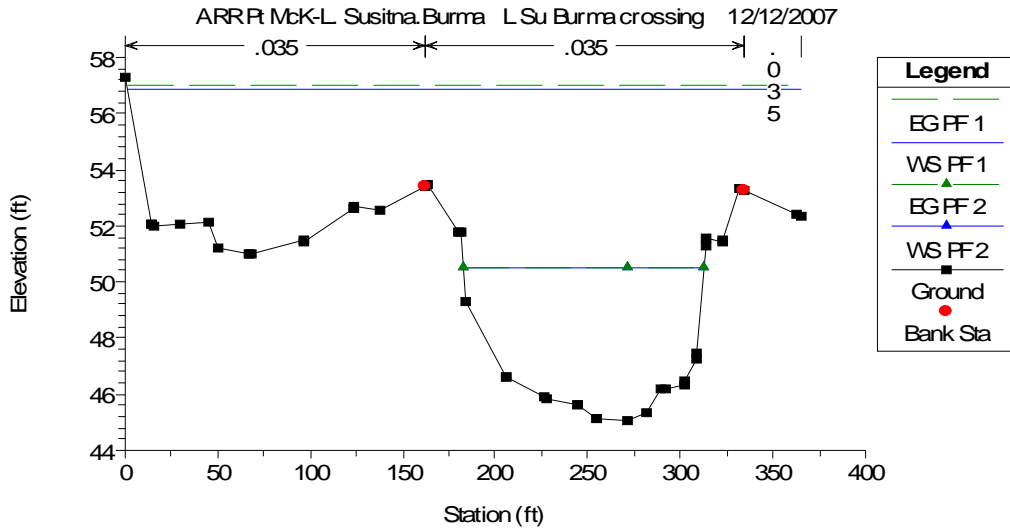


Figure G-2 HEC-RAS Typical Cross-Section

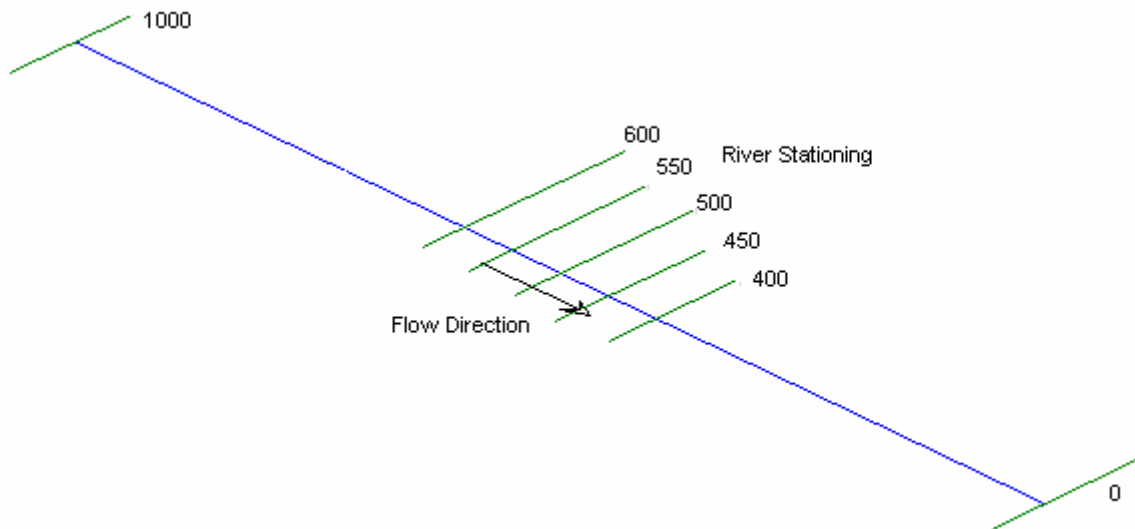


Figure G-3 HEC-RAS Model Schematic

The boundary conditions were specified as normal flow with the energy slopes upstream and downstream of  $S(\text{Willow}) = 0.0015$ ;  $S(\text{Little Susitna-Miller}) = 0.001$ ; and  $S(\text{Little Susitna-Burma}) = 0.00046$ . The slope values were determined from a linear fit to the channel bottom elevation data. A Manning  $n = .035$  was used for the main channel and overbanks for each bridge location. One flow rate (designated by HEC-RAS as a profile or PF on the graphs) was used for each model; the estimated 100-year peak flows of 5976-cfs, 5787-cfs, and 5892-cfs for Willow Little Susitna-Miller, and Little Susitna - Burma respectively. Each crossing site was modeled with two scenarios: without and with the imposed minimum flow opening. The HEC-RAS feature of “ineffective area” was used to provide an approximate estimation of the upstream backwater effect of the restricted opening. The criteria that no more than an approximate 1-foot rise at the upstream section (Station 550) over the no-bridge scenario was used for each bridge location. This was accomplished by using a method that applied sequentially narrower bridge openings until the aforementioned criteria was reached. A sample result for the cross-section

used above is shown below in Figure G-4. Note that the HEC-RAS model treats the outer limit points as vertical flows limits for computation purposes. This is an acceptable for conservative minimum opening width results. This analysis was made for each of the three bridge sites. The results are summarized below:

Bridge	100-Year Flow	Minimum Opening Width
Willow Creek	5946-cfs	86-feet
L. Susitna-Miller	5787-cfs	59-feet
L. Susitna-Burma	5892-cfs	47-feet

It should be noted that the final design for the bridge opening width will likely be significantly wider than the minimum opening widths indicated above, since considerations for fill and piers within the waterway will be negotiated. Additional hydraulic analysis will need to be completed for final design once more information, such as abutment and pier locations, is known. Furthermore, the three bridges cross designated anadromous waterways; therefore, sizing for fish passage will also need to be performed for final design.

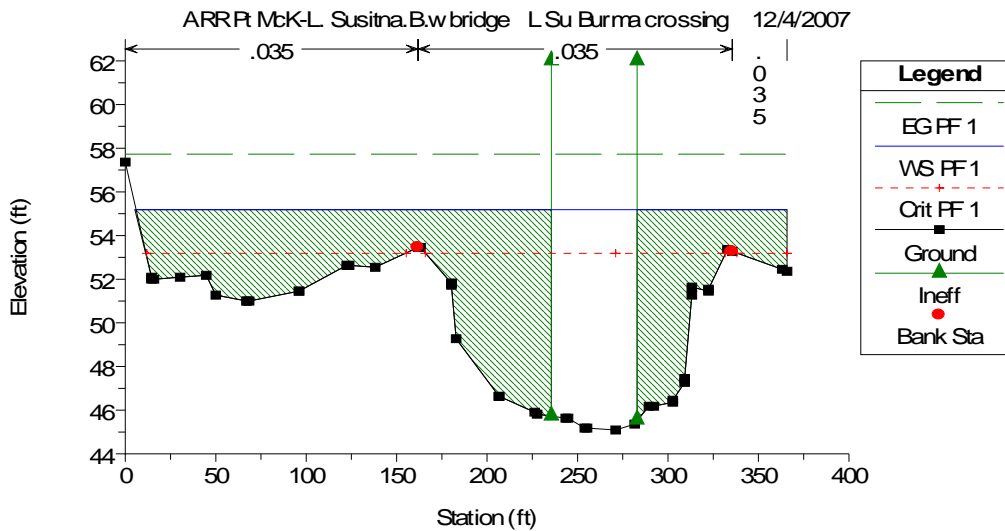


Figure G-4 HEC-RAS Typical Result

#### G.4 Culvert Sizing

Culverts were sized to convey the 100-year flow event with a head to a depth of 1.5 times the culvert diameter (AREMA Manual for Railway Engineering 2006 Section 4.8.2). The maximum flow (full pipe flow) was calculated using Manning’s equation as computed by FHWA’s HY-8 culvert design software for 24-, 36-, 48-, and 72-inch diameter, corrugated metal pipes (Manning’s N = 0.023) at 1% slope, with an assumed length of 80 feet. The flow-based culvert sizings are shown in Table G-1. For the purposed of obtaining a cost estimate, several assumptions were made:

- Those drainage areas that had 100-year flow rates greater than the calculated maximum flow for a 72-inch culvert have been designated as “Drainage Structures.” The selection and sizing of these structures will be determined during the final design process. Possible structures could include, but are not limited to, multi-plate circular and arch culverts; pre-cast arches; and single or multiple short span bridges.

- Anadromous streams are currently sized for flow; however, sizing for fish passage will be negotiated with jurisdictional agencies and included in the final design.
- Conveyance structures within 1,500' of trail crossings were sized up to "Drainage Structures" with the intention of routing the trail underneath the track.

The 100-year flows calculated for each of the analyzed drainage areas and the maximum flows for each of the four culvert diameters are shown in Table G-1.

### **G.5 Minor Drainages and Additional Quantities**

As described in Section G.1, a flow accumulation grid was computed using ArcHydro GIS software to define the pattern of drainage from the digital elevation model. To further define the drainage lines, a stream network was created using the same software. While the additional drainage lines located using this method were not sized for flow (due to limitations in the digital elevation data and the current lack of survey data for the project), culverts will likely be installed at these approximate locations. Therefore, to improve the accuracy of the cost estimate, 80-foot long, 48-inch diameter culverts were specified at locations where the stream network crossed the alignment, but no 100-year flow rates are shown. Several of these drainages occurred in close proximity to existing trail crossings. In order to avoid construction of additional crossing structures, conveyance structures within 1500-feet of trail crossings were sized up to "Drainage Structures" as described previously. These culverts, along with flow sized culverts, bridges, other drainage structures, and extensions for existing culverts, are shown in the attached Table G-2, attached at the end of this appendix.

To account for unknown drainages along each alignment, an additional 36", 80' long culvert was added for every 1 mile of track in each segment. These quantities are listed in Table G-2, attached at the end of this appendix, as "Additional" Segments and show an unspecified stationing. Maintenance and preservation of hydraulic connectivity of wetlands is of special concern; therefore, an additional 24", 60' long equalization culvert was added for every 1000' of track through wetlands in each segment. These quantities are listed in Table G-2, attached at the end of this appendix, as "Equalization" Segments and also show an unspecified stationing.

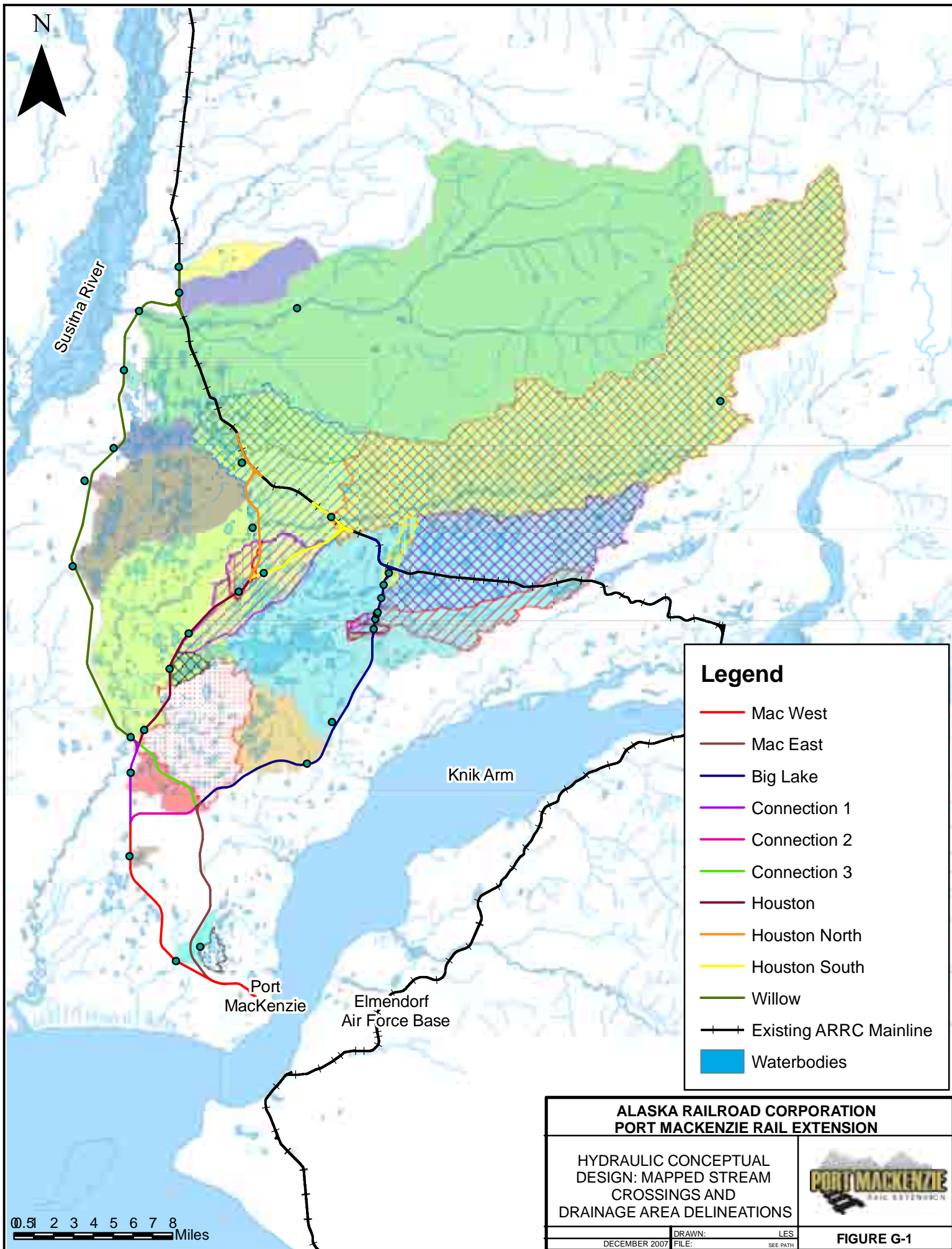
### **G.6 Hydraulic Conceptual Design Summary**

It is important to note that the hydraulic conceptual design described herein was intended to ascertain an approximate construction cost for whatever hydraulic conveyance structures are eventually designed for the selected alignment. The structure types and sizes specified above and quantified below are preliminary and may be refined as the project design progresses. A summary of the quantities of major bridges, minor bridges, and the lengths of each of the four sizes of culverts included in each segment and alignment alternative are shown in Tables G-3 and G-4, respectively.

<b>Table G-3 Quantity Summary by Segment</b>										
	<b>Willow</b>	<b>Conn1</b>	<b>Conn2</b>	<b>Conn3</b>	<b>Houston</b>	<b>Houston North</b>	<b>Houston South</b>	<b>Big Lake</b>	<b>Mac East</b>	<b>Mac West</b>
<b>River Bridges</b>	2	0	0	0	0	1	1	0	0	0
<b>Minor Bridges</b>	5	2	0	0	2	1	0	6	0	0
<b>24-Inch Culvert (ft)</b>	1140	420	240	120	1320	1380	240	1160	600	1200
<b>36-Inch Culvert (ft)</b>	2540	400	320	400	880	880	720	1760	1120	1120
<b>48-Inch Culvert (ft)</b>	750	400	320	80	480	310	270	400	320	960
<b>72-Inch Culvert (ft)</b>	80	0	0	0	80	30	0	60	0	0

<b>Table G-4 Quantity Summary by Alignment Alternative</b>								
	<b>Mac West, Conn 1, Willow</b>	<b>Mac West, Conn 1, Houston, Houston North</b>	<b>Mac West, Conn 1, Houston, Houston South</b>	<b>Mac West, Conn 2, Big Lake</b>	<b>Mac East, Conn 3, Willow</b>	<b>Mac East, Conn 3, Houston, Houston North</b>	<b>Mac East, Conn 3, Houston, Houston South</b>	<b>Mac East, Big Lake</b>
<b>River Bridges</b>	2	1	1	0	2	1	1	0
<b>Minor Bridges</b>	7	5	4	6	5	3	2	6
<b>24-Inch Culvert (ft)</b>	2760	4320	3180	2600	1860	3420	2280	1760
<b>36-Inch Culvert (ft)</b>	4060	3280	3120	3200	4060	3280	3120	2880
<b>48-Inch Culvert (ft)</b>	2110	2150	2110	1680	1150	1190	1150	720
<b>72-Inch Culvert (ft)</b>	80	110	80	60	80	110	80	60





*(This page intentionally left blank.)*

Table G-2 Drainage Crossing List

Alignment Segment	Station	Mile-post	Water Body Type	Conveyance Type	Culvert Diameter (inches)	Quantity	Culvert Length (feet)	Extension Length (feet)	Fisheries	Navigation	Size-Controlling Factor
Mac East	45+73.32	T-0.9	UMD	Culvert	48	1	80	-		None	
Mac East	65+37.63	T-1.2	Wetland	Culvert	48	1	80	-		None	
Mac East	130+46.23	ME-2.5	UMD	Culvert	48	1	80	-		None	
Mac East	changed	ME-4.5	Stream	Culvert	36	1	80	-	Possible	Possible	Flow
Mac East	389+68.43	ME-7.4	UMD	Culvert	48	1	80	-		None	
Mac East Additional	Unspecified	NA	UMD	Culvert	36	13	80	-	NA	NA	
Mac East Equalization	Unspecified	NA	Wetland	Culvert	24	10	60	-	NA	NA	
Mac West	195+84.69	MW-3.7	UMD	Culvert	48	1	80	-		None	
Mac West	265+06.52	MW-5.0	Stream	Culvert	48	1	80	-	Possible	Possible	Flow
Mac West	276+19.99	MW-5.2	UMD	Culvert	48	1	80	-		None	
Mac West	330+98.80	MW-6.3	UMD	Culvert	48	1	80	-		None	
Mac West	358+53.21	MW-6.8	UMD	Culvert	48	1	80	-		None	
Mac West	382+42.08	MW-7.2	UMD	Culvert	48	1	80	-		None	
Mac West	511+40.33	MW-9.7	UMD	Culvert	48	1	80	-		None	
Mac West	439+14.28	MW-8.3	UMD	Culvert	48	1	80	-		None	
Mac West	461+74.48	MW-8.8	UMD	Culvert	48	1	80	-		None	
Mac West	491+06.15	MW-9.3	UMD	Culvert	48	1	80	-		None	
Mac West	534+03.07	MW-10.1	UMD	Culvert	48	1	80	-		None	
Mac West	597+09.07	MW-11.3	Stream	Culvert	36	1	80	-	Possible	Possible	Flow
Mac West	634+88.01	MW-12.0	UMD	Culvert	48	1	80	-		None	
Mac West Additional	Unspecified	NA	UMD	Culvert	36	13	80	-	NA	NA	
Mac West Equalization	Unspecified	NA	Wetland	Culvert	24	20	60	-	NA	NA	
Conn 1	11+20.07	C1-0.2	Wetland	Culvert	48	1	80	-		None	
Conn 1	35+29.20	C1-0.7	Wetland	Culvert	48	1	80	-		None	
Conn 1	47+47.04	C1-0.9	Wetland	Culvert	48	1	80	-		None	
Conn 1	58+29.20	C1-1.1	Wetland	Culvert	48	1	80	-		None	
Conn 1	118+68.21	C1-2.3	UMD	Drainage Structure	NA	1	80	-		None	Trail
Conn 1	137+13.20	C1-2.6	Stream	Culvert	NA	1	80	-	Anadromous	Possible	Flow
Conn 1	157+84.70	C1-3.0	Wetland	Culvert	48	1	80	-		None	
Conn 2	10+21.66	C2-0.2	Wetland	Culvert	48	1	80	-		None	
Conn 2	90+19.38	C2-1.7	UMD	Culvert	48	1	80	-		None	
Conn 2	98+20.86	C2-1.9	UMD	Culvert	48	1	80	-		None	
Conn 2	118+98.84	C2-2.3	UMD	Culvert	48	1	80	-		None	
Conn 3		C3-1.5	UMD	Culvert	36	1	80	-		None	
Conn 3		C3-2.2	Wetland	Culvert	24	1	80	-		None	
Conn 3	157+36.22	C3-3.0	Wetland	Culvert	48	1	80	-		None	
Conn 3		C3-3.6	UMD	Culvert	36	1	80	-		None	
Conn 1 Additional	Unspecified	NA	UMD	Culvert	36	5	80	-	NA	NA	
Conn 1 Equalization	Unspecified	NA	Wetland	Culvert	24	7	60	-	NA	NA	
Conn 2 Additional	Unspecified	NA	UMD	Culvert	36	4	80	-	NA	NA	
Conn 2 Equalization	Unspecified	NA	Wetland	Culvert	24	4	60	-	NA	NA	
Conn 3 Additional	Unspecified	NA	UMD	Culvert	36	3	80	-	NA	NA	
Conn 3 Equalization	Unspecified	NA	Wetland	Culvert	24	1	60	-	NA	NA	
Willow	28+99.68	W-0.6	Willow	Bridge	NA	1	-	-	Anadromous	Navigable	Flow
Willow	128+90.21	W-2.4	UMD	Culvert	48	1	80	-		None	
Willow	128+90.21	W-8.6	UMD	Culvert	36	1	80	-		None	
Willow	529+01.04	W-10.0	Stream	Drainage Structure	NA	1	-	-	Anadromous	Possible	Flow
Willow	727+48.56	W-13.8	UMD	Culvert	48	1	80	-		None	
Willow	761+24.87	W-14.4	Stream	Culvert	36	1	80	-	Possible	Possible	Flow
Willow	836+23.01	W-15.8	UMD	Culvert	48	1	80	-		None	
Willow	867+76.52	W-16.4	UMD	Culvert	48	1	80	-		None	
Willow	883+09.84	W-16.7	Stream	Culvert	72	1	80	-	Possible	Possible	Flow
Willow	1032+99.57	W-19.6	Wetland	Drainage Structure	NA	1	80	-		None	Trail
Willow	1100+73.35	W-20.9	Stream	Culvert	36	1	80	-	Possible	Possible	Flow
Willow	1131+74.79	W-21.4	UMD	Culvert	48	1	80	-		None	
Willow	1198+07.28	W-22.7	UMD	Culvert	48	1	80	-		None	
Willow	1219+09.17	W-23.1	Wetland	Drainage Structure	NA	1	80	-		None	Trail
Willow	1269+20.88	W-24.0	Little	Bridge	NA	1	-	-	Anadromous	Navigable	Flow
Willow	1310+43.59	W-24.8	UMD	Culvert	48	1	80	-		None	
Willow	1343+94.53	W-25.5	UMD	Culvert	48	1	80	-		None	
Willow	1353+27.20	W-25.6	UMD	Culvert	48	1	80	-		None	
Willow Additional	Unspecified	NA	UMD	Culvert	36	28	80	-	NA	NA	
Willow Equalization	Unspecified	NA	Wetland	Culvert	24	19	60	-	NA	NA	
Houston	39+60.05	H-0.8	Stream	Drainage Structure	NA	1	-	-	Anadromous	Possible	Flow
Houston	98+04.98	H-1.2	Wetland	Culvert	24	1	80	-		None	
Houston	98+04.98	H-1.9	Wetland	Culvert	48	1	80	-		None	
Houston	145+12.99	H-2.8	UMD	Culvert	48	1	80	-		None	
Houston	226+06.18	H-4.3	Stream	Culvert	72	1	80	-	Anadromous	Possible	Flow
Houston	98+04.98	H-5.8	Wetland	Culvert	36	1	80	-		None	
Houston	333+88.41	H-6.3	Stream	Drainage Structure	NA	1	-	-	Anadromous	Possible	Flow
Houston	374+92.74	H-7.1	UMD	Culvert	48	1	80	-		None	
Houston	438+54.94	H-8.3	UMD	Culvert	48	1	80	-		None	
Houston	497+45.95	H-9.4	UMD	Culvert	48	1	80	-		None	
Houston	505+48.22	H-9.6	Stream	Culvert	48	1	80	-	Possible	Possible	Flow
Houston Additional	Unspecified	NA	UMD	Culvert	36	11	80	-	NA	NA	
Houston Equalization	Unspecified	NA	Wetland	Culvert	24	20	60	-	NA	NA	
Houston North	63+81.81	HN-1.2	UMD	Culvert	48	1	80	-		None	
Houston North	142+75.61	HN-2.7	Wetland	Culvert	48	1	80	-		None	
Houston North	170+19.53	HN-3.2	River	Bridge	NA	1	-	-	Anadromous	Possible	Flow
Houston North	299+32.72	HN-5.7	Stream	Drainage Structure	NA	1	-	-	Anadromous	None	Flow
Houston North Additional	Unspecified	NA	UMD	Culvert	36	8	80	-	NA	NA	
Houston North Equalization	Unspecified	NA	Wetland	Culvert	24	23	60	-	NA	NA	
Houston South	40+24.41	HS-0.8	UMD	Culvert	48	1	80	-		None	
Houston South	50+54.34	HS-1.0	Stream	Culvert	36	1	80	-	Possible	Possible	Flow
Houston South	72+67.94	HS-1.4	UMD	Culvert	48	1	80	-		None	
Houston South	98+43.90	HS-1.9	UMD	Culvert	48	1	80	-		None	
Houston South Additional	Unspecified	NA	UMD	Culvert	36	8	80	-	NA	NA	
Houston South Equalization	Unspecified	NA	Wetland	Culvert	24	4	60	-	NA	NA	
Big Lake	218+17.48	B-4.1	UMD	Culvert	48	1	80	-		None	
Big Lake	671+88.45	B-5.9	Wetland	Culvert	24	1	80	-		None	
Big Lake	337+24.99	B-6.4	Stream	Drainage Structure	NA	1	80	-	Anadromous	Possible	Fish
Big Lake	671+88.45	B-7.2	UMD	Culvert	36	1	80	-		None	
Big Lake	671+88.45	B-8.4	Wetland	Culvert	24	1	80	-		None	
Big Lake	473+59.63	B-9.0	Stream	Drainage Structure	NA	1	-	-	Anadromous	Possible	Flow
Big Lake	671+88.45	B-9.9	Wetland	Culvert	24	1	80	-		None	
Big Lake	671+88.45	B-11.9	Wetland	Culvert	24	1	80	-		None	
Big Lake	671+88.45	B-12.7	Wetland	Culvert	48	1	80	-		None	
Big Lake	710+49.73	B-13.5	Wetland	Culvert	48	1	80	-		None	
Big Lake	753+65.36	B-14.3	Stream	Culvert	24	1	80	-	Possible	Possible	Flow
Big Lake	763+90.56	B-14.5	UMD	Culvert	48	1	80	-		None	
Big Lake	779+75.27	B-14.8	Stream	Culvert	36	1	80	-		Possible	Flow
Big Lake	795+15.78	B-15.1	Stream	Culvert	36	1	80	-		Possible	Flow
Big Lake	801+39.38	B-15.2	Stream	Drainage Structure	NA	1	-	-	Anadromous	Possible	Flow
Big Lake	834+37.26	B-15.8	UMD	Culvert	48	1	80	-		None	
Big Lake	839+73.28	B-15.9	Stream	Drainage Structure	NA	1	-	-	Anadromous	Possible	Flow
Big Lake	874+84.99	B-16.6	Stream	Drainage Structure	NA	1	80	-	Anadromous	Possible	Trail
Big Lake	919+57.06	B-17.4	Stream	Drainage Structure	NA	1	80	-	Anadromous	Possible	Trail

*(This page intentionally left blank.)*

Alignment Segment	Station	Mile-post	Water Body Type	Conveyance Type	Culvert Diameter (inches)	Quantity	Culvert Length (feet)	Extension Length (feet)	Fisheries	Navigation	Size-Controlling Factor
Big Lake Equalization	Unspecified	NA	Wetland	Culvert	24	14	60	-	NA	NA	
Big Lake Additional	Unspecified	NA	UMD	Culvert	36	19	80	-	NA	NA	
Big Lake Existing Mainline	MP170.55	MP-170.0	UMD	Culvert	60	1	70	30		None	Match Existing
Big Lake Existing Mainline	MP169.96	MP-170.5	UMD	Culvert	60	1	96	30	NA	NA	Match Existing
Houston South Existing Mainline	MP173.25	MP-173.3	UMD	Culvert	48	1	82	30		None	Match Existing
Houston South Existing Mainline	MP174.3	MP-174.3	River	Bridge	NA	1	-	-	Anadromous	Navigable	Match Existing
Houston North Existing Mainline	MP178.5	MP-178.5		Culvert	48	1	42	30		None	Match Existing
Houston North Existing Mainline	MP178.85	MP-178.9		Culvert	36	1	54	30	NA	NA	Match Existing
Houston North Existing Mainline	MP178.97	MP-179.0		Culvert	36	1	53	30	NA	NA	Match Existing
Houston North Existing Mainline	MP179.1	MP-179.1		Culvert	48	1	42	30	NA	NA	Match Existing
Houston North Existing Mainline	MP179.4	MP-179.4	UMD	Culvert	60	1	60	30		None	Match Existing
Houston North Existing Mainline	MP179.5	MP-179.5	UMD	Culvert	48	1	50	30		None	Match Existing
Houston North Existing Mainline	MP179.6	MP-179.6		Culvert	36	1	22	30	NA	NA	Match Existing
Houston North Existing Mainline	MP179.67	MP-179.7		Culvert	36	1	22	30	NA	NA	Match Existing
Houston North Existing Mainline	MP179.8	MP-179.8		Culvert	48	1	47	30	NA	NA	Match Existing
Houston North Existing Mainline	MP179.94	MP-179.9		Culvert	48	1	43	30	NA	NA	Match Existing
Houston North Existing Mainline	MP180.34	MP-180.3	UMD	Culvert	36	1	48	30		None	Match Existing
Houston North Existing Mainline	MP180.43	MP-180.4		Culvert	36	1	53	30	NA	NA	Match Existing
Houston North Existing Mainline	MP180.52	MP-180.5		Culvert	36	1	43	30	NA	NA	Match Existing
Houston North Existing Mainline	MP180.6	MP-180.6	UMD	Culvert	36	1	14	30		None	Match Existing
Willow Existing Mainline	MP188.16	MP-188.2	Wetland	Culvert	48	1	53	30		None	Match Existing
Willow Existing Mainline	MP189.0	MP-189.0	Stream	Bridge	NA	1	-	-	Anadromous	Boat	Match Existing
Willow Existing Mainline	MP189.32	MP-189.3	Wetland	Culvert	36	1	36	30	NA	NA	Match Existing
Willow Existing Mainline	MP189.58	MP-189.6	Wetland	Culvert	36	1	40	30	NA	NA	Match Existing
Willow Existing Mainline	MP190.3	MP-190.3	Stream	Bridge	NA	1	-	-	Anadromous	None	Match Existing

NOTES:

Stream - Designates drainage channels mapped by Matanuska-Susitna Borough's GIS Division compiled based on tax parcel maps and orthoimagery

UMD - Designates an "Un-Mapped Drainage" identified by analysis of digital elevation data

Drainage Structures will be determined during the final design process and multi-plate culverts; pre-cast arches; and single or multiple short

Anadromous streams are currently sized for flow; however, sizing for fish passage will also need to be performed for final

Conveyance structures within 1500' of trail crossings were sized up to drainage structures

An additional 36", 80' long culvert was added for every 1 mile of track in each segment

An additional 24", 60' long equalization culverts were added for every 1000' of track through wetland in each segment

Structures highlighted in yellow are not shown on plan and profile sheets

*(This page intentionally left blank.)*

## **Appendix H: Preliminary Geological Investigations**

October 26, 2007

Tryck Nyman Hayes, Inc.  
911 West 8<sup>th</sup> Avenue, Suite 300  
Anchorage, Alaska 99501

Attn: Mr. Norm Gutcher, P.E.

FAX: (907) 279-0543

**RE: GEOTECHNICAL RECONNAISSANCE SUMMARY, MAT-SU RAIL  
EXTENSION, MATANUSKA-SUSITNA BOROUGH, ALASKA**

This letter report documents the results of reconnaissance studies by Shannon & Wilson for the proposed Mat Su Rail Extension through the Mat-Su Valley, Mat Su Borough, Alaska. The purpose of this study was to visit the various alignments being considered for the extension in the field and gather preliminary geotechnical information along these alignments. The data collected in during this phase of reconnaissance is to be used in preliminary, comparative costing analysis in the route selection process. Presented in this letter is a description of our reconnaissance efforts, field observations made during reconnaissance, and results from hand probing.

Shannon & Wilson performed this work in general accordance with our August 30, 2007 proposal. Mr. Norm Gutcher of Tryck Nyman Hayes, Inc. authorized the work verbally on September 3, 2007.

**Project Description**

The project area includes several potential alignments that extend from Port MacKenzie in the south to the existing rail between Mile Post 53 and 73 of the Glenn Highway in the north. The alignments are named and organized in this report as the Mac West, Mac East, East, Lake Creek, Central, Houston, Conn (1, 2, and 3), and West Alignments. A project overview map is provided in Figure 1 that shows the alignments addressed in this report. We understand that the project is currently in the route selection phase and that relative construction cost is one of the factors that will be analyzed when selecting the preferred alignment. The construction of the new railroad line will be impacted by the type of terrain and general soil conditions traversed by the various alignments.



To assist in developing relative costs for construction of the alignments, our reconnaissance focused on characterizing the presence and thicknesses of soft and/or compressible soils (soils that would need to be replaced during construction) along the subject alignments. Our reconnaissance focused on areas that appeared to be poorly drained and/or near stream or river features that cross the alignments. The results of our reconnaissance should be used only in preliminary route selection and relative cost comparisons. Our reconnaissance was such that a relatively small number of areas were visited (compared to the total lengths of the alignments). The data presented herein should be confirmed with further explorations once a route is selected.

### **Field Activities**

Our field explorations included aerial observations, subsurface peat probing, and surface observations along the potential alignments. Reconnaissance was conducted with helicopter support as well as from existing roadway access where possible. Aerial observations and helicopter support during field efforts was performed with a Robinson R-44 helicopter, operated by Jayhawk Air of Anchorage, Alaska. Probing was conducted with ½-inch outer diameter (OD) probes. The probes comprised four, 5-foot lengths that could be connected together to form one 20-foot long probe, but typically, the rods were used to form two 10-foot probes. The probes were fitted with a removable tip that could be replaced with a thermister probe to measure soil temperatures at the bottom of the probe.

Prior to field work, we were provided with coordinates for points spaced every 500 feet along the various alignments. We plotted those points on United States Geological Society (USGS) quadrangle maps and entered into handheld Global Positioning System (GPS) units for location control during field efforts. We were also provided with land ownership maps of the project area and you informed us that State of Alaska and Matanuska-Susitna Borough lands were open to our access. We did not access private land, land owned by native corporations, or Mental Health Trust lands as access rights would have caused unacceptable delays to the field work.

The first step of the reconnaissance was to fly the various alignments in the helicopter and collect video coverage of the corridors. During the fly-over, we also noted areas that would be of interest for further observations during the reconnaissance. After the fly-over, an engineer and a geologist from Shannon & Wilson used the helicopter to access the noted areas of interest along

the alignment. Areas visited with the helicopter support were those that were generally inaccessible with existing roads. We conducted helicopter supported reconnaissance for three days. Additional reconnaissance was performed for two days in areas accessible by the existing road system.

At each site visited, our representatives noted the general terrain and ground conditions. We also conducted probing along the alignment and occasionally collected ground temperature measurements with the thermister probe. Probe rods were pushed by hand until the probe was completely penetrated (i.e. no refusal) or until refusal (it could not be penetrated further by hand). Where possible, the probe bottom conditions were noted to indicate the approximate mineral soil conditions (e.g. dense granular soil is discernable from softer, cohesive soils when the tip of the probe reaches refusal). When used, the thermister probe was typically left in the ground for approximately 10 minutes to allow the probe to equilibrate with the soil temperatures. Notes collected during this activity including probe depths and measured ground temperatures are included on the attached field notes. The field notes are tied to the station numbering (every 500 feet along the proposed alignments) provided by you prior to our mobilization to the field. Location control in the field was gained using handheld GPS units (accurate to approximately 30 feet).

In addition to the boggy areas that were visited during the reconnaissance, we observed ground conditions at the more prominent stream crossings along the alignments. The data from these areas is also included on the attached field note spread sheets. At the crossing areas, we noted the approximate flow rate of the water, width of the drainage feature, and the channel bottom conditions if visible.

### **Data Reduction**

The data collected during our reconnaissance was used to produce the maps attached to this letter. In summary, the probing data was used to estimate the amount and location of organic soils along each of the alignments. Note that organic soils will likely persist on the ground surface over the entirety of each of the alignments to some extent. Most of the organic soils will comprise vegetative mats and organic silts several feet thick in areas that have not been developed. The attached maps highlight areas where soft and/or compressible soils including peat bogs generally greater than several feet exist. The organic soil thicknesses on the attached maps indicate the range in depths measured by our probes in areas that were visited. In some

cases, significant peat bogs were not able to be visited due to lack of access permission. In these areas, we observed the fly-over video and the probe data in surrounding areas and estimated the likely peat depths based on these observations calibrated to probing data in neighboring areas of similar appearance.

### LIMITATIONS

The analyses and conclusions contained in this report are based on site conditions as they presently exist. It is assumed that the reconnaissance is representative of the surface and subsurface conditions throughout the site, i.e., the conditions everywhere are not significantly different from those disclosed by the explorations.

Unanticipated conditions are commonly encountered during construction and cannot fully be determined by merely conducting reconnaissance or advancing probes. Such unexpected conditions frequently require that additional expenditures be made to attain a properly constructed project. This information should be used in route comparisons only and once a preferred route is selected, additional explorations should be conducted to characterize the subsurface conditions along the preferred alignment.

Shannon & Wilson has prepared the attachment *Important Information About Your Geotechnical/Environmental Report* to assist you and others in understanding the use and limitations of the reports.

Sincerely,

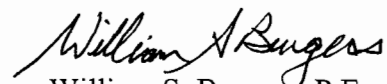
**SHANNON & WILSON, INC.**

Prepared By:



Kyle Brennan  
Geotechnical Engineer

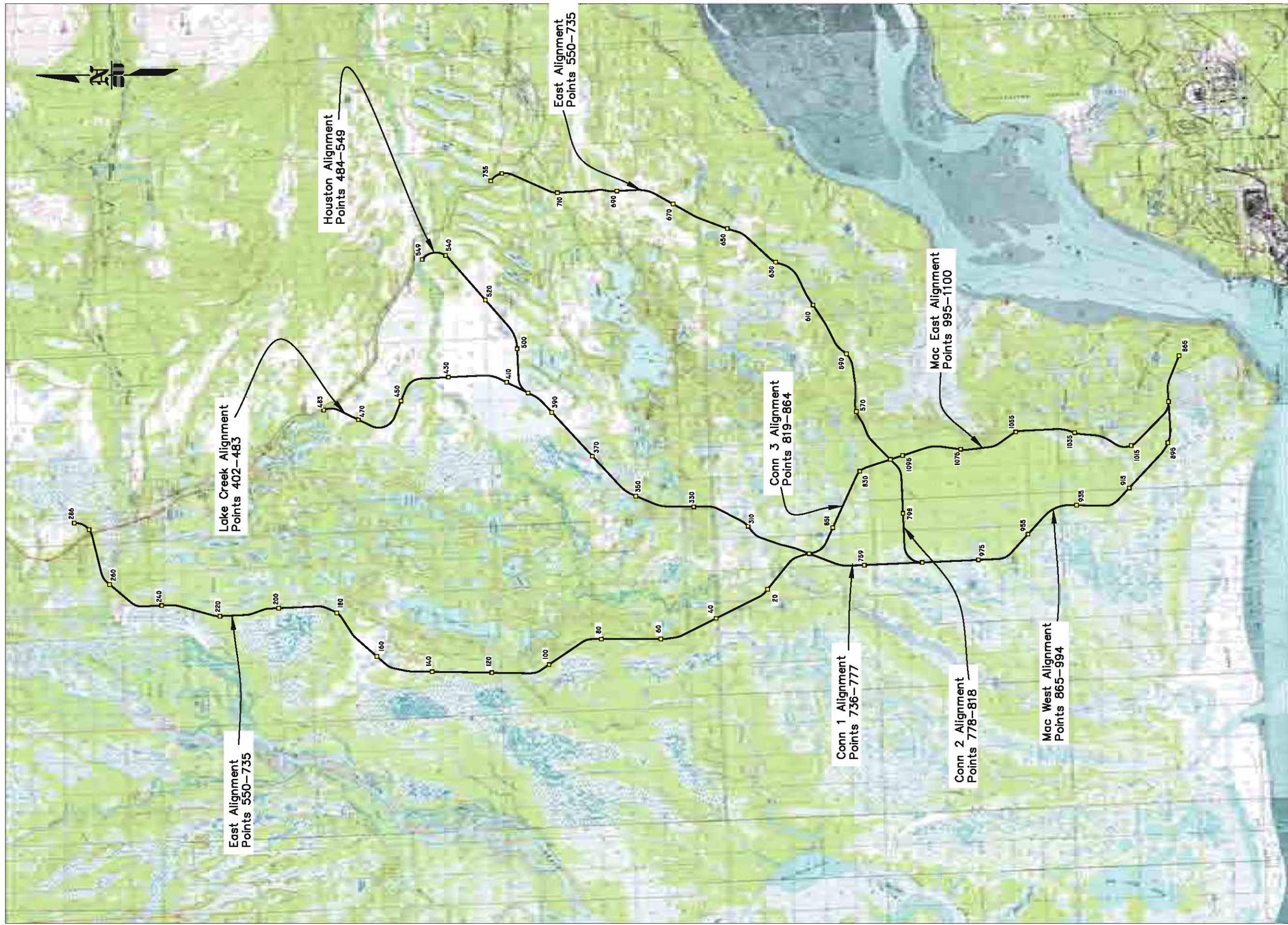
Approved By:



William S. Burgess, P.E.  
Associate

Attachments: Figure 1: Site Plan  
Alignment Maps  
Field Notes  
Important Information About Your Geotechnical/Environmental Report

*(This page intentionally left blank.)*



**LEGEND**

□ 267  
Survey Point



MAT-SU RAIL EXTENSION  
MAT-SU BOROUGH, ALASKA

**SITE PLAN**

OCTOBER 2007



SHANNON & WILSON, INC.  
Geotechnical & Environmental Consultants

32-1-01932

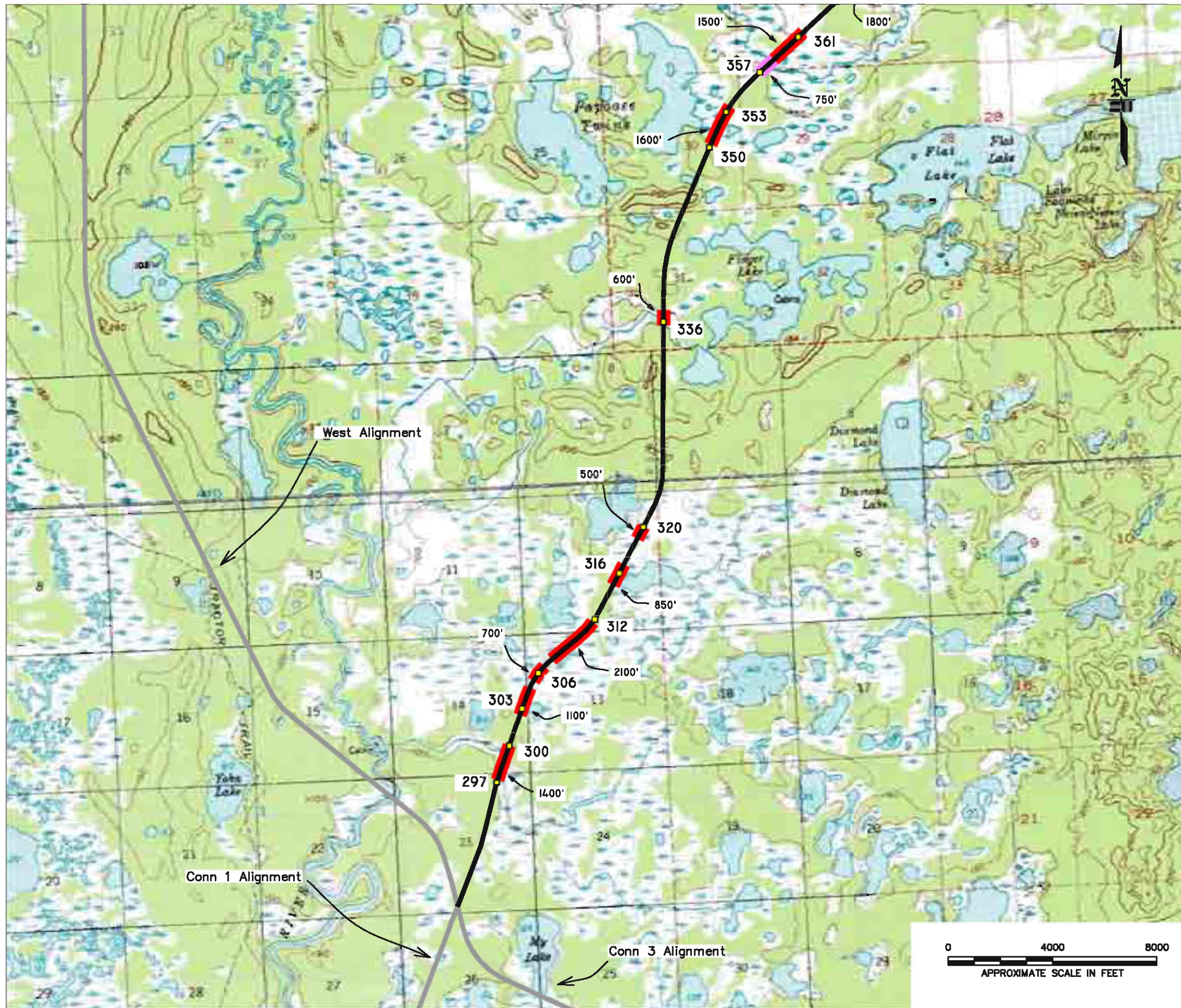
FIG. 1

*(This page intentionally left blank.)*

**ATTACHMENT A**  
**ALIGNMENT MAPS**

*(This page ntentionally left blank.)*





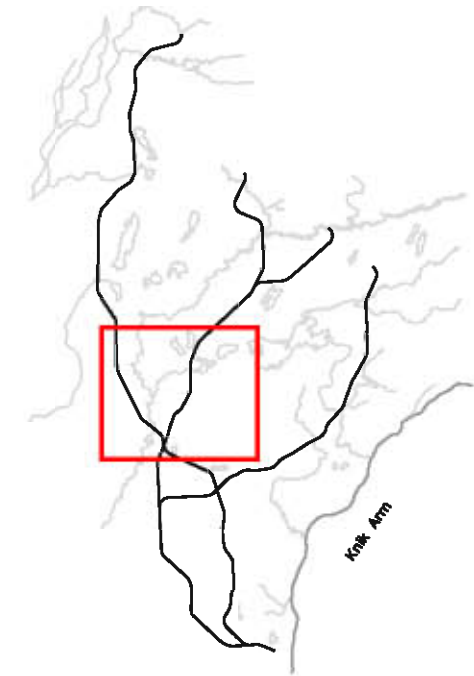
**LEGEND**

- 267 Survey Point
- 1000' Approximate length of delineated bog section

**APPROXIMATE RAILWAY ALIGNMENT**

- 0 to 3 feet silty soils with organics
- 3 to 5 feet soft/compressible soils
- 5 to 10 feet soft/compressible soils
- 10 to 15 feet soft/compressible soils

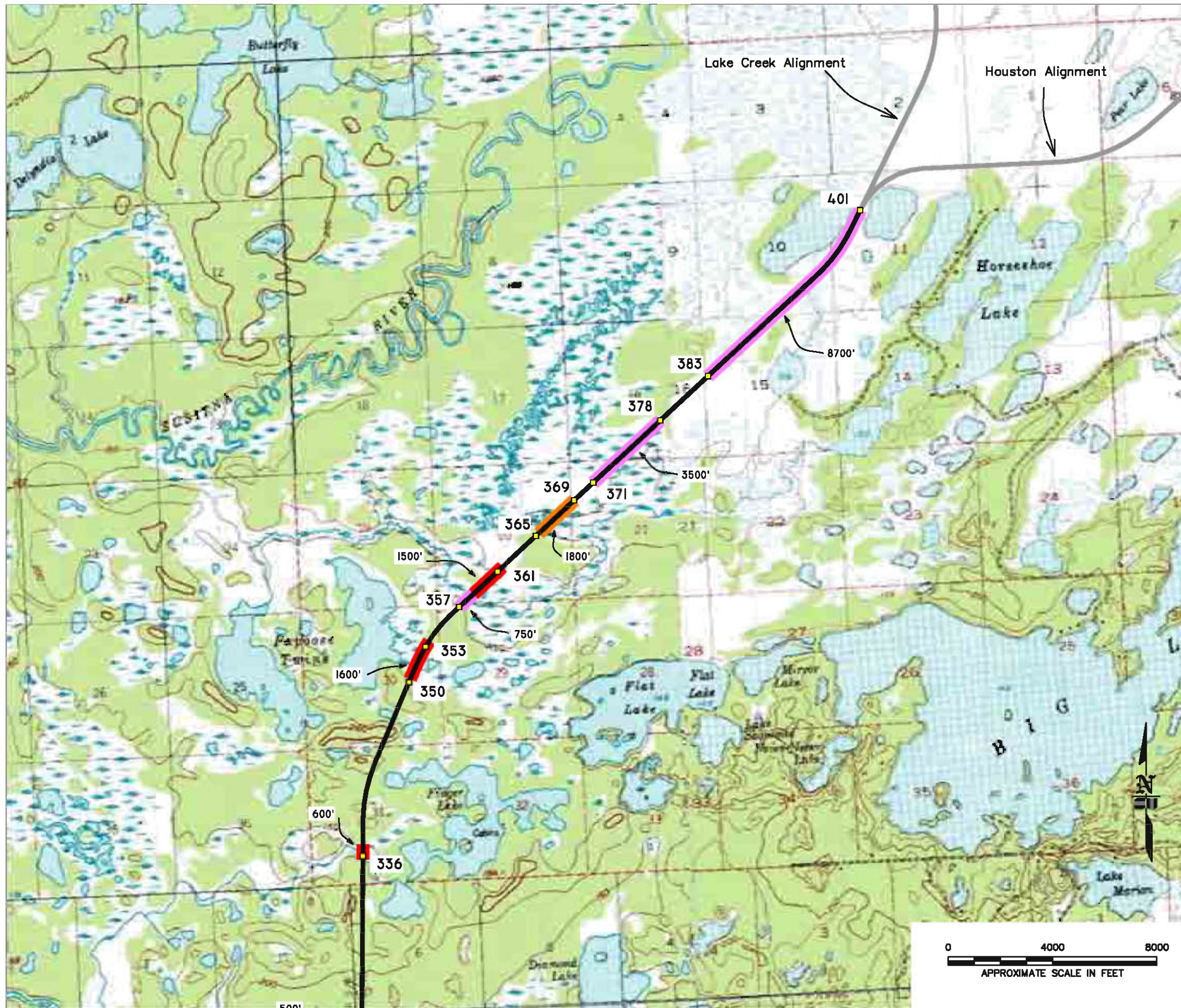
**OVERVIEW MAP**



- NOTES:**
1. BASEMAP ADAPTED FROM USGS 1:63,000 SCALE MAP COVERAGE.
  2. RAILWAY ALIGNMENT IS ADAPTED FROM POINT LOCATIONS PROVIDED BY HDR ALASKA.
  3. BOG DELINEATIONS ALONG THE ALIGNMENT ARE ESTIMATED FROM USGS MAP COVERAGE AND FIELD NOTES.

MAT-SU RAIL EXTENSION MAT-SU BOROUGH, ALASKA	
<b>ALIGNMENT MAP</b> CENTRAL ALIGNMENT POINTS 287 - 364	
OCTOBER 2007	32-I-01932
<b>SHANNON &amp; WILSON, INC.</b> <small>Geotechnical &amp; Environmental Consultants</small>	<b>FIG. 2</b> <small>SHT 1 OF 2</small>

*(This page intentionally left blank.)*



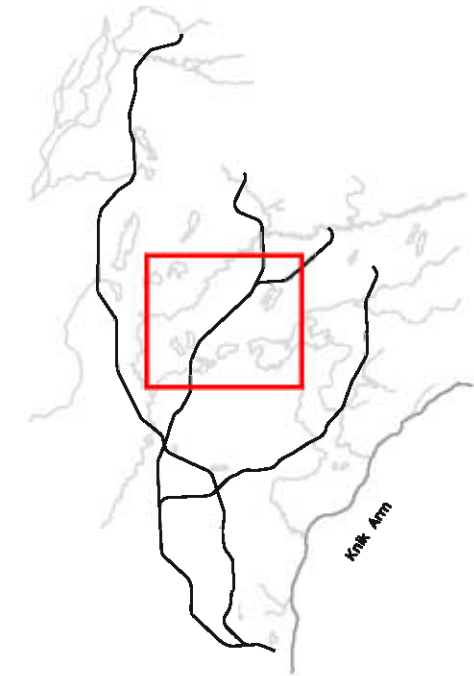
LEGEND

- 267 Survey Point
- ┌───┐ 1000' Approximate length of delineated bog section

APPROXIMATE RAILWAY ALIGNMENT

- 0 to 3 feet silty soils with organics
- 3 to 5 feet soft/compressible soils
- 5 to 10 feet soft/compressible soils
- 10 to 15 feet soft/compressible soils

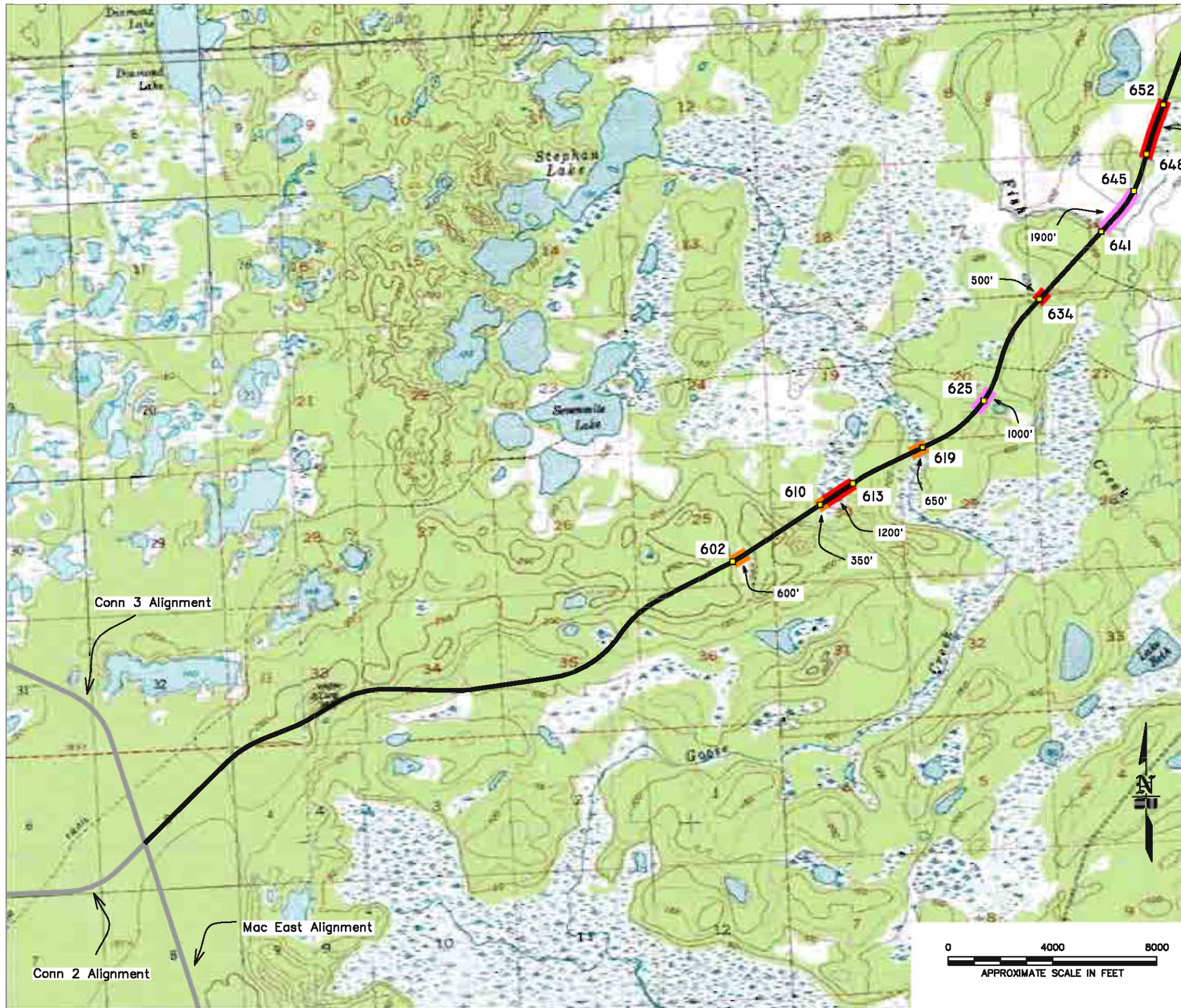
OVERVIEW MAP



- NOTES:
1. BASEMAP ADAPTED FROM USGS 1:63,000 SCALE MAP COVERAGE.
  2. RAILWAY ALIGNMENT IS ADAPTED FROM POINT LOCATIONS PROVIDED BY HDR ALASKA.
  3. BOG DELINEATIONS ALONG THE ALIGNMENT ARE ESTIMATED FROM USGS MAP COVERAGE AND FIELD NOTES.

MAT-SU RAIL EXTENSION MAT-SU BOROUGH, ALASKA	
ALIGNMENT MAP CENTRAL ALIGNMENT POINTS 325 - 401	
OCTOBER 2007	32-1-01932
<b>SHANNON &amp; WILSON, INC.</b> <small>Geotechnical &amp; Environmental Consultants</small>	<b>FIG. 2</b> <small>SHT 2 OF 2</small>

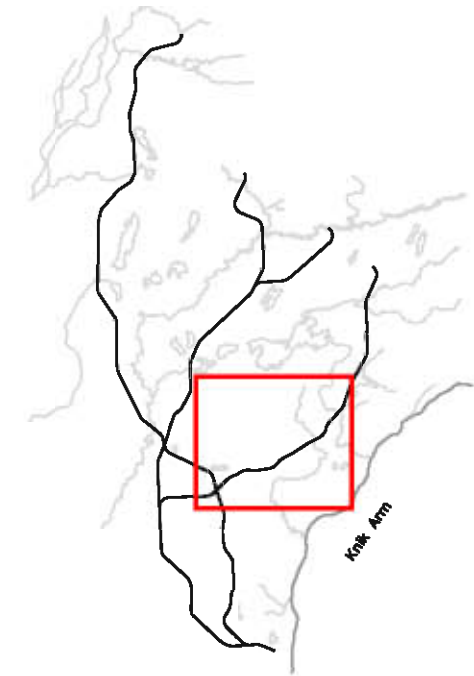
*(This page intentionally left blank.)*



LEGEND

- 267 Survey Point
  - ┌──┐ 1000' Approximate length of delineated bog section
- APPROXIMATE RAILWAY ALIGNMENT
- 0 to 3 feet silty soils with organics
  - 3 to 5 feet soft/compressible soils
  - 5 to 10 feet soft/compressible soils
  - 10 to 15 feet soft/compressible soils

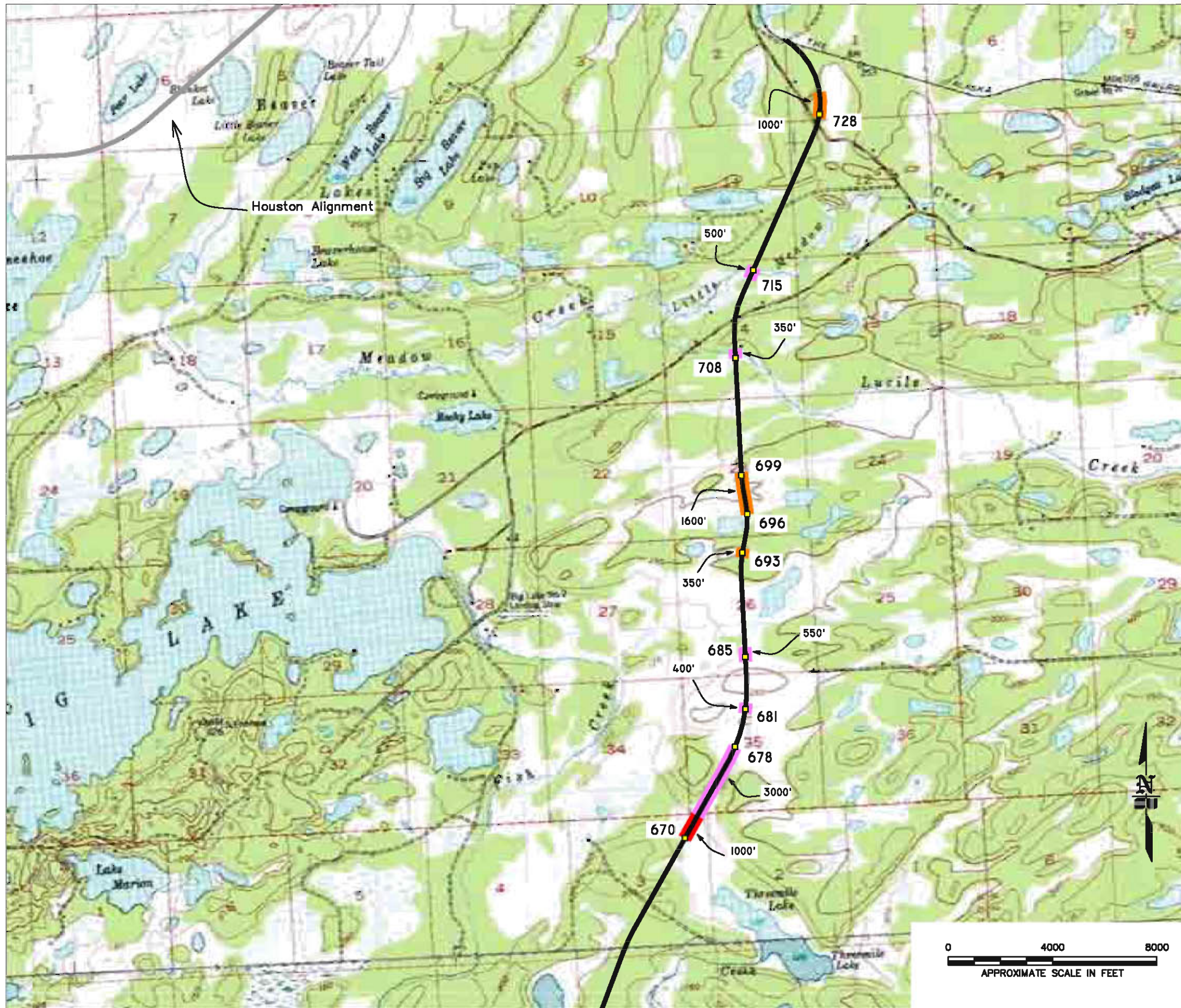
OVERVIEW MAP



- NOTES:
1. BASEMAP ADAPTED FROM USGS 1:63,000 SCALE MAP COVERAGE.
  2. RAILWAY ALIGNMENT IS ADAPTED FROM POINT LOCATIONS PROVIDED BY HDR ALASKA.
  3. BOG DELINEATIONS ALONG THE ALIGNMENT ARE ESTIMATED FROM USGS MAP COVERAGE AND FIELD NOTES.

MAT-SU RAIL EXTENSION MAT-SU BOROUGH, ALASKA	
<b>ALIGNMENT MAP</b> EAST ALIGNMENT POINTS 550 - 658	
OCTOBER 2007	32-I-01932
<b>SHANNON &amp; WILSON, INC.</b> Geotechnical & Environmental Consultants	
<b>FIG. 3</b> SHT 1 OF 2	

*(This page intentionally left blank.)*



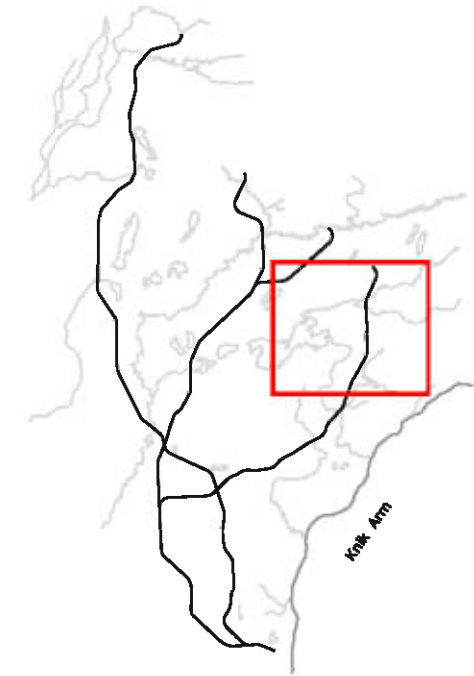
LEGEND

- 267 Survey Point
- ↔ 1000' Approximate length of delineated bog section

APPROXIMATE RAILWAY ALIGNMENT

- 0 to 3 feet silty soils with organics
- 3 to 5 feet soft/compressible soils
- 5 to 10 feet soft/compressible soils
- 10 to 15 feet soft/compressible soils

OVERVIEW MAP

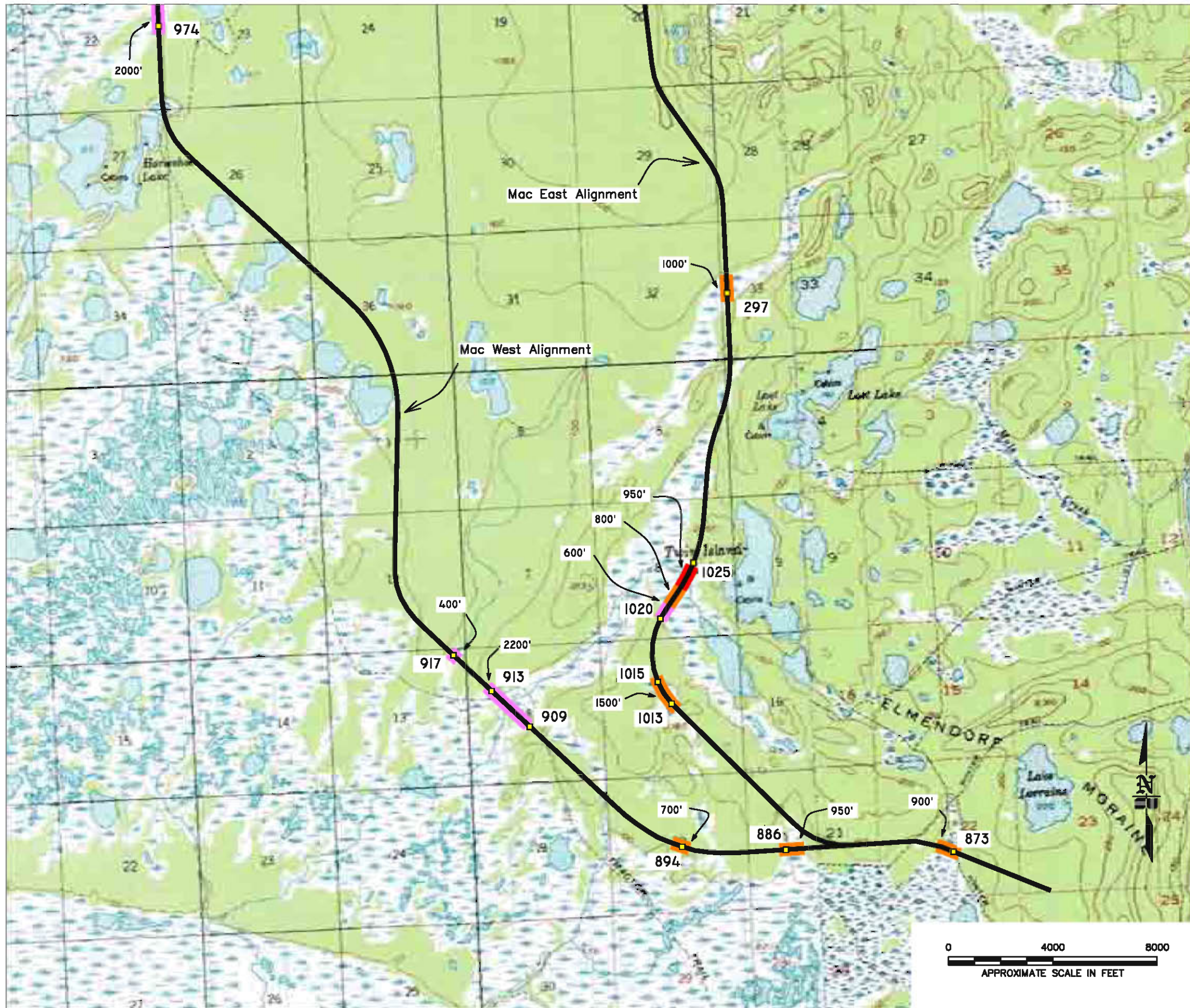


- NOTES:
1. BASEMAP ADAPTED FROM USGS 1:63,000 SCALE MAP COVERAGE.
  2. RAILWAY ALIGNMENT IS ADAPTED FROM POINT LOCATIONS PROVIDED BY HDR ALASKA.
  3. BOG DELINEATIONS ALONG THE ALIGNMENT ARE ESTIMATED FROM USGS MAP COVERAGE AND FIELD NOTES.

MAT-SU RAIL EXTENSION MAT-SU BOROUGH, ALASKA	
ALIGNMENT MAP EAST ALIGNMENT POINTS 656 - 735	
OCTOBER 2007	32-I-01932
<b>SHANNON &amp; WILSON, INC.</b> Geotechnical & Environmental Consultants	
<b>FIG. 3</b> SHT 2 OF 2	

*(This page intentionally left blank.)*

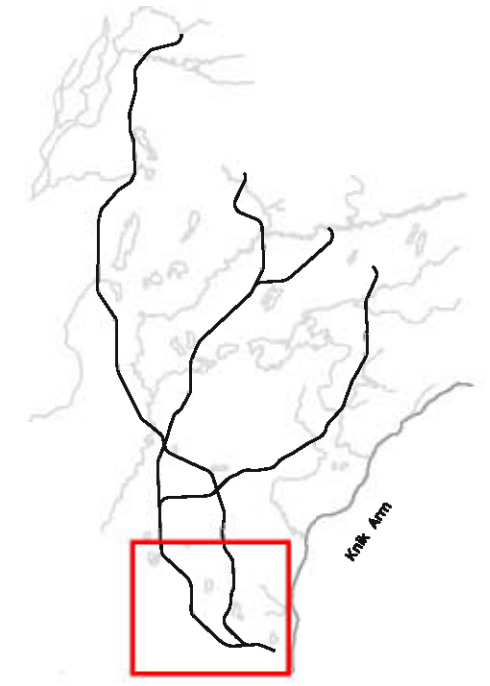




**LEGEND**

- 267 Survey Point
  - 1000' Approximate length of delineated bog section
- APPROXIMATE RAILWAY ALIGNMENT**
- 0 to 3 feet silty soils with organics
  - 3 to 5 feet soft/compressible soils
  - 5 to 10 feet soft/compressible soils
  - 10 to 15 feet soft/compressible soils

**OVERVIEW MAP**



- NOTES:**
1. BASEMAP ADAPTED FROM USGS 1:63,000 SCALE MAP COVERAGE.
  2. RAILWAY ALIGNMENT IS ADAPTED FROM POINT LOCATIONS PROVIDED BY HDR ALASKA.
  3. BOG DELINEATIONS ALONG THE ALIGNMENT ARE ESTIMATED FROM USGS MAP COVERAGE AND FIELD NOTES.

MAT-SU RAIL EXTENSION MAT-SU BOROUGH, ALASKA	
<b>ALIGNMENT MAP</b>	
MAC WEST & EAST ALIGNMENTS SOUTHERN PORTION	
OCTOBER 2007	32-I-01932
SHANNON & WILSON, INC. Geotechnical & Environmental Consultants	FIG. 4 SHT 1 OF 2

*(This page intentionally left blank.)*



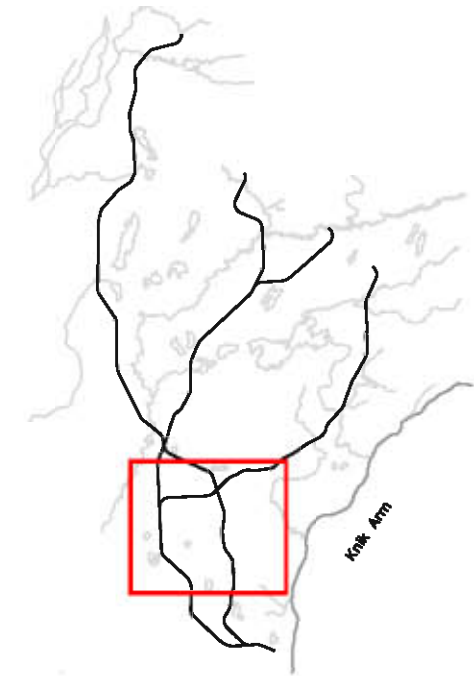
**LEGEND**

- 267 Survey Point
- 1000' Approximate length of delineated bog section

**APPROXIMATE RAILWAY ALIGNMENT**

- 0 to 3 feet silty soils with organics
- 3 to 5 feet soft/compressible soils
- 5 to 10 feet soft/compressible soils
- 10 to 15 feet soft/compressible soils

**OVERVIEW MAP**



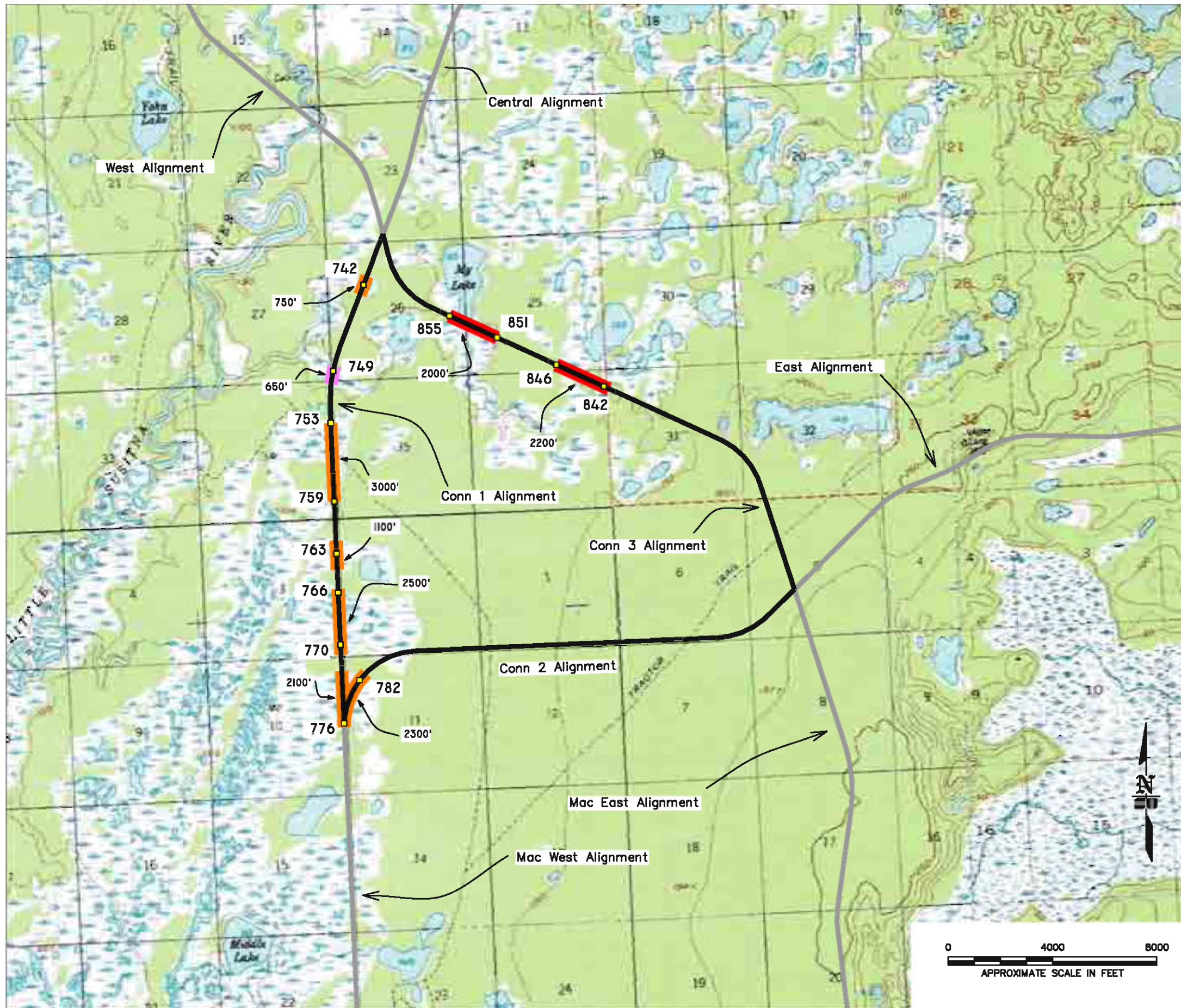
**NOTES:**

1. BASEMAP ADAPTED FROM USGS 1:63,000 SCALE MAP COVERAGE.
2. RAILWAY ALIGNMENT IS ADAPTED FROM POINT LOCATIONS PROVIDED BY HDR ALASKA.
3. BOG DELINEATIONS ALONG THE ALIGNMENT ARE ESTIMATED FROM USGS MAP COVERAGE AND FIELD NOTES.



MAT-SU RAIL EXTENSION MAT-SU BOROUGH, ALASKA	
<b>ALIGNMENT MAP</b>	
MAC WEST & EAST ALIGNMENTS NORTHERN PORTION	
OCTOBER 2007	32-I-01932
SHANNON & WILSON, INC. <small>Geotechnical &amp; Environmental Consultants</small>	FIG. 4 SHT 2 OF 2

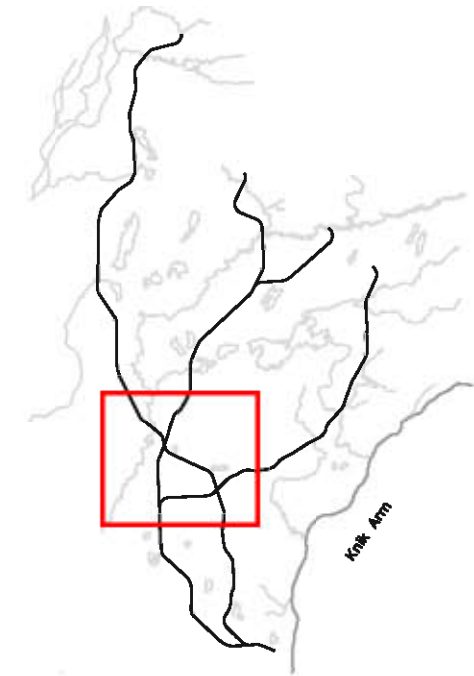
*(This page intentionally left blank.)*



**LEGEND**

- 267 Survey Point
  - 1000' Approximate length of delineated bog section
- APPROXIMATE RAILWAY ALIGNMENT**
- 0 to 3 feet silty soils with organics
  - 3 to 5 feet soft/compressible soils
  - 5 to 10 feet soft/compressible soils
  - 10 to 15 feet soft/compressible soils

**OVERVIEW MAP**

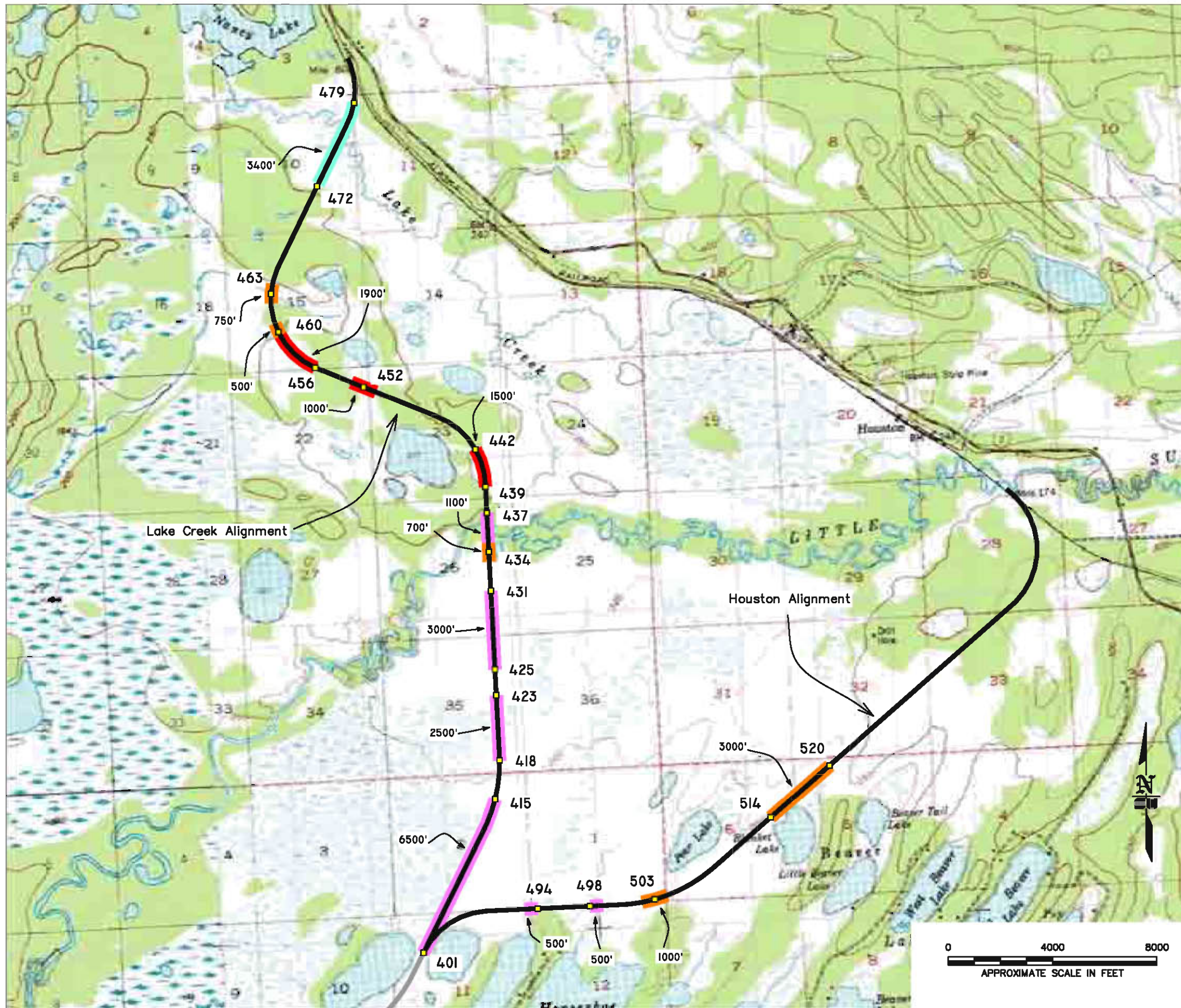


- NOTES:**
1. BASEMAP ADAPTED FROM USGS 1:63,000 SCALE MAP COVERAGE.
  2. RAILWAY ALIGNMENT IS ADAPTED FROM POINT LOCATIONS PROVIDED BY HDR ALASKA.
  3. BOG DELINEATIONS ALONG THE ALIGNMENT ARE ESTIMATED FROM USGS MAP COVERAGE AND FIELD NOTES.



MAT-SU RAIL EXTENSION MAT-SU BOROUGH, ALASKA	
<b>ALIGNMENT MAP</b> CONN 1, 2 & 3 ALIGNMENTS	
OCTOBER 2007	32-I-01932
SHANNON & WILSON, INC. <small>Geotechnical &amp; Environmental Consultants</small>	FIG. 5

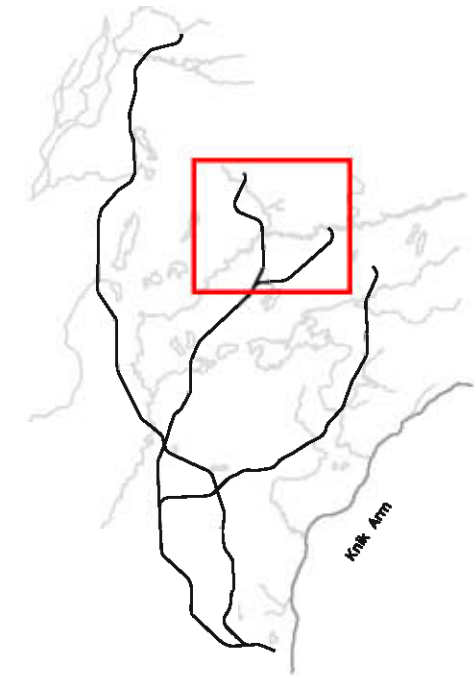
*(This page intentionally left blank.)*



**LEGEND**

- 267 Survey Point
- ↖ 1000' Approximate length of delineated bog section
- APPROXIMATE RAILWAY ALIGNMENT**
- 0 to 3 feet silty soils with organics
- 3 to 5 feet soft/compressible soils
- 5 to 10 feet soft/compressible soils
- 10 to 15 feet soft/compressible soils
- 20+ feet soft/compressible soils

**OVERVIEW MAP**



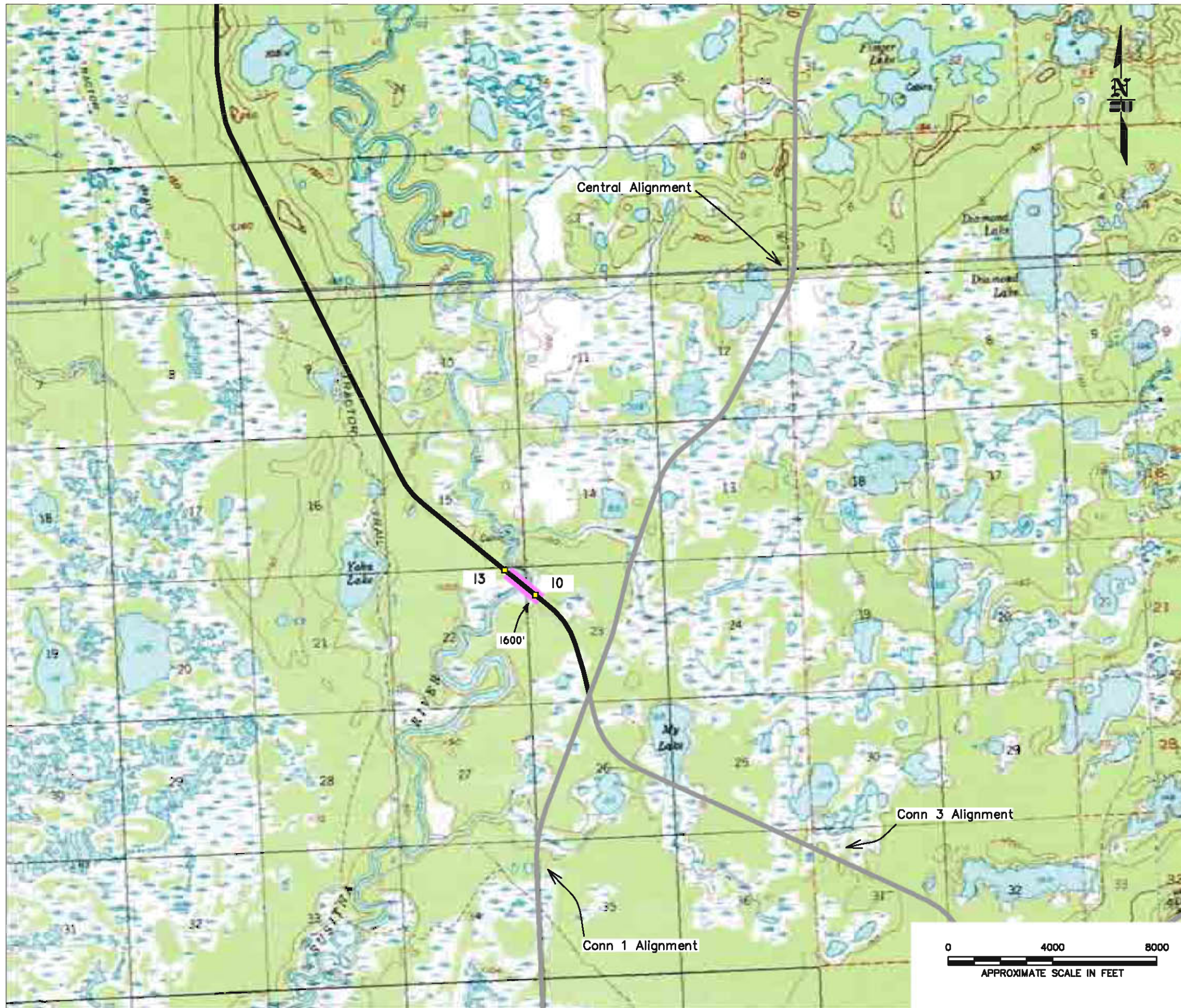
- NOTES:**
1. BASEMAP ADAPTED FROM USGS 1:63,000 SCALE MAP COVERAGE.
  2. RAILWAY ALIGNMENT IS ADAPTED FROM POINT LOCATIONS PROVIDED BY HDR ALASKA.
  3. BOG DELINEATIONS ALONG THE ALIGNMENT ARE ESTIMATED FROM USGS MAP COVERAGE AND FIELD NOTES.



MAT-SU RAIL EXTENSION MAT-SU BOROUGH, ALASKA	
<b>ALIGNMENT MAP</b> LAKE CREEK & HOUSTON ALIGNMENTS	
OCTOBER 2007	32-I-01932
<b>SHANNON &amp; WILSON, INC.</b> Geotechnical & Environmental Consultants	
<b>FIG. 6</b>	

*(This page intentionally left blank.)*





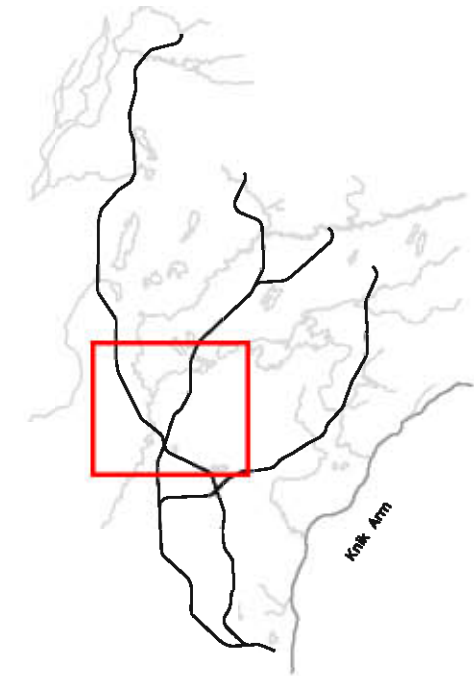
LEGEND

- 267 Survey Point
- ↖ 1000' Approximate length of delineated bog section

APPROXIMATE RAILWAY ALIGNMENT

- 0 to 3 feet silty soils with organics
- 3 to 5 feet soft/compressible soils
- 5 to 10 feet soft/compressible soils
- 10 to 15 feet soft/compressible soils

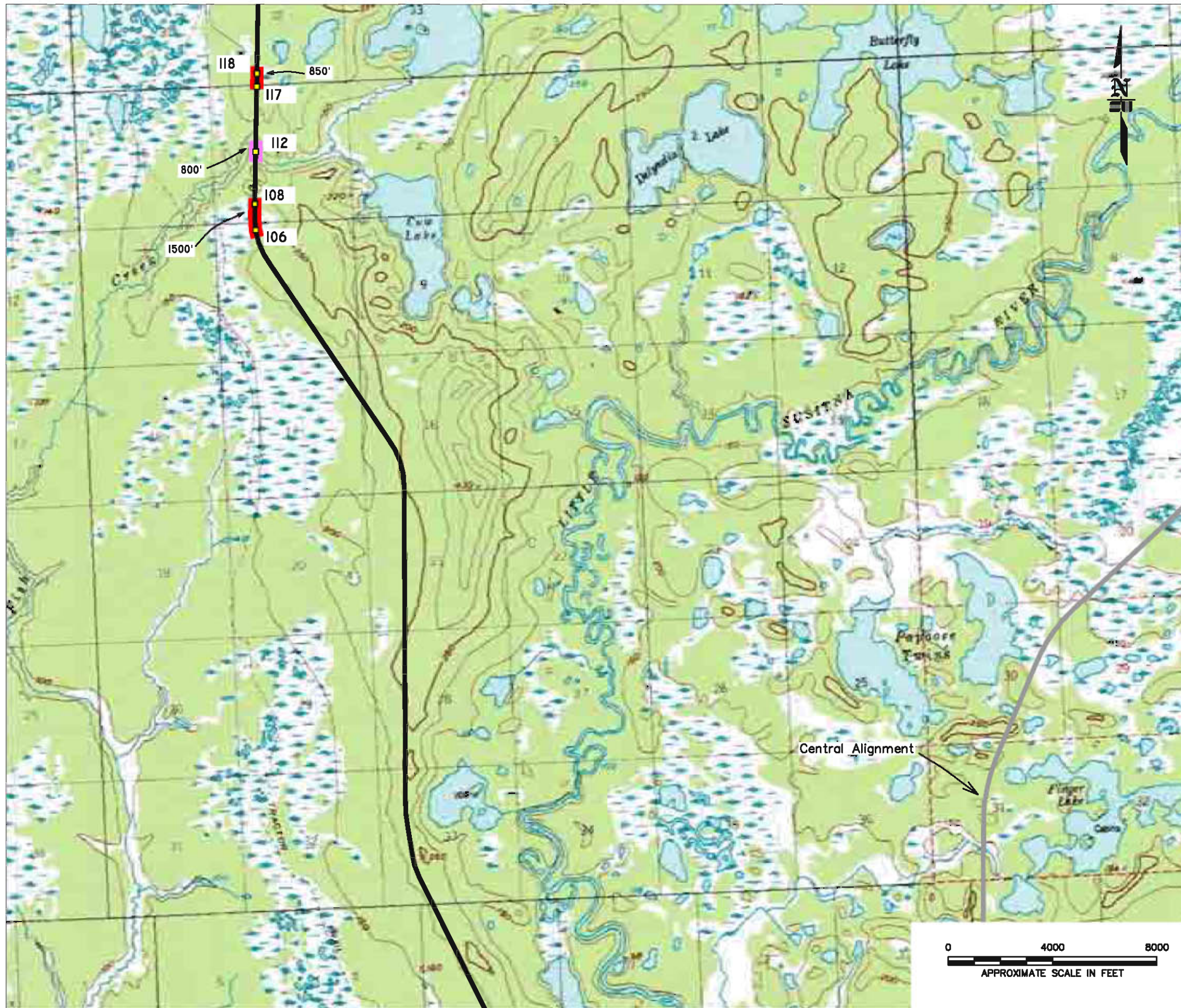
OVERVIEW MAP



- NOTES:
1. BASEMAP ADAPTED FROM USGS 1:63,000 SCALE MAP COVERAGE.
  2. RAILWAY ALIGNMENT IS ADAPTED FROM POINT LOCATIONS PROVIDED BY HDR ALASKA.
  3. BOG DELINEATIONS ALONG THE ALIGNMENT ARE ESTIMATED FROM USGS MAP COVERAGE AND FIELD NOTES.

MAT-SU RAIL EXTENSION MAT-SU BOROUGH, ALASKA	
ALIGNMENT MAP WEST ALIGNMENT POINTS I - 42	
OCTOBER 2007	32-I-01932
<b>SHANNON &amp; WILSON, INC.</b> <small>Geotechnical &amp; Environmental Consultants</small>	<b>FIG. 7</b> <small>SHT 1 OF 4</small>

*(This page intentionally left blank.)*



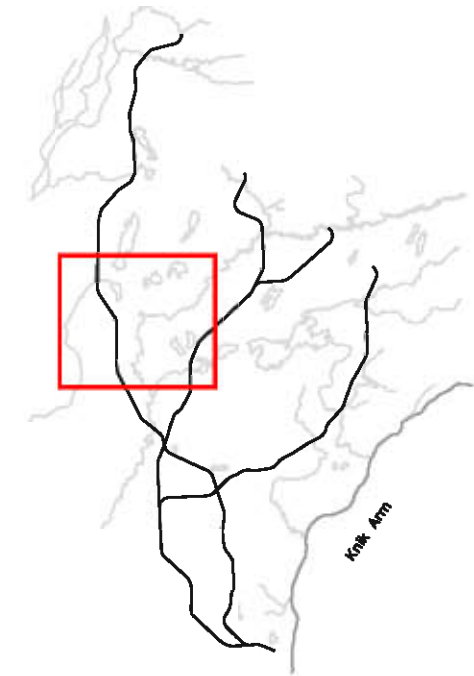
LEGEND

- 267 Survey Point
- 1000' Approximate length of delineated bog section

APPROXIMATE RAILWAY ALIGNMENT

- 0 to 3 feet silty soils with organics
- 3 to 5 feet soft/compressible soils
- 5 to 10 feet soft/compressible soils
- 10 to 15 feet soft/compressible soils

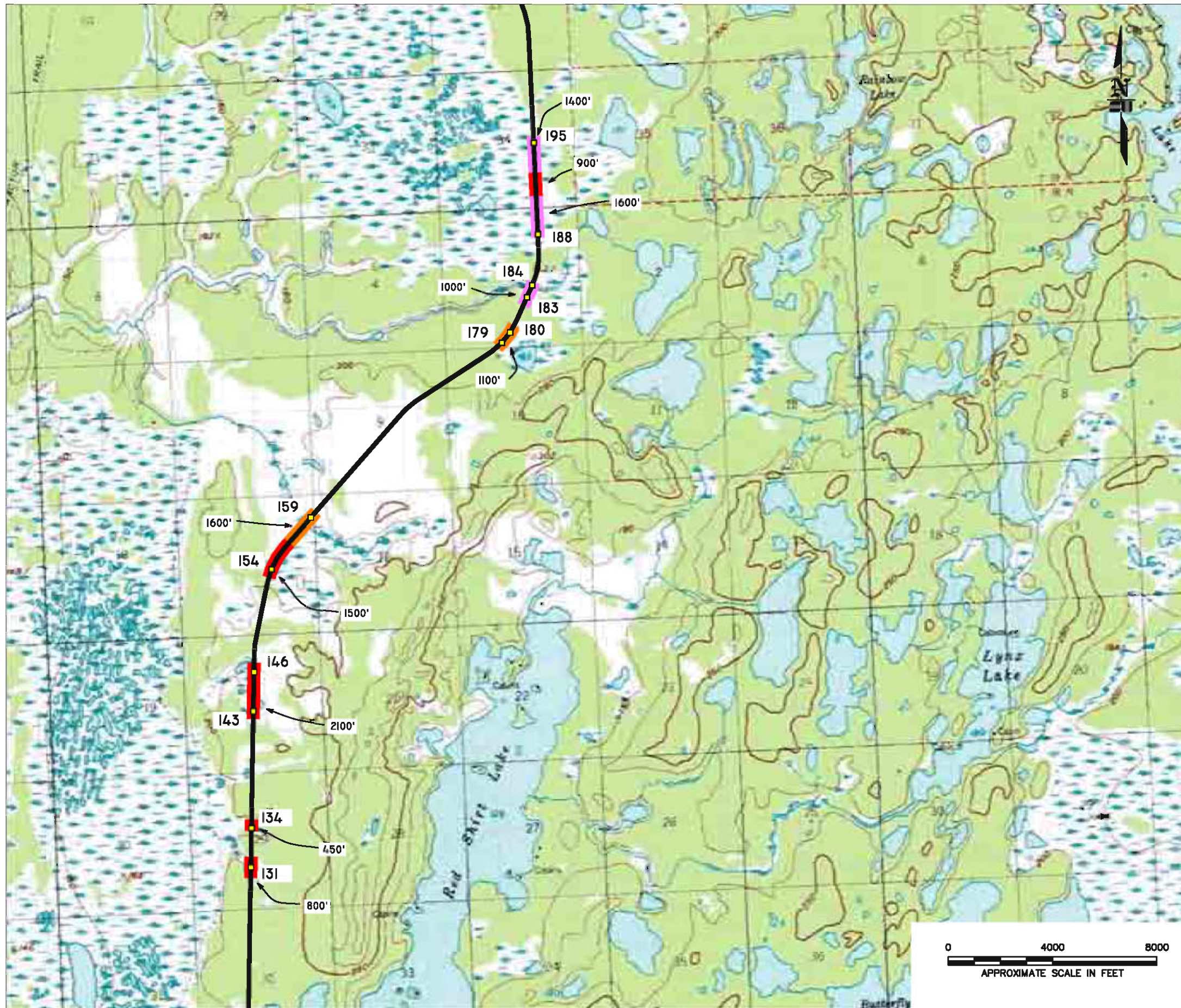
OVERVIEW MAP



- NOTES:
1. BASEMAP ADAPTED FROM USGS 1:63,000 SCALE MAP COVERAGE.
  2. RAILWAY ALIGNMENT IS ADAPTED FROM POINT LOCATIONS PROVIDED BY HDR ALASKA.
  3. BOG DELINEATIONS ALONG THE ALIGNMENT ARE ESTIMATED FROM USGS MAP COVERAGE AND FIELD NOTES.

MAT-SU RAIL EXTENSION MAT-SU BOROUGH, ALASKA	
ALIGNMENT MAP WEST ALIGNMENT POINTS 42 - 123	
OCTOBER 2007	32-I-01932
<b>SHANNON &amp; WILSON, INC.</b> <small>Geotechnical &amp; Environmental Consultants</small>	<b>FIG. 7</b> <small>SHT 2 OF 4</small>

*(This page intentionally left blank.)*



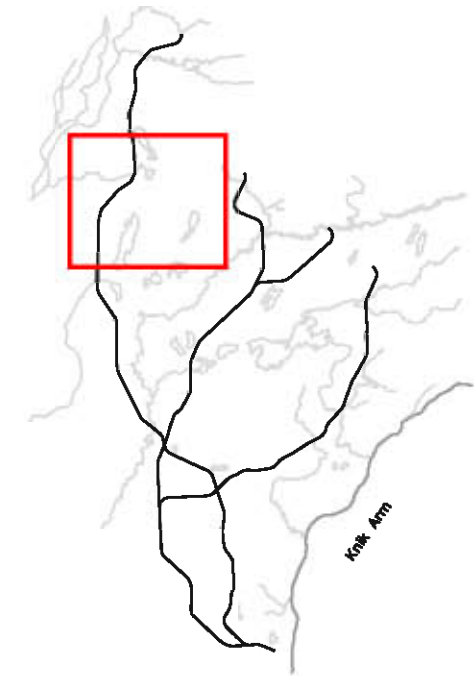
LEGEND

- 267 Survey Point
- 1000' Approximate length of delineated bog section

APPROXIMATE RAILWAY ALIGNMENT

- 0 to 3 feet silty soils with organics
- 3 to 5 feet soft/compressible soils
- 5 to 10 feet soft/compressible soils
- 10 to 15 feet soft/compressible soils

OVERVIEW MAP

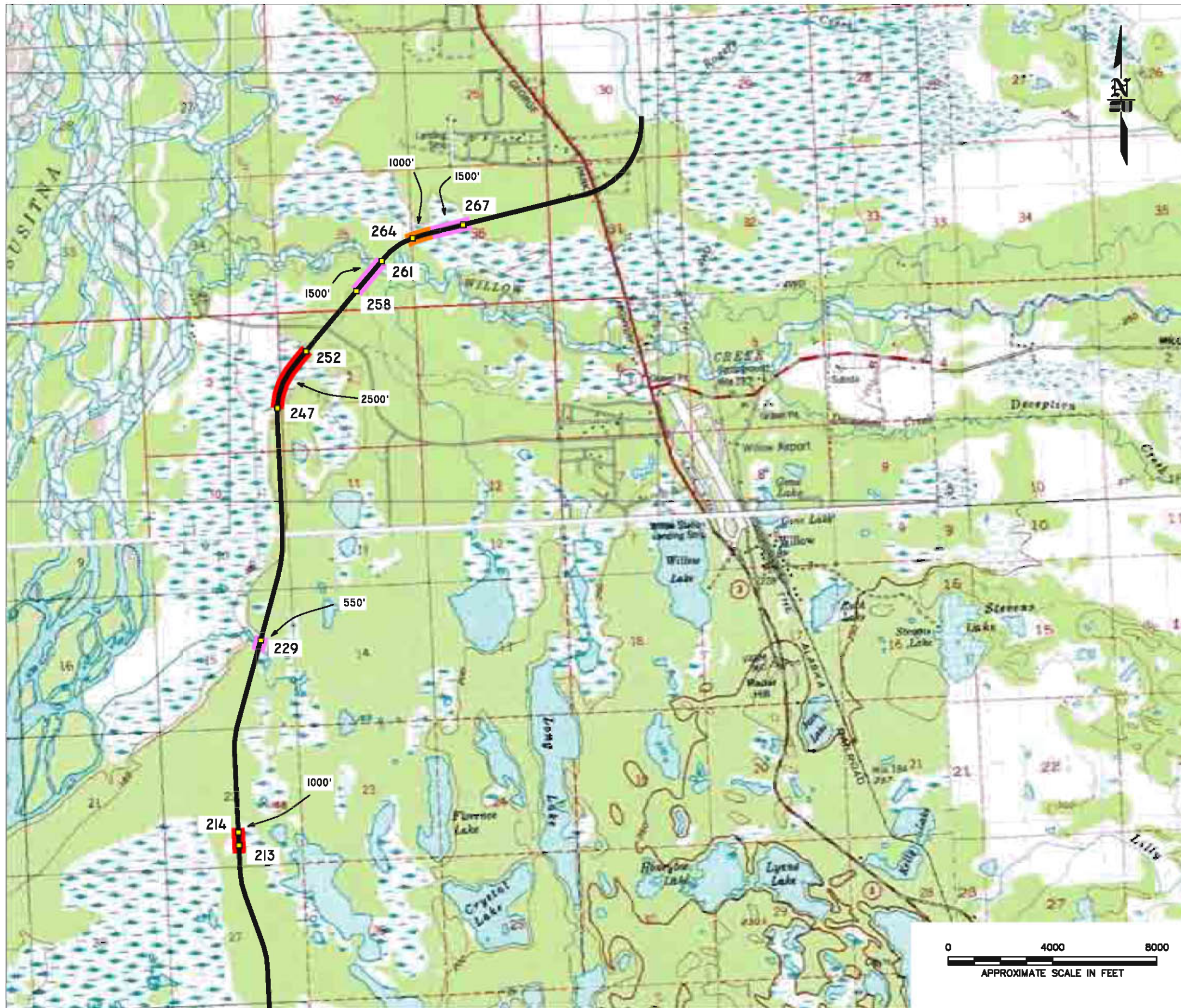


- NOTES:
1. BASEMAP ADAPTED FROM USGS 1:63,000 SCALE MAP COVERAGE.
  2. RAILWAY ALIGNMENT IS ADAPTED FROM POINT LOCATIONS PROVIDED BY HDR ALASKA.
  3. BOG DELINEATIONS ALONG THE ALIGNMENT ARE ESTIMATED FROM USGS MAP COVERAGE AND FIELD NOTES.



MAT-SU RAIL EXTENSION MAT-SU BOROUGH, ALASKA	
ALIGNMENT MAP WEST ALIGNMENT POINTS 121-205	
OCTOBER 2007	32-I-01932
<b>SHANNON &amp; WILSON, INC.</b> <small>Geotechnical &amp; Environmental Consultants</small>	<b>FIG. 7</b> <small>SHT 3 OF 4</small>

*(This page intentionally left blank.)*



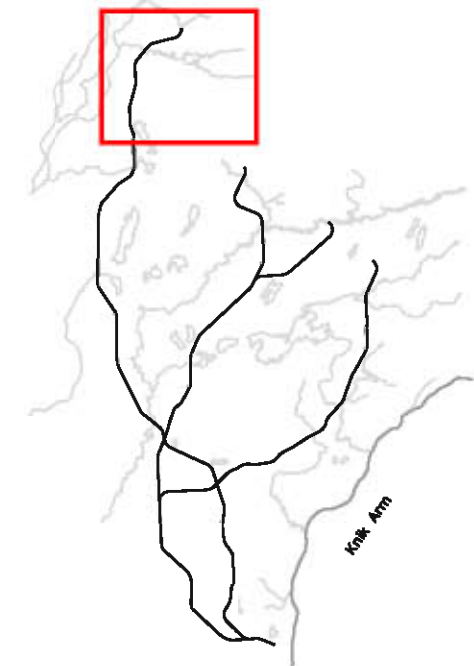
**LEGEND**

- 267 Survey Point
- 1000' Approximate length of delineated bog section

**APPROXIMATE RAILWAY ALIGNMENT**

- 0 to 3 feet silty soils with organics
- 3 to 5 feet soft/compressible soils
- 5 to 10 feet soft/compressible soils
- 10 to 15 feet soft/compressible soils

**OVERVIEW MAP**



**NOTES:**

1. BASEMAP ADAPTED FROM USGS 1:63,000 SCALE MAP COVERAGE.
2. RAILWAY ALIGNMENT IS ADAPTED FROM POINT LOCATIONS PROVIDED BY HDR ALASKA.
3. BOG DELINEATIONS ALONG THE ALIGNMENT ARE ESTIMATED FROM USGS MAP COVERAGE AND FIELD NOTES.



MAT-SU RAIL EXTENSION MAT-SU BOROUGH, ALASKA	
<b>ALIGNMENT MAP</b> WEST ALIGNMENT POINTS 201 - 286	
OCTOBER 2007	32-I-01932
SHANNON & WILSON, INC. Geotechnical & Environmental Consultants	FIG. 7 SHT 4 OF 4

*(This page intentionally left blank.)*



**ATTACHMENT B**  
**FIELD NOTES**

*(This page intentionally left blank.)*

## West Alignment (Points 1-286) September 2007

Point #	Probe Depth (feet)	Site Description	Photo #
11	5	North abutment for Little Su crossing. Dense alder and willow cover with tall grass. No hilly terrain on either side. River banks are 4 to 5 feet high. South side is gravel. North side is sand. Probe feels sandy.	077-080
12	4.5	Tall spruce and cottonwood grove.	
105+250	6	Spruce trees at edge of boggy area. Water on ground surface.	060-066
106	12	Bog with scattered, small spruce.	
107	>10	Boggy area 40 feet east of pond. Pond is about 6 feet deep with muddy bottom.	
107+100	>10	Boggy with medium dense spruce and willow cover. Pond to the west.	
117+25	>10	Bog with scattered, low spruce. Water on ground surface.	056-059
130	>10	Boggy with medium dense spruce and willow cover. Water on ground surface.	
131	>10	Hummocky bog with tall grass and scattered willows. Lies in a depression with small outlet to the north.	
142	4	Edge of forest and bog.	
143	9	Bog with scattered, small spruce.	
143+250	9	Small bog with scattered spruce trees. Water on ground surface. Surrounded by higher, well drained ground. Bottom of probe feels hard.	052-055
154	>10	Open bog. Forest area about 450 feet to the west was probed. Probe depth was 3.5 feet.	
155	>10	Edge of open, "floating" bog. Water on ground surface.	
156	>10	Bog with scattered, small spruce.	
157	4 to 6	Boggy area near small drainage. Tall grass and willow with scattered spruce. Very wet. Surface water too deep to get next to drainage.	044-051
184	3	Beaver pond on Rolly Ck. Bog is wet above pond level with water on the ground surface. Vegetation is tall grass and scattered spruce. Bottom of probe feels hard and sandy.	040-043
188	3	Bog extends north to Point 193+200. Southern 1,000 feet is significantly more marshy with water on the ground surface.	037-039
190	3	Spruce stand in bog extending north to Point 193+200. Tree cover is more dense. Bottom of probe is hard but doesn't feel granular.	034-036
192	>10	Spruce stand in bog extending north to Point 193+200.	031-033
193+200	2	North end of large bog to the north of Rolly Ck. Mostly tall grass with scattered spruce and birch trees. Bottom of probe feels granular (sandy) but soft.	028-030
228+250	2 to 3	Small drainage valley approximately 300 feet wide with 40 to 60-foot bluff on either side. Valley is flat and grass covered. Stream is 2 to 3 feet wide with gravelly sand bottom. Bottom of probe feels coarse.	020-027
248+150	>10	Boggy with medium dense willow cover.	
248+400	>10	Boggy with medium dense willow cover.	
250+60	>15	Tall, hummocky grass with dense willows.	
251	13	Boggy area with scattered willows 50 feet north of small drainage.	
258+50	3 to 4	Small drainage west of 50-foot tall bluff. Boggy ground with tall grass and standing water in drainage. Bottom of probe feels coarse.	009-012
260	4 to 6	South edge of Willow Ck. Crossing. Generally flat with cottonwoods and scattered spruce. Stream bottom and bars consist of relatively clean gravelly sand.	002-008, 013, 014
264+200	8	Edge of open bog and bog with scattered willows.	
265	5	Edge of open bog and bog with scattered willows. Thermister reading near this point was 12460 Ohms (5.4 degrees Celsius) at 5 feet bgs.	
266	4.5	Boggy with hummocks. Probed >10 feet, 200 feet to the south of Point 266 in an open area of the bog.	015-019
267	4.5	Boggy, hummocky with scattered willows.	
267+150	3.5	Edge of bog with scattered spruce.	

**Central Alignment (Points 287-401) September 2007**

Point #	Probe Depth (feet)	Site Description	Photo #
298+200	12	Unnamed drainage approx. 5 to 10 feet wide and 6 feet deep. Marshy with tall grass. Water on ground surface.	087-093
304	>10	Boggy with areas of scattered spruce.	094-096
305	2 to 3	Upland area between bogs. About +20 feet elevation over bog.	097, 098
306	>10	Boggy area near pond.	099-101
307	3	North edge of forested area between bogs.	
308	14	West edge of small lake. Lake depth at edge is 7 feet.	102, 103
309	>10	Open bog near north edge of small lake.	
310	>10	Boggy with areas of scattered spruce.	
336+100	11	Boggy area near small drainage. Drainage is approx. 2 feet wide and 3 feet deep with slow flow. Low grass and willow cover.	104-106
357+400	4.5	Boggy area near stream. Stream is approx. 6 feet wide and 3 feet deep with sandy bottom and very slow flow. Probe feels like loose sand.	107-109
359	14	Floating bog. Thermister reading is 13531 Ohms (3.7 degrees Celsius) at 14 feet bgs.	110-112
392+400	3.5	Grassy bog with small drainage on the south end. Drainage is approx. 2 feet wide and 2 feet deep. Probe bottom feels sandy.	
393	3.5	Mossy bog. Not as wet as 392+400	
394	3.5	Moss bog with scattered spruce.	

**Lake Creek Alignment (Points 402-483) September 2007**

Point #	Probe Depth (feet)	Site Description	Photo #
418	4	Bog with areas of scattered spruce. Bottom of probe feels sandy.	
420	6	Same bog as Point 418 with less spruce cover. Water on ground surface.	
425	4	Mossy bog with scattered, low spruce. Small hill to the south rises approximately 30 to 50 feet over bog. Bottom of probe is solid and coarse.	
426	6	Mossy bog with scattered, low spruce. Bottom of probe feels sandy but soft.	
430+150	3.5	Small open section in bog that is generally covered with light to medium spruce cover. Bottom is solid.	
434+400	7	Open area in oxbow off the Little Su. Tall grass but well drained. Bottom is hard.	
435+250	3	Near Little Su crossing with high willows on the river banks. Banks are approximately 3 feet high and show silty soils.	
441	>10	Bog with medium spruce cover. Small hill at bog exit to the north along the alignment. Power lines also across the bog to the north.	
452	>10	Boggy area with scattered spruce.	
457-459	>10	Open bog with spots of floating mat. Water on ground surface. Hills to the east.	
460	8	Bog with light spruce cover.	
463+250	5	Boggy area that shows a drainage on USGS map. No drainage noticeable on ground. Scattered short spruce and moderate shrub and grass cover. Hard granular probe bottom.	
473	>10	South edge of Lake Creek bog. Dense spruce and willow vegetation. Hills to the south.	
474	>10	Bog with scattered, low spruce.	
475	>20	Open bog with spots of floating mat. Water on ground surface.	
476	>10	Lake Creek bog near creek.	
476+350	>20	Boggy area on the Lake Creek Crossing. Standing water on ground surface up to 3 feet deep up to 200 feet away from visible open water. Hard pushing with probe.	162-166
478	>20	Boggy area near Lake Creek with scattered spruce. Water on ground surface. Thermister reading is 13594 Ohms (3.6 degrees Celsius) at 15 feet bgs.	159-161

**Houston Alignment (Points 484-549) September 2007**

Point #	Probe Depth (feet)	Site Description	Photo #
493+300	2.5	Boggy area near stream. Bottom of probe is sandy and probe could be advanced to 6 feet through loose sand.	167-169

**East Alignment (Points 550-735) September 2007**

Point #	Probe Depth (feet)	Site Description	Photo #
610	4	Hummocky bog with medium dense willow and spruce cover. Loose sand at bottom. Pushed probe to 10 feet after contact.	
611	>10	Open, hummocky bog with hills to the south. Some residences on the hillside. Very wet.	
619	7	Bog near drainage. Floating grass mat. Probe to 2 feet on north edge of mat.	
624	4	Boggy area not on map. Approximately 200 feet wide with birch covered hills (20 to 30 feet high) on either side. Probe bottom feels sandy.	
634+150	>10	Bog with scattered, low spruce. Bog is about 300 to 400 feet wide with low hills on either side.	
641+400	N/A	Near Fish Creek crossing. Water is high, 1 to 2 feet above normal? Sand/gravel bottom.	
642	3.5	Bog near Fish Creek with scattered spruce and tall grass. Water on ground surface. Probe bottom feels like sand/gravel.	
649	>10	Bog with scattered, low spruce. Tree cover gets more dense to the south.	
650	>10	Open, floating bog. Water on ground surface.	
651	>10	Bog with scattered spruce and floating mats. Water on ground surface.	
652	>10	Bog with scattered spruce.	
671	>10	Near drainage that flows into Threemile Lake. Tall grass and scattered alders.	
678+200	3.5	Burn area. Small bog area with scattered, burned spruce trunks between low hills.	
685+200	5	Boggy area with dense, low spruce cover. Area consists of alternating low hills and bogs.	
693	9.5	Boggy area with sparse spruce cover. Bottom of probe feels granular.	150-152
714	N/A	Little Meadow Creek approx. 500 feet west of crossing (due to private property). Creek is approx. 15 to 25 feet wide and 1 to 5 feet deep with a gravelly sand bottom. Creek banks are steep and approx. 1 to 2 feet high. Peat looks to be around 2 to 3 feet thick.	153-155
728+270	7.5	Boggy area with scattered spruce trees. No standing water on surface. Bottom of probe is hard/abrupt.	156-158

**Conn 1 Alignment (Points 736-777) September 2007**

Point #	Probe Depth (feet)	Site Description	Photo #
749	2 to 4	Boggy area with flowing drainage. Low brush and grass vegetation. Drainage is 6 to 8 feet wide and approximately 2 feet deep with sandy bottom. Surrounding area is hilly and spruce covered.	147-149
750	2	Dense spruce cover at edge of boggy area. Bottom of probe feels sandy.	145, 146

**Conn 3 Alignment (Points 819-864) September 2007**

Point #	Probe Depth (feet)	Site Description	Photo #
845	13	Route skirts the south edge of bog. Water on ground surface.	081-083
854	>10	Southern tip of My Lake. Floating bog with small drainage exiting the lake. Water on ground surface. Probed to 3 feet bgs in a small spruce stand about 300 feet east of Point 854.	084-086

**Mac West Alignment (Points 865-994) September 2007**

Point #	Probe Depth (feet)	Site Description	Photo #
882	2.5	Open area in tall spruce and birch forest. Silty soil with 6 inch peat mat.	118, 119
885	9	Boggy area with dense spruce trees. Water on ground surface	120-122
886	7.5	Bog with scattered spruce trees. Some water on ground surface.	
893	8	Boggy spot in trees. Tall grass cover and water on ground surface.	
910+125	4	Small open bog to the north of Point 910. Probe to 2 feet bgs at Point 910 in scattered spruce bog.	
911+100	3	Boggy area near drainage. Standing water on ground surface. Drainage is 2 to 4 feet wide and stagnant.	
968	N/A	West edge of Horseshoe Lake. Alignment cuts across edge of the lake.	141-144

**Mac East Alignment (Points 995-1100) September 2007**

Point #	Probe Depth (feet)	Site Description	Photo #
1014	9.5	Open bog surrounded by low-lying spruce bog. Water on ground surface.	123-125
1015	8	Same boggy area as Point 1014.	126-128
1021	5	Bog with scattered spruce. Opens to the north.	129, 130
1022	9	Floating bog.	131-134
1023	16	Floating bog with scattered spruce trees to the north. Bottom of probe feels sandy.	135-137
1047	6	Very wet, boggy area with floating mat. Tall grass and scattered, low spruce. Bog is surrounded by 6 to 8-foot high bluff.	138-140

**ATTACHMENT C**

**IMPORTANT INFORMATION ABOUT YOUR  
GEOTECHNICAL/ENVIRONMENTAL REPORT**

*(This page intentionally left blank.)*





Date: October 2007  
To: Tryck Nyman Hayes, Inc.  
Re: Mat-Su Rail Extension, Mat-Su Borough,  
Alaska

## **Important Information About Your Geotechnical/Environmental Report**

### **CONSULTING SERVICES ARE PERFORMED FOR SPECIFIC PURPOSES AND FOR SPECIFIC CLIENTS.**

Consultants prepare reports to meet the specific needs of specific individuals. A report prepared for a civil engineer may not be adequate for a construction contractor or even another civil engineer. Unless indicated otherwise, your consultant prepared your report expressly for you and expressly for the purposes you indicated. No one other than you should apply this report for its intended purpose without first conferring with the consultant. No party should apply this report for any purpose other than that originally contemplated without first conferring with the consultant.

### **THE CONSULTANT'S REPORT IS BASED ON PROJECT-SPECIFIC FACTORS.**

A geotechnical/environmental report is based on a subsurface exploration plan designed to consider a unique set of project-specific factors. Depending on the project, these may include: the general nature of the structure and property involved; its size and configuration; its historical use and practice; the location of the structure on the site and its orientation; other improvements such as access roads, parking lots, and underground utilities; and the additional risk created by scope-of-service limitations imposed by the client. To help avoid costly problems, ask the consultant to evaluate how any factors that change subsequent to the date of the report may affect the recommendations. Unless your consultant indicates otherwise, your report should not be used: (1) when the nature of the proposed project is changed (for example, if an office building will be erected instead of a parking garage, or if a refrigerated warehouse will be built instead of an unrefrigerated one, or chemicals are discovered on or near the site); (2) when the size, elevation, or configuration of the proposed project is altered; (3) when the location or orientation of the proposed project is modified; (4) when there is a change of ownership; or (5) for application to an adjacent site. Consultants cannot accept responsibility for problems that may occur if they are not consulted after factors, which were considered in the development of the report, have changed.

### **SUBSURFACE CONDITIONS CAN CHANGE.**

Subsurface conditions may be affected as a result of natural processes or human activity. Because a geotechnical/environmental report is based on conditions that existed at the time of subsurface exploration, construction decisions should not be based on a report whose adequacy may have been affected by time. Ask the consultant to advise if additional tests are desirable before construction starts; for example, groundwater conditions commonly vary seasonally.

Construction operations at or adjacent to the site and natural events such as floods, earthquakes, or groundwater fluctuations may also affect subsurface conditions and, thus, the continuing adequacy of a geotechnical/environmental report. The consultant should be kept apprised of any such events, and should be consulted to determine if additional tests are necessary.

### **MOST RECOMMENDATIONS ARE PROFESSIONAL JUDGMENTS.**

Site exploration and testing identifies actual surface and subsurface conditions only at those points where samples are taken. The data were extrapolated by your consultant, who then applied judgment to render an opinion about overall subsurface conditions. The actual interface between materials may be far more gradual or abrupt than your report indicates. Actual conditions in areas not sampled may differ from those predicted in your report. While nothing can be done to prevent such situations, you and your consultant can work together to help reduce their impacts. Retaining your consultant to observe subsurface construction operations can be particularly beneficial in this respect.

## **A REPORT'S CONCLUSIONS ARE PRELIMINARY.**

The conclusions contained in your consultant's report are preliminary because they must be based on the assumption that conditions revealed through selective exploratory sampling are indicative of actual conditions throughout a site. Actual subsurface conditions can be discerned only during earthwork; therefore, you should retain your consultant to observe actual conditions and to provide conclusions. Only the consultant who prepared the report is fully familiar with the background information needed to determine whether or not the report's recommendations based on those conclusions are valid and whether or not the contractor is abiding by applicable recommendations. The consultant who developed your report cannot assume responsibility or liability for the adequacy of the report's recommendations if another party is retained to observe construction.

## **THE CONSULTANT'S REPORT IS SUBJECT TO MISINTERPRETATION.**

Costly problems can occur when other design professionals develop their plans based on misinterpretation of a geotechnical/environmental report. To help avoid these problems, the consultant should be retained to work with other project design professionals to explain relevant geotechnical, geological, hydrogeological, and environmental findings, and to review the adequacy of their plans and specifications relative to these issues.

## **BORING LOGS AND/OR MONITORING WELL DATA SHOULD NOT BE SEPARATED FROM THE REPORT.**

Final boring logs developed by the consultant are based upon interpretation of field logs (assembled by site personnel), field test results, and laboratory and/or office evaluation of field samples and data. Only final boring logs and data are customarily included in geotechnical/environmental reports. These final logs should not, under any circumstances, be redrawn for inclusion in architectural or other design drawings, because drafters may commit errors or omissions in the transfer process.

To reduce the likelihood of boring log or monitoring well misinterpretation, contractors should be given ready access to the complete geotechnical engineering/environmental report prepared or authorized for their use. If access is provided only to the report prepared for you, you should advise contractors of the report's limitations, assuming that a contractor was not one of the specific persons for whom the report was prepared, and that developing construction cost estimates was not one of the specific purposes for which it was prepared. While a contractor may gain important knowledge from a report prepared for another party, the contractor should discuss the report with your consultant and perform the additional or alternative work believed necessary to obtain the data specifically appropriate for construction cost estimating purposes. Some clients hold the mistaken impression that simply disclaiming responsibility for the accuracy of subsurface information always insulates them from attendant liability. Providing the best available information to contractors helps prevent costly construction problems and the adversarial attitudes that aggravate them to a disproportionate scale.

## **READ RESPONSIBILITY CLAUSES CLOSELY.**

Because geotechnical/environmental engineering is based extensively on judgment and opinion, it is far less exact than other design disciplines. This situation has resulted in wholly unwarranted claims being lodged against consultants. To help prevent this problem, consultants have developed a number of clauses for use in their contracts, reports and other documents. These responsibility clauses are not exculpatory clauses designed to transfer the consultant's liabilities to other parties; rather, they are definitive clauses that identify where the consultant's responsibilities begin and end. Their use helps all parties involved recognize their individual responsibilities and take appropriate action. Some of these definitive clauses are likely to appear in your report, and you are encouraged to read them closely. Your consultant will be pleased to give full and frank answers to your questions.

The preceding paragraphs are based on information provided by the  
ASFE/Association of Engineering Firms Practicing in the Geosciences, Silver Spring, Maryland

November 20, 2007

Tryck Nyman Hayes, Inc.  
911 West 8<sup>th</sup> Avenue, Suite 300  
Anchorage, Alaska 99501

Attn: Mr. Norm Gutcher, P.E.

FAX: (907) 279-0543

**RE: REVISED BORROW MATERIAL AVAILABILITY, MAT-SU RAIL  
EXTENSION, MAT-SU BOROUGH, ALASKA**

This letter report provides a “broad brush” evaluation of borrow material availability for use in the planned Mat Su Rail Extension through the Mat-Su Valley, Mat-Su Borough, Alaska. Shannon & Wilson recently completed a geotechnical reconnaissance along the concept alignments that are being evaluated for planning the rail extension. The reconnaissance study included evaluating landforms and probing soft or compressible soils and the results of the study are reported in our October 26, 2007 letter report titled *Geotechnical Reconnaissance, Mat-Su Rail Extension, Mat-Su Borough, Alaska*. With the reconnaissance report as a starting point, this communication on borrow material availability also considers aerial imagery, USGS topography maps, surface geology maps, and our general understanding of the geological setting of the project vicinity.

**Geological Setting:**

The terrain traversed by the potential rail alignments are dominated by glacially derived landforms. The project vicinity has been subject to several glacial advances that have completely or partially covered the proposed corridors with glacial ice. As a result, the soils in the area have been extensively worked and/or deposited by glacial action including ice transport and glacial meltwater transport. The most recent glacial advance known as the Naptowne Glaciation created and shaped much of the landforms that are visible today in the project vicinity. This glacial advance transported soil and rock debris from the Chugach and Talkeetna Mountains and deposited moraine and outwash soils in the Upper Cook Inlet.

Given this geologic history, we believe that the most prominent soil types (not including organic and bog soils) in the project area consist of moraines and outwash deposits. Moraine soils usually contain a wide variety gradation ranges and can include clay through boulder sized particles. Because of their gradation and the manner in which they were placed, these soils tend to be relatively dense. Typically there is no distinctly layering to these deposits and they are

typically found in (and beneath) topographically high areas. The undulating topography of much of the project area likely comprises these soil deposits.

Outwash soil is deposited by meltwater running off or from under the glacial ice mass and are categorized as glaciofluvial. As sediment-laden water runs off of the glacier, it typically loses much of its velocity as it begins to run over the ground surface. At the decrease in velocity, the coarser sediment (sand and gravel) is deposited and finer sediments (silt and clay) remain in suspension. The resultant outwash deposit is usually a large, flat plane that extends out from the toe of the glacier, consisting of relatively clean sand and gravel. Typically, the gradation of the outwash soils is coarser the closer they are to the toe of the glacier. Other glaciofluvial deposits may exist in other areas of the project; however, these deposits are likely to be isolated and relatively small in comparison to the moraine deposits.

The landforms in the project area suggest an apparent boundary between moraine soils and outwash soils. We have included an approximate boundary between these two regions on Figure 1.

#### **Field Observations:**

During our reconnaissance efforts, we observed the general terrain along the alignments being considered for the rail extension. Due to typically dense vegetative cover, the soil conditions over most of the areas visited was obscured from view. Our limited observations in the field agree with the generalizations made above regarding potential soil types for moraine and outwash deposits. It is our opinion that much of the soils in the outwash area will comprise sand and gravel soils that are relatively clean. Experience in the area suggests that the outwash soils may tend to contain more sand than gravel, but isolated areas of increased gravel soils are likely present. In the northern reaches of the project area (morainal areas) in existing road cuts, the soils appear to contain more gravel and fine grained fractions (silt and clay).

#### **Borrow Sources:**

Due to the lateral extent of this project, multiple sources of borrow will be needed to complete construction. Given our experience in the area, we believe that, on average, most of the moraine soils found in the upland portions of the project area are likely embankable with careful moisture control. As such, the railroad will likely be able to be developed by balancing cuts and fills through the hilly upland areas. Larger borrow sources will also have to be developed in the upland areas to provide fill for expansive bogs where fill will not be recoverable along the alignment.

Given our experience in the project area and the geological terrains described above, we believe that the vicinity can be divided into three regions: outwash, moraine, and low-lying bogs. These three areas are indicated in the site plan on Figure 1. The boundaries shown on Figure 1 are based on topography and our limited observations in the field and should be considered a rough approximation of the actual boundaries for planning purposes. In general, we believe that borrow sources developed in the outwash region will likely yield relatively clean sand and gravel using conventional borrow source mining techniques. We anticipate that ground water in these areas is likely to be relatively deep and borrow sources will, as a result, be able to be developed relatively deeply. We anticipate that soils in the outwash area will be relatively consistent and uniform in developed borrow sources.

Borrow sources developed in the moraine regions will likely encountered variable soil conditions. Topography in these areas tends to be hummocky and isolated areas of poorly drained soils and shallow water are present. The isolated water could prove problematic in developing some of the moraine areas as borrow sources as these soils will likely be more silty and therefore, more difficult to handle under wet conditions. Because of the terrain and isolated areas of water, borrow sources in the moraine areas will likely need to be smaller in scale to mine isolated topographic highs.

We do not believe that borrow sources in the low-lying bog areas will be able to provide a significant amount of fill for this project. Many of the boggy areas along the project alignments are scattered with isolated topographic highs, however, they are usually less than about 10 to 20 feet above the bog areas and limited in lateral extent. We do not believe that dredging is a viable option in developing borrow sources below the bog surface in these areas due to likely permitting implications and potentially silty and clayey soil conditions beneath the bogs.

### **LIMITATIONS**

The analyses and conclusions contained in this report are based on site conditions as they presently exist. It is assumed that the landform analysis and related soil type interpretations included herein are representative of the conditions throughout project vicinity, i.e., the conditions everywhere are not significantly different from those assumed in this letter.

Unanticipated conditions are commonly encountered during construction and cannot fully be determined by merely taking soil samples or advancing borings. Such unexpected conditions frequently require that additional expenditures be made to attain a properly constructed project. The information included in this letter should be used for planning purposes only and once a preferred alignment is selected, we recommend follow-on work to further evaluate and characterize potential borrow sources for this project.

Mat-Su Rail Extension Borrow Availability,  
Mat-Su Borough, Alaska  
November 20, 2007  
Page 4

**SHANNON & WILSON, INC.**

Shannon & Wilson has prepared the attachment *Important Information About Your Geotechnical/Environmental Report* to assist you and others in understanding the use and limitations of the reports.

Sincerely,

**SHANNON & WILSON, INC.**

Prepared By:



Kyle Brennan, P.E.  
Senior Geotechnical Engineer

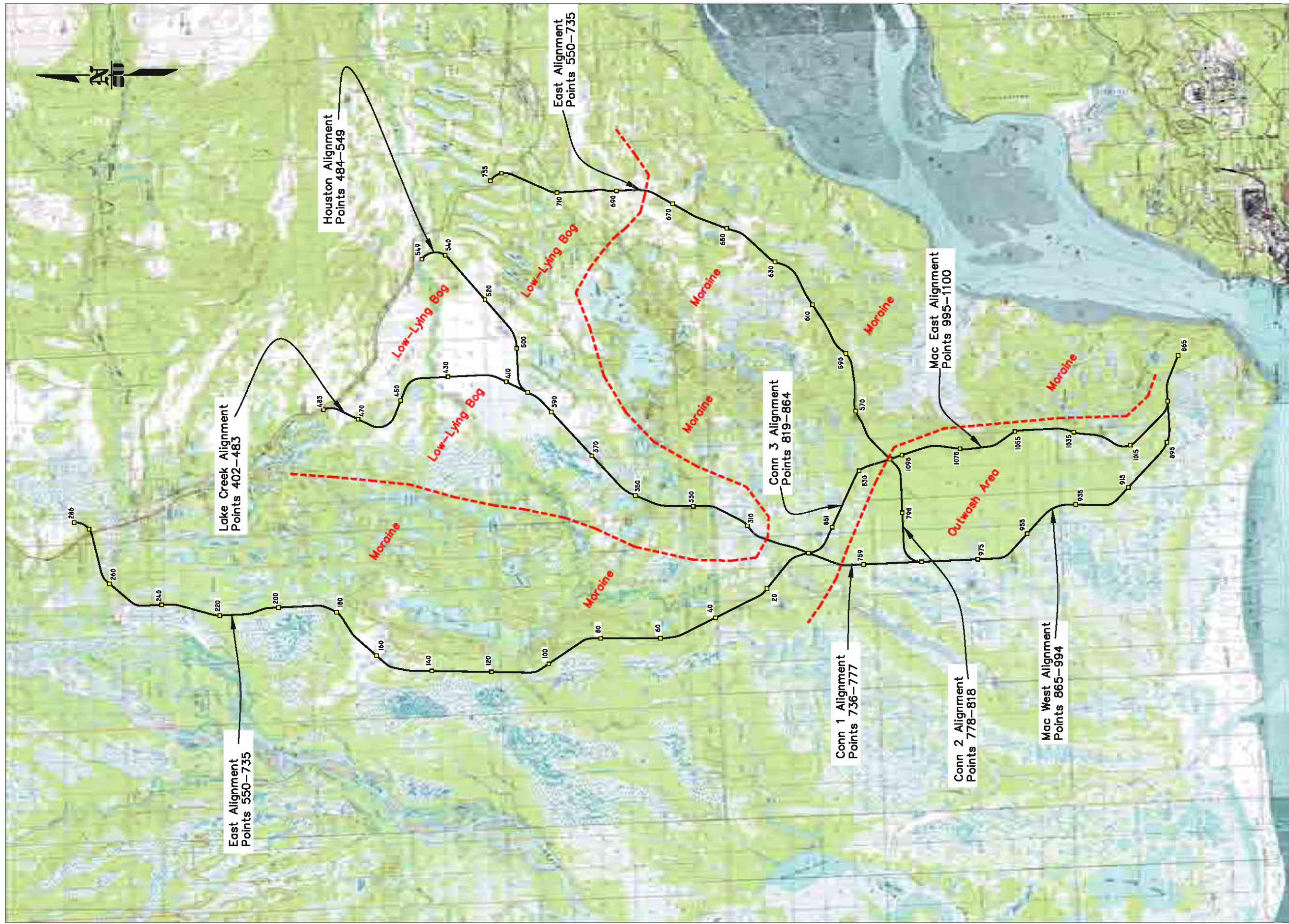
Approved By:



William S. Burgess, P.E.  
Senior Associate

Attachments: Figure 1: Site Plan  
Important Information About Your Geotechnical/Environmental Report

32-1-01932r1



**LEGEND**

□ 267  
 Survey Point



MAT-SU RAIL EXTENSION  
 MAT-SU BOROUGH, ALASKA

**SITE PLAN**

OCTOBER 2007



32-1-01932  
 FIG. 1

*(This page intentionally left blank.)*



**ATTACHMENT A**  
**IMPORTANT INFORMATION ABOUT YOUR**  
**GEOTECHNICAL/ENVIRONMENTAL REPORT**

32-1-01932

*(This page intentionally left blank.)*



Date: October 2007  
To: Tryck Nyman Hayes, Inc.  
Re: Mat-Su Rail Extension, Mat-Su Borough,  
Alaska

## **Important Information About Your Geotechnical/Environmental Report**

### **CONSULTING SERVICES ARE PERFORMED FOR SPECIFIC PURPOSES AND FOR SPECIFIC CLIENTS.**

Consultants prepare reports to meet the specific needs of specific individuals. A report prepared for a civil engineer may not be adequate for a construction contractor or even another civil engineer. Unless indicated otherwise, your consultant prepared your report expressly for you and expressly for the purposes you indicated. No one other than you should apply this report for its intended purpose without first conferring with the consultant. No party should apply this report for any purpose other than that originally contemplated without first conferring with the consultant.

### **THE CONSULTANT'S REPORT IS BASED ON PROJECT-SPECIFIC FACTORS.**

A geotechnical/environmental report is based on a subsurface exploration plan designed to consider a unique set of project-specific factors. Depending on the project, these may include: the general nature of the structure and property involved; its size and configuration; its historical use and practice; the location of the structure on the site and its orientation; other improvements such as access roads, parking lots, and underground utilities; and the additional risk created by scope-of-service limitations imposed by the client. To help avoid costly problems, ask the consultant to evaluate how any factors that change subsequent to the date of the report may affect the recommendations. Unless your consultant indicates otherwise, your report should not be used: (1) when the nature of the proposed project is changed (for example, if an office building will be erected instead of a parking garage, or if a refrigerated warehouse will be built instead of an unrefrigerated one, or chemicals are discovered on or near the site); (2) when the size, elevation, or configuration of the proposed project is altered; (3) when the location or orientation of the proposed project is modified; (4) when there is a change of ownership; or (5) for application to an adjacent site. Consultants cannot accept responsibility for problems that may occur if they are not consulted after factors, which were considered in the development of the report, have changed.

### **SUBSURFACE CONDITIONS CAN CHANGE.**

Subsurface conditions may be affected as a result of natural processes or human activity. Because a geotechnical/environmental report is based on conditions that existed at the time of subsurface exploration, construction decisions should not be based on a report whose adequacy may have been affected by time. Ask the consultant to advise if additional tests are desirable before construction starts; for example, groundwater conditions commonly vary seasonally.

Construction operations at or adjacent to the site and natural events such as floods, earthquakes, or groundwater fluctuations may also affect subsurface conditions and, thus, the continuing adequacy of a geotechnical/environmental report. The consultant should be kept apprised of any such events, and should be consulted to determine if additional tests are necessary.

### **MOST RECOMMENDATIONS ARE PROFESSIONAL JUDGMENTS.**

Site exploration and testing identifies actual surface and subsurface conditions only at those points where samples are taken. The data were extrapolated by your consultant, who then applied judgment to render an opinion about overall subsurface conditions. The actual interface between materials may be far more gradual or abrupt than your report indicates. Actual conditions in areas not sampled may differ from those predicted in your report. While nothing can be done to prevent such situations, you and your consultant can work together to help reduce their impacts. Retaining your consultant to observe subsurface construction operations can be particularly beneficial in this respect.

## **A REPORT'S CONCLUSIONS ARE PRELIMINARY.**

The conclusions contained in your consultant's report are preliminary because they must be based on the assumption that conditions revealed through selective exploratory sampling are indicative of actual conditions throughout a site. Actual subsurface conditions can be discerned only during earthwork; therefore, you should retain your consultant to observe actual conditions and to provide conclusions. Only the consultant who prepared the report is fully familiar with the background information needed to determine whether or not the report's recommendations based on those conclusions are valid and whether or not the contractor is abiding by applicable recommendations. The consultant who developed your report cannot assume responsibility or liability for the adequacy of the report's recommendations if another party is retained to observe construction.

## **THE CONSULTANT'S REPORT IS SUBJECT TO MISINTERPRETATION.**

Costly problems can occur when other design professionals develop their plans based on misinterpretation of a geotechnical/environmental report. To help avoid these problems, the consultant should be retained to work with other project design professionals to explain relevant geotechnical, geological, hydrogeological, and environmental findings, and to review the adequacy of their plans and specifications relative to these issues.

## **BORING LOGS AND/OR MONITORING WELL DATA SHOULD NOT BE SEPARATED FROM THE REPORT.**

Final boring logs developed by the consultant are based upon interpretation of field logs (assembled by site personnel), field test results, and laboratory and/or office evaluation of field samples and data. Only final boring logs and data are customarily included in geotechnical/environmental reports. These final logs should not, under any circumstances, be redrawn for inclusion in architectural or other design drawings, because drafters may commit errors or omissions in the transfer process.

To reduce the likelihood of boring log or monitoring well misinterpretation, contractors should be given ready access to the complete geotechnical engineering/environmental report prepared or authorized for their use. If access is provided only to the report prepared for you, you should advise contractors of the report's limitations, assuming that a contractor was not one of the specific persons for whom the report was prepared, and that developing construction cost estimates was not one of the specific purposes for which it was prepared. While a contractor may gain important knowledge from a report prepared for another party, the contractor should discuss the report with your consultant and perform the additional or alternative work believed necessary to obtain the data specifically appropriate for construction cost estimating purposes. Some clients hold the mistaken impression that simply disclaiming responsibility for the accuracy of subsurface information always insulates them from attendant liability. Providing the best available information to contractors helps prevent costly construction problems and the adversarial attitudes that aggravate them to a disproportionate scale.

## **READ RESPONSIBILITY CLAUSES CLOSELY.**

Because geotechnical/environmental engineering is based extensively on judgment and opinion, it is far less exact than other design disciplines. This situation has resulted in wholly unwarranted claims being lodged against consultants. To help prevent this problem, consultants have developed a number of clauses for use in their contracts, reports and other documents. These responsibility clauses are not exculpatory clauses designed to transfer the consultant's liabilities to other parties; rather, they are definitive clauses that identify where the consultant's responsibilities begin and end. Their use helps all parties involved recognize their individual responsibilities and take appropriate action. Some of these definitive clauses are likely to appear in your report, and you are encouraged to read them closely. Your consultant will be pleased to give full and frank answers to your questions.

The preceding paragraphs are based on information provided by the  
ASFE/Association of Engineering Firms Practicing in the Geosciences, Silver Spring, Maryland

December 20, 2007

Tryck Nyman Hayes, Inc.  
911 West 8<sup>th</sup> Avenue, Suite 300  
Anchorage, Alaska 99501

Attn: Mr. Mike Pochop, P.E.

FAX: (907) 279-0543

**RE: REFINED EVALUATION OF BORROW MATERIAL AVAILABILITY, PORT  
MACKENZIE RAIL EXTENSION, MAT-SU BOROUGH, ALASKA**

This letter report provides a refined evaluation of borrow material availability for use in the planned Port Mackenzie Extension through the Mat-Su Valley, Mat-Su Borough, Alaska. Shannon & Wilson recently completed a geotechnical reconnaissance along the concept alignments that are being evaluated for planning the rail extension. The results of the reconnaissance study were documented in an October 26, 2007 letter report titled *Geotechnical Reconnaissance, Mat-Su Rail Extension, Mat-Su Borough, Alaska*. The reconnaissance study included evaluating landforms and probing soft or compressible soils. Building on the geotechnical reconnaissance, Shannon & Wilson initiated an evaluation of borrow material based on USGS topography maps, surface geology maps, and our general understanding of the geological setting of the project vicinity. The findings of this study were reported in a November 20, 2007 letter report titled *Revised Borrow Material Availability, Mat-Su Rail Extension, Mat-Su Borough, Alaska*.

This refined borrow source evaluation reiterates the geological setting presented in our November 20 letter and goes to the next step of evaluating aerial photography for evidence of existing borrow pits and then locating topographic features that could potentially be developed for new borrow sites along the project alignments. Existing features believed to be borrow sites are shown geographically on an aerial photo base image in Figure 1. The borrow sites within approximately one mile of the project alignments are numbered on Figure 1 and listed in Table 1 with location information in terms of Township, Range, and Section. For each pit, the Soil Conservation Service (SCS) Soil classifications are also presented in Table 1. Similarly, Figure 2 shows the geographical location of potential new borrow sites with associated location and soil classification information presented in Table 1.

A brief note is needed about the concept alignments shown on Figures 1 and 2. The alignments shown on Figure 2 correspond with the alignments flown and probed during Shannon & Wilson's geotechnical reconnaissance. The corridors shown on Figure 1 correspond with minor realignment that are intended to take advantage of more favorable site conditions

identified in the field reconnaissance along with other administrative factors. The two generations of alignments are purposely shown to bridge the time span between field reconnaissance and research of potential borrow pits. The alignments are slightly different, as can be seen by comparing Figures 1 and 2, but the differences are inconsequential in terms of the availability of borrow material.

**Geological Setting:**

The terrain traversed by the potential rail alignments are dominated by glacially derived landforms. The project vicinity has been subject to several glacial advances that have completely or partially covered the proposed corridors with glacial ice. As a result, the soils in the area have been extensively worked and/or deposited by glacial action including ice transport and glacial meltwater transport. The most recent glacial advance known as the Naptowne Glaciation created and shaped much of the landforms that are visible today in the project vicinity. This glacial advance transported soil and rock debris from the Chugach and Talkeetna Mountains and deposited moraine and outwash soils in the Upper Cook Inlet.

Given this geologic history, we believe that the most prominent soil types (not including organic and bog soils) in the project area consist of moraines and outwash deposits. Moraine soils usually contain a wide variety gradation ranges and can include clay through boulder sized particles. Because of their gradation and the manner in which they were placed, these soils tend to be relatively dense. Typically there is no distinct layering to these deposits and they are commonly found in (and beneath) topographically high areas. The undulating topography of much of the project area likely comprises these soil deposits.

Outwash soil is deposited by meltwater running off or from under the glacial ice mass and are categorized as glaciofluvial. As sediment-laden water runs off of the glacier, it typically loses much of its velocity as it begins to run over the ground surface. At the decrease in velocity, the coarser sediment (sand and gravel) is deposited and finer sediments (silt and clay) remain in suspension. The resultant outwash deposit is usually a large, flat plane that extends out from the toe of the glacier, consisting of relatively clean sand and gravel. Typically, the gradation of the outwash soils is coarser the closer they are to the toe of the glacier. Other glaciofluvial deposits may exist in other areas of the project; however, these deposits are likely to be isolated and relatively small in comparison to the moraine deposits. Eolian or wind borne deposits of sand and silt are also locally present in the Susitna River drainage along the western margin of the Port Mackenzie Rail Extension project area.

The landforms in the project area suggest an apparent boundary between moraine soils and outwash soils. We have included an approximate boundary between these two regions overlaid on the USGS topographic base map in Figure 2.

**Existing Borrow Sources:**

Over 30 existing borrow sites were identified within about one mile of the Port Mackenzie Rail Extension concept alignments from a detailed review of aerial photography. The one mile distance was arbitrarily chosen to filter the existing borrow sites considering cost factors related to the haul distance. Nearly 70 additional borrow pits were also evident at a slightly greater distance away from the concept alignments. Many of these pits are located close to the Parks Highway and Knik-Goose Bay Road, or in the road and residential areas surrounding Big Lake. We anticipate that many of the pits are small and/or on private property such that they would be difficult to develop for large quantities of borrow as will be needed for the rail extension project. An area for future study of borrow availability would be to research the parcel size and ownership records for the existing borrow pits identified in Table 1 and then proceed to contact responsible parties regarding the feasibility of mining additional borrow for the rail project. The remainder of this communication focuses on the suitability of material that may be accessible in the borrow sites listed in Table 1.

**Mac East Alignment** – Five existing borrow pits were identified within one mile of the Mac East alignment. Pits 1, 2, 3, and 5 appear to be situated in the outwash plain and would likely produce relatively clean granular soil after stripping a few feet of silty overburden. Shannon & Wilson conducted a borrow source evaluation in 2003 for the Mat-Su Borough that characterized the Pit 1 and the moraine east and south of Lake Loraine near the south end of the Port Mackenzie Rail Extension. The results of the study were presented in an October 2003 *Sand & Gravel Assessment, Elmendorf Moraine, Port Mackenzie, Alaska*. Two borings close to Pit 1 encountered sand or gravelly sand with fines contents in the 3 to 6 percent range. The water table during drilling in September 2003 was within about 5 to 10 feet of the ground surface. Therefore, large quantities of non-frost-susceptible borrow may be available in expanding Pit 1, but it may be necessary to dredge and stockpile borrow material to control moisture contents.

The *Sand & Gravel Assessment* study identified a potential for extracting up to 27 million cubic yards of coarse grained materials (clean to silty, sandy gravel to gravelly sand) from the moraine in the study area (portions of Sections 23, 24, and 26). Pit 4 is situated in the moraine complex east of the Mac East alignment; borrow material from this pit would likely comprise a mixture of silt and sand with a minor gravel fraction. The moraine soil is locally moisture sensitive and poorly drained so segregation may be needed to produce borrow from this pit suitable for

compaction with moisture/density control and the resulting embankment may be frost susceptible.

**East Alignment** – Seven existing borrow pits are located in the East Alignment corridor. Pit 6 is close to the “T” where the Knik-Goose Bay road intersects Port Mackenzie Road. Based on landforms and SCS classifications, this pit appears to be on the northeast margin of the outwash plain and should reasonably produce good quality borrow material. Pits 7 through 12 are in the moraine to bog transition zone. Except for Pit 8, SCS classifications suggest that these pits should produce predominantly coarse granular soils. However, it may be necessary to move 5 feet or more of silty overburden to produce usable borrow from these pits and the borrow material would likely require segregation and stockpiling to control moisture contents.

**Conn 1, 2, and 3 Alignments** – Five existing borrow pits (Pits 13 through 17) fall in the approximately triangular area defined by the three Conn alignments. These pits all appear to be within or at the northern margin of the outwash plain and should reasonably produce good quality borrow material. As evident from the aerial photograph, much of the Conn triangle is currently developed for farming because of the rich silt loam cap overlying the granular outwash material. The silt cap would need to be stripped to expose usable borrow.

**Central Alignment** – One existing pit (Pit 18) was observed between the Papoose Twins lakes within one mile of the Central alignment. According to the SCS classifications, borrow from this pit may be predominantly fine-grained silty soil, which would be consistent with the general moraine landform. Consequently, borrow from this pit may be moisture sensitive and frost susceptible and difficult to incorporate into embankments without experiencing long-term settlement, seasonal frost movements, and/or vulnerability to seismic-induced distress.

**Lake Creek and Houston Alignments** – Four existing borrow pits (Pits 19 through 22) are located close to the Parks Highway near the Lake Creek and Houston alignments. These pits appear to be close to the northeast margin of a bog complex where SCS classifications favor predominantly granular soil but indicate the soils may locally consist of predominantly fine-grained material (note SCS Classification for Pit 19). These pits were likely developed for construction of the Parks Highway and/or the Alaska Railroad Anchorage to Fairbanks track, so material data reports for borrow previously excavated from the pits may be available from the ADOT or ARRC. Shannon & Wilson contacted the ADNRR to attempt to identify State borrow pits and was told the records are not readily available, so further research is needed.

**West Alignment** – Nine existing borrow pits (Pits 23 through 31) were identified within one mile of the West alignment. Pit 23 was noted to be a gravel pit in the SCS data and contained no further information on soil classifications. Based on the SCS classifications, the soil type most



likely to be encountered in the remaining pits is likely sand with minor gravel and silt fractions except for Pit 24 where silt contents may be dominant.

**Potential New Borrow Sources:**

Shannon & Wilson identified ten potential new borrow sources along the project concept alignments. We concentrated on topographic highs along portions of alignment where there were notable gaps between existing borrow sources. Potential Pits 1 and 2 on Figure 2 represent locations that Shannon & Wilson evaluated for the Mat-Su Borough in conjunction with work on the planned Mat-Su Prison project (reference March 2007, *Geotechnical Report, Mat-Su Borough Borrow Source Assessment, Point Mackenzie, Alaska*). Pit 1 corresponds to explorations conducted close to the intersection of Alsop Road and Port Mackenzie Road. The native soils in the level of rolling plain at this location appear to correspond to glacial outwash and are classified as sandy gravel or gravelly sand with typically less than 6 percent fines (i.e., high quality granular borrow meeting the criteria for non-frost-susceptible [NFS] fill that will be needed for the frost cap on the railroad embankment). The native soils in the higher ground north of Alsop Road are encountered as complex mixtures of silt, sand, and gravel (20 to 30 or more percent fines); these soils correspond to moraine deposits and are typically more difficult to use as engineered fill. Pit 2 is located between Port Mackenzie Road and the west end of Goose Bay encountered NFS sandy gravel and gravelly sand, but these desirable materials were often interbedded with layers of silty soil. Pit 2 could produce high quality borrow but the NFS material would have to be segregated from the siltier, frost-susceptible soil during excavation.

Potential Pits 3 and 4 are located in the moraine landform along the East Alignment. SCS classifications for these locations suggest that the soils may be predominantly silty. In practice, we would anticipate that the soils would be similar to the upland soils encountered in prior work near Pit 1 (i.e., complex mixtures of silt, sand, and gravel), which are not ideal for use as embankment fill material.

Pits 5 through 8 are located in the moraine landform along the West Alignment. Similar to Pits 3 and 4, we anticipate that these locations would likely produce complex mixtures of silt, sand, and gravel and this assessment corresponds to the SCS classifications, which suggest that there will be 15 percent or less of gravel or sand. We believe that the West Alignment can likely be constructed by balancing cut and fill, but caution that the cut soil may be moisture sensitive and frost susceptible. Therefore, frost cap NFS fill material could be "manufactured" by processing borrow from the West Alignment potential borrow pits, or the NFS fill could be imported from distant pits in the outwash plain or from existing pits along the Parks Highway.

Pits 9 and 10 correspond with topographic highs that rise above the bog landform of the Central Alignment. SCS classifications for these locations suggest that soil in these hills could be a mixture of silt, sand, and gravel not unlike the soil encountered in the uplands near Pit 1 (see above). In our opinion, the topographic highs represented by Pits 9 and 10 are likely moraine landforms and borrow extracted from these pits may not be ideal for use as embankment fill material. Similar to the West Alignment, it is possible the topographic highs could be mined for borrow to construct some of the Central Alignment embankment, but the frost cap NFS fill material would need to be manufactured or imported from distant pits in the outwash plain or along the Parks Highway.

The Mac West Alignment has not specifically been addressed in the prior discussion of existing borrow sources (none were noted in our aerial photo review) and no distinct topographic highs were designated as potential borrow pits. However, we believe that the Mac West alignment skirts the west side of the outwash plain and that pits excavated periodically along much of the Mac West alignment may encounter granular soil suitable for embankment construction. Note that these soils are apt to be of finer texture (sand instead of gravel) because they are further from the face of the glacial ice as it retreated to the north or east.

#### **Field Observations:**

During our reconnaissance efforts, we observed the general terrain along the alignments being considered for the rail extension. Due to typically dense vegetative cover, the soil conditions over most of the areas visited was obscured from view. Our limited observations in the field agree with the generalizations made above regarding potential soil types for moraine and outwash deposits. It is our opinion that much of the soils in the outwash area will comprise sand and gravel soils that are relatively clean. Experience in the area suggests that the outwash soils may tend to contain more sand than gravel, but isolated areas of increased gravel soils are likely present. Explorations for the potential borrow pit near Alsop Road (Pit 1 on Figure 2) encountered sufficient cobbles that it may be economical to segregate out rock for crushed aggregates needed to construct the rail bed. In the northern reaches of the project area (morainal areas) in existing road cuts, the soils appear to contain more gravel and fine-grained fractions (silt and clay).

#### **Borrow Sources:**

Due to the lateral extent of this project, multiple sources of borrow will be needed to complete construction. Given our experience in the area, we believe that, on average, most of the moraine soils found in the upland portions of the project area are likely embankable with careful moisture control. As such, the railroad will likely be able to be developed by balancing cuts and

fills through the hilly upland areas. Larger borrow sources will also have to be developed in the upland areas to provide fill for expansive bogs where fill will not be recoverable along the alignment.

Given our experience in the project area and the geological terrains described above, we believe that the vicinity can be divided into three regions: outwash, moraine, and low-lying bogs; these three areas are indicated on Figure 2. The boundaries shown on Figure 2 are based on topography and our limited observations in the field and should be considered a rough approximation of the actual boundaries for planning purposes. In general, we believe that borrow sources developed in the outwash region will likely yield relatively clean sand and gravel using conventional borrow source mining techniques. We anticipate that groundwater in these areas is likely to be relatively deep and borrow sources will, as a result, be able to be developed relatively deeply. We anticipate that soils in the outwash area will be relatively consistent and uniform in developed borrow sources. A cap of 3 to 5 feet of organic silty surface soil will need to be stripped to expose the desired borrow in the outwash plain.

Borrow sources developed in the moraine regions will likely encountered variable soil conditions. Topography in these areas tends to be hummocky and isolated areas of poorly drained soils and shallow water are present. The isolated water could prove problematic in developing some of the moraine areas as borrow sources as these soils will likely be more silty and therefore, more difficult to handle under wet conditions. Because of the terrain and isolated areas of water, borrow sources in the moraine areas will likely need to be smaller in scale to mine isolated topographic highs.

We do not believe that borrow sources in the low-lying bog areas will be able to provide a significant amount of fill for this project. Many of the boggy areas along the project alignments are scattered with isolated topographic highs, however, they are usually less than about 10 to 20 feet above the bog areas and limited in lateral extent. We do not believe that dredging is a viable option in developing borrow sources below the bog surface in these areas due to likely permitting implications and potentially silty and clayey soil conditions beneath the bogs.

### LIMITATIONS

The analyses and conclusions contained in this report are based on site conditions as they presently exist. It is assumed that the landform analysis and related soil type interpretations included herein are representative of the conditions throughout project vicinity, i.e., the conditions everywhere are not significantly different from those assumed in this letter.

Port Mackenzie Rail Extension Borrow Availability,  
Mat-Su Borough, Alaska  
December 21, 2007  
Page 8

**SHANNON & WILSON, INC.**

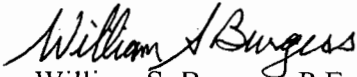
Unanticipated conditions are commonly encountered during construction and cannot fully be determined by merely taking soil samples or advancing borings. Such unexpected conditions frequently require that additional expenditures be made to attain a properly constructed project. The information included in this letter should be used for planning purposes only and once a preferred alignment is selected, we recommend follow-on work to further evaluate and characterize potential borrow sources for this project.

Shannon & Wilson has prepared the attachment *Important Information About Your Geotechnical/Environmental Report* to assist you and others in understanding the use and limitations of the reports.

Sincerely,

**SHANNON & WILSON, INC.**

Prepared By:

  
William S. Burgess, P.E.  
Senior Associate

Attachments: Figure 1: Existing Borrow Pits  
Figure 2: Potential Borrow Pits  
Important Information About Your Geotechnical/Environmental Report

32-1-01932

TABLE 1 - BORROW PIT DETAILS

Shannon &amp; Wilson, Inc.

Number	Alignment	Township	Range	Section	Existing Pits (Aerial Photos)		
					Soils Conservation Classification		
1	Mac East	T14N	R4W	23	Kichatna-Delyndia Silt Loams, 0 to 4 Percent Slopes	15 to 50% Gravel Soils	85% or more Sand Soils
2	Mac East	T15N	R4W	32	Kashwitna Silt Loam, 0 to 3 percent Slopes	85% or more Gravel Soils	85% or more Sand Soils
3	Mac East	T15N	R4W	28/33	Kashwitna Silt Loam, 0 to 3 percent Slopes	85% or more Gravel Soils	85% or more Sand Soils
4	Mac East	T15N	R4W	27/28	Estelle Silt Loam, Sloping and Moderately Steep	15% or less Gravel Soils	15% or less Sand Soils
5	Mac East	T15N	R4W	20	Kashwitna Silt Loam, Undulating	85% or more Gravel Soils	85% or more Sand Soils
6	East	T15N	R4W	4	Kashwitna Silt Loam, 0 to 3 percent Slopes	85% or more Gravel Soils	85% or more Sand Soils
7	East	T17N	R3W	26/28	Kichatna-Deception Complex, Sloping and Moderately Steep	50 to 85% Gravel Soils	50 to 85% Sand Soils
8	East	T17N	R3W	25	Deception Silt Loam, Rolling	15% or less Gravel Soils	15% or less Sand Soils
9	East	T17N	R3W	24/25	Kichatna Silt Loam Sloping and Moderately Steep	85% or More Gravel Soils	85% or More Sand Soils
10	East	T17N	R3W	14	Kichatna Silt Loam, 0 to 3 percent Slopes	85% or More Gravel Soils	85% or More Sand Soils
11	East	T17N	R3W	13	Kichatna Silt Loam, 0 to 3 percent Slopes	85% or More Gravel Soils	85% or More Sand Soils
12	East	T17N	R3W	13	Kichatna Silt Loam, 0 to 3 percent Slopes	85% or More Gravel Soils	85% or More Sand Soils
13	Conn 2	T15N	R5W	1	Kashwitna Silt Loam, 0 to 3 percent Slopes	85% or more Gravel Soils	85% or more Sand Soils
14	Conn 3	T16N	R5W	36	Kichatna Silt Loam, Sloping and Moderately Steep	85% or more Gravel Soils	85% or more Sand Soils
15	Conn 3	T16N	R5W	31	Kashwitna Silt Loam, 0 to 3 Percent Slopes	85% or more Gravel Soils	85% or more Sand Soils
16	Conn 3	T16N	R5W	36	Kichatna Silt Loam, Sloping and Moderately Steep	85% or more Gravel Soils	85% or more Sand Soils
17	Conn 1	T16N	R5W	26	Kashwitna Silt Loam, 0 to 3 Percent Slopes	85% or more Gravel Soils	85% or more Sand Soils

TABLE 1 - BORROW PIT DETAILS

Shannon &amp; Wilson, Inc.

Number	Alignment	Township	Range	Section	Existing Pits (Aerial Photos)		
					Soils Conservation Classification		
18	Central	T17N	R5W	30	Estelle, Hilly-Disappoint Complex	15% or Less Gravel Soils	15% or Less Sand Soils
19	Lake Creek	T18N	R3W	13	Estelle Silt Loam, Undulating	15% or Less Gravel Soils	15% or Less Sand Soils
20	Lake Creek	T18N	R3W	18	Kashwitna Silt Loam, 0 to 3 Percent Slopes	85% or more Gravel Soils	85% or more Sand Soils
21	Houston	T18N	R3W	21/22	Kichatna Silt Loam, Undulating	85% or more Gravel Soils	85% or more Sand Soils
22	Houston	T18N	R3W	27	Kichatna Silt Loam, 0 to 3 Percent Slopes	85% or more Gravel Soils	85% or more Sand Soils
23	West	T18N	R5W	10	Pits, Gravel	Undocumented	Undocumented
24	West	T18N	R5W	11	Estelle Silt Loam, Rolling	15% or Less Gravel Soils	15% or Less Sand Soils
25	West	T19N	R5W	22	Kashwitna Silt Loam, 0 to 3 Percent Slopes	85% or more Gravel Soils	85% or more Sand Soils
26	West	T19N	R5W	26	Kichatna Silt Loam, Sloping and Moderately Steep	85% or more Gravel Soils	85% or more Sand Soils
27	West	T19N	R5W	11	Nancy Silt Loam, 0 to 3 Percent Slopes	85% or more Gravel Soils	85% or more Sand Soils
28	West	T19N	R5W	2	Benka Silt Loam, 0 to 3 Percent Slopes	15% or Less Gravel Soils	85% or more Sand Soils
29	West	T20N	R4W	31	Benka Silt Loam, 0 to 3 Percent Slopes	15% or Less Gravel Soils	85% or more Sand Soils
30	West	T20N	R4W	30	Benka Silt Loam, 0 to 3 Percent Slopes	15% or Less Gravel Soils	85% or more Sand Soils
31	West	T20N	R4W	13	Benka Silt Loam, 0 to 3 Percent Slopes	15% or Less Gravel Soils	85% or more Sand Soils

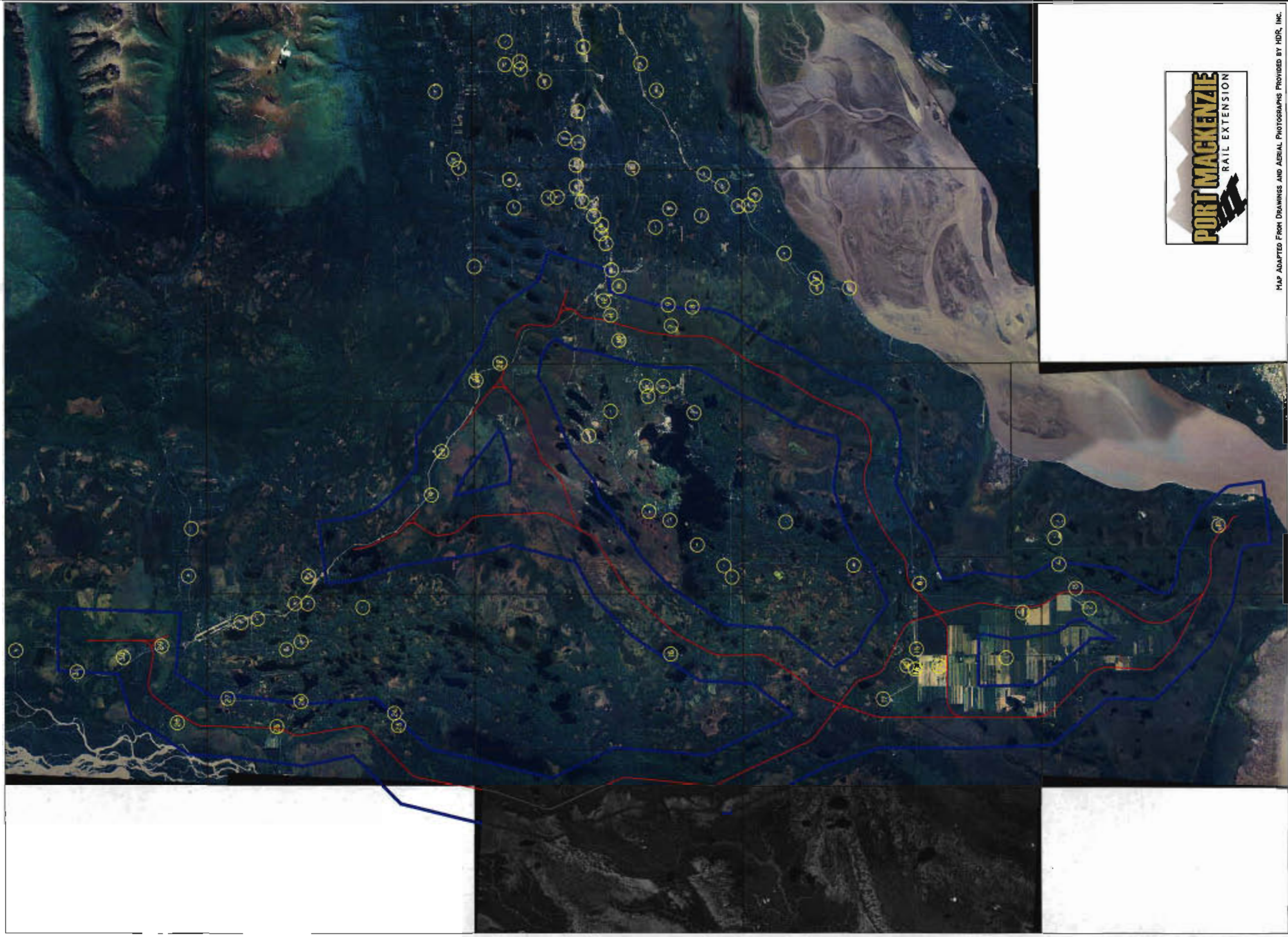
TABLE 1 - BORROW PIT DETAILS

Shannon &amp; Wilson, Inc.

Number	Alignment	Township	Range	Potential Pits (Topographic Highs)			
				Section	Soils Conservation Classification		
1	Mac East	T15N	R4W	20/21	Kashwitna Silt Loam, 0 to 3 Percent Slopes	85% or more Gravel Soils	85% or more Sand Soils
2	Mac East	T15N	R4W	9	Kashwitna Silt Loam, 0 to 3 Percent Slopes	85% or more Gravel Soils	85% or more Sand Soils
3	East	T16N	R4W	33	Estelle Silt Loam, Steep and Sloping	15% or less Gravel Soils	15% or less Sand Soils
4	East	T16N	R3W	25/36	Estelle Silt Loam, Rolling	15% or less Gravel Soils	15% or less Sand Soils
5	West	T17N	R5W	16/21/28	Estelle Silt Loam, Steep and Sloping	15% or less Gravel Soils	15% or less Sand Soils
6	West	T18N	R5W	21/28/33	Whitsol Silt Loam, Silty Substratum, Sloping and Moderately Steep	15% or less Gravel Soils	15% or less Sand Soils
7	West	T18N	R5W	16/21	Whitsol Silt Loam, Silty Substratum, Sloping and Moderately Steep	15% or less Gravel Soils	15% or less Sand Soils
8	West	T18N	R5W	10/11/14	Estelle Silt Loam, Sloping and Moderately Steep	15% or less Gravel Soils	15% or less Sand Soils
9	Central	T16N	R4/5W	6/1	Kichatna-Deception Complex, Sloping and Moderately Steep	50 to 85% Gravel Soils	50 to 85% Sand Soils
10	Central	T17N	R4W	30	Kichatna-Deception Complex, Steep and Sloping	15 to 50% Gravel Soils	15 to 50% Sand Soils

*(This page intentionally left blank.)*





MAP ADAPTED FROM DRAWINGS AND AERIAL PHOTOGRAPHS PROVIDED BY HDR, INC.

PORT MACKENZIE RAIL EXTENSION  
MAT-SU BOROUGH, ALASKA

EXISTING BORROW PITS

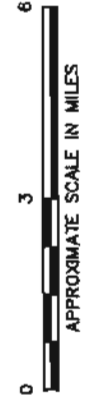
DECEMBER 2007 32-I-0932

SHANNON & WILSON, INC.  
Geotechnical & Environmental Consultants

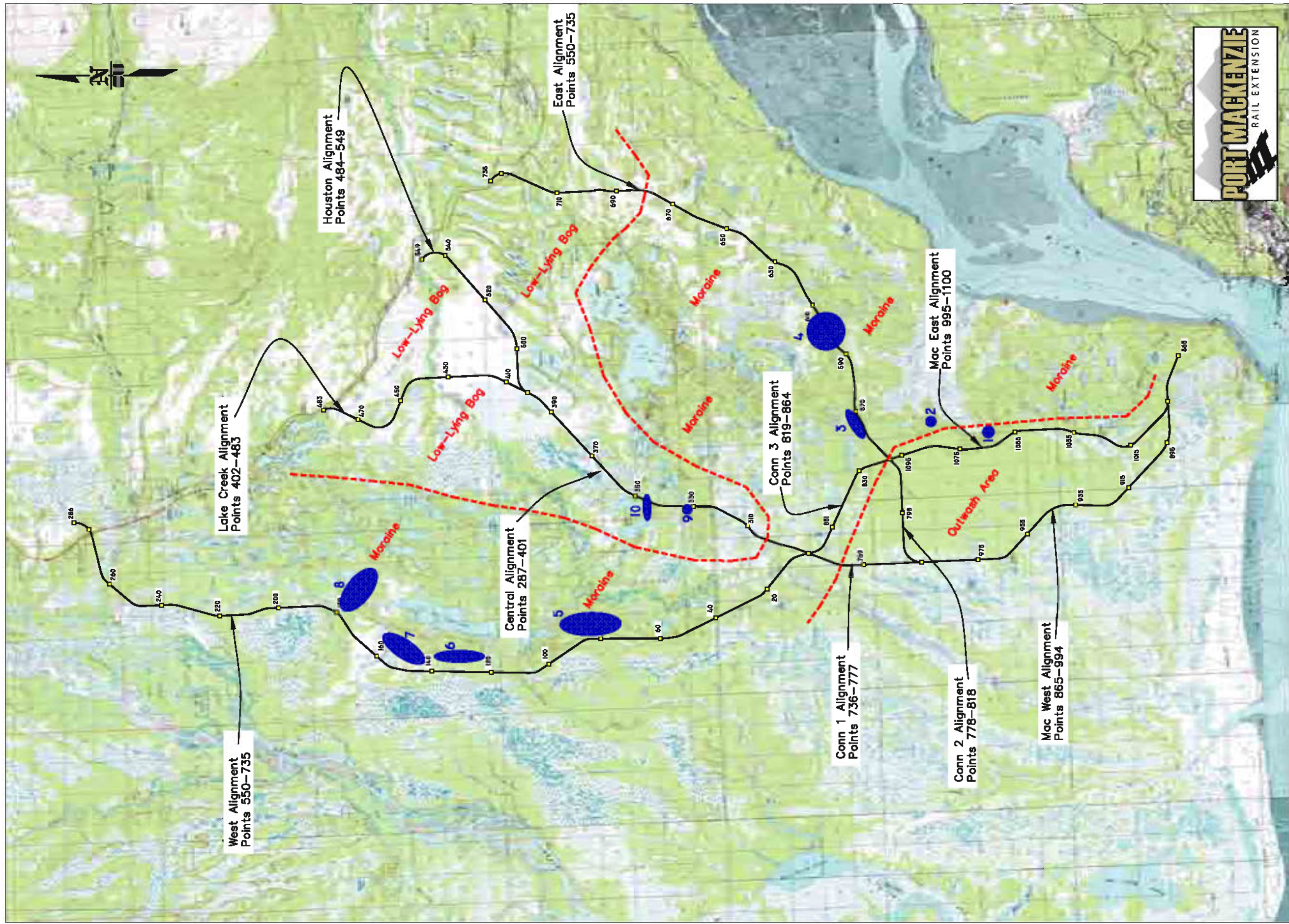
FIG. 1

**LEGEND**

- ① APPROXIMATE LOCATION OF EXISTING BORROW PIT NUMBER 1. REFERENCE TABLE 1 FOR PIT DETAILS.
- APPROXIMATE LOCATION OF 1-MILE CORRIDOR AROUND ALIGNMENT
- APPROXIMATE LOCATION OF PROPOSED ALIGNMENT



*(This page intentionally left blank.)*



DRAWING ADAPTED FROM ALL TOPO MAPS ANCHORAGE A-8, AK, 1952, ANCHORAGE B-8, AK, 1952, ANCHORAGE C-8, AK, 1950, ANCHORAGE D-8, AK, 1949, TYONEK B-1, AK, 1958, TYONEK B-2, AK, 1958, TYONEK C-1, AK, 1958, TYONEK C-2, AK, 1958, TYONEK D-1, AK, 1993 QUADRANGLES.

**LEGEND**

267  
 SURVEY POINT

APPROXIMATE LOCATION OF POTENTIAL BORROW PIT NUMBER 1, BASED ON TOPOGRAPHIC HIGHS

REFERENCE TABLE 1 FOR PIT DETAILS



PORT MACKENZIE RAIL EXTENSION  
 MAT-SU BOROUGH, ALASKA

POTENTIAL BORROW PITS

DECEMBER 2007

32-1-01932

SHANNON & WILSON, INC.  
 Geotechnical & Environmental Consultants

FIG. 2

*(This page intentionally left blank.)*



Date: December 21, 2007  
To: Mike Pochop, TNH-Hansen  
Port Mackenzie Rail Extension, Mat-Su  
Borough, Alaska

## **Important Information About Your Geotechnical/Environmental Report**

### **CONSULTING SERVICES ARE PERFORMED FOR SPECIFIC PURPOSES AND FOR SPECIFIC CLIENTS.**

Consultants prepare reports to meet the specific needs of specific individuals. A report prepared for a civil engineer may not be adequate for a construction contractor or even another civil engineer. Unless indicated otherwise, your consultant prepared your report expressly for you and expressly for the purposes you indicated. No one other than you should apply this report for its intended purpose without first conferring with the consultant. No party should apply this report for any purpose other than that originally contemplated without first conferring with the consultant.

### **THE CONSULTANT'S REPORT IS BASED ON PROJECT-SPECIFIC FACTORS.**

A geotechnical/environmental report is based on a subsurface exploration plan designed to consider a unique set of project-specific factors. Depending on the project, these may include: the general nature of the structure and property involved; its size and configuration; its historical use and practice; the location of the structure on the site and its orientation; other improvements such as access roads, parking lots, and underground utilities; and the additional risk created by scope-of-service limitations imposed by the client. To help avoid costly problems, ask the consultant to evaluate how any factors that change subsequent to the date of the report may affect the recommendations. Unless your consultant indicates otherwise, your report should not be used: (1) when the nature of the proposed project is changed (for example, if an office building will be erected instead of a parking garage, or if a refrigerated warehouse will be built instead of an unrefrigerated one, or chemicals are discovered on or near the site); (2) when the size, elevation, or configuration of the proposed project is altered; (3) when the location or orientation of the proposed project is modified; (4) when there is a change of ownership; or (5) for application to an adjacent site. Consultants cannot accept responsibility for problems that may occur if they are not consulted after factors, which were considered in the development of the report, have changed.

### **SUBSURFACE CONDITIONS CAN CHANGE.**

Subsurface conditions may be affected as a result of natural processes or human activity. Because a geotechnical/environmental report is based on conditions that existed at the time of subsurface exploration, construction decisions should not be based on a report whose adequacy may have been affected by time. Ask the consultant to advise if additional tests are desirable before construction starts; for example, groundwater conditions commonly vary seasonally.

Construction operations at or adjacent to the site and natural events such as floods, earthquakes, or groundwater fluctuations may also affect subsurface conditions and, thus, the continuing adequacy of a geotechnical/environmental report. The consultant should be kept apprised of any such events, and should be consulted to determine if additional tests are necessary.

### **MOST RECOMMENDATIONS ARE PROFESSIONAL JUDGMENTS.**

Site exploration and testing identifies actual surface and subsurface conditions only at those points where samples are taken. The data were extrapolated by your consultant, who then applied judgment to render an opinion about overall subsurface conditions. The actual interface between materials may be far more gradual or abrupt than your report indicates. Actual conditions in areas not sampled may differ from those predicted in your report. While nothing can be done to prevent such situations, you and your consultant can work together to help reduce their impacts. Retaining your consultant to observe subsurface construction operations can be particularly beneficial in this respect.

## **A REPORT'S CONCLUSIONS ARE PRELIMINARY.**

The conclusions contained in your consultant's report are preliminary because they must be based on the assumption that conditions revealed through selective exploratory sampling are indicative of actual conditions throughout a site. Actual subsurface conditions can be discerned only during earthwork; therefore, you should retain your consultant to observe actual conditions and to provide conclusions. Only the consultant who prepared the report is fully familiar with the background information needed to determine whether or not the report's recommendations based on those conclusions are valid and whether or not the contractor is abiding by applicable recommendations. The consultant who developed your report cannot assume responsibility or liability for the adequacy of the report's recommendations if another party is retained to observe construction.

## **THE CONSULTANT'S REPORT IS SUBJECT TO MISINTERPRETATION.**

Costly problems can occur when other design professionals develop their plans based on misinterpretation of a geotechnical/environmental report. To help avoid these problems, the consultant should be retained to work with other project design professionals to explain relevant geotechnical, geological, hydrogeological, and environmental findings, and to review the adequacy of their plans and specifications relative to these issues.

## **BORING LOGS AND/OR MONITORING WELL DATA SHOULD NOT BE SEPARATED FROM THE REPORT.**

Final boring logs developed by the consultant are based upon interpretation of field logs (assembled by site personnel), field test results, and laboratory and/or office evaluation of field samples and data. Only final boring logs and data are customarily included in geotechnical/environmental reports. These final logs should not, under any circumstances, be redrawn for inclusion in architectural or other design drawings, because drafters may commit errors or omissions in the transfer process.

To reduce the likelihood of boring log or monitoring well misinterpretation, contractors should be given ready access to the complete geotechnical engineering/environmental report prepared or authorized for their use. If access is provided only to the report prepared for you, you should advise contractors of the report's limitations, assuming that a contractor was not one of the specific persons for whom the report was prepared, and that developing construction cost estimates was not one of the specific purposes for which it was prepared. While a contractor may gain important knowledge from a report prepared for another party, the contractor should discuss the report with your consultant and perform the additional or alternative work believed necessary to obtain the data specifically appropriate for construction cost estimating purposes. Some clients hold the mistaken impression that simply disclaiming responsibility for the accuracy of subsurface information always insulates them from attendant liability. Providing the best available information to contractors helps prevent costly construction problems and the adversarial attitudes that aggravate them to a disproportionate scale.

## **READ RESPONSIBILITY CLAUSES CLOSELY.**

Because geotechnical/environmental engineering is based extensively on judgment and opinion, it is far less exact than other design disciplines. This situation has resulted in wholly unwarranted claims being lodged against consultants. To help prevent this problem, consultants have developed a number of clauses for use in their contracts, reports and other documents. These responsibility clauses are not exculpatory clauses designed to transfer the consultant's liabilities to other parties; rather, they are definitive clauses that identify where the consultant's responsibilities begin and end. Their use helps all parties involved recognize their individual responsibilities and take appropriate action. Some of these definitive clauses are likely to appear in your report, and you are encouraged to read them closely. Your consultant will be pleased to give full and frank answers to your questions.

The preceding paragraphs are based on information provided by the  
ASFE/Association of Engineering Firms Practicing in the Geosciences, Silver Spring, Maryland

## **Appendix I: Cultural Resource Probability Modeling**

## Alaska Rail Road Archaeological Survey and Inventory

### Pre-historic and Ethno--historical Patterns of Land Use and Settlement

Prehistoric sites have been found in upland areas along major water ways dating from <sup>14</sup>C 8,000 yrs. B. P. (Before Present) to approximately 3,000 yrs. B.P. People inhabiting those upland areas may have been the progenitors of succeeding generations of Dena'ina. Traditionally, land use by the Dena'ina Athabascans was along streams and lakes that supported anadromous fish runs. Winters were spent in villages during a time of minimum food procurement. Villages along Knik Arm were inhabited on a semi-sedentary basis; they were located at the mouth of important river confluences with Knik Arm and on numerous inland lakes. During the spring and fall most of the population moved away to fish camps where whole families would have been involved in procuring and processing fish. In the late fall, families went hunting in the mountains; men pursued large game, while women and children set snares for small fur bearers such as rabbits and squirrels.

In the Port MacKenzie Rail Road study area the Nancy Lake and Goose Bay lowlands both prehistorically and historically likely provided good locations for hunting water fowl and fur bearing animals. Uplands surrounding Lakes and swampy areas, such as the Susitna lowlands and Goose Bay, provided excellent areas to camp. Prehistorically, promontories served as lookout areas and vantage points for spotting big game. Historically, they provided good locations for camps when hunting, gathering berries and/or pursuing water fowl. Shem Pete (Kari and Fall 2003) called Goose Bay "Tustl'agh" (Rear Portage). It was a popular goose hunting area for the Knik Dena'ina.

Art Theodore, a Dena'ina elder, remembered the description his uncle gave him of how they had traditionally hunted geese in the Goose Bay area. His uncle, as a little boy, would accompany his father when they went goose hunting. They set up and staked pit snares in locations where geese were likely to land. To increase their odds, they put feed out to entice the birds. Once the birds were caught he and his father clubbed them. When he was 4-5 years old often the geese were bigger than he, making him fearful of them as he tried to club them. Later they started using traps, but were cautious in their use because metal traps often amputated a bird's leg, enabling the geese to fly away. When traps were used, his uncle was taught to wrap cloth or rope around the "jaws" so the traps would not cut through and amputate a leg (Kari and Fall 2003: 273).

In the early twentieth century a cannery was built at Goose Bay. Shem Pete, the late Dena'ina elder well versed in the geography of the region, mentioned:

"In 1915 I left Susitna in a boat and came over to Goose Bay cannery. They had just built a cannery there. There were Chinamen there Lot of people there. I saw that cannery there in 1914 and 1915" (Kari and Fall 2003: 273).

Tustl'aghtnu (Rear Portage Creek) refers to trails and portages connecting Goose Bay with a number of other important sites such as: Seven Mile Lake, Big Lake, Papoose and Twin Lake, in addition to the Little Susitna River and a trail north to Red Shirt Lake (Kari and Fall 2003: 273). Art Theodore reported that his father had a trapper's cabin at Seven Mile Lake on a trail leading from Knik.

Shem Pete mentioned that villages were generally located at the confluence of a river with a lake. A good example of such a site was his description of the village at the confluence of Red Shirt Lake with Fish Creek. His son Billy added that people would place basket traps in the shallows near an outlet. Fish traps could be four to eight feet long and as wide as four feet. The area surrounding the mouth of Red Shirt Lake harbored a large community. Both Shem Pete and his son, Billy, elaborated at great length on the availability of fish and other subsistence foods in that



community (Kari and Fall 2003: 101). Annual fish runs provided the most stable and easy subsistence resource. Other important village sites included a village at the upper end of Red Shirt Lake, Cow Lake, Flat Horn Lake, and Indian Bay on the southeast shore of Nancy Lake. Cow Lake was reportedly the first stop-over village when traveling from Red Shirt Lake (Fall 1981: 382); it had been abandoned prior to the village at Red Shirt Lake. Shem Pete's mother was familiar with the village when she was young (Fall 1981: 382). Trails radiated out from all the above villages, linking them to Goose Bay, in addition to villages along the Susitna River.

## **Methodology**

The area of investigation for the Alaska Rail Road spur alternates encompasses the region south of Willow to Port MacKenzie. To the north it is bound by the railroad, on the east side by Knik Arm, while its western boundary basically follows the Susitna River. The specific objective was to examine topographic maps and shade areas likely to reveal historic sites based on ethnographic reports and previous archaeological data. Areas within the Area of Potential Effect (APE), adjacent to and in the vicinity of the proposed spurs, were also addressed.

Prior to outlining areas on the map it was necessary to study historical and ethnographic data on settlement patterns and land uses by people who lived in the region, both historically and prehistorically. Based on those patterns, areas of greatest sensitivity for archaeological sites were identified and broken down into two separate categories: prehistoric and proto-historic. The latter category also included the ethno-historic period; both overlap the prehistoric with the historic period. Once the literature research was conducted, topographic maps were examined to determine areas most and least likely to encompass historic sites. Potential areas for archaeological sites were identified on topographic maps submitted for the Port MacKenzie R. R. spur alternates. An on-the-ground cultural resource walk-over survey was recommended for areas identified on the maps. Archaeological surveys conducted will need to identify and examine evidence of cultural activity left on the landscape, both historically and prehistorically.

Archaeological sites comprise material remains of past events or landscape features and alterations created by cultural activities. The latter includes depressions where cache pits were used for food storage; depressions where houses once stood; and earthen foundation walls. Subsurface archaeological sites can only be found through shovel testing. Shovel testing may yield artifacts and /or cultural disturbance in sub-surface strata; such as birch bark lined caches, hearths and post molds.

There is a fine line between historic and prehistoric sites when identifying sites characteristic of Native American dwellings and subsistence activities. Many Athabascan traditional activities and subsistence practices continued into the early 20th Century. If the date of use is attributable to more recent times, a site may be referred to as proto-historic or ethno-historic; both describe the period that overlaps the prehistoric unwritten and historic written eras. Proto-historic is used for the period during initial Russian settlement on the Kenai, while the ethno-historic period may include the Russian-American trading period with the Upper Cook Inlet Dena'ina. The Dena'ina controlled the interior fur trade by acting as middlemen for the Russians. Most trade was carried out on Dena'ina terms (Fall 1981). Information for that period may be gleaned from Russian records, Russian Orthodox Church documents and from cultural observations written by Euro-Americans and from Dena'ina oral histories.

In the final report, based on a cultural resource survey, it is anticipated that archaeological site identification may include pre-historic sites, dating from the pre-Russian undocumented era; proto and ethno-historic sites, reflecting the Russian-American and Early-American fur trade; and the subsequent gold rush era that continued into the early 20th Century. The ethno-historic era is dependant on ethnographic data, historical records, oral histories, cultural documentation and

material culture. During the late Russian American period, dating between 1835-1840, the smallpox epidemic swept through Athabascan territory. It decimated Dena'ina numbers by 50 percent. Dena'ina Athabascan people thus received a crippling blow to their culture. Subsequent flu epidemics further depopulated many areas. Because of the decimation of many villages in the Upper Cook Inlet Susitna region, much of the collective traditional cultural knowledge was lost; however their villages in the form of house and cache pit depressions have been left as cultural footprints on the landscape. Those are the remains that are important to interpret before they too are lost to development.

The map submitted to the Alaska Rail Road included areas likely to yield important data on archaeological sites. In order to carry out an archaeological survey, areas of importance and sensitivity to archaeological sites (as highlighted on the map) would include:

- a) the confluence of a river with a lake;
- b) the confluence of two rivers;
- c) bluffs above major waterways;
- d) promontories that may have provided good look-outs for prehistoric camps;
- e) promontories near or adjacent to swamps where water fowl might have congregated; and
- f) bluffs surrounding inland lakes and streams

Staff for the Cultural Resources Division of the Matanuska-Susitna Borough submitted topographic maps identifying areas sensitive to sites within a region heavily used and populated by the Dena'ina people. Most of the information on Dena'ina land use patterns was based mainly on one man's oral history: Shem Pete related numerous areas used by his family both in the recent and historic past (Kari and Fall 2003). It is obvious from his recollections and from oral history handed down to him, that the region has the potential to hold a greater number of villages and subsistence areas than the recollections of one person. The above information and suggested survey areas did not include post 20th Century historical data on the built environment.

## APPENDIX J: TRAIN ENERGY CALCULATIONS

Calculations of train energy for the Pt. MacKenzie Rail Extension alternatives were prepared as an input to the evaluation matrix, presented in Chapter 5. The values obtained, in units of horsepower-hours, are approximations intended for comparison of the alternatives and not absolute values of energy consumption.

### J.1 Assumptions for Calculations

The train energy calculations were based on the following basic assumptions:

- A design train of 100 cars, each weighing 125 tons. This results in a design train weight of 12,500 tons. It was assumed that the weight of the locomotives would be balanced by a fewer number of cars or lighter loadings in certain cars.
- Resistance forces considered were internal train resistance, grade resistance, and curvature resistance.
- Atmospheric resistance and wind were not considered.
- The resistance of the entire train was calculated based on the grade and curvature over a distance of alignment where these two values were constant. No attempt was made to account for the variation in curvature or grade that would typically be found within the length of the design train. This assumption is appropriate for values intended solely for comparison and not to determine absolute values.
- The Combined Resistance values were not permitted to be less than zero. While a train descending a grade will have a negative resistance and thus accelerate, the energy produced by internal combustion locomotives cannot be less than zero. Again, this is an appropriate for values intended solely for comparison and not to determine absolute values.
- The design velocity of the design train was set at a uniform 60 miles per hour per ARRC standards.

### J.2 Calculation Methodology

The calculations found on the following pages were developed with a consistent methodology and resistance factors (Hay, 1982). First, using the conceptual alignments and profiles found in Appendix B and the ARRC track chart for the existing mainline, station limits were established for zones of uniform grade and curvature. The internal train resistance was calculated as a constant value, the product of the design train weight and a resistance factor of 4.5lb/ton of train weight. For each zone, a curve resistance was determined as the product of the design train weight and a curve resistance factor of 0.8 lb/ton of train weight. A zone value for grade resistance was derived from the grade resistance factor of 20 lb/ton multiplied by the train weight. The sum of the three resistances is identified as the Combined Resistance.

The Power Required to move the train within a given zone was calculated as the product of the Combined Resistance and the Design Velocity of 60 mph, with the appropriate factors to produce the results in horsepower. Time Between Stations values were derived from the length of each zone multiplied by the velocity, again adjusting units to seconds. Finally the approximate energy used within each zone was calculated as the product of the Power Required and the Time Between Stations, including a factor to convert the time component from seconds to hours, to produce a result in horsepower-hours.

The calculations are presented on the following pages. Table J-1 is a summary of the energy values for each alignment segment, along with the combination of these segmental values into an overall value for each alignment option between Port MacKenzie and Milepost 188.9 just north of Willow. Once the total energy was determined for each alignment, a determination was made of the median value, the middle value of the set of values for the eight alignments. The difference between each alignment's energy value and the median value was calculated and is displayed at the bottom of the table for comparison.

The subsequent pages are the calculations of energy for each alignment segment.

### **J.3 References**

Hay, William Walter. 1982. *Railroad Engineering* (2nd ed.). John Wiley & Sons, Inc.: New York.

Table J-1: Train Energy Summary - Northbound

Alignment Segments	Estimated Required Energy, Horsepower-Hours							
	Mac West, Connection 1, Willow	Mac West, Connection 1, Houston, Houston North	Mac West, Connection 1, Houston, Houston South	Mac West, Connection 2, Big Lake	Mac East, Connection 3, Willow	Mac East, Connection 3, Houston, Houston North	Mac East, Connection 3, Houston, Houston South	Mac East, Big Lake
Mac West	1744.0	1744.0	1744.0	1744.0				
Mac East					2666.7	2666.7	2666.7	2666.7
Connection 1	621.4	621.4	621.4					
Connection 2				1595.8				
Connection 3					291.2	291.2	291.2	
Willow	5702.0				5702.0			
Houston		2560.2	2560.2			2560.2	2560.2	
Houston North		1741.5				1741.5		
Houston South			1917.7				1917.7	
Big Lake				4646.0				4646.0
Existing Mainline								
MP 170.3-188.9				3624.7				3624.7
MP 174.0-188.9			2654.4				2654.4	
MP 179.3-188.9		1864.6				1864.6		
Total	8067.4	8531.7	9497.7	11610.5	8659.9	9124.2	10090.2	10937.4
Median Value	9311.0							
Departure from Median	-13%	-8%	2%	25%	-7%	-2%	8%	17%

Table J-2: Mac West Alignment, Northbound - Train Energy Calculations

Design Train is 100 cars @ 125 tons/car =

12,500 tons

Station	Train Resistance (4.5lb/ton) (lb)	Curve Component		Grade Component		Combined Resistance R (>0) (lb)	Design Velocity V (mph)	Power Required P=R*V (hp)	Time Between Stations (sec)	Energy E=P*T (hp-hour)
		Curvature D (degrees)	Resistance D(0.8lb/ton) (lb)	Gradient G (%)	Resistance G*(20lb/ton) (lb)					
0+00										
12+56	56250	5	50000	0.00	0	106250	60	17000	14.3	67.5
23+46	56250	0	0	0.00	0	56250	60	9000	12.4	31.0
25+00	56250	4	40000	0.00	0	96250	60	15400	1.8	7.7
31+63	56250	4	40000	-0.50	-125000	0	60	0	7.5	0.0
37+83	56250	0	0	-0.50	-125000	0	60	0	7.0	0.0
44+63	56250	4	40000	-0.50	-125000	0	60	0	7.7	0.0
45+00	56250	4	40000	-0.50	-125000	0	60	0	0.4	0.0
63+14	56250	0	0	-0.10	-25000	31250	60	5000	20.6	28.6
80+21	56250	2	20000	-0.10	-25000	51250	60	8200	19.4	44.2
106+54	56250	0	0	-0.10	-25000	31250	60	5000	29.9	41.5
142+63	56250	0.83	8300	-0.10	-25000	39550	60	6328	41.0	72.1
150+00	56250	0.83	8300	-0.10	-25000	39550	60	6328	8.4	14.8
200+00	56250	0.83	8300	0.08	20000	84550	60	13528	56.8	213.4
247+21	56250	0	0	-0.24	-60000	0	60	0	53.6	0.0
264+17	56250	1	10000	-0.24	-60000	6250	60	1000	19.3	5.4
299+11	56250	0	0	-0.24	-60000	0	60	0	39.7	0.0
300+00	56250	0	0	-0.24	-60000	0	60	0	1.0	0.0
323+26	56250	2	20000	0.00	0	76250	60	12200	26.4	89.5
360+00	56250	2	20000	0.00	0	76250	60	12200	41.8	141.7
381+06	56250	0	0	0.20	50000	106250	60	17000	23.9	112.9
430+03	56250	1	10000	0.20	50000	116250	60	18600	55.6	287.3
440+00	56250	1	10000	0.20	50000	116250	60	18600	11.3	58.4
502+30	56250	0	0	-0.12	-30000	26250	60	4200	70.8	82.6
520+00	56250	0	0	-0.12	-30000	26250	60	4200	20.1	23.5
547+46	56250	1	10000	0.00	0	66250	60	10600	31.2	91.9
663+59	56250	0	0	0.00	0	56250	60	9000	132.0	330.0
										1744.0

Table J-3: Mac East Alignment, Northbound - Train Energy Calculations

Design Train is 100 cars @ 125 tons/car =

12,500 tons

Station	Train Resistance (4.5lb/ton) (lb)	Curve Component		Grade Component		Combined Resistance R (>0) (lb)	Design Velocity V (mph)	Power Required P=R*V (hp)	Time Between Stations (sec)	Energy E=P*T (hp-hour)
		Curvature D (degrees)	Resistance D(0.8lb/ton) (lb)	Gradient G (%)	Resistance G*(20lb/ton) (lb)					
0+00										
12+56	56250	5	50000	0.00	0	106250	60	17000	14.3	67.5
23+46	56250	0	0	0.00	0	56250	60	9000	12.4	31.0
25+00	56250	4	40000	0.00	0	96250	60	15400	1.8	7.7
31+63	56250	4	40000	-0.50	-125000	0	60	0	7.5	0.0
37+83	56250	0	0	-0.50	-125000	0	60	0	7.0	0.0
44+63	56250	4	40000	-0.50	-125000	0	60	0	7.7	0.0
45+00	56250	0	0	-0.50	-125000	0	60	0	0.4	0.0
63+14	56250	0	0	-0.10	-25000	31250	60	5000	20.6	28.6
80+21	56250	2	20000	-0.10	-25000	51250	60	8200	19.4	44.2
95+00	56250	0	0	-0.10	-25000	31250	60	5000	16.8	23.3
106+54	56250	0	0	0.00	0	56250	60	9000	13.1	32.8
150+00	56250	0.83	8300	0.00	0	64550	60	10328	49.4	141.7
172+84	56250	0.83	8300	0.20	50000	114550	60	18328	26.0	132.4
215+59	56250	0	0	0.20	50000	106250	60	17000	48.6	229.5
282+88	56250	1	10000	0.20	50000	116250	60	18600	76.5	395.3
300+00	56250	0	0	0.20	50000	106250	60	17000	19.5	92.1
361+75	56250	0	0	0.00	0	56250	60	9000	70.2	175.5
377+32	56250	2	20000	0.00	0	76250	60	12200	17.7	60.0
390+00	56250	0	0	0.00	0	56250	60	9000	14.4	36.0
418+19	56250	0	0	0.50	125000	181250	60	29000	32.0	257.8
434+55	56250	2	20000	0.50	125000	201250	60	32200	18.6	166.4
460+00	56250	0	0	0.50	125000	181250	60	29000	28.9	232.8
463+31	56250	0	0	0.12	30000	86250	60	13800	3.8	14.6
477+57	56250	2	20000	0.12	30000	106250	60	17000	16.2	76.5
516+22	56250	0	0	0.12	30000	86250	60	13800	43.9	168.3
523+59	56250	2	20000	0.12	30000	106250	60	17000	8.4	39.7
550+00	56250	0	0	0.12	30000	86250	60	13800	30.0	115.0
561+22	56250	0	0	-0.28	-70000	0	60	0	12.8	0.0
587+07	56250	1	10000	-0.28	-70000	0	60	0	29.4	0.0
615+00	56250	0	0	-0.28	-70000	0	60	0	31.7	0.0
649+49	56250	0	0	0.00	0	56250	60	9000	39.2	98.0
										2666.7

Table J-4: Connection 1 Alignment, Northbound - Train Energy Calculations

Design Train is 100 cars @ 125 tons/car =

12,500 tons

Station	Train Resistance (4.5lb/ton) (lb)	Curve Component		Grade Component		Combined Resistance R (>0) (lb)	Design Velocity V (mph)	Power Required P=R*V (hp)	Time Between Stations (sec)	Energy E=P*T (hp-hour)
		Curvature D (degrees)	Resistance D(0.8lb/ton) (lb)	Gradient G (%)	Resistance G*(20lb/ton) (lb)					
0+00										
0+40	56250	0	0	0.00	0	56250	60	9000	0.5	1.3
139+44	56250	0	0	-0.12	-30000	26250	60	4200	158.0	184.3
159+03	56250	1	10000	-0.12	-30000	36250	60	5800	22.3	35.9
160+00	56250	0	0	-0.12	-30000	26250	60	4200	1.1	1.3
207+00	56250	0	0	0.40	100000	156250	60	25000	53.4	370.8
216+75	56250	0	0	0.00	0	56250	60	9000	11.1	27.8
										621.4



Table J-5: Connection 2 Alignment, Northbound - Train Energy Calculations

Design Train is 100 cars @ 125 tons/car =

12,500 tons

Station	Train Resistance (4.5lb/ton) (lb)	Curve Component		Grade Component		Combined Resistance R (>0) (lb)	Design Velocity V (mph)	Power Required P=R*V (hp)	Time Between Stations (sec)	Energy E=P*T (hp-hour)
		Curvature D (degrees)	Resistance D(0.8lb/ton) (lb)	Gradient G (%)	Resistance G*(20lb/ton) (lb)					
0+00										
30+00	56250	2	20000	0.00	0	76250	60	12200	34.1	115.6
45+01	56250	2	20000	0.48	120000	196250	60	31400	17.1	149.2
161+02	56250	0	0	0.48	120000	176250	60	28200	131.8	1032.4
182+11	56250	2	20000	0.48	120000	196250	60	31400	24.0	209.3
190+00	56250	0	0	0.48	120000	176250	60	28200	9.0	70.5
196+57	56250	0	0	0.00	0	56250	60	9000	7.5	18.8
										1595.8

Table J-6: Connection 3 Alignment, Northbound - Train Energy Calculations

Design Train is 100 cars @ 125 tons/car =

12,500 tons

Station	Train Resistance (4.5lb/ton) (lb)	Curve Component		Grade Component		Combined Resistance R (>0) (lb)	Design Velocity V (mph)	Power Required P=R*V (hp)	Time Between Stations (sec)	Energy E=P*T (hp-hour)
		Curvature D (degrees)	Resistance D(0.8lb/ton) (lb)	Gradient G (%)	Resistance G*(20lb/ton) (lb)					
0+00										
23+00	56250	0	0	0.00	0	56250	60	9000	26.1	65.3
42+15	56250	0	0	-0.50	-125000	0	60	0	21.8	0.0
65+99	56250	2	20000	-0.50	-125000	0	60	0	27.1	0.0
136+13	56250	0	0	-0.50	-125000	0	60	0	79.7	0.0
158+32	56250	2	20000	-0.50	-125000	0	60	0	25.2	0.0
166+00	56250	0	0	-0.50	-125000	0	60	0	8.7	0.0
182+52	56250	0	0	0.00	0	56250	60	9000	18.8	47.0
199+44	56250	2	20000	0.00	0	76250	60	12200	19.2	65.1
239+44	56250	0	0	0.00	0	56250	60	9000	45.5	113.8
										291.2

Table J-7: Willow Alignment, Northbound - Train Energy Calculations

Design Train is 100 cars @ 125 tons/car =

12,500 tons

Station	Train Resistance (4.5lb/ton) (lb)	Curve Component		Grade Component		Combined Resistance R (>0) (lb)	Design Velocity V (mph)	Power Required P=R*V (hp)	Time Between Stations (sec)	Energy E=P*T (hp-hour)
		Curvature D (degrees)	Resistance D(0.8lb/ton) (lb)	Gradient G (%)	Resistance G*(20lb/ton) (lb)					
0+00										
10+00	56250	0	0	0.00	0	56250	60	9000	11.4	28.5
60+00	56250	0	0	-0.32	-80000	0	60	0	56.8	0.0
68+92	56250	0	0	0.50	125000	181250	60	29000	10.1	81.4
82+54	56250	2	0	0.50	125000	181250	60	29000	15.5	124.9
240+00	56250	0	0	0.50	125000	181250	60	29000	178.9	1441.1
252+31	56250	0	20000	0.28	70000	146250	60	23400	14.0	91.0
267+73	56250	2	0	0.28	70000	126250	60	20200	17.5	98.2
380+00	56250	0	0	0.28	70000	126250	60	20200	127.6	716.0
408+30	56250	0	20000	0.00	0	76250	60	12200	32.2	109.1
423+34	56250	2	0	0.00	0	56250	60	9000	17.1	42.8
480+00	56250	0	0	0.00	0	56250	60	9000	64.4	161.0
542+81	56250	0	20000	-0.32	-80000	0	60	0	71.4	0.0
560+00	56250	2	0	-0.32	-80000	0	60	0	19.5	0.0
564+58	56250	2	0	0.32	80000	136250	60	21800	5.2	31.5
646+45	56250	0	20000	0.32	80000	156250	60	25000	93.0	645.8
650+51	56250	2	20000	0.32	80000	156250	60	25000	4.6	31.9
690+00	56250	0	0	0.32	80000	136250	60	21800	44.9	271.9
770+00	56250	0	20000	-0.40	-100000	0	60	0	90.9	0.0
782+33	56250	0	0	-0.50	-125000	0	60	0	14.0	0.0
799+86	56250	2	0	-0.50	-125000	0	60	0	19.9	0.0
860+00	56250	0	0	-0.50	-125000	0	60	0	68.3	0.0
898+44	56250	0	20000	0.20	50000	126250	60	20200	43.7	245.2
926+11	56250	2	0	0.20	50000	106250	60	17000	31.4	148.3
980+00	56250	0	0	0.20	50000	106250	60	17000	61.2	289.0
1018+80	56250	0	20000	-0.12	-30000	46250	60	7400	44.1	90.7
1036+44	56250	2	0	-0.12	-30000	26250	60	4200	20.0	23.3
1045+56	56250	0	0	-0.12	-30000	26250	60	4200	10.4	12.1
1060+00	56250	1	20000	-0.12	-30000	46250	60	7400	16.4	33.7
1069+64	56250	1	0	-0.24	-60000	0	60	0	11.0	0.0
1189+82	56250	0	10000	-0.24	-60000	6250	60	1000	136.6	37.9
1200+00	56250	2	10000	-0.24	-60000	6250	60	1000	11.6	3.2
1204+16	56250	2	0	0.00	0	56250	60	9000	4.7	11.8
1252+92	56250	0	20000	0.00	0	76250	60	12200	55.4	187.7
1256+94	56250	2	20000	0.00	0	76250	60	12200	4.6	15.6
1285+81	56250	0	0	0.00	0	56250	60	9000	32.8	82.0
1300+00	56250	2	20000	0.00	0	76250	60	12200	16.1	54.6
1316+94	56250	2	0	0.20	50000	106250	60	17000	19.3	91.1
1357+27	56250	0	20000	0.20	50000	126250	60	20200	45.8	257.0
1365+18	56250	3	20000	0.20	50000	126250	60	20200	9.0	50.5
1369+91	56250	0	0	0.20	50000	106250	60	17000	5.4	25.5
1394+31	56250	3	30000	0.20	50000	136250	60	21800	27.7	167.7
										5702.0

Table J-8: Houston Alignment, Northbound - Train Energy Calculations

Design Train is 100 cars @ 125 tons/car =

12,500 tons

Station	Train Resistance (4.5lb/ton) (lb)	Curve Component		Grade Component		Combined Resistance R (>0) (lb)	Design Velocity V (mph)	Power Required P=R*V (hp)	Time Between Stations (sec)	Energy E=P*T (hp-hour)
		Curvature D (degrees)	Resistance D(0.8lb/ton) (lb)	Gradient G (%)	Resistance G*(20lb/ton) (lb)					
0+00										
29+64	56250	0	0	-0.08	-20000	36250	60	5800	33.7	54.3
36+82	56250	2	20000	-0.08	-20000	56250	60	9000	8.2	20.5
47+84	56250	0	0	-0.08	-20000	36250	60	5800	12.5	20.1
57+68	56250	1	10000	-0.08	-20000	46250	60	7400	11.2	23.0
76+07	56250	0	0	-0.08	-20000	36250	60	5800	20.9	33.7
91+00	56250	0.5	5000	-0.08	-20000	41250	60	6600	17.0	31.2
94+31	56250	0.5	5000	0.32	80000	141250	60	22600	3.8	23.9
108+22	56250	0	0	0.32	80000	136250	60	21800	15.8	95.7
108+74	56250	1	10000	0.32	80000	146250	60	23400	0.6	3.9
144+87	56250	0	0	0.32	80000	136250	60	21800	41.1	248.9
162+46	56250	2	20000	0.32	80000	156250	60	25000	20.0	138.9
220+79	56250	0	0	0.32	80000	136250	60	21800	66.3	401.5
249+99	56250	1	10000	0.32	80000	146250	60	23400	33.2	215.8
277+00	56250	0	0	0.32	80000	136250	60	21800	30.7	185.9
306+47	56250	0	0	-0.36	-90000	0	60	0	33.5	0.0
321+22	56250	1	10000	-0.36	-90000	0	60	0	16.8	0.0
349+00	56250	0	0	-0.36	-90000	0	60	0	31.6	0.0
414+26	56250	0	0	0.32	80000	136250	60	21800	74.2	449.3
440+00	56250	0.5	5000	0.32	80000	141250	60	22600	29.3	183.9
444+31	56250	0.5	5000	0.08	20000	81250	60	13000	4.9	17.7
519+58	56250	0	0	0.08	20000	76250	60	12200	85.5	289.8
534+19	56250	2	20000	0.08	20000	96250	60	15400	16.6	71.0
547+47	56250	0	0	0.08	20000	76250	60	12200	15.1	51.2
										2560.2

Table J-9: Houston North Alignment, Northbound - Train Energy Calculations

Design Train is 100 cars @ 125 tons/car =

12,500 tons

Station	Train Resistance (4.5lb/ton) (lb)	Curve Component		Grade Component		Combined Resistance R (>0) (lb)	Design Velocity V (mph)	Power Required P=R*V (hp)	Time Between Stations (sec)	Energy E=P*T (hp-hour)
		Curvature D (degrees)	Resistance D(0.8lb/ton) (lb)	Gradient G (%)	Resistance G*(20lb/ton) (lb)					
0+00										
21+39	56250	2	20000	0.40	100000	176250	60	28200	24.3	190.4
86+93	56250	0	0	0.40	100000	156250	60	25000	74.5	517.4
100+00	56250	1	10000	0.40	100000	166250	60	26600	14.9	110.1
119+14	56250	1	10000	0.12	30000	96250	60	15400	21.8	93.3
133+98	56250	0	0	0.12	30000	86250	60	13800	16.9	64.8
148+72	56250	2	20000	0.12	30000	106250	60	17000	16.8	79.3
160+00	56250	0	0	0.12	30000	86250	60	13800	12.8	49.1
225+00	56250	0	0	0.50	125000	181250	60	29000	73.9	595.3
232+67	56250	0	0	-0.40	-100000	0	60	0	8.7	0.0
241+42	56250	2	20000	-0.40	-100000	0	60	0	9.9	0.0
276+42	56250	0	0	-0.40	-100000	0	60	0	39.8	0.0
300+00	56250	3	30000	-0.40	-100000	0	60	0	26.8	0.0
309+63	56250	3	30000	0.00	0	86250	60	13800	10.9	41.8
										1741.5

Table J-10: Houston South Alignment, Northbound - Train Energy Calculations

Design Train is 100 cars @ 125 tons/car = 12,500 tons

Station	Train Resistance (4.5lb/ton) (lb)	Curve Component		Grade Component		Combined Resistance R (>0) (lb)	Design Velocity V (mph)	Power Required P=R*V (hp)	Time Between Stations (sec)	Energy E=P*T (hp-hour)
		Curvature D (degrees)	Resistance D(0.8lb/ton) (lb)	Gradient G (%)	Resistance G*(20lb/ton) (lb)					
0+00										
51+79	56250	0	0	0.20	50000	106250	60	17000	58.9	278.1
83+25	56250	1	10000	0.20	50000	116250	60	18600	35.8	185.0
100+00	56250	0	0	0.20	50000	106250	60	17000	19.0	89.7
184+09	56250	0	0	0.50	125000	181250	60	29000	95.6	770.1
200+00	56250	2	20000	0.50	125000	201250	60	32200	18.1	161.9
204+26	56250	2	20000	0.20	50000	126250	60	20200	4.8	26.9
226+47	56250	0	0	0.20	50000	106250	60	17000	25.2	119.0
243+24	56250	2	20000	0.20	50000	126250	60	20200	19.1	107.2
260+00	56250	0	0	0.20	50000	106250	60	17000	19.0	89.7
260+04	56250	0	0	-0.40	-100000	0	60	0	0.0	0.0
281+25	56250	2	20000	-0.40	-100000	0	60	0	24.1	0.0
305+07	56250	0	0	-0.40	-100000	0	60	0	27.1	0.0
310+00	56250	3	30000	-0.40	-100000	0	60	0	5.6	0.0
330+72	56250	3	30000	0.00	0	86250	60	13800	23.5	90.1
										1917.7

Table J-11: Big Lake Alignment, Northbound - Train Energy Calculations

Design Train is 100 cars @ 125 tons/car =

12,500 tons

Station	Train Resistance (4.5lb/ton) (lb)	Curve Component		Grade Component		Combined Resistance R (>0) (lb)	Design Velocity V (mph)	Power Required P=R*V (hp)	Time Between Stations (sec)	Energy E=P*T (hp-hour)
		Curvature D (degrees)	Resistance D(0.8lb/ton) (lb)	Gradient G (%)	Resistance G*(20lb/ton) (lb)					
0+00										
60+00	56250	0	0	0.36	90000	146250	60	23400	68.2	443.3
66+28	56250	0	0	0.00	0	56250	60	9000	7.1	17.8
85+41	56250	2	20000	0.00	0	76250	60	12200	21.7	73.5
99+37	56250	0	0	0.00	0	56250	60	9000	15.9	39.8
123+69	56250	1.5	15000	0.00	0	71250	60	11400	27.6	87.4
130+00	56250	0	0	0.00	0	56250	60	9000	7.2	18.0
145+92	56250	0	0	0.40	100000	156250	60	25000	18.1	125.7
167+78	56250	0.5	5000	0.40	100000	161250	60	25800	24.8	177.7
180+00	56250	0	0	0.40	100000	156250	60	25000	13.9	96.5
194+77	56250	0	0	-0.50	-125000	0	60	0	16.8	0.0
206+93	56250	0.5	5000	-0.50	-125000	0	60	0	13.8	0.0
234+16	56250	0	0	-0.50	-125000	0	60	0	30.9	0.0
292+99	56250	0.75	7500	-0.50	-125000	0	60	0	66.9	0.0
300+11	56250	0	0	-0.50	-125000	0	60	0	8.1	0.0
329+65	56250	1	10000	-0.50	-125000	0	60	0	33.6	0.0
342+15	56250	0	0	-0.50	-125000	0	60	0	14.2	0.0
355+00	56250	1.25	12500	-0.50	-125000	0	60	0	14.6	0.0
388+38	56250	1.25	12500	0.20	50000	118750	60	19000	37.9	200.0
430+00	56250	0	0	0.20	50000	106250	60	17000	47.3	223.4
454+21	56250	0	0	-0.50	-125000	0	60	0	27.5	0.0
462+65	56250	1	10000	-0.50	-125000	0	60	0	9.6	0.0
500+00	56250	0	0	-0.50	-125000	0	60	0	42.4	0.0
529+57	56250	0	0	0.50	125000	181250	60	29000	33.6	270.7
538+32	56250	1	10000	0.50	125000	191250	60	30600	9.9	84.2
568+27	56250	0	0	0.50	125000	181250	60	29000	34.0	273.9
577+01	56250	1	10000	0.50	125000	191250	60	30600	9.9	84.2
600+00	56250	0	0	0.50	125000	181250	60	29000	26.1	210.3
650+77	56250	0	0	0.32	80000	136250	60	21800	57.7	349.4
682+95	56250	1	10000	0.32	80000	146250	60	23400	36.6	237.9
725+27	56250	0	0	0.32	80000	136250	60	21800	48.1	291.3
732+22	56250	2	20000	0.32	80000	156250	60	25000	7.9	54.9
739+77	56250	0	0	0.32	80000	136250	60	21800	8.6	52.1
740+00	56250	2	20000	0.32	80000	156250	60	25000	0.3	2.1
744+19	56250	2	20000	-0.40	-100000	0	60	0	4.8	0.0
766+50	56250	0	0	-0.40	-100000	0	60	0	25.4	0.0
772+00	56250	2	20000	-0.40	-100000	0	60	0	6.3	0.0
810+00	56250	0	0	-0.40	-100000	0	60	0	43.2	0.0
848+78	56250	0	0	0.50	125000	181250	60	29000	44.1	355.3
853+43	56250	2	20000	0.50	125000	201250	60	32200	5.3	47.4
881+28	56250	0	0	0.50	125000	181250	60	29000	31.6	254.6
893+35	56250	3	30000	0.50	125000	211250	60	33800	13.7	128.6
899+54	56250	0	0	0.50	125000	181250	60	29000	7.0	56.4
936+06	56250	3	30000	0.50	125000	211250	60	33800	41.5	389.6
										4646.0

Table J-12: Existing ARRC Mainline, Northbound - Train Energy Calculations

Design Train is 100 cars @ 125 tons/car =

12,500 tons

ARRC Milepost	Station	Train Resistance (4.5lb/ton) (lb)	Curve Component		Grade Component		Combined Resistance R (>0) (lb)	Design Velocity V (mph)	Power Required P=R*V (hp)	Time Between Stations (sec)	Energy E=P*T (hp-hour)
			Curvature D (degrees)	Resistance D(0.8lb/ton) (lb)	Gradient G (%)	Resistance G*(20lb/ton) (lb)					
170.30	0+00										
170.86	29+57	56250	3	30000	0.71	177500	263750	60	42200	33.6	393.9
171.20	47+52	56250	0	0	0.71	177500	233750	60	37400	20.4	211.9
171.33	54+38	56250	2.46	24600	0.71	177500	258350	60	41336	7.8	89.6
171.70	73+92	56250	2.46	24600	-0.34	-85000	0	60	0	22.2	0.0
171.85	81+84	56250	2.46	24600	0.15	37500	118350	60	18936	9.0	47.3
171.92	85+54	56250	0	0	0.15	37500	93750	60	15000	4.2	17.5
172.20	100+32	56250	0	0	-0.30	-75000	0	60	0	16.8	0.0
172.54	118+27	56250	0	0	0.54	135000	191250	60	30600	20.4	173.4
173.20	153+12	56250	0	0	-0.82	-205000	0	60	0	39.6	0.0
173.72	180+58	56250	0.75	7500	-0.82	-205000	0	60	0	31.2	0.0
173.78	183+75	56250	0.75	7500	-0.02	-5000	58750	60	9400	3.6	9.4
173.98	194+31	56250	0	0	-0.02	-5000	51250	60	8200	12.0	27.3
174.26	209+09	56250	0	0	0.17	42500	98750	60	15800	16.8	73.7
174.49	221+23	56250	0	0	-0.44	-110000	0	60	0	13.8	0.0
174.60	227+04	56250	1.02	10200	-0.44	-110000	0	60	0	6.6	0.0
174.69	231+79	56250	1.02	10200	-0.13	-32500	33950	60	5432	5.4	8.1
174.94	244+99	56250	0	0	-0.13	-32500	23750	60	3800	15.0	15.8
175.07	251+85	56250	0.92	9200	-0.13	-32500	32950	60	5272	7.8	11.4
175.14	255+55	56250	0	0	-0.13	-32500	23750	60	3800	4.2	4.4
175.40	269+28	56250	0	0	0.05	12500	68750	60	11000	15.6	47.7
175.65	282+48	56250	0	0	-0.10	-25000	31250	60	5000	15.0	20.8
175.94	297+79	56250	0	0	-0.18	-45000	11250	60	1800	17.4	8.7
176.10	306+24	56250	0	0	0.23	57500	113750	60	18200	9.6	48.5
176.19	310+99	56250	0	0	-0.31	-77500	0	60	0	5.4	0.0
176.44	324+19	56250	0.92	9200	-0.31	-77500	0	60	0	15.0	0.0
176.47	325+77	56250	0.92	9200	0.00	0	65450	60	10472	1.8	5.2
176.69	337+39	56250	0	0	0.00	0	56250	60	9000	13.2	33.0
177.04	355+87	56250	0.97	9700	0.00	0	65950	60	10552	21.0	61.6
177.09	358+51	56250	0.97	9700	-0.15	-37500	28450	60	4552	3.0	3.8
177.16	362+21	56250	0	0	-0.15	-37500	18750	60	3000	4.2	3.5
177.35	372+24	56250	0	0	0.08	20000	76250	60	12200	11.4	38.6
177.54	382+27	56250	0	0	-0.06	-15000	41250	60	6600	11.4	20.9
177.90	401+28	56250	1.97	19700	-0.06	-15000	60950	60	9752	21.6	58.5
177.97	404+98	56250	1.97	19700	0.24	60000	135950	60	21752	4.2	25.4
178.29	421+88	56250	0	0	0.24	60000	116250	60	18600	19.2	99.2
178.55	435+61	56250	0	0	0.66	165000	221250	60	35400	15.6	153.4
178.64	440+36	56250	0	0	-0.49	-122500	0	60	0	5.4	0.0
178.78	447+75	56250	0.43	4300	-0.49	-122500	0	60	0	8.4	0.0
178.98	458+31	56250	0	0	-0.49	-122500	0	60	0	12.0	0.0
179.26	473+09	56250	0	0	0.03	7500	63750	60	10200	16.8	47.6
179.41	481+01	56250	0	0	-0.03	-7500	48750	60	7800	9.0	19.5
179.48	484+71	56250	1.5	15000	-0.03	-7500	63750	60	10200	4.2	11.9
179.68	495+27	56250	1.5	15000	0.39	97500	168750	60	27000	12.0	90.0
179.82	502+66	56250	0	0	-0.14	-35000	21250	60	3400	8.4	7.9
180.01	512+69	56250	0	0	-0.68	-170000	0	60	0	11.4	0.0
180.20	522+72	56250	0	0	0.48	120000	176250	60	28200	11.4	89.3
180.45	535+92	56250	0	0	0.72	180000	236250	60	37800	15.0	157.5
180.52	539+62	56250	0.9	9000	0.72	180000	245250	60	39240	4.2	45.8

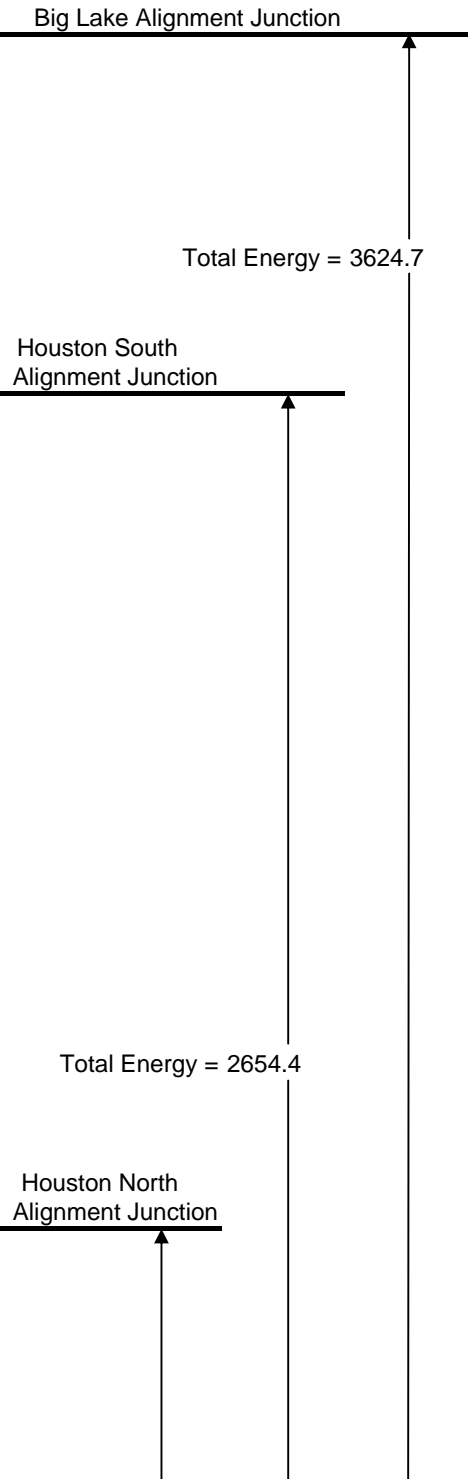




Table J-12: Existing ARRC Mainline, Northbound - Train Energy Calculations

Design Train is 100 cars @ 125 tons/car =

12,500 tons

ARRC Milepost	Station	Train Resistance (4.5lb/ton) (lb)	Curve Component		Grade Component		Combined Resistance R (>0) (lb)	Design Velocity V (mph)	Power Required P=R*V (hp)	Time Between Stations (sec)	Energy E=P*T (hp-hour)
			Curvature D (degrees)	Resistance D(0.8lb/ton) (lb)	Gradient G (%)	Resistance G*(20lb/ton) (lb)					
180.68	548+07	56250	0.9	9000	0.11	27500	92750	60	14840	9.6	39.6
180.73	550+71	56250	0.9	9000	-0.01	-2500	62750	60	10040	3.0	8.4
180.95	562+33	56250	0	0	-0.01	-2500	53750	60	8600	13.2	31.5
181.02	566+03	56250	0	0	-0.78	-195000	0	60	0	4.2	0.0
181.24	577+65	56250	1.7	17000	-0.78	-195000	0	60	0	13.2	0.0
181.34	582+93	56250	0	0	-0.78	-195000	0	60	0	6.0	0.0
181.40	586+10	56250	0	0	-0.01	-2500	53750	60	8600	3.6	8.6
181.56	594+55	56250	2.75	27500	-0.01	-2500	81250	60	13000	9.6	34.7
181.80	607+22	56250	0	0	-0.01	-2500	53750	60	8600	14.4	34.4
182.00	617+78	56250	0	0	-0.09	-22500	33750	60	5400	12.0	18.0
182.02	618+84	56250	0	0	0.47	117500	173750	60	27800	1.2	9.3
182.14	625+18	56250	4.1	41000	0.47	117500	214750	60	34360	7.2	68.7
182.28	632+57	56250	4.1	41000	0.53	132500	229750	60	36760	8.4	85.8
182.52	645+24	56250	0	0	0.53	132500	188750	60	30200	14.4	120.8
182.82	661+08	56250	0	0	1.02	255000	311250	60	49800	18.0	249.0
182.98	669+53	56250	0	0	0.31	77500	133750	60	21400	9.6	57.1
183.07	674+28	56250	0	0	-0.31	-77500	0	60	0	5.4	0.0
183.12	676+92	56250	5.98	59800	-0.31	-77500	38550	60	6168	3.0	5.1
183.23	682+73	56250	5.98	59800	-1.17	-292500	0	60	0	6.6	0.0
183.34	688+54	56250	5.98	59800	-0.25	-62500	53550	60	8568	6.6	15.7
183.58	701+21	56250	5.92	59200	-0.03	-7500	107950	60	17272	14.4	69.1
183.70	707+55	56250	0	0	-0.03	-7500	48750	60	7800	7.2	15.6
184.09	728+14	56250	0	0	-0.65	-162500	0	60	0	23.4	0.0
184.80	765+63	56250	0	0	-0.36	-90000	0	60	0	42.6	0.0
184.97	774+61	56250	0	0	-0.73	-182500	0	60	0	10.2	0.0
185.12	782+53	56250	0.45	4500	-0.73	-182500	0	60	0	9.0	0.0
185.15	784+11	56250	0.45	4500	-0.03	-7500	53250	60	8520	1.8	4.3
185.40	797+31	56250	0	0	-0.03	-7500	48750	60	7800	15.0	32.5
186.06	832+16	56250	0	0	-0.40	-100000	0	60	0	39.6	0.0
186.11	834+80	56250	0.92	9200	-0.40	-100000	0	60	0	3.0	0.0
186.39	849+58	56250	0.92	9200	-0.96	-240000	0	60	0	16.8	0.0
187.06	884+96	56250	0	0	-0.96	-240000	0	60	0	40.2	0.0
187.28	896+58	56250	1.93	19300	0.00	0	75550	60	12088	13.2	44.3
187.42	903+97	56250	0	0	0.00	0	56250	60	9000	8.4	21.0
187.89	928+79	56250	0	0	1.08	270000	326250	60	52200	28.2	408.9
188.09	939+35	56250	0	0	0.00	0	56250	60	9000	12.0	30.0
188.25	947+80	56250	1.47	14700	0.00	0	70950	60	11352	9.6	30.3
188.41	956+25	56250	1.47	14700	-0.59	-147500	0	60	0	9.6	0.0
188.93	983+71	56250	0	0	-0.59	-147500	0	60	0	31.2	0.0

Total Energy = 1864.6

Willow Alignment Junction

*(This page intentionally left blank.)*



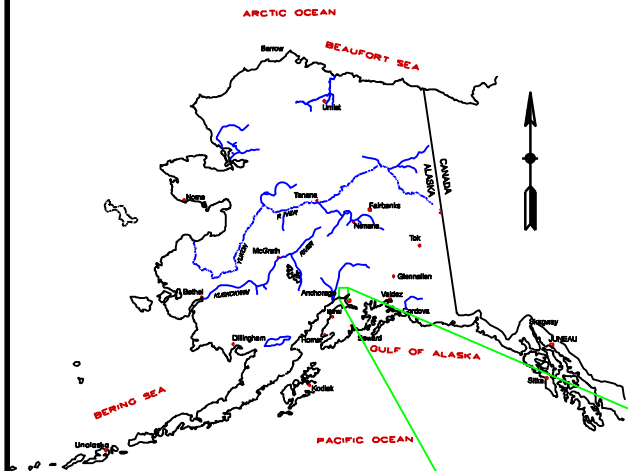
# Conceptual Plan Set

*Port Mackenzie Rail Extension Project — Preliminary Environmental and Alternatives Report*



Prepared for the Matanuska-Susitna Borough and the Alaska Railroad Corporation

# ALASKA RAILROAD CORPORATION PORT MACKENZIE RAIL EXTENSION PORT MACKENZIE TO EXISTING MAINLINE CONCEPTUAL DESIGN JANUARY 2008



**LOCATION MAP**

SCALE: N.T.S.



**OWNER**

**ARRC**  
ALASKA RAILROAD CORPORATION  
327 W. SHIP CREEK AVE.  
ANCHORAGE, AK 99501  
(907) 265-2543

**CIVIL**

**TNH-HANSON, LLC**  
911 W. 8TH AVE.  
ANCHORAGE, AK 99501  
(907) 279-0543

**ENVIRONMENTAL**

**HDR, INC.**  
2525 C STREET #305  
ANCHORAGE, AK 99503  
(907) 644-2000


**GEOTECHNICAL**

**SHANNON & WILSON, INC.**  
5430 FAIRBANKS STREET, SUITE 3  
ANCHORAGE, AK 99518  
(907) 561-2120

**CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION**

AUTHORED BY: TNH-HANSON, LLC

REV.	DATE	BY	REVISION



**CONCEPTUAL DESIGN  
PORT MACKENZIE TO EXISTING MAINLINE**

DESIGNED BY: MAP	SCALE: NONE	AFE NO.:
DRAWN BY: DEM	DATE: JANUARY 2008	ACAD FILE:
CHECKED BY: MAP		DWG NO. <b>A1</b>
APPROVED BY:		<b>1</b> OF <b>48</b>

# ABBREVIATIONS LIST

AC	ASPHALT CONCRETE	NIC	NOT IN CONTRACT
ADA	FEDERAL STANDARDS FOR ACCESSIBLE DESIGN	NTS	NOT TO SCALE
AK	ALASKA	NW	NORTHWEST
ARRC	ALASKA RAILROAD CORPORATION	OH/E	OVERHEAD ELECTRIC
ASCE	AMERICAN SOCIETY OF CIVIL ENGINEERS	PC	POINT OF CURVATURE
ASTM	AMERICAN SOCIETY FOR TESTING AND MATERIALS	PF	POINT OF FROG
BOP	BOTTOM OF PIPE	PI	POINT OF INTERSECTION
C&G	CURB AND GUTTER	POL	PETROLEUM OIL AND LUBRICANTS
C/L	CENTERLINE	PRC	POINT OF REVERSE CURVATURE
CMP	CORRUGATED METAL PIPE	PS	POINT OF SWITCH
COMM	COMMUNICATION LINE	PSI	POUNDS PER SQUARE INCH
CONC	CONCRETE	PT	POINT OF TANGENCY
CPEP	CORRUGATED POLYETHYLENE PIPE	PVC	POINT OF VERTICAL CURVATURE
CSP	CORRUGATED STEEL PIPE	PVI	POINT OF VERTICAL INTERSECTION
CY	CUBIC YARD	PVT	POINT OF VERTICAL TANGENCY
DBL	DOUBLE	PVMT	PAVEMENT
DEMO	DEMOLISH	R	RADIUS
DIA	DIAMETER	RD	ROAD
E	EASTING	RH	RIGHT HAND
ELV	ELEVATION	RLD	ROCK LINED DITCH
EXIST	EXISTING	RT	RIGHT
FF	FINISHED FLOOR ELEVATION	ROW	RIGHT-OF-WAY
FL	FLOW LINE OF GUTTER	SCH	SCHEDULE
FT	FOOT/FEET	SDCBFI	STORMDRAIN CATCH BASIN FIELD INLET
FH	FIRE HYDRANT	SDCBMH	STORMDRAIN CATCH BASIN MANHOLE
HP	HIGH POINT	SE	SOUTHEAST
L	LENGTH	STA	STATION
LH	LEFT HAND	TBD	TO BE DETERMINED
LP	LOW POINT	TBM	TEMPORARY BENCH MARK
LT	LEFT	TC	TOP BACK OF CURB
LF	LINEAR FEET	T/O	TURNOUT
ME	MATCH EXISTING	T/R	TOP OF RAIL
MP	MILEPOST	T/T	TOP OF TIE
MSB	MATANUSKA-SUSITNA BOROUGH	U/G	UNDERGROUND
N	NORTHING	UON	UNLESS OTHERWISE NOTED ON PLANS
NE	NORTHEAST		

# SYMBOL LEGEND

EXISTING (E)	PROPOSED (P)	EXISTING (E)	PROPOSED (P)
—	—	RECREATIONAL AREA AND WILDLIFE REFUGE BOUNDARY	
—30—	—	CONTOURS	
	—	VALLEY GUTTER WITH DRAINAGE DIRECTION	
	—	TRENCH DRAIN CATCH BASIN	
	—	TRENCH DRAIN WITH DRAINAGE DIRECTION	
	—	DRAINAGE SWALE	
	—	ROCK LINED DITCH	
—SD—	—	STORM PIPE	
	—	ENCASED STORM PIPE	
	—	STORM SUBDRAIN PIPE (SOLID)	
	—	STORM SUBDRAIN PIPE (PERFORATED)	
	—	STORM DRAIN CLEANOUT W/ SWEEP DIRECTION	
	—	DRAINAGE DIRECTION	
⊗	⊗	STORM MANHOLE – AREA INLET (FLAT GRATE)	
⊙	⊙	STORM MANHOLE (SOLID LID)	
	⊗	AC PAVEMENT (PLAN VIEW)	
	⊗	RECYCLED ASPHALT PAVEMENT (RAP)	
	⊗	CONCRETE SURFACING (PLAN VIEW)	
	⊗	MODULAR CROSSING PADS (PLAN VIEW)	
	⊗	VEHICLE ACCESS (PLAN VIEW), SEE TYPICAL SECTIONS	
	⊗	AC PAVEMENT (SECTIONS)	
	⊗	LEVELING COURSE (SECTIONS)	
	⊗	TYPE II FILL MATERIAL (SECTIONS)	
	⊗	PAVEMENT DEMOLITION	
□—□—□	□—□—□	GUARD RAIL OR FENCE	
—S—	—S—	SANITARY SEWER PIPE	
—W—	—W—	WATER PIPE	
—UG/E—	—UG/E—	UNDERGROUND ELECTRIC	
—OH/E—	—OH/E—	OVERHEAD ELECTRIC	
UG/COMM	UG/COMM	UNDERGROUND COMMUNICATIONS	
—UG/FO—	—UG/FO—	UNDERGROUND FIBER OPTIC	
	⊗	SERVICE ISLAND (REFER TO SITE & ELECTRICAL SHEETS)	
	S2-2	STORM PIPE OR STORM MANHOLE STRUCTURE NUMBER	
		SHEET NUMBER STRUCTURE APPEARS ON STORM PIPING SHTS.	
		P= PIPE	
		S= MANHOLE STRUCTURE	
		RAIL SIGNAL	
		HYDRANT	
		WATER VALVE	
		BOLLARDS	
		CATCH BASIN	
		STREET LAMP	
		GAS METER	
		ELECTRIC METER	
		ELECTRIC VAULT	
		ELECTRIC MANHOLE	
		JUNCTION BOX	
		SANITARY SEWER MANHOLE	
		VEGETATION	
		TRASH CAN	
		CULVERT	
		PIPE FLOW DIRECTION	
		RELOCATED SIGN W/ POST	
		RAILROAD TRACKS	
		RAILROAD TRACKS TO BE DEMOLISHED	
		ROAD	
		UTILITY POLE	
		SURVEY CONTROL POINTS	
		TOP OF RAIL ELEVATION (ON LEFT)	
		EXISTING GRADE ELEVATION (ON RIGHT)	
		ARRC MILEPOST	
		FIBER OPTIC LINES	
		BRIDGE	

# GENERAL NOTE

EXISTING GROUND SHOWN IN PROFILE IS BASED ON A DIGITAL ELEVATION MODEL (DEM), AVAILABLE FROM THE US GEOLOGICAL SURVEY (USGS) AT: <http://edc2usgs.gov/geodata/index.php>.

<b>CONCEPTUAL PLANS NOT FOR CONSTRUCTION</b>	AUTHORED BY: TNH-HANSON, LLC	
	DESIGNED BY: DEM	SCALE: NONE
	DRAWN BY: DEM	DATE: JANUARY 2008
CHECKED BY: MAP	APPROVED BY:	AFE NO.: ACAD FILE: DWG NO. <b>A2</b> OF <b>48</b>
REV. DATE BY REVISION		



TITLE: **CONCEPTUAL DESIGN  
PORT MACKENZIE TO EXISTING MAINLINE**



**SHEET TITLE**

PLAN AND PROFILE, TERMINAL	T1	PLAN AND PROFILE, WILLOW	W1 - W11
PLAN AND PROFILE, MAC WEST	MW1 - MW4	PLAN AND PROFILE, HOUSTON	H1 - H4
PLAN AND PROFILE, MAC EAST	ME1 - ME4	PLAN AND PROFILE, HOUSTON NORTH	HN1 - HN3
PLAN AND PROFILE, CONNECTION 1	C1-1 - C1-2	PLAN AND PROFILE, HOUSTON SOUTH	HS1 - HS3
PLAN AND PROFILE, CONNECTION 2	C2-1 - C2-2	PLAN AND PROFILE, BIG LAKE	B1 - B8
PLAN AND PROFILE, CONNECTION 3	C3-1 - C3-2		

**CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION**

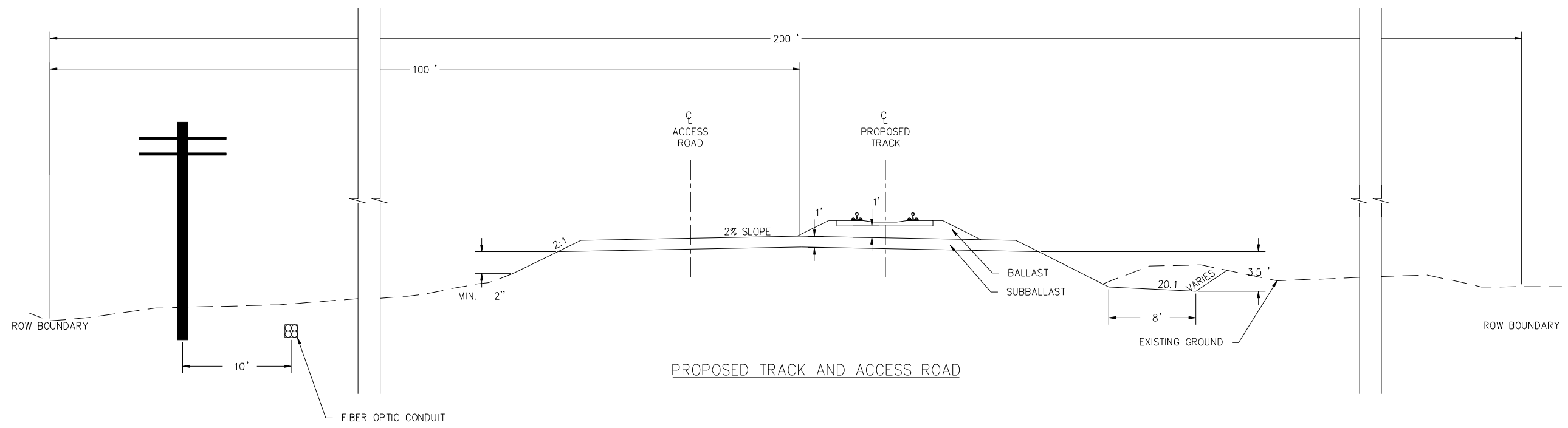
AUTHORED BY: TNH-HANSON, LLC			
REV.	DATE	BY	REVISION

ALIGNMENT	ARRC COMMUNICATION TOWERS TO BE CONSTRUCTED
ALL ALIGNMENTS	1
WILLOW	2 & 3
HOUSTON	2 & 4
BIG LAKE	5

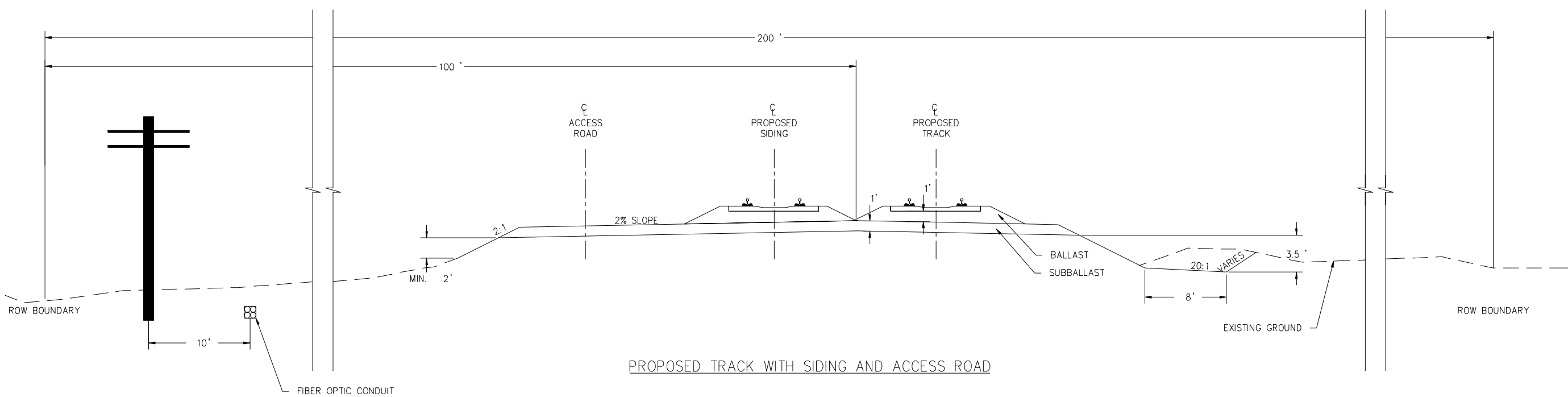


**PLAN AND PROFILE  
MAP KEY**

DESIGNED BY: DEM	SCALE: NONE	AFE NO.:
DRAWN BY: DEM	DATE: JANUARY 2008	ACAD FILE:
CHECKED BY: MAP		DWG NO. <b>A3</b>
APPROVED BY:		<b>3</b> OF <b>48</b>



PROPOSED TRACK AND ACCESS ROAD



PROPOSED TRACK WITH SIDING AND ACCESS ROAD

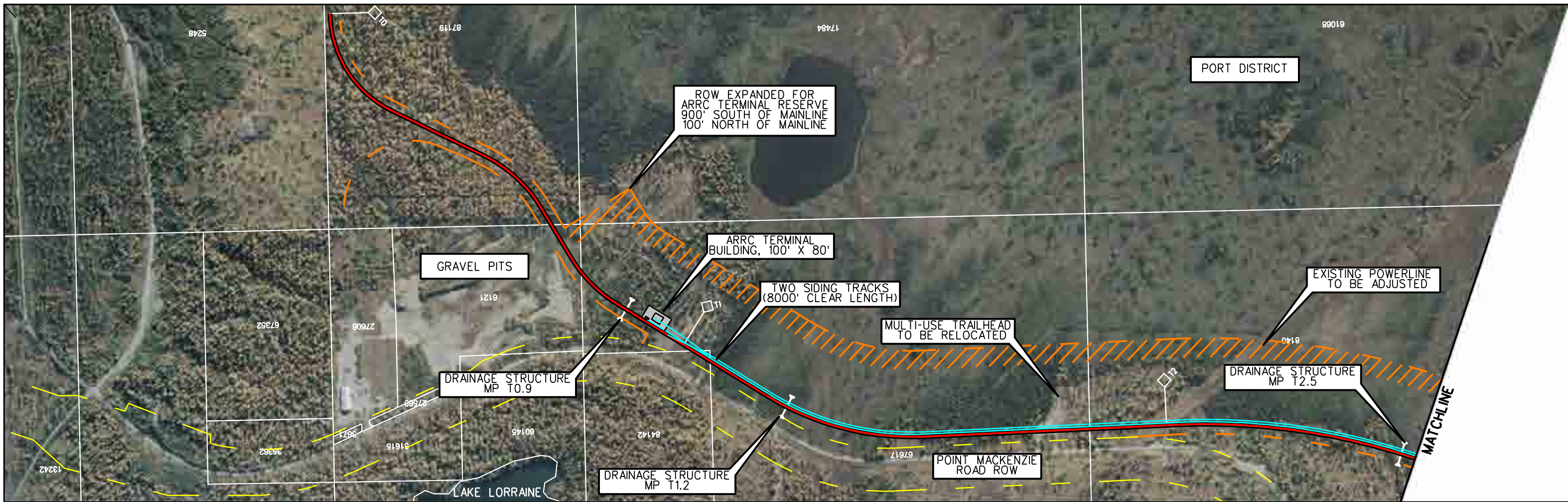


**CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION**

REV.	DATE	BY	REVISION

AUTHORED BY: TNH-HANSON, LLC			
DESIGNED BY:	MAP	SCALE:	NONE
DRAWN BY:	TGL	DATE:	JANUARY 2008
CHECKED BY:	MAP	AFE NO.:	
APPROVED BY:		ACAD FILE:	
		DWG NO.:	<b>A4</b>
			<b>4</b> OF <b>48</b>

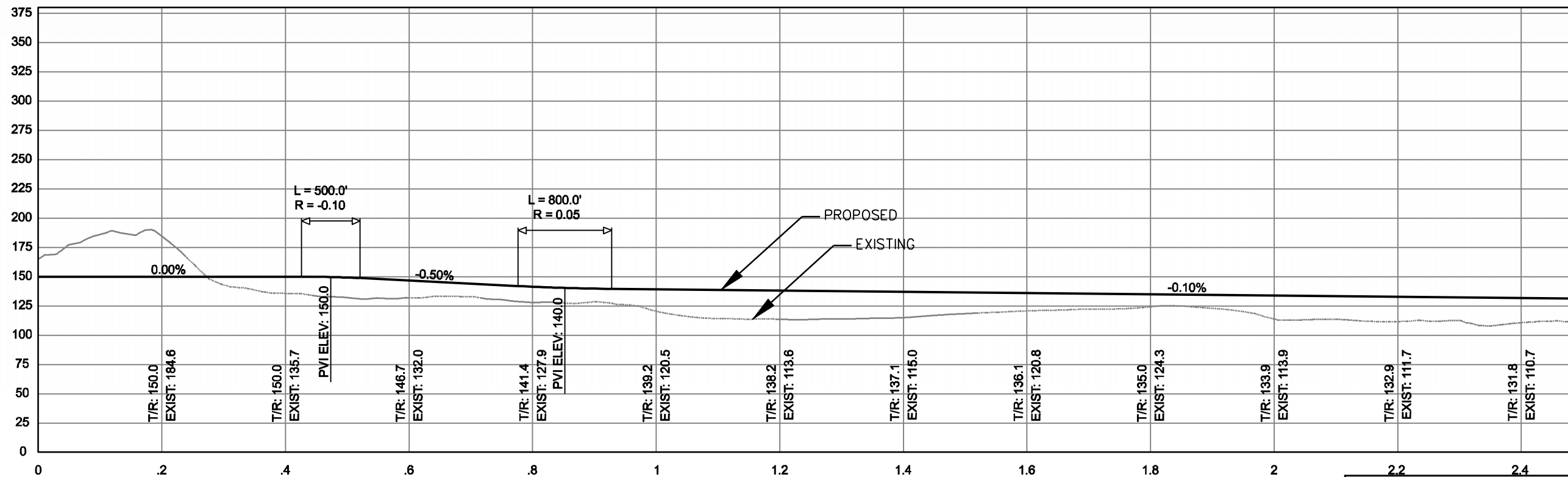




← TO PORT MACKENZIE

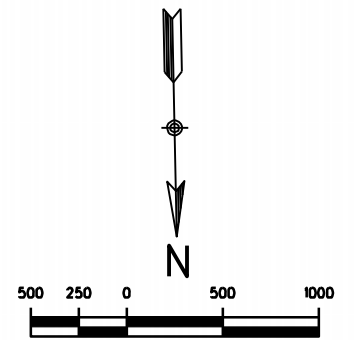
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



PLAN AND PROFILE  
MAC WEST & MAC EAST, MP T0.0-T2.5

CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

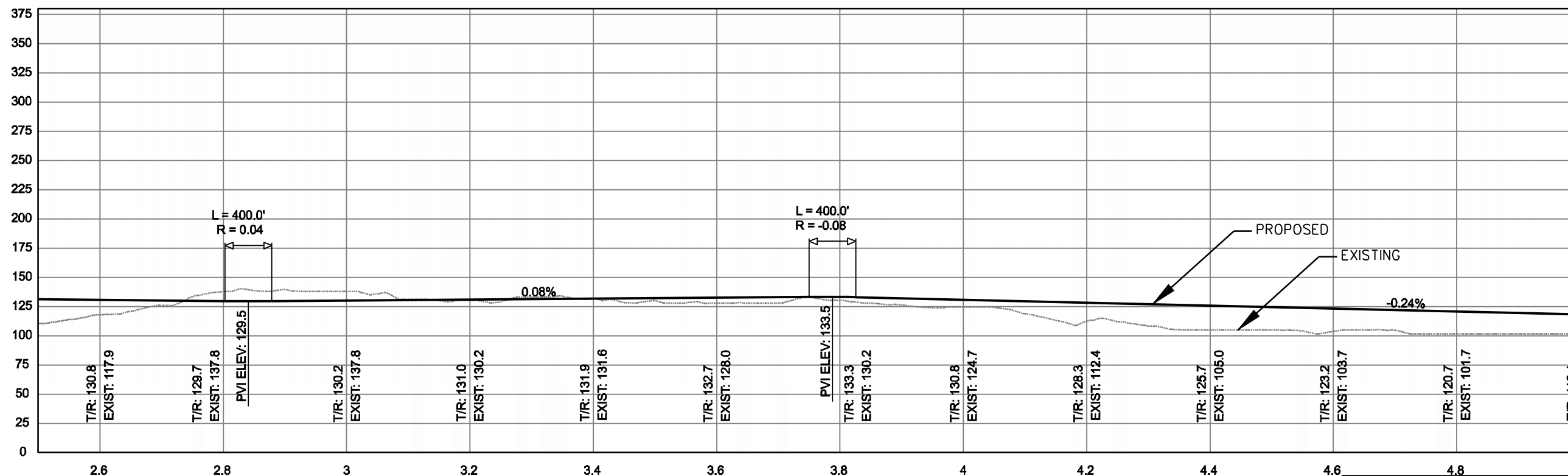
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	5 OF 48
DRAWN BY: DEM			
CHECKED BY: MAP			
APPROVED BY:			



← TO PORT MACKENZIE

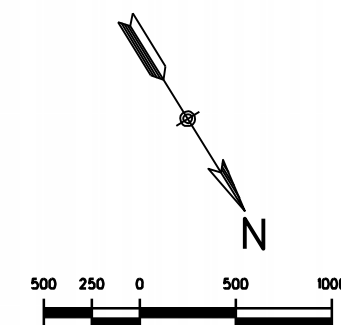
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA  
REFER TO VOLUME 2, APPENDIX G  
PRELIMINARY HYDROLOGY REPORT

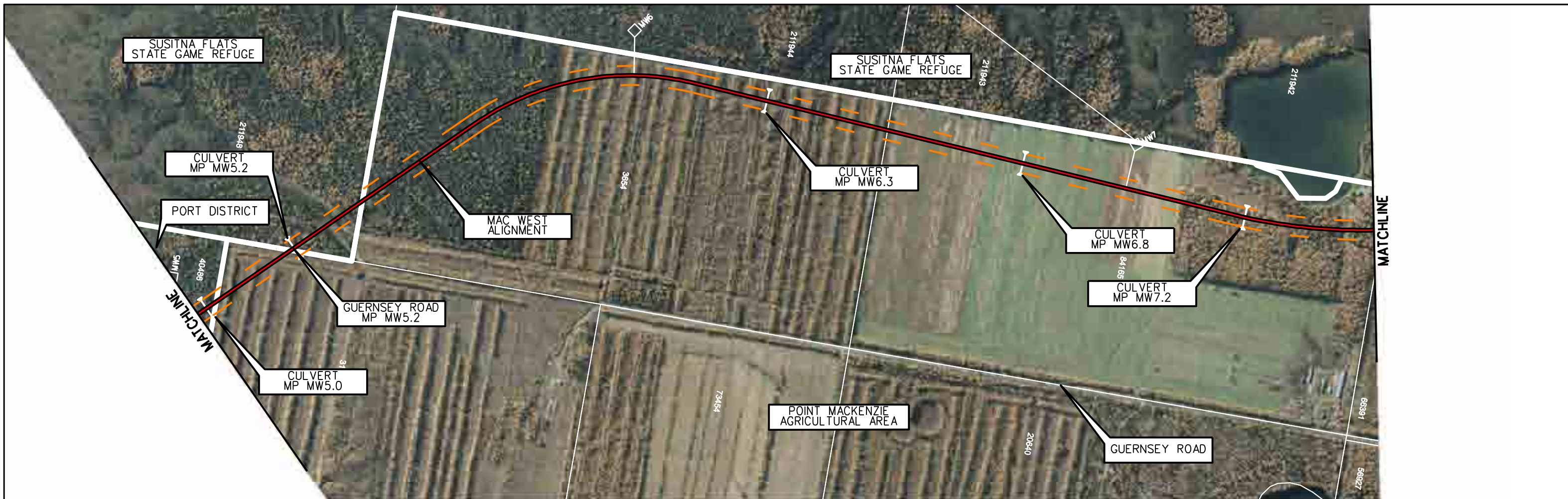


PLAN AND PROFILE  
MAC WEST, MP T2.5-MW5.0

CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

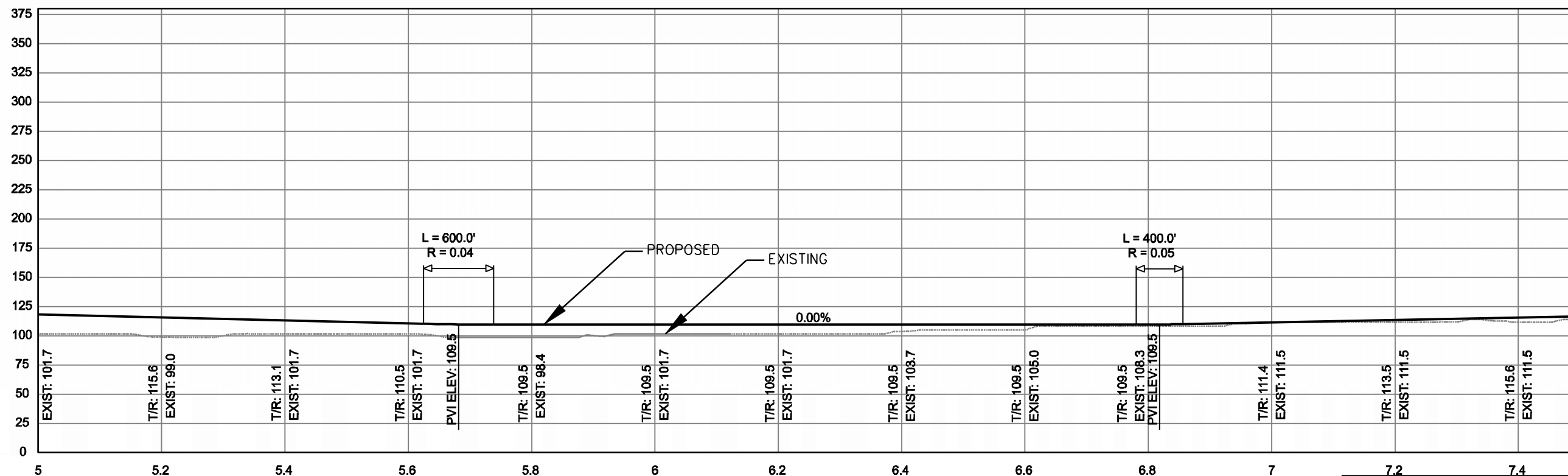
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	DATE: JANUARY 2008
DRAWN BY: DEM	CHECKED BY: MAP	APPROVED BY:	APPROVED BY:
DWG NO. MW1		6 OF 48	



← TO PORT MACKENZIE

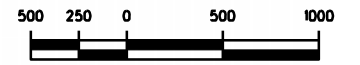
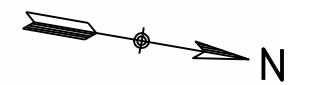
PLAN

TO EXISTING MAINLINE →



PROFILE

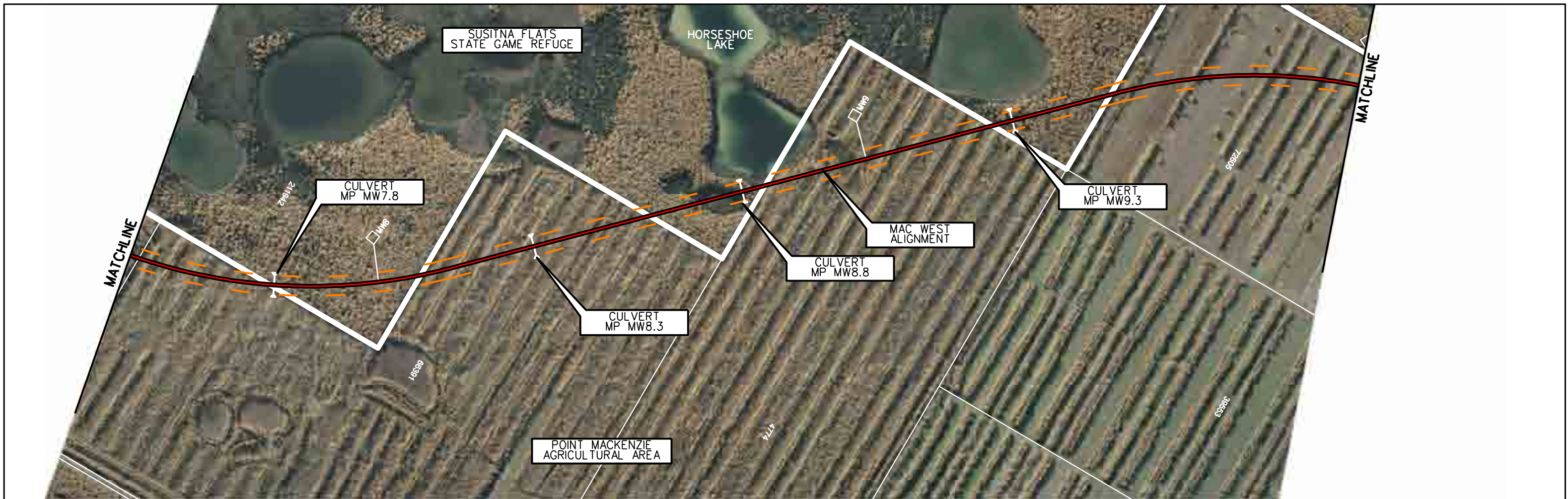
FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

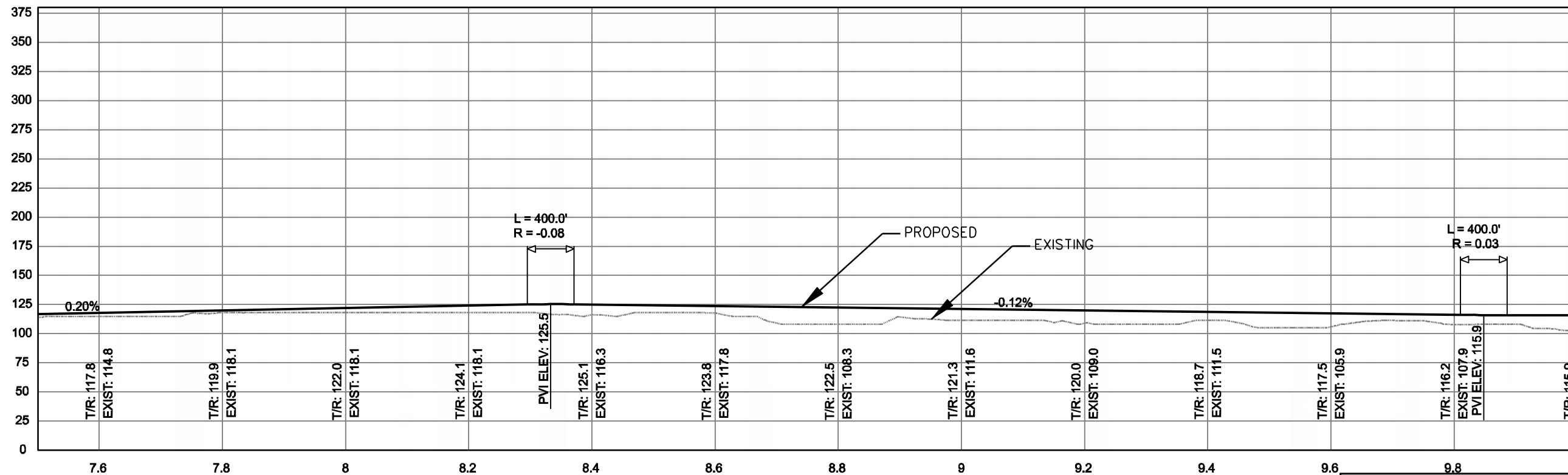
<b>PLAN AND PROFILE MAC WEST, MP MW5.0-MW7.5</b>			
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	7 OF 48
DRAWN BY: DEM	CHECKED BY: MAP	APPROVED BY:	
MAP NO: MW2 DWG NO: 7			



← TO PORT MACKENZIE

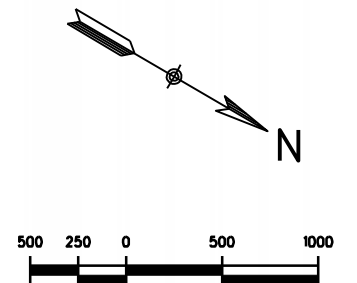
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



PLAN AND PROFILE  
MAC WEST, MP MW7.5-MW10.0

CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

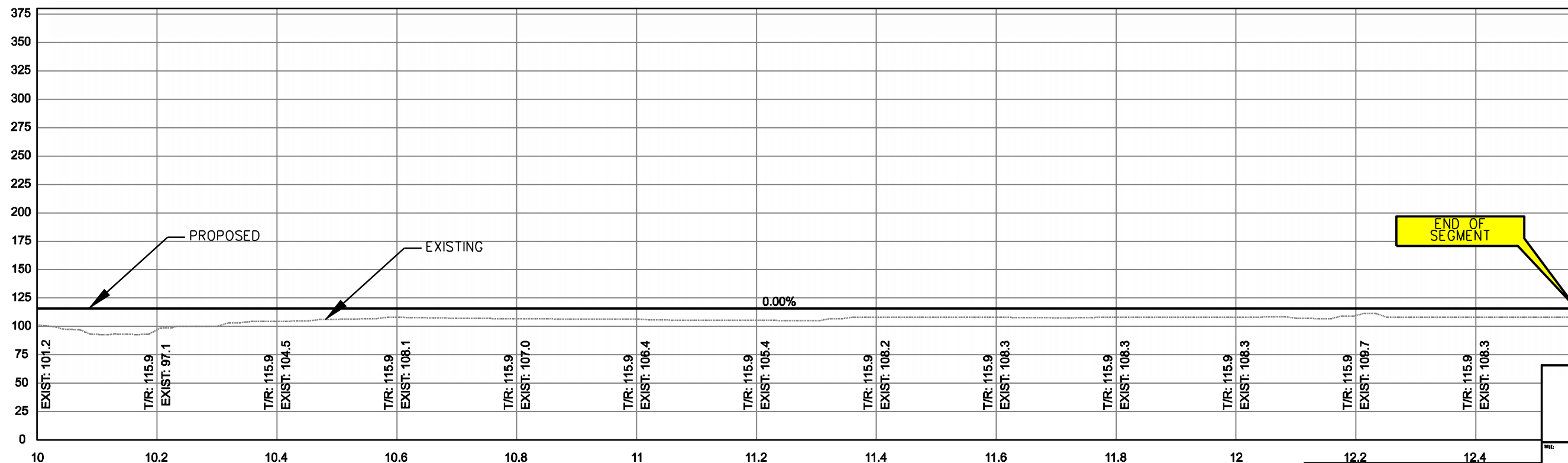
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	APP. NO.:
DRAWN BY: DEM			ACAD FILE:
CHECKED BY: MAP			DWG NO. MW3
APPROVED BY:			8 OF 48



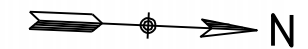
← TO PORT MACKENZIE

PLAN

TO EXISTING MAINLINE →



FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



PLAN AND PROFILE  
MAC WEST, MP MW10.0-MW12.6

CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

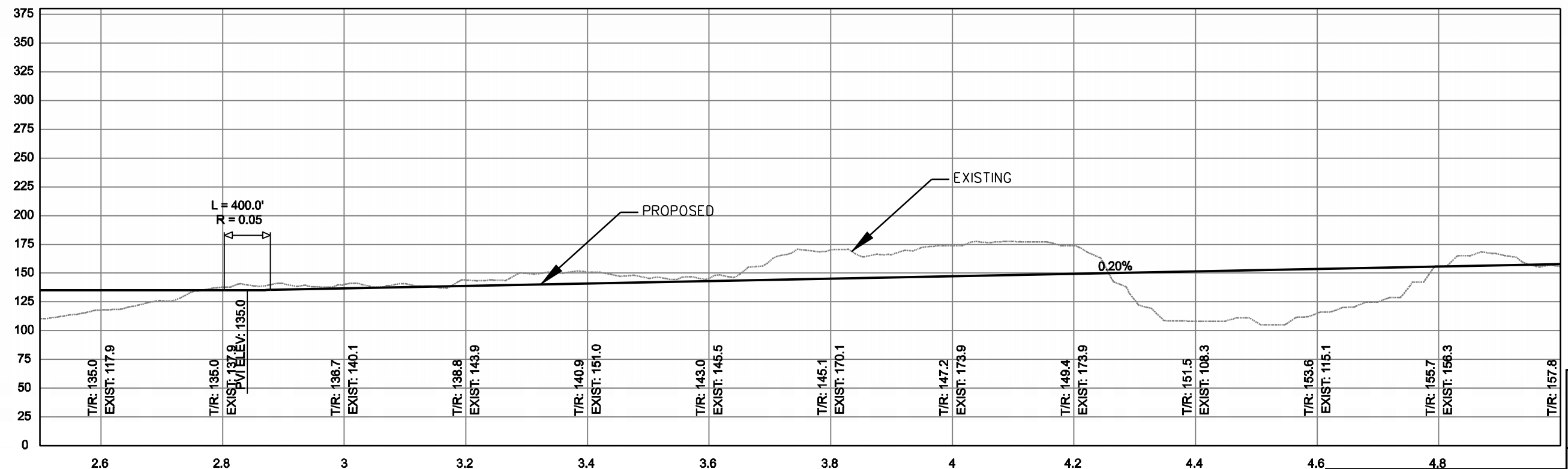
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	APP. NO. MW4
DRAWN BY: DEM			9 OF 48
CHECKED BY: MAP			
APPROVED BY:			



← TO PORT MACKENZIE

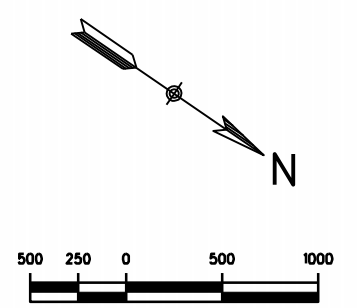
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

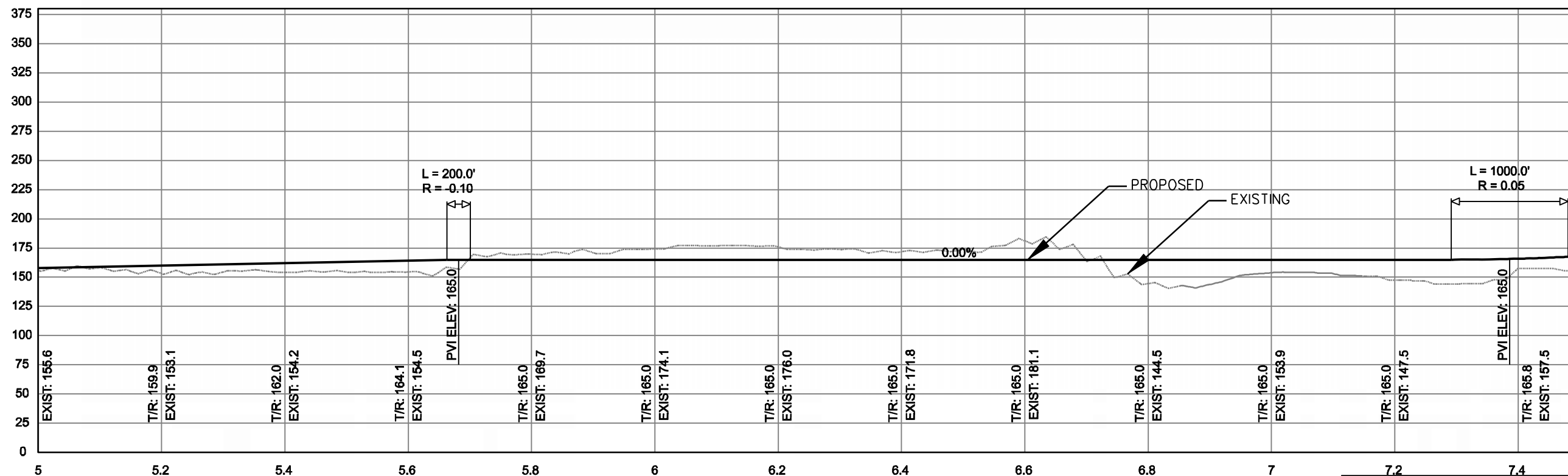
TITLE: <b>PLAN AND PROFILE          MAC EAST, MP ME2.5-ME5.0</b>		
DESIGNED BY: MAP	SCALE: AS SHOWN	AFE NO: -
DRAWN BY: DEM	DATE: JANUARY 2008	ACAD FILE: -
CHECKED BY: MAP		DWG NO: ME1
APPROVED BY: -		10 OF 48



← TO PORT MACKENZIE

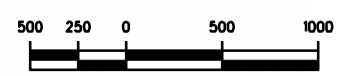
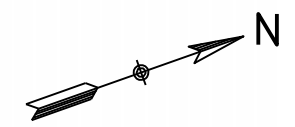
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G, PRELIMINARY HYDROLOGY REPORT



CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

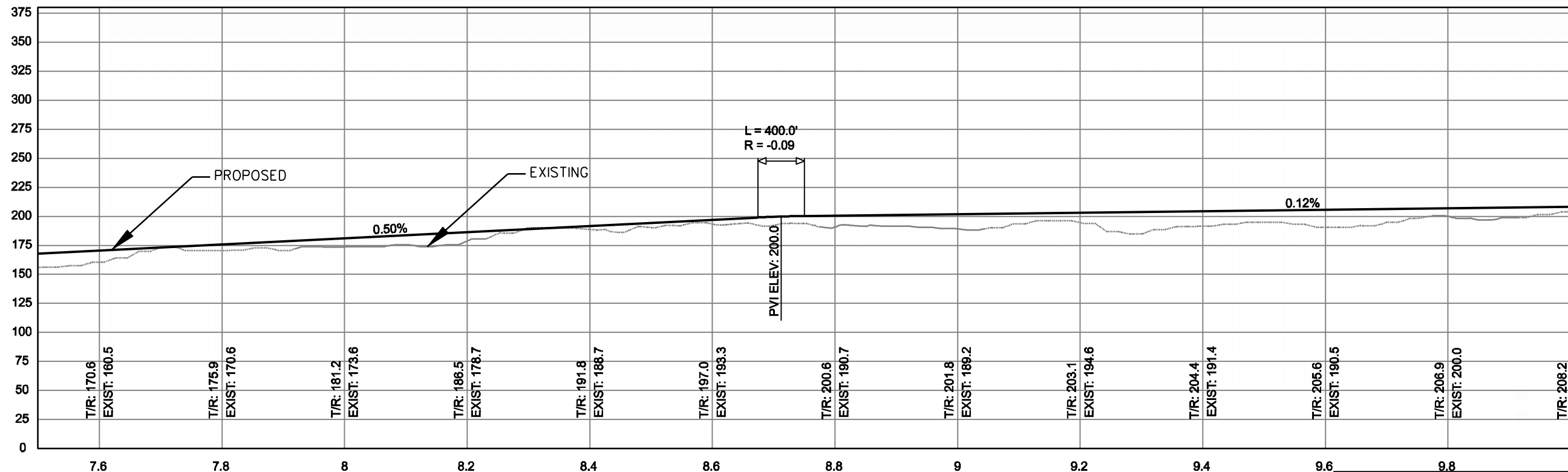
<b>PLAN AND PROFILE MAC EAST, MP ME5.0-ME7.5</b>		
DESIGNED BY: MAP	SCALE: AS SHOWN	AFE NO: _____
DRAWN BY: DEM	DATE: JANUARY 2008	ACAD FILE: _____
CHECKED BY: MAP		DWG NO: <b>ME2</b>
APPROVED BY: _____		<b>11</b> OF <b>48</b>



← TO PORT MACKENZIE

PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

TITLE: <b>PLAN AND PROFILE          MAC EAST, MP ME7.5-ME10.0</b>		
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008
DRAWN BY: DEM	CHECKED BY: MAP	APPROVED BY:
DWG NO. <b>ME3</b>		SHEET NO. <b>12</b> OF <b>48</b>

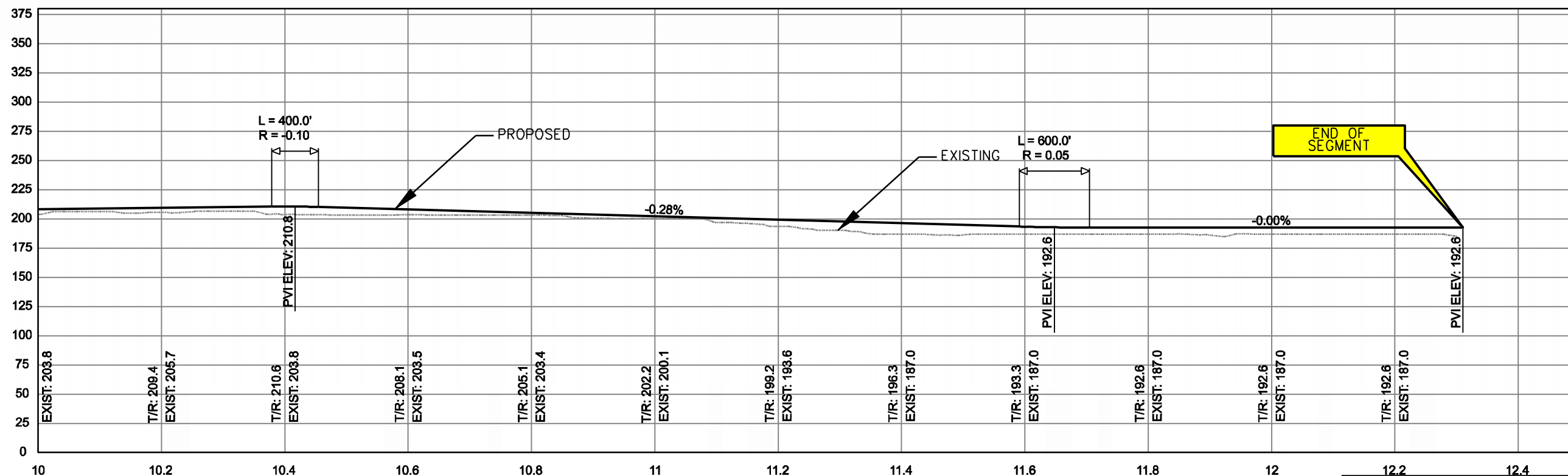




← TO PORT MACKENZIE

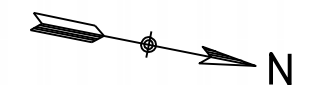
PLAN

TO EXISTING MAINLINE →



PROFILE

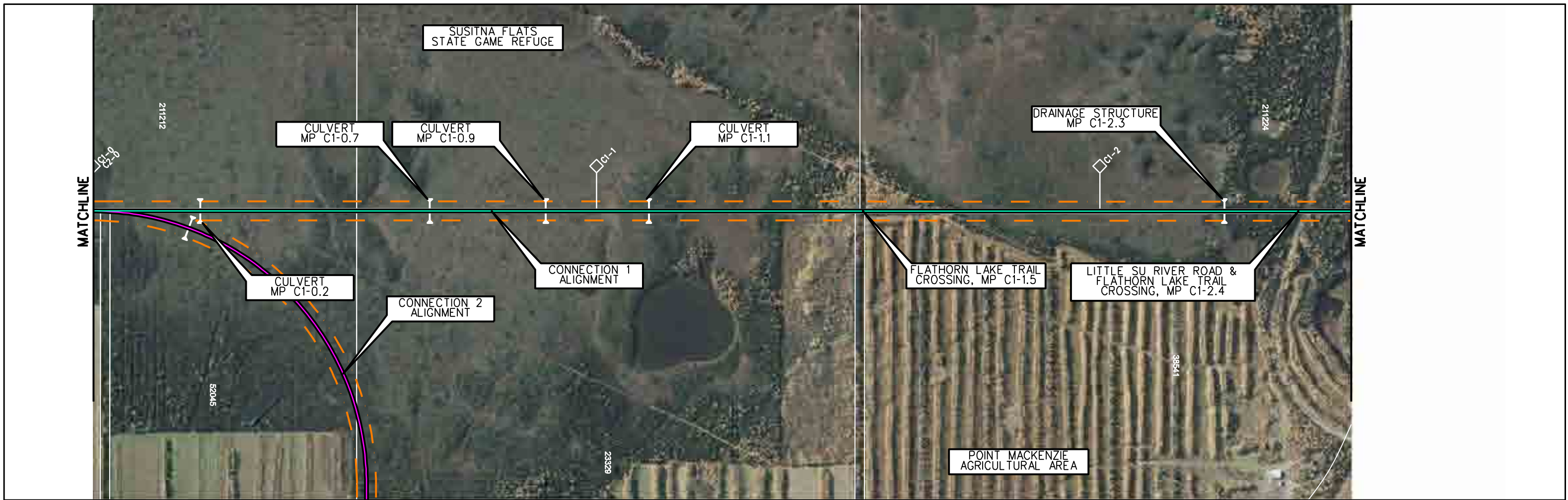
FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



CONCEPTUAL PLANS NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

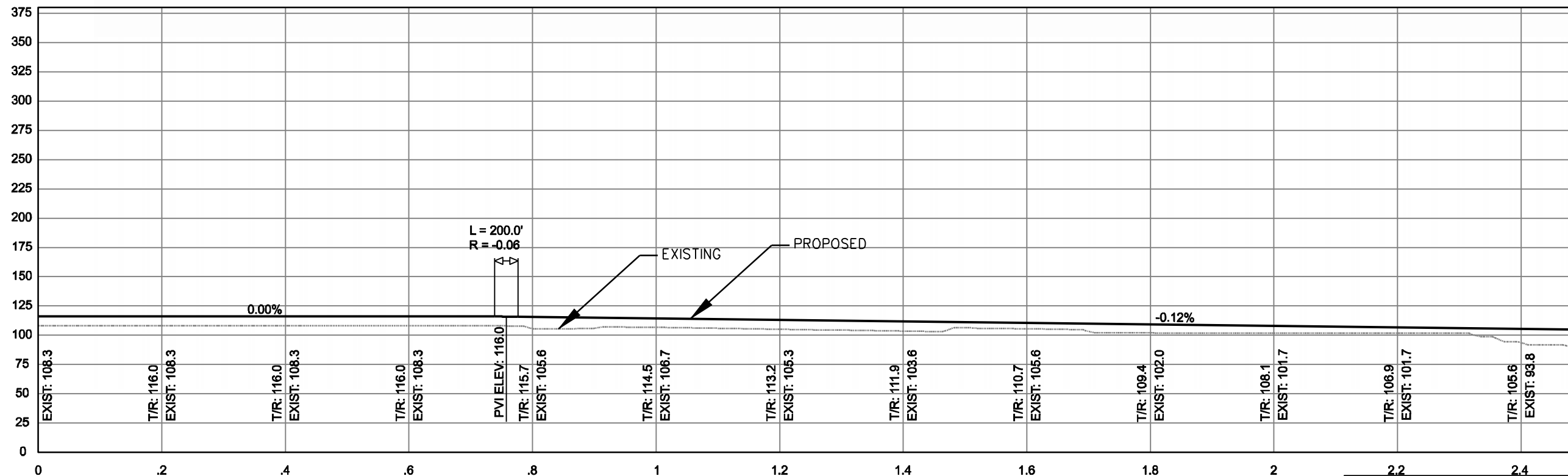
PROJECT: PLAN AND PROFILE MAC EAST, MP ME10.0-ME12.3		
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008
DRAWN BY: DEM	CHECKED BY: MAP	APPROVED BY:
SHEET NO: ME4 OF 48		13 OF 48



← TO PORT MACKENZIE

PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT

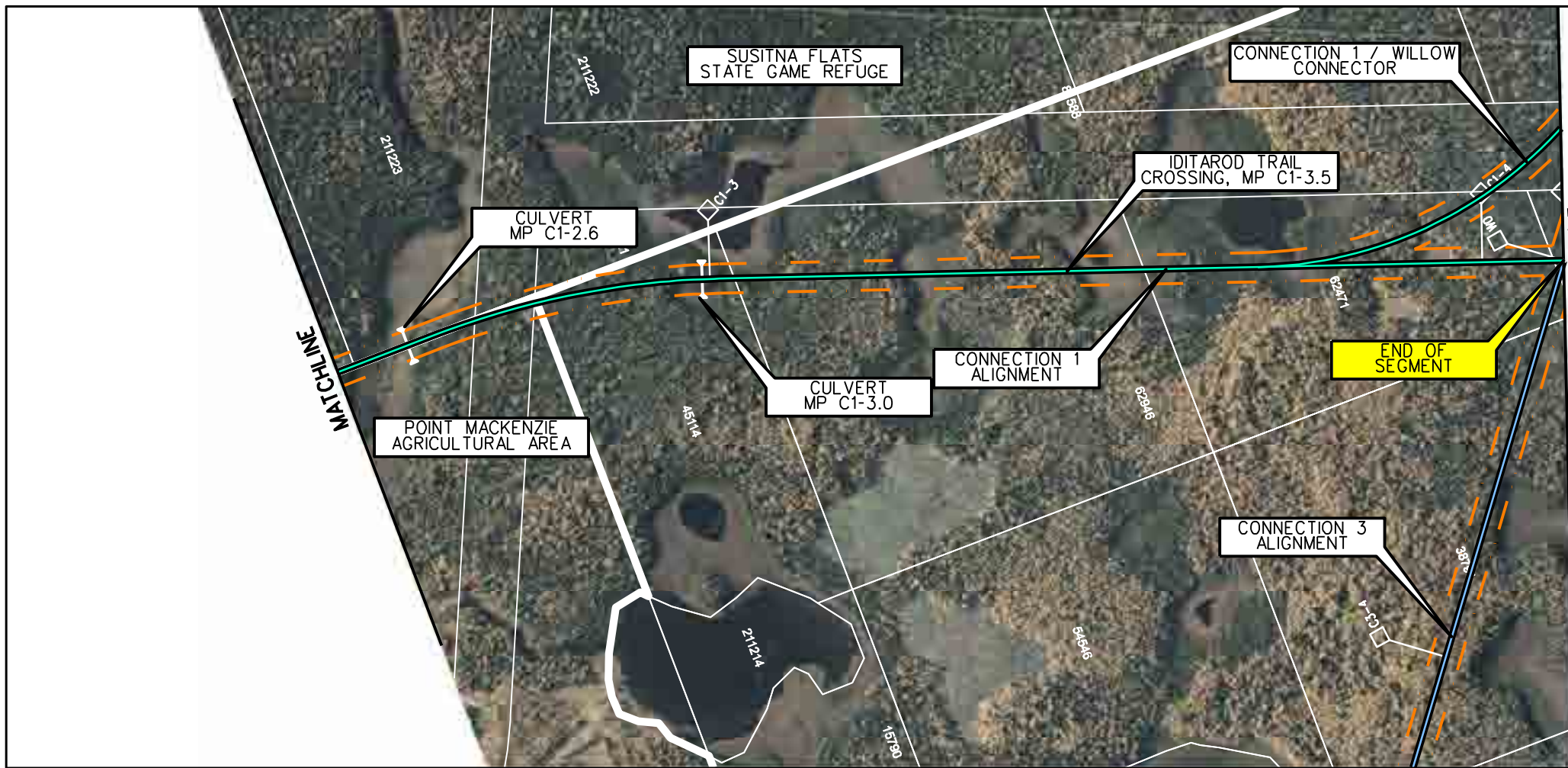


PLAN AND PROFILE CONNECTION 1, MP C1-0.0 - C1-2.5

CONCEPTUAL PLANS NOT FOR CONSTRUCTION

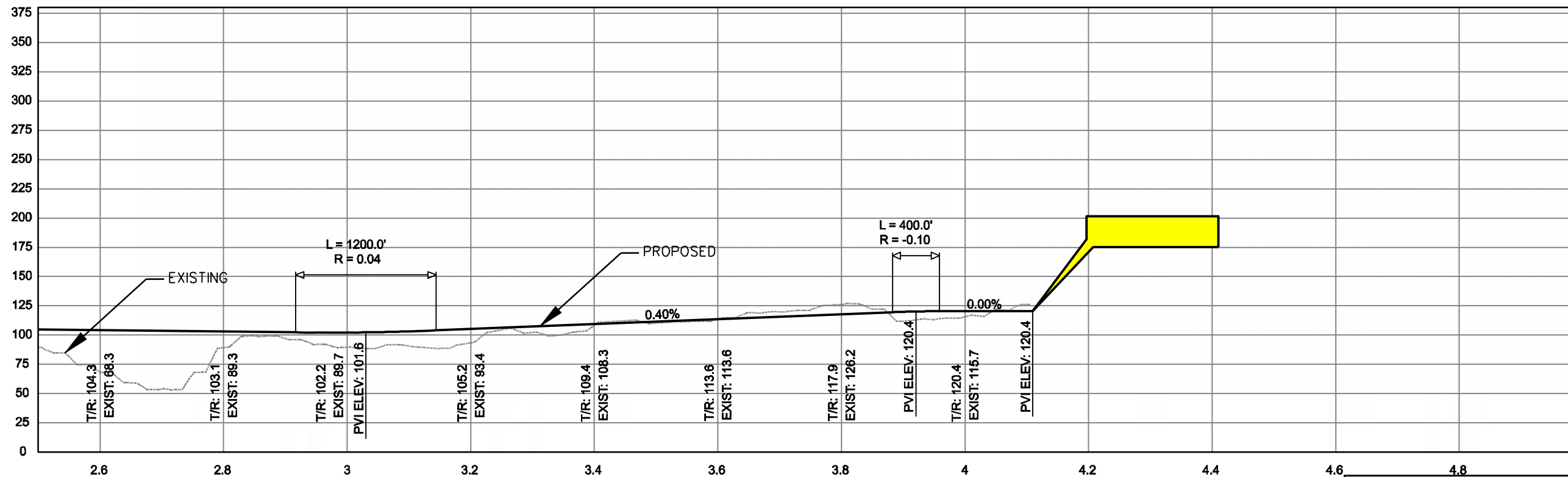
AUTHORED BY TNH-HANSON, LLC

DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	APP. NO.:
DRAWN BY: DEM			ACAD FILE:
CHECKED BY: MAP			DWG NO. C1-1
APPROVED BY:			14 OF 48

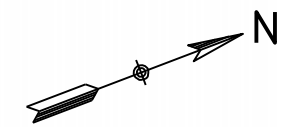


← TO PORT MACKENZIE

TO EXISTING MAINLINE →



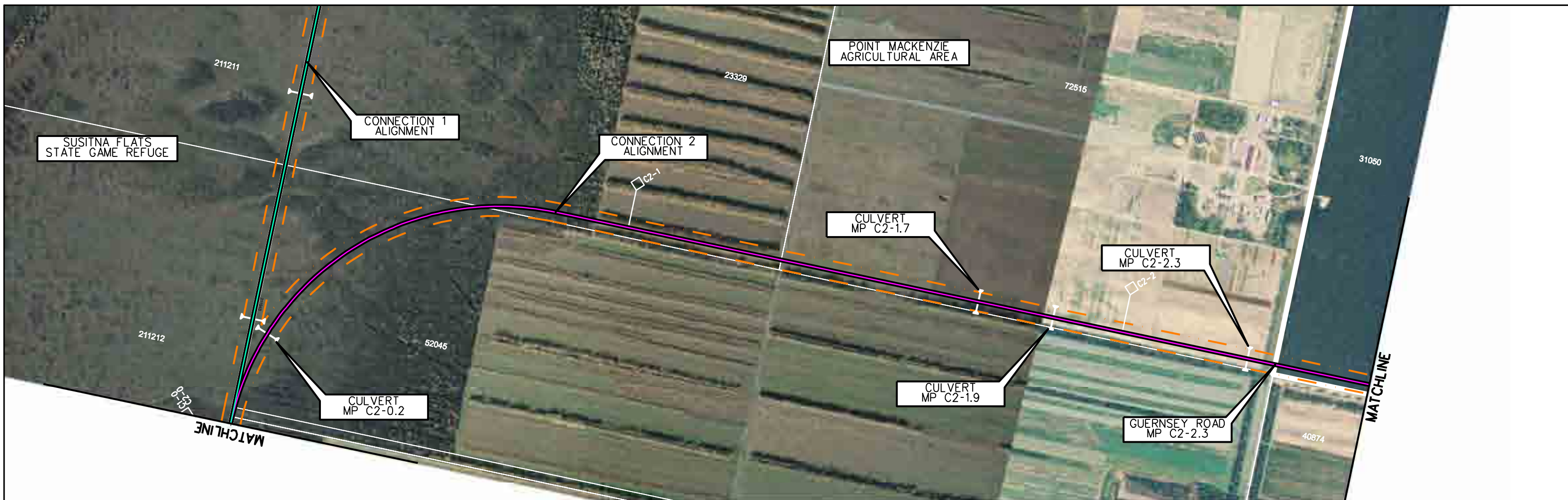
FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



**CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION**

AUTHORED BY TNH-HANSON, LLC

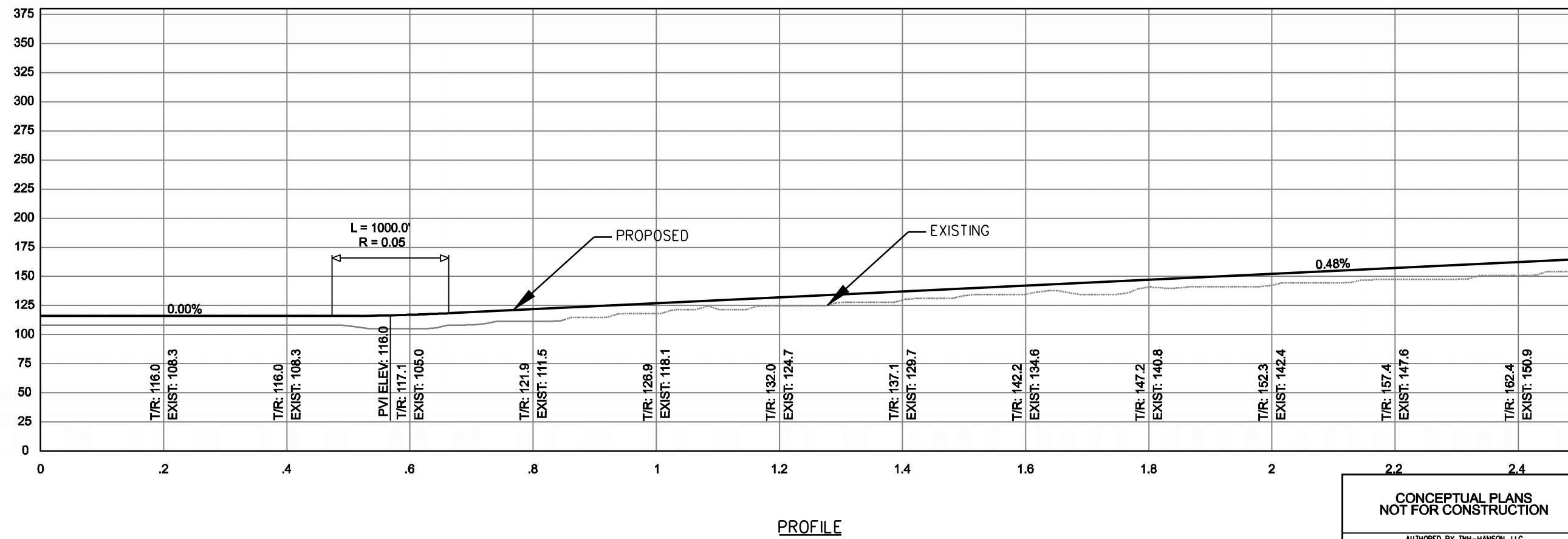
<b>PLAN AND PROFILE CONNECTION 1, MP C1-2.5 - C1-4.1</b>		
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008
DRAWN BY: DEM	CHECKED BY: MAP	APPROVED BY:
DWG NO. <b>C1-2</b>		15 OF 48



← TO PORT MACKENZIE

PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G, PRELIMINARY HYDROLOGY REPORT



PLAN AND PROFILE  
CONNECTION 2, MP C2-0.0 - C2-2.5

CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

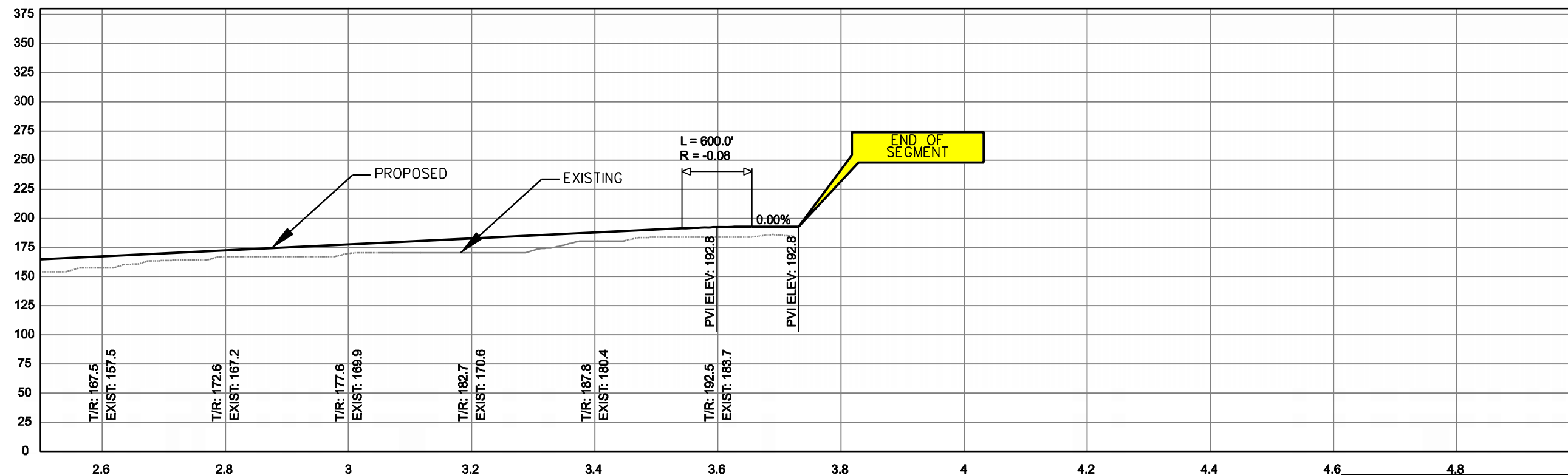
DESIGNED BY: MAP	SCALE: AS SHOWN	AFE NO:
DRAWN BY: DEM	DATE: JANUARY 2008	ACAD FILE:
CHECKED BY: MAP		DWG NO: C2-1
APPROVED BY:		16 OF 48



← TO PORT MACKENZIE

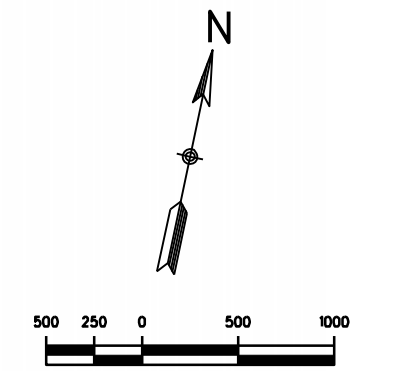
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

PLAN AND PROFILE  
CONNECTION 2, MP C2-2.5 - C2-3.7

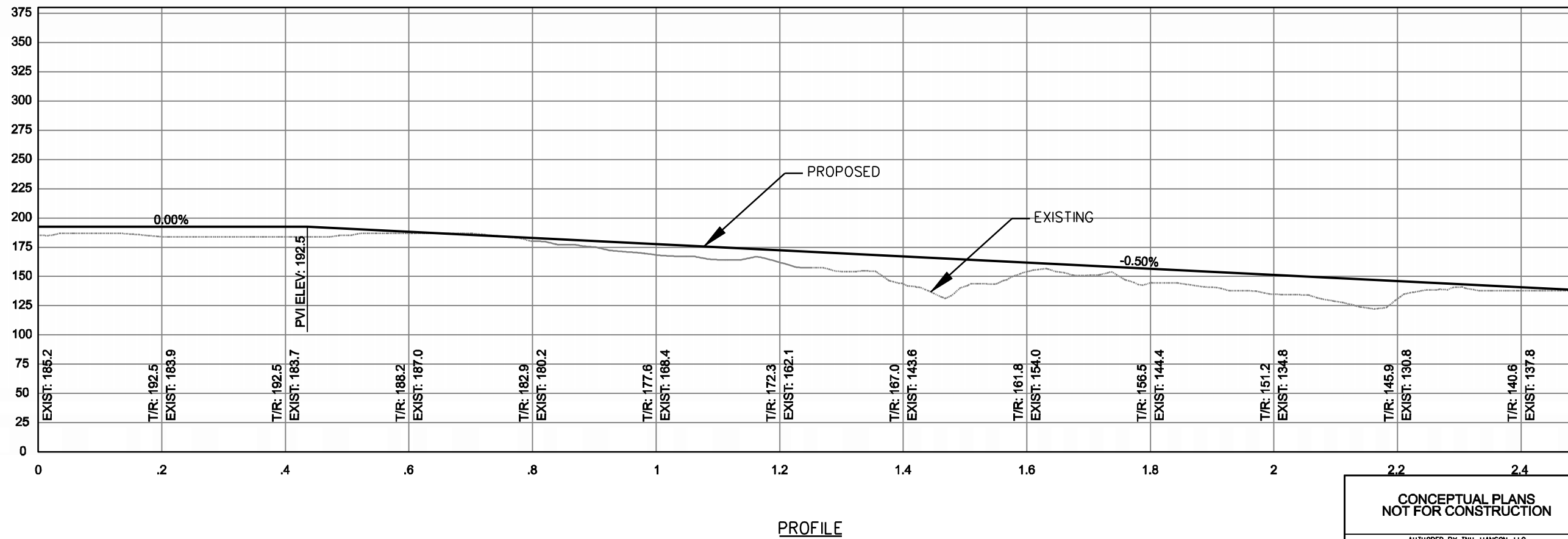
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	APP NO: C2-2
DRAWN BY: DEM			17 OF 48
CHECKED BY: MAP			
APPROVED BY:			



← TO PORT MACKENZIE

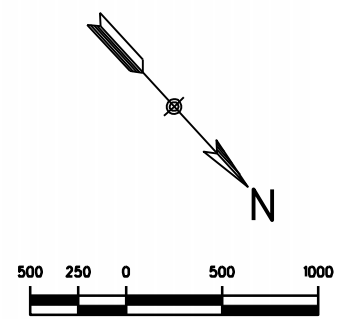
PLAN

TO EXISTING MAINLINE →



PROFILE

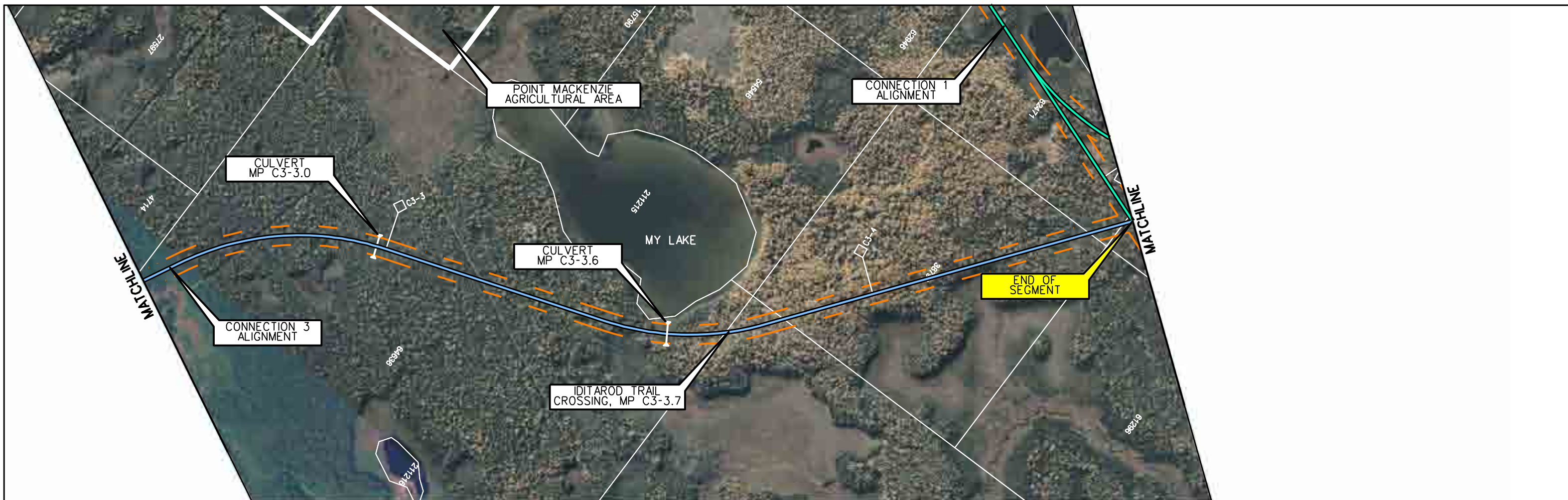
FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

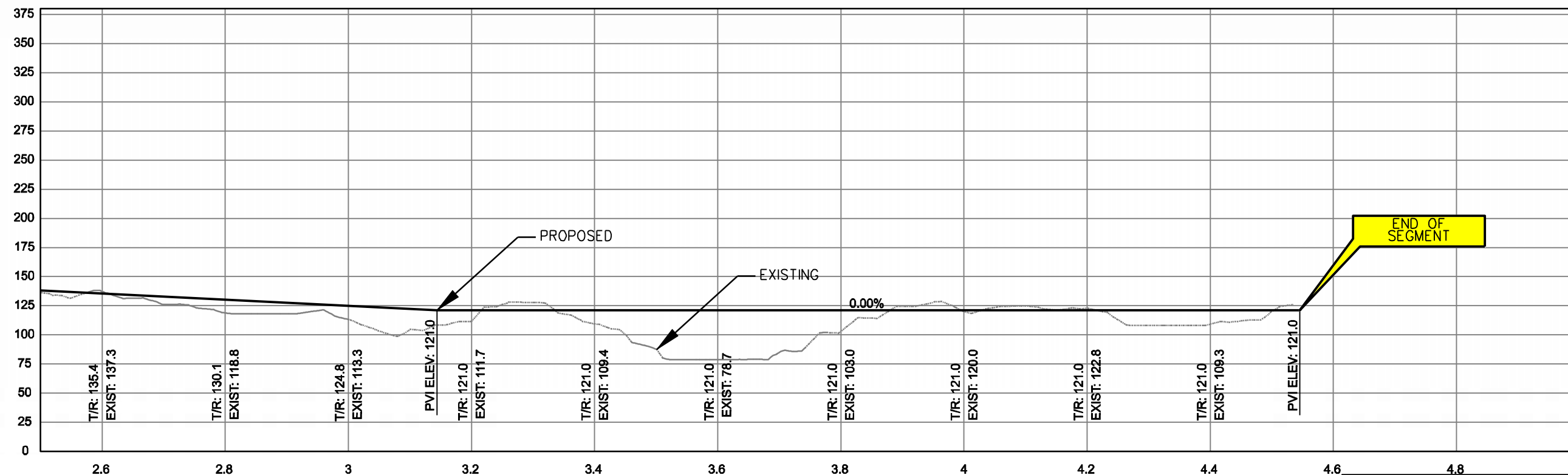
TITLE: <b>PLAN AND PROFILE          CONNECTION 3, MP C3-0.0 - C3-2.5</b>		
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008
DRAWN BY: DEM		
CHECKED BY: MAP		
APPROVED BY:		
DWG NO. <b>C3-1</b> 18 OF 48		



← TO PORT MACKENZIE

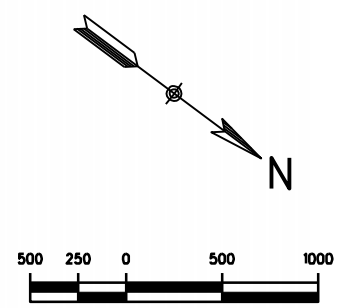
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT

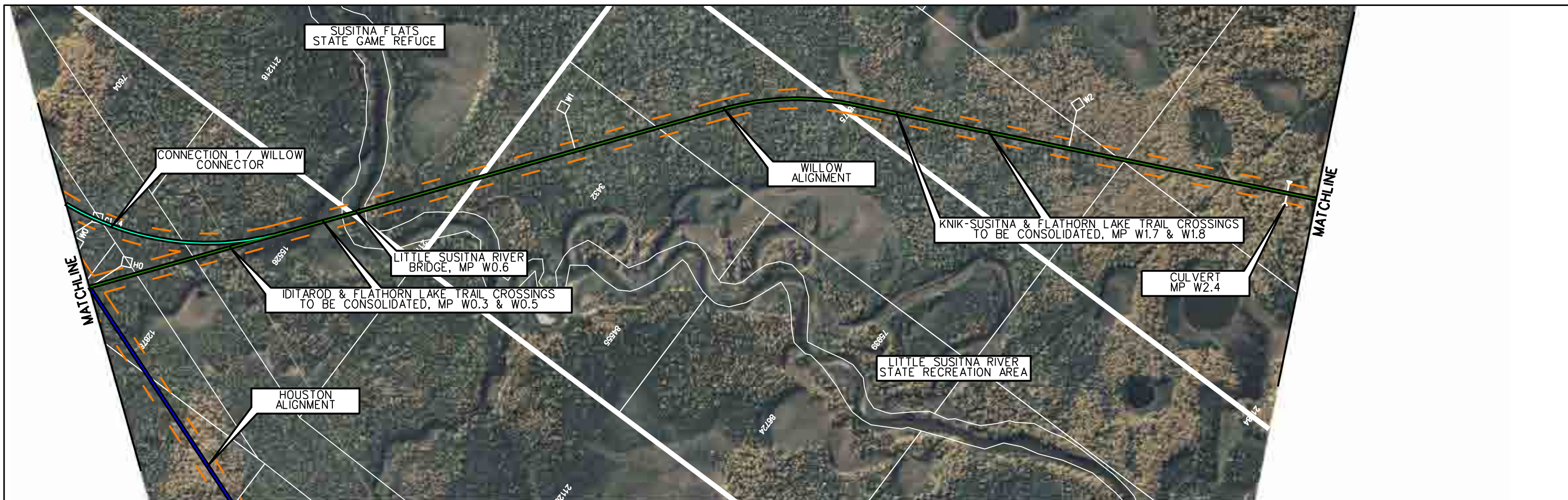


PLAN AND PROFILE  
CONNECTION 3, MP C3-2.5 - C3-4.5

CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

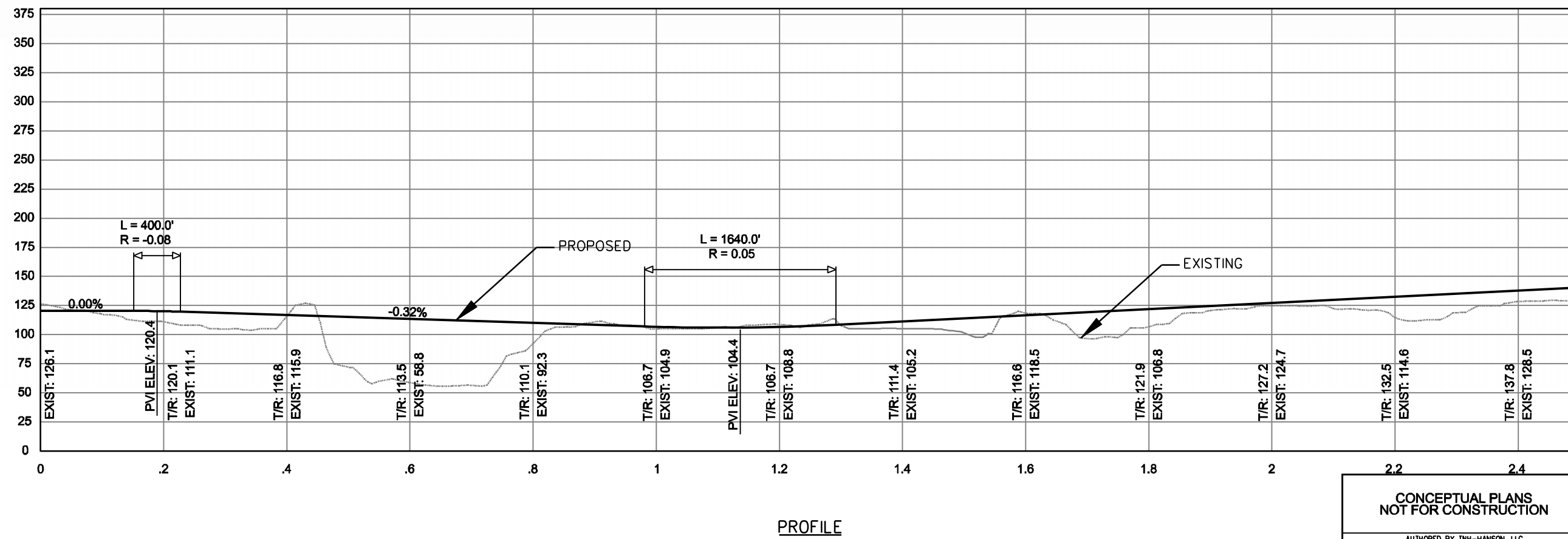
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	APP. NO.:
DRAWN BY: DEM			ACAD FILE:
CHECKED BY: MAP			DWG NO. C3-2
APPROVED BY:			19 OF 48



← TO PORT MACKENZIE

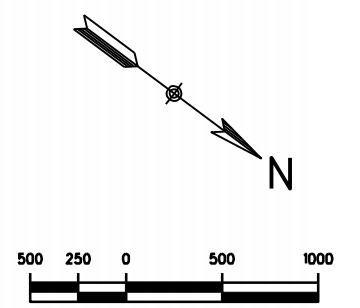
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



PLAN AND PROFILE WILLOW, MP W0.0-W2.5

CONCEPTUAL PLANS NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	W1
DRAWN BY: DEM			20 OF 48
CHECKED BY: MAP			
APPROVED BY:			

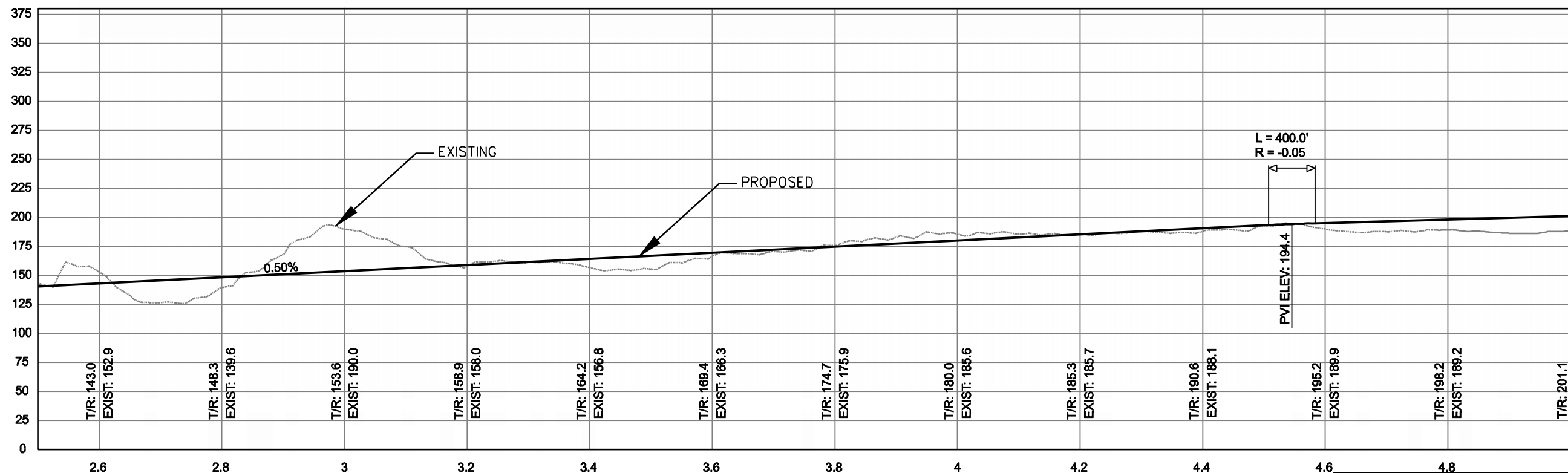




← TO PORT MACKENZIE

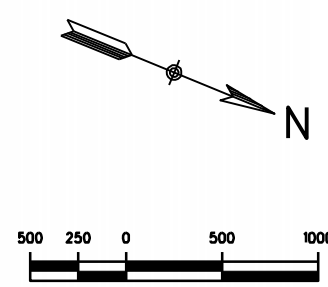
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

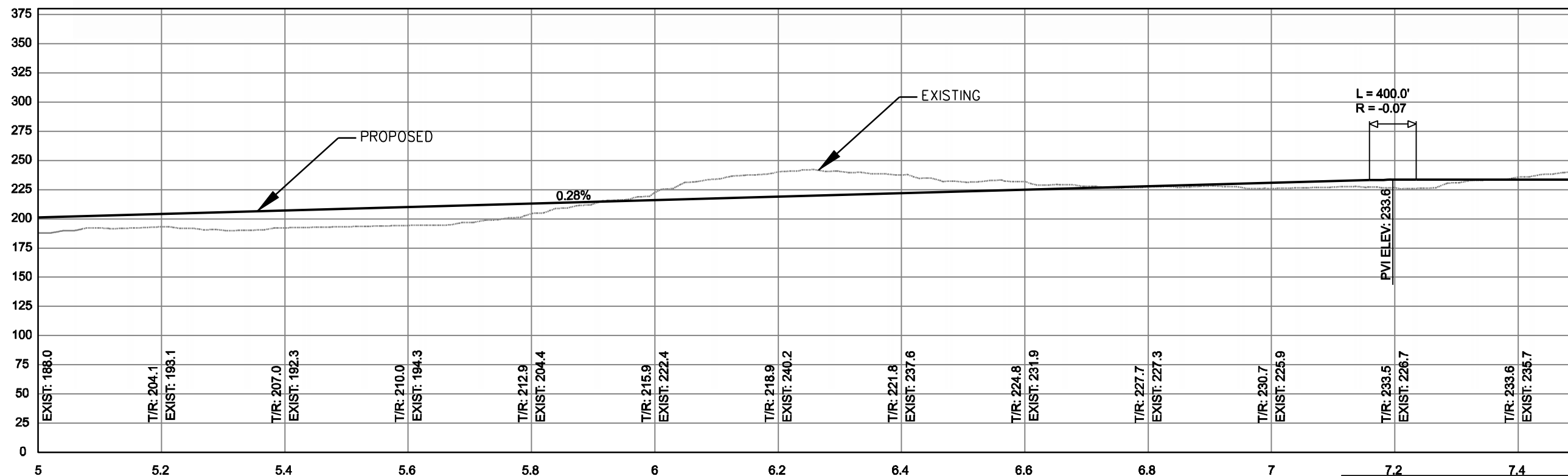
DESIGNED BY: MAP DRAWN BY: DEM CHECKED BY: MAP APPROVED BY:			SCALE: AS SHOWN DATE: JANUARY 2008	SHEET NO.: W2 OF 48
--	--	--	---------------------------------------	------------------------



← TO PORT MACKENZIE

PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G, PRELIMINARY HYDROLOGY REPORT



CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

PLAN AND PROFILE  
WILLOW, MP W5.0-W7.5

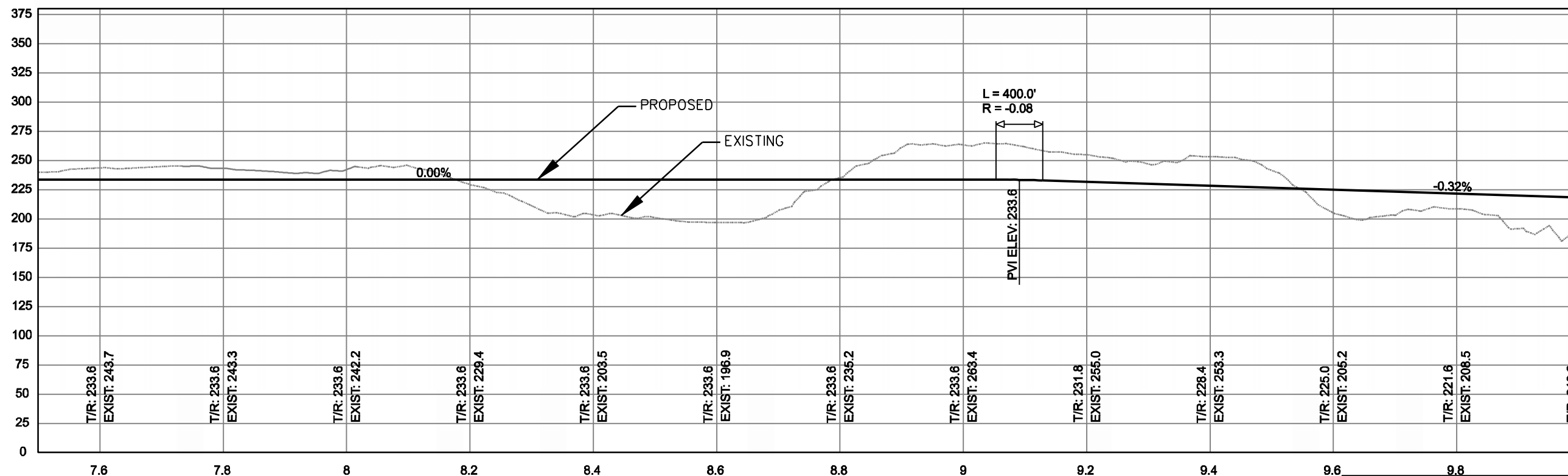
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	W3
DRAWN BY: DEM			22 OF 48
CHECKED BY: MAP			
APPROVED BY:			



← TO PORT MACKENZIE

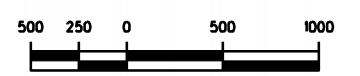
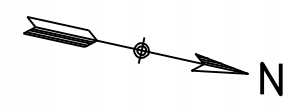
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



PLAN AND PROFILE  
WILLOW, MP W7.5-W10.0

CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	W4
DRAWN BY: DEM			23 OF 48
CHECKED BY: MAP			
APPROVED BY:			



MATCHLINE

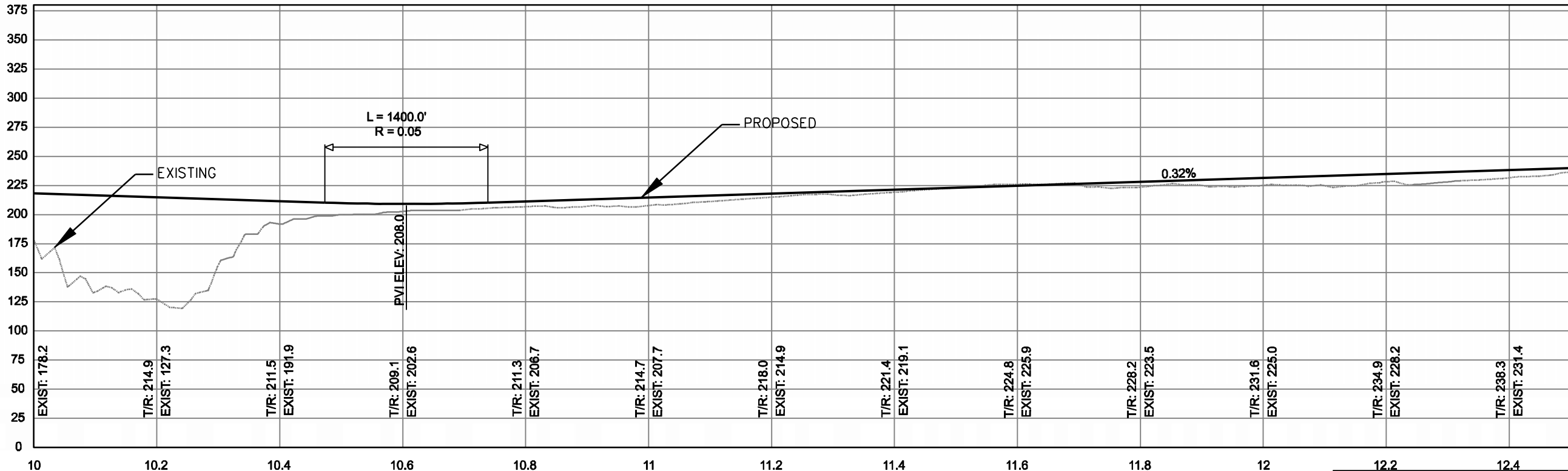
WILLOW ALIGNMENT

FISH CREEK DRAINAGE STRUCTURE, MP W10.0

← TO PORT MACKENZIE

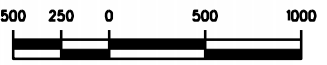
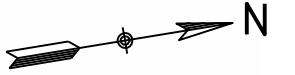
TO EXISTING MAINLINE →

PLAN



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



CONCEPTUAL PLANS NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

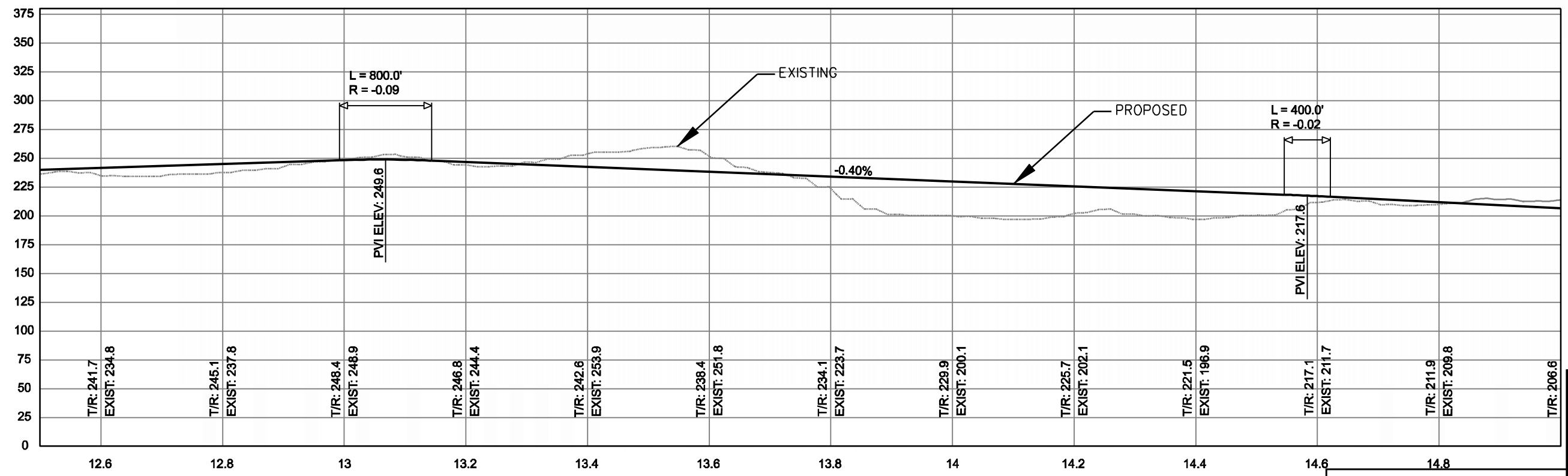
<p>PROJECT: <b>PLAN AND PROFILE WILLOW, MP W10.0-W12.5</b></p>		
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008
DRAWN BY: DEM	CHECKED BY: MAP	APPROVED BY:
<p>W5 OF 48</p>		<p>24 OF 48</p>



← TO PORT MACKENZIE

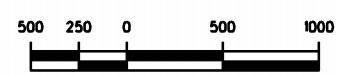
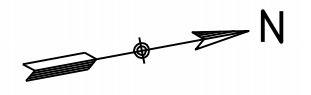
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

PLAN AND PROFILE  
WILLOW, MP W12.5-W15.0

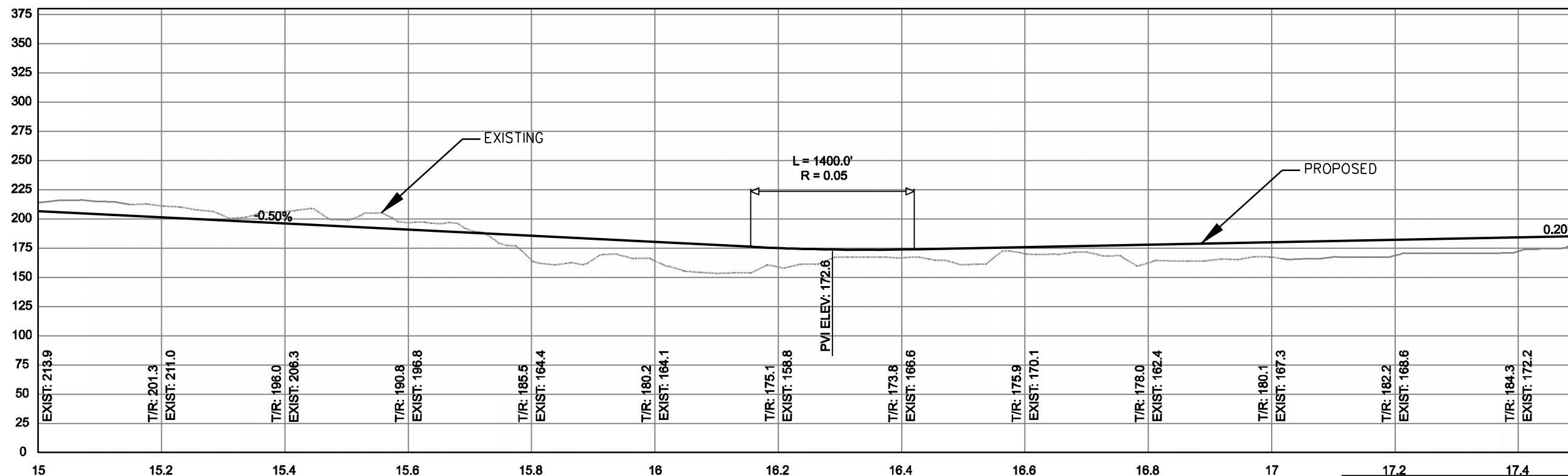
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	W6
DRAWN BY: DEM			25 OF 48
CHECKED BY: MAP			
APPROVED BY:			



← TO PORT MACKENZIE

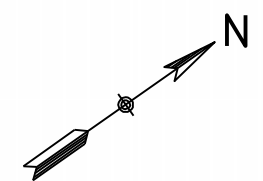
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

PLAN AND PROFILE  
WILLOW, MP W15.0-W17.5

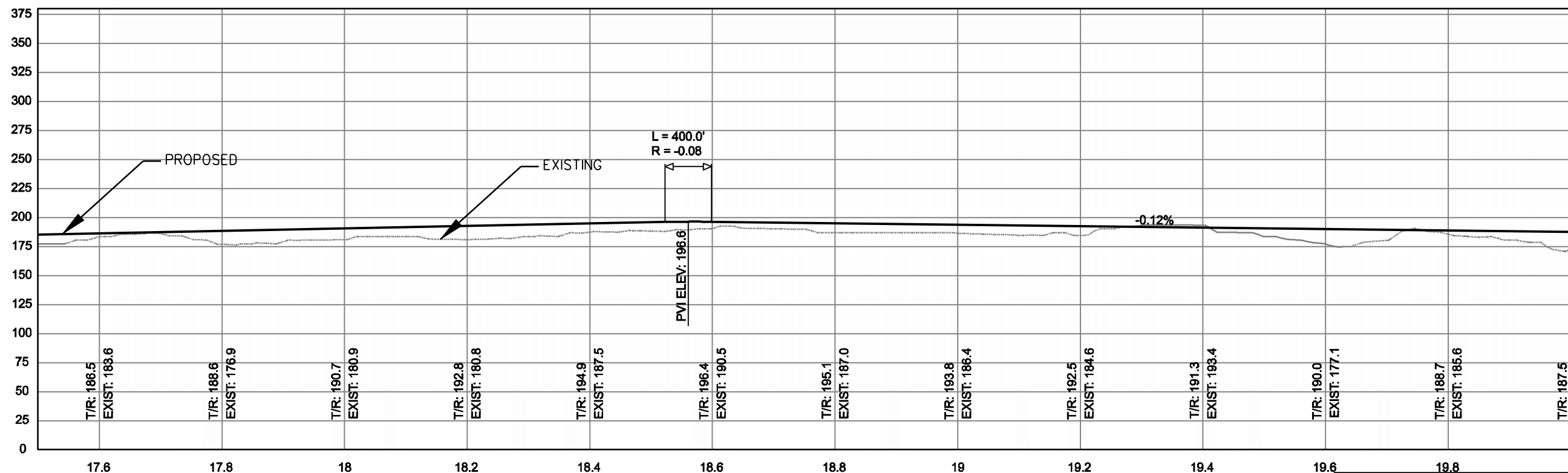
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	W7
DRAWN BY: DEM			26 OF 48
CHECKED BY: MAP			
APPROVED BY:			



← TO PORT MACKENZIE

PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



**CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION**

AUTHORED BY TNH-HANSON, LLC

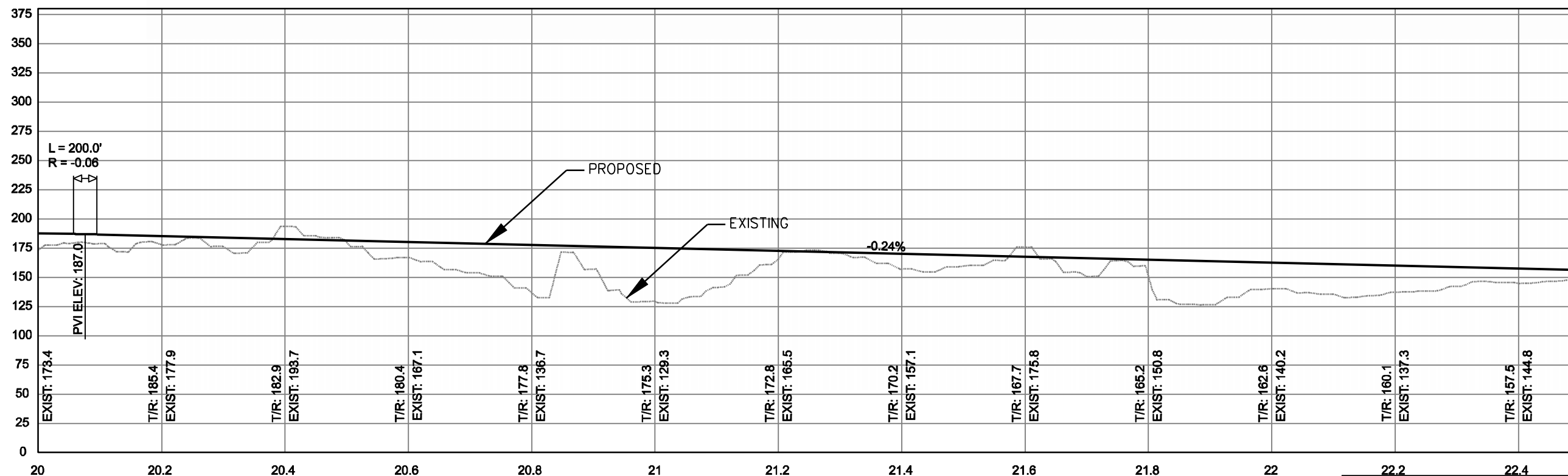
<p>PLAN AND PROFILE WILLOW, MP W17.5-W20.0</p>			
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	W8
DRAWN BY: DEM			27 OF 48
CHECKED BY: MAP			
APPROVED BY:			



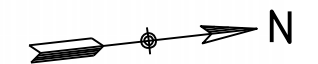
← TO PORT MACKENZIE

PLAN

TO EXISTING MAINLINE →



FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



PLAN AND PROFILE  
WILLOW, MP W20.0-W22.5

CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	W9
DRAWN BY: DEM			28 OF 48
CHECKED BY: MAP			
APPROVED BY:			

PROFILE

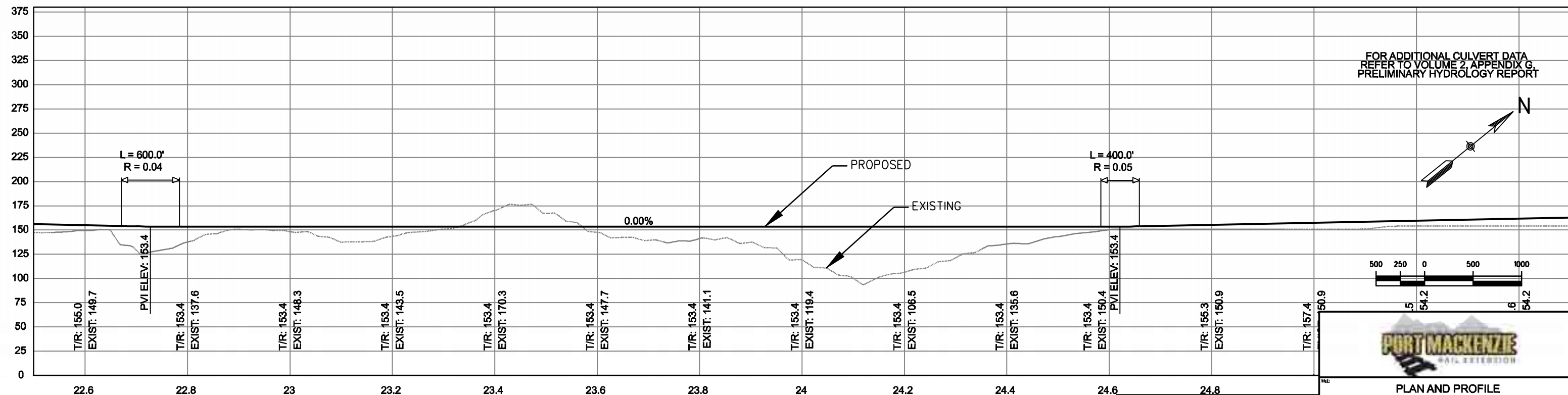




← TO PORT MACKENZIE

PLAN

TO EXISTING MAINLINE →



FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT

PROFILE

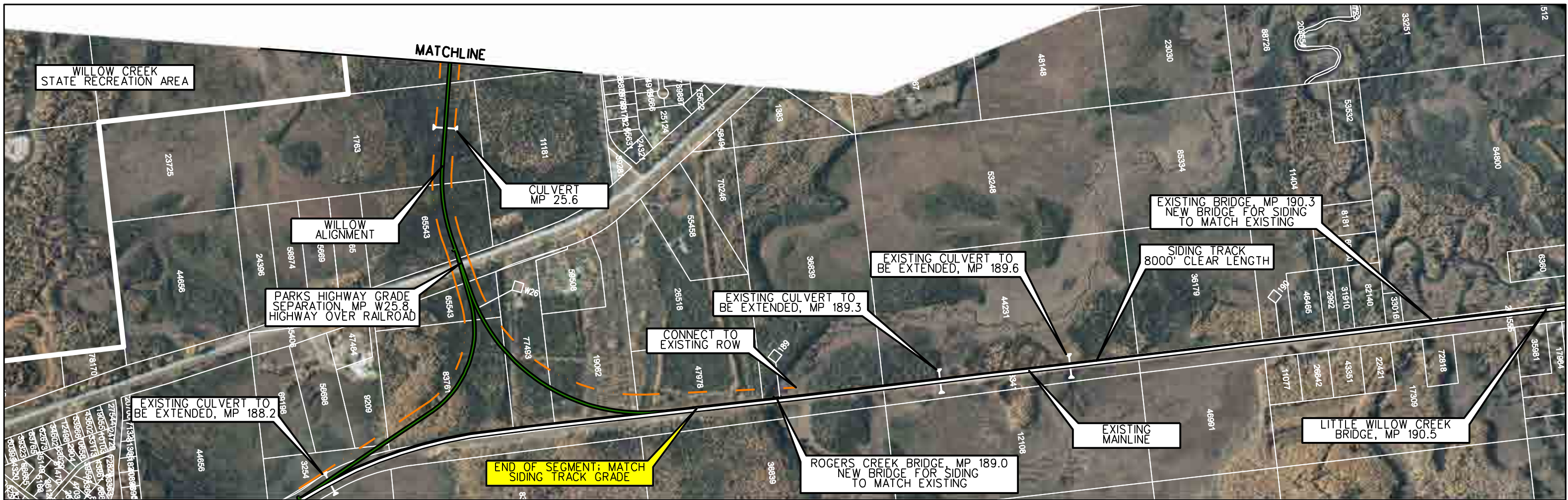
CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC



PLAN AND PROFILE  
WILLOW, MP W22.5-W25.5

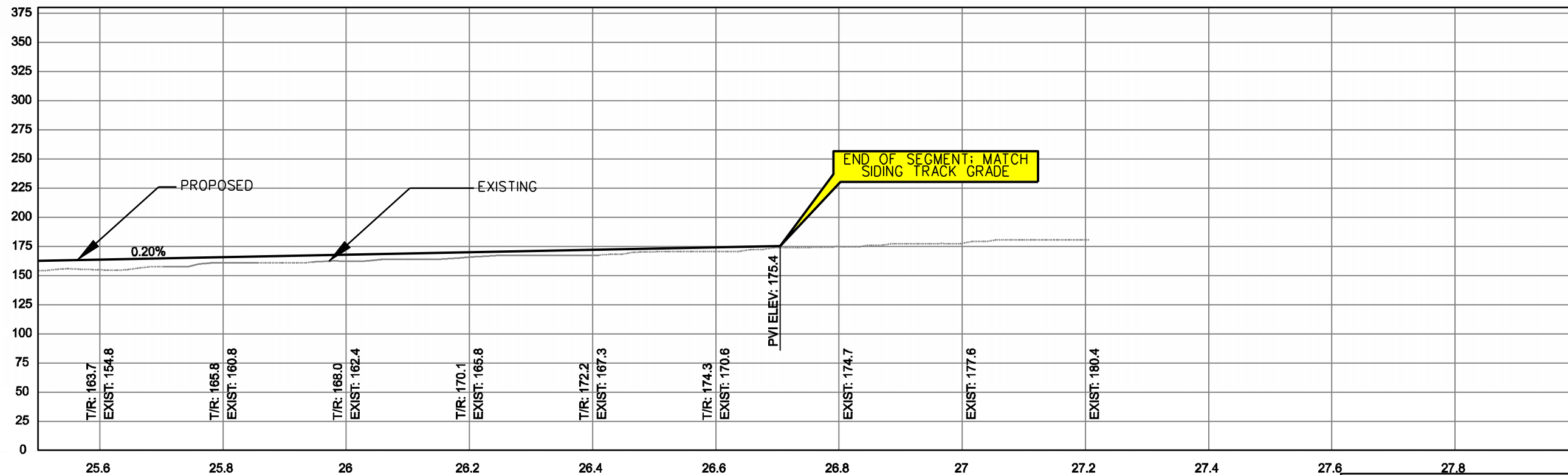
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	W10
DRAWN BY: DEM			29 OF 48
CHECKED BY: MAP			
APPROVED BY:			



← TO ANCHORAGE

PLAN

TO FAIRBANKS →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT

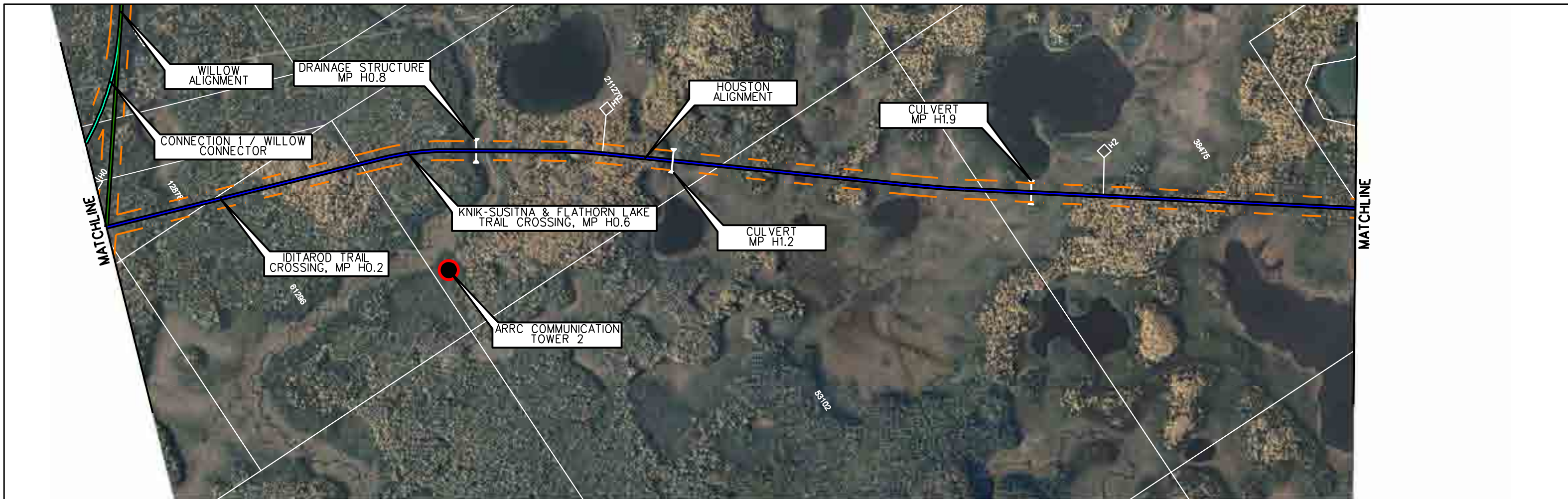


PLAN AND PROFILE  
WILLOW, MP W25.5-W28.2

CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

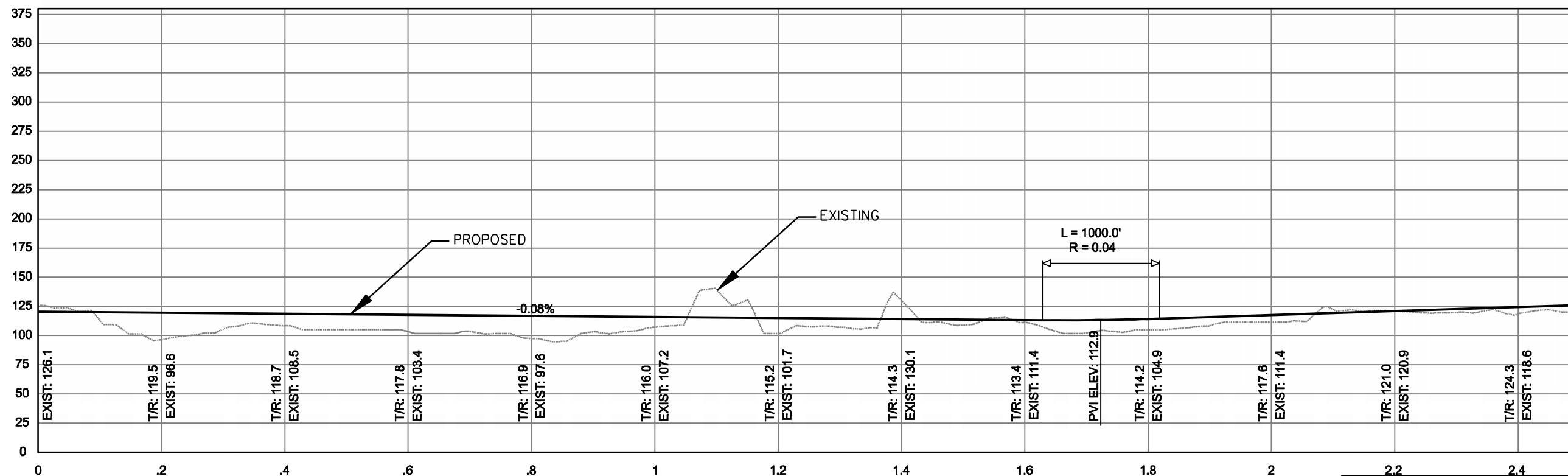
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	W11
DRAWN BY: DEM			30 OF 48
CHECKED BY: MAP			
APPROVED BY:			



← TO PORT MACKENZIE

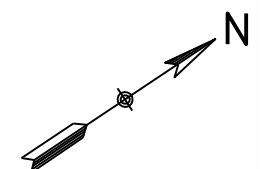
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA  
 REFER TO VOLUME 2, APPENDIX G  
 PRELIMINARY HYDROLOGY REPORT



PLAN AND PROFILE  
 HOUSTON, MP H0.0-H2.5

CONCEPTUAL PLANS  
 NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

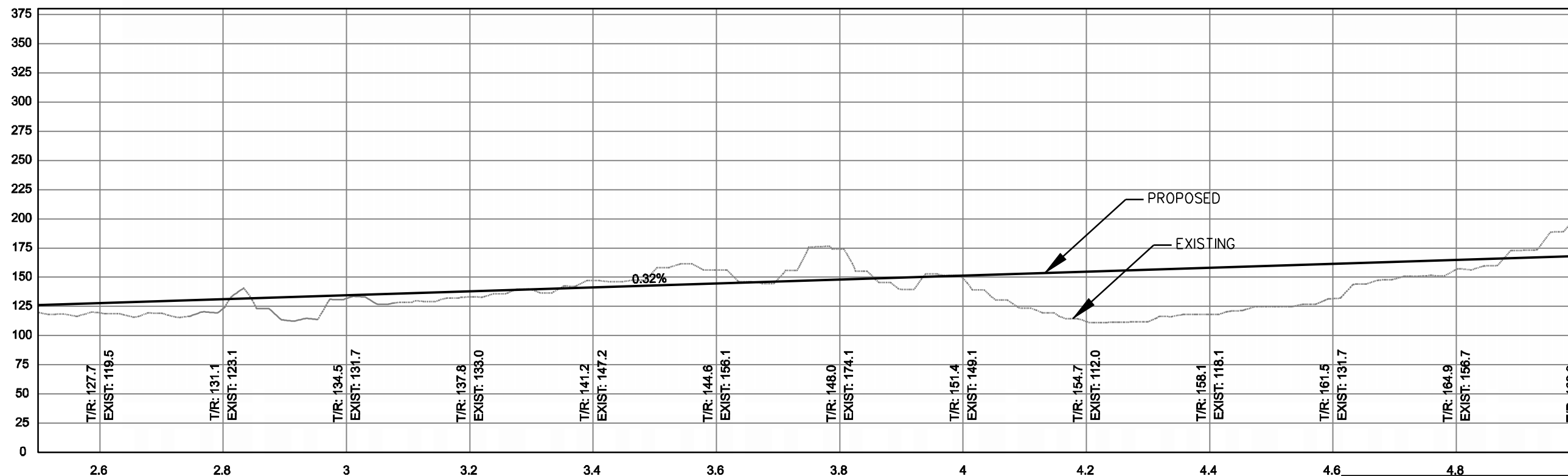
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	31 OF 48
DRAWN BY: DEM			
CHECKED BY: MAP			
APPROVED BY:			



← TO PORT MACKENZIE

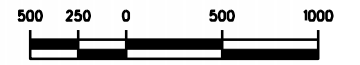
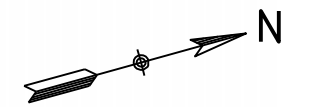
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G, PRELIMINARY HYDROLOGY REPORT



CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

PLAN AND PROFILE  
HOUSTON, MP H2.5-H5.0

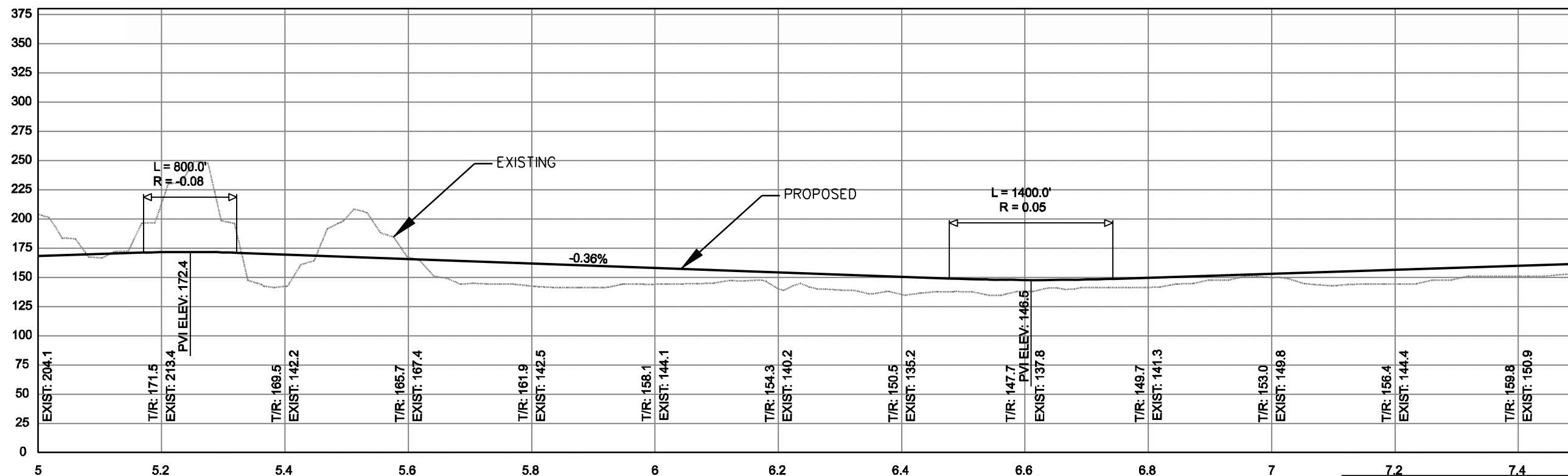
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	APP. NO.:
DRAWN BY: DEM			ACAD FILE:
CHECKED BY: MAP			DWG NO. H2
APPROVED BY:			32 OF 48



← TO PORT MACKENZIE

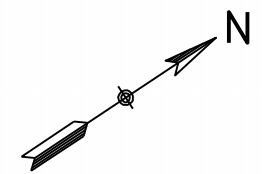
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

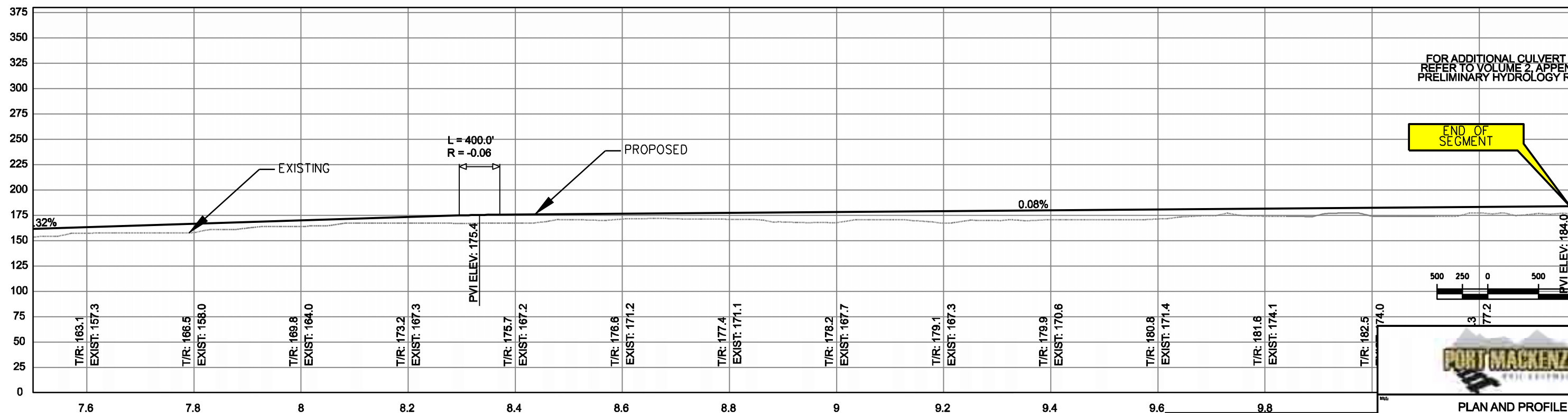
PLAN AND PROFILE HOUSTON, MP H5.0-H7.5			
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	33 OF 48
DRAWN BY: DEM			
CHECKED BY: MAP			
APPROVED BY:			



← TO PORT MACKENZIE

PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT

END OF SEGMENT

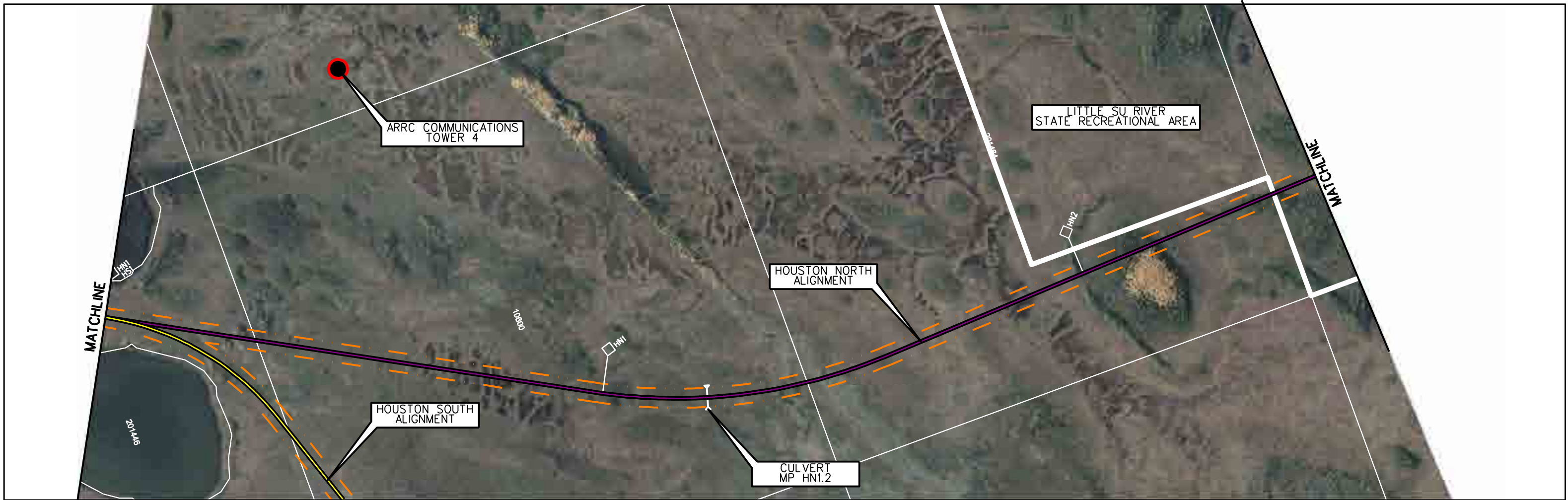


CONCEPTUAL PLANS NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

PLAN AND PROFILE HOUSTON, MP H7.5-H10.4

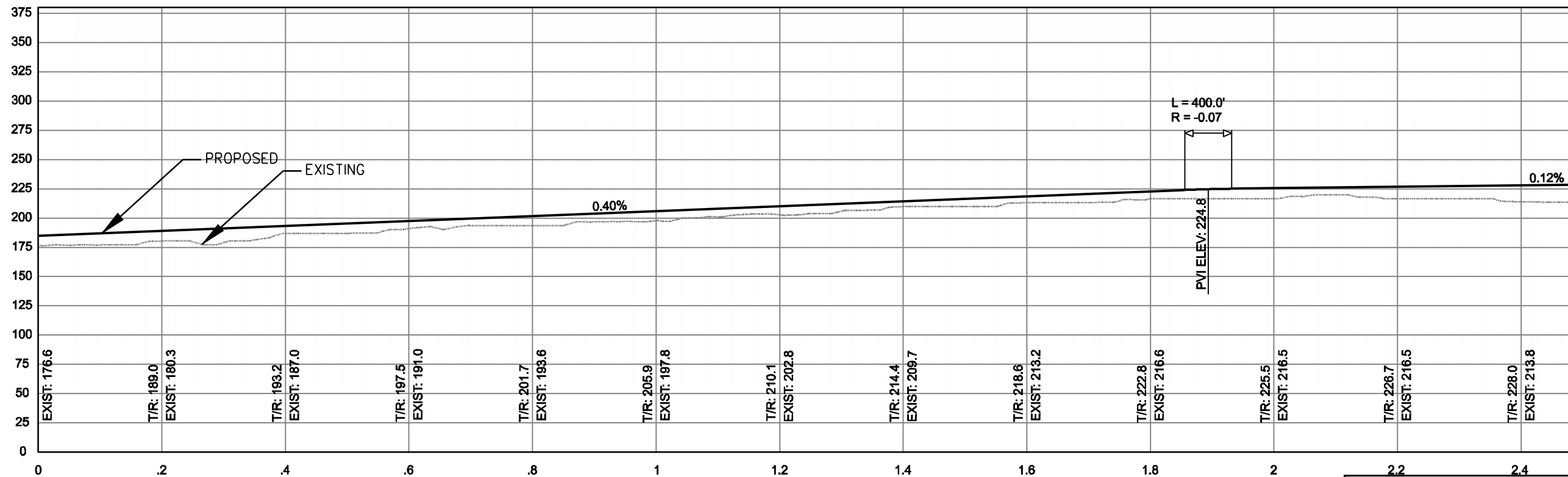
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	DATE: JANUARY 2008
DRAWN BY: DEM			
CHECKED BY: MAP			
APPROVED BY:			



← TO PORT MACKENZIE

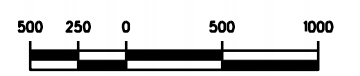
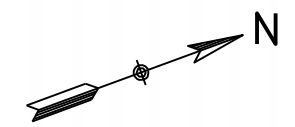
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



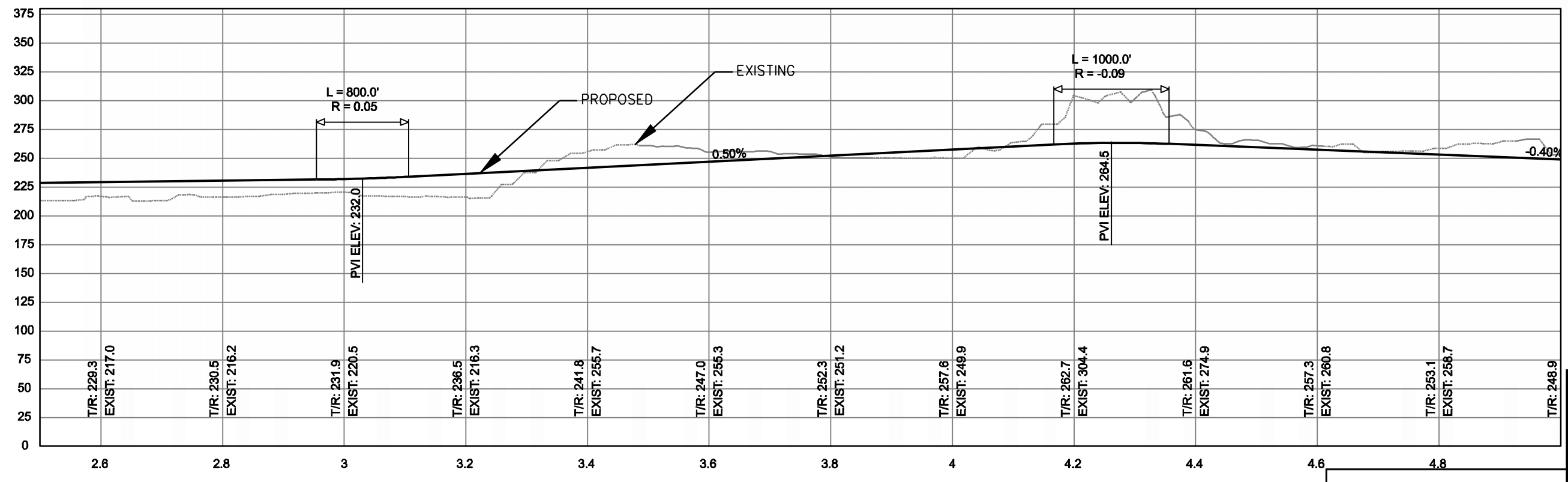
CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

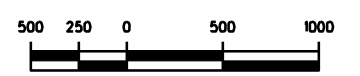
PROJECT		
PLAN AND PROFILE HOUSTON NORTH, MP HN0.0-HN2.5		
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008
DRAWN BY: DEM	CHECKED BY: MAP	APPROVED BY:
DWG NO. HN1		35 OF 48



← TO PORT MACKENZIE PLAN TO EXISTING MAINLINE →

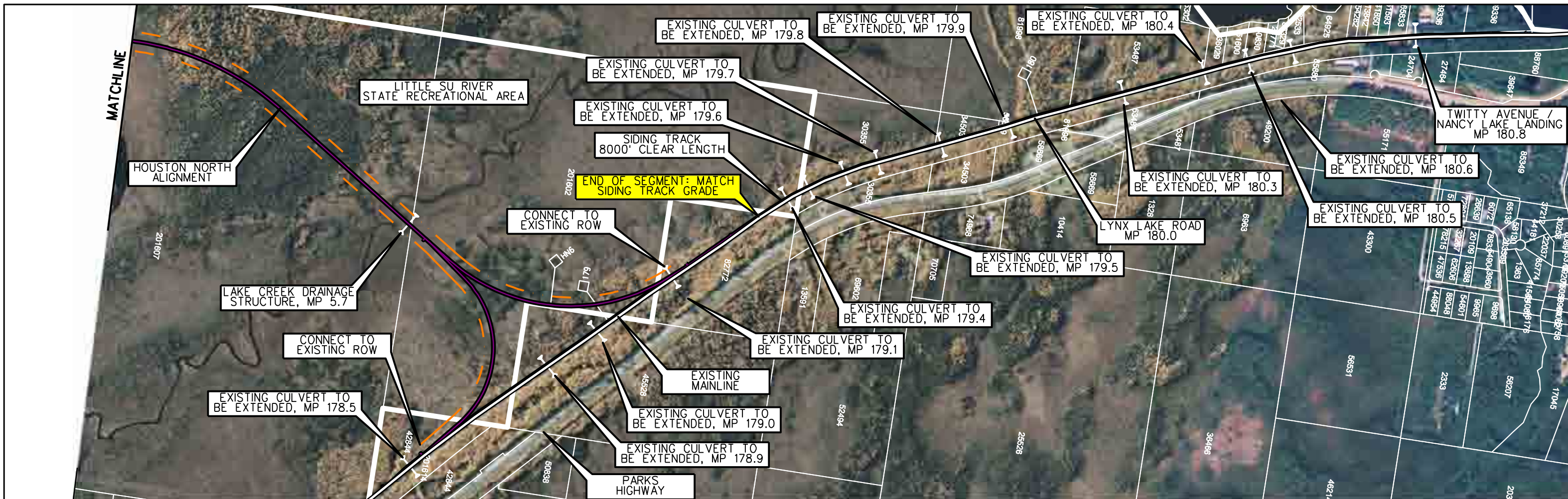


FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT

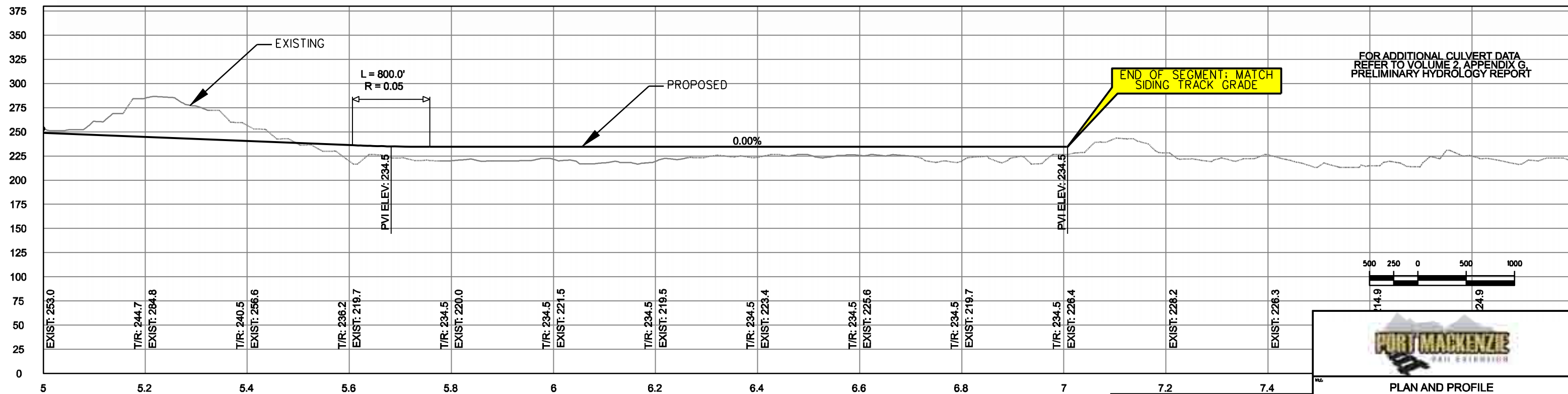


<b>PLAN AND PROFILE</b> <b>HOUSTON NORTH, MP HN2.5-HN5.0</b>		
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008
DRAWN BY: DEM	CHECKED BY: MAP	APPROVED BY:
CONCEPTUAL PLANS NOT FOR CONSTRUCTION		AUTHORED BY: TNH-HANSON, LLC
ACAD FILE: DWG NO. <b>HN2</b>		SHEET NO. <b>36</b> OF <b>48</b>





← TO ANCHORAGE PLAN TO FAIRBANKS →



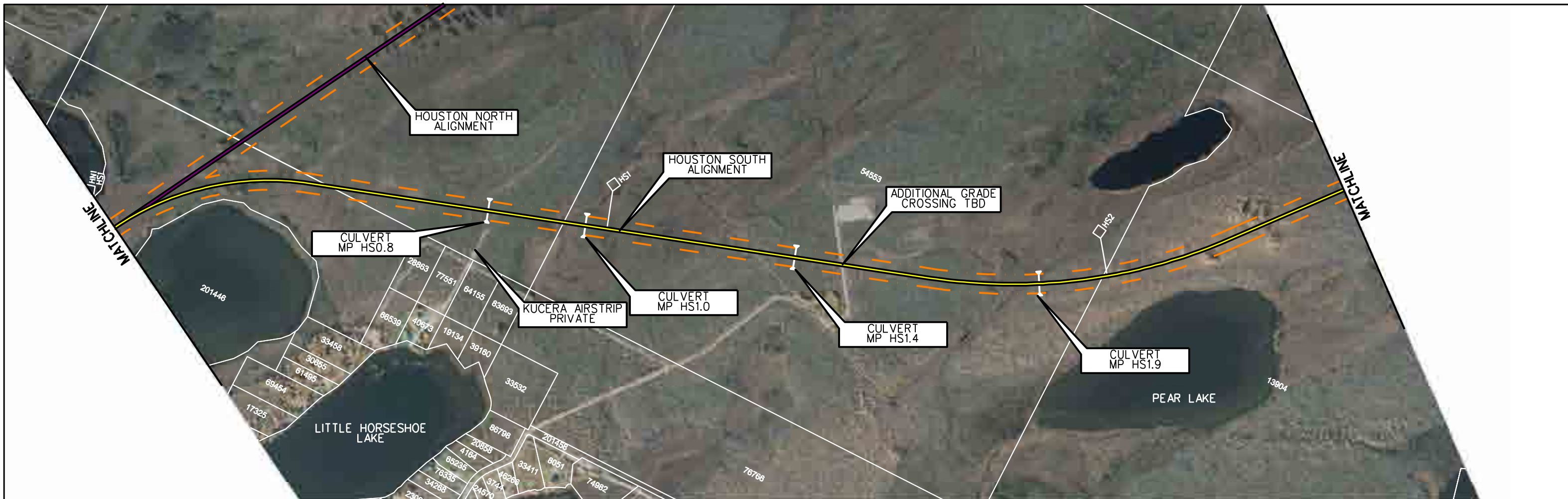
FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT

PROFILE

CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION  
AUTHORED BY TNH-HANSON, LLC

PLAN AND PROFILE  
HOUSTON NORTH, MP HN5.0-HN8.1

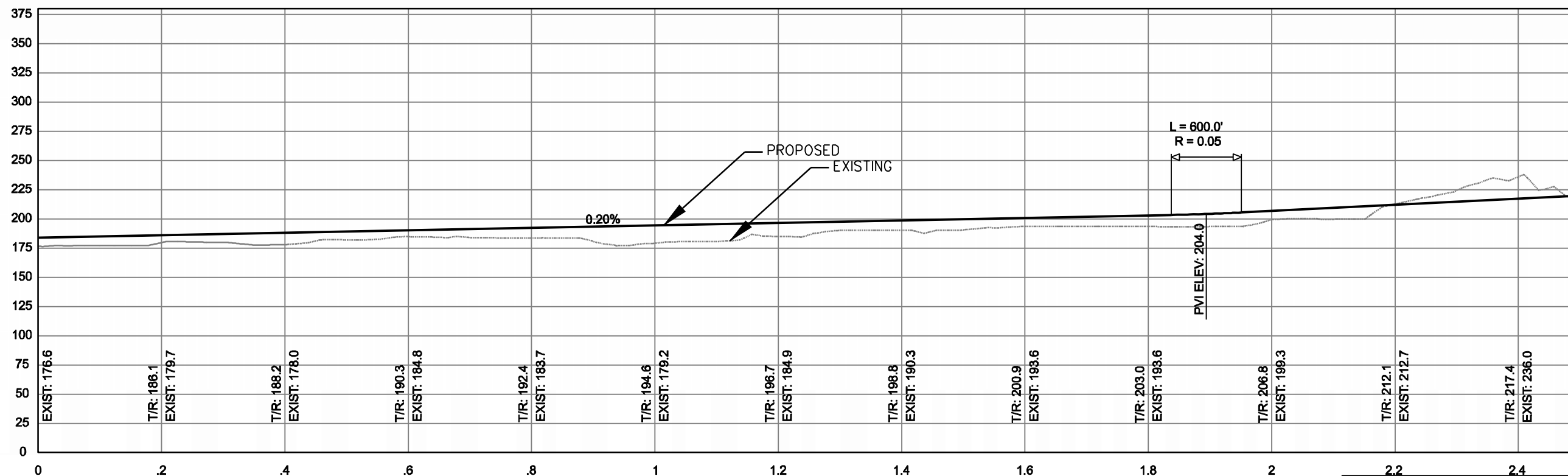
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	DATE: JANUARY 2008
DRAWN BY: DEM	CHECKED BY: MAP	APPROVED BY:	APPROVED BY:
DWG NO. HN3		37 OF 48	



← TO PORT MACKENZIE

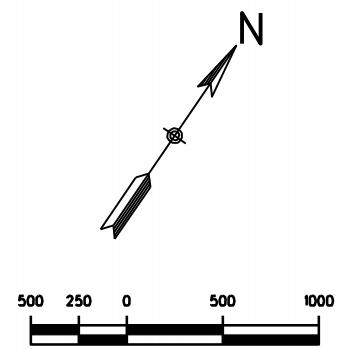
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



PLAN AND PROFILE HOUSTON SOUTH, MP HS0.0-HS2.5

CONCEPTUAL PLANS NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

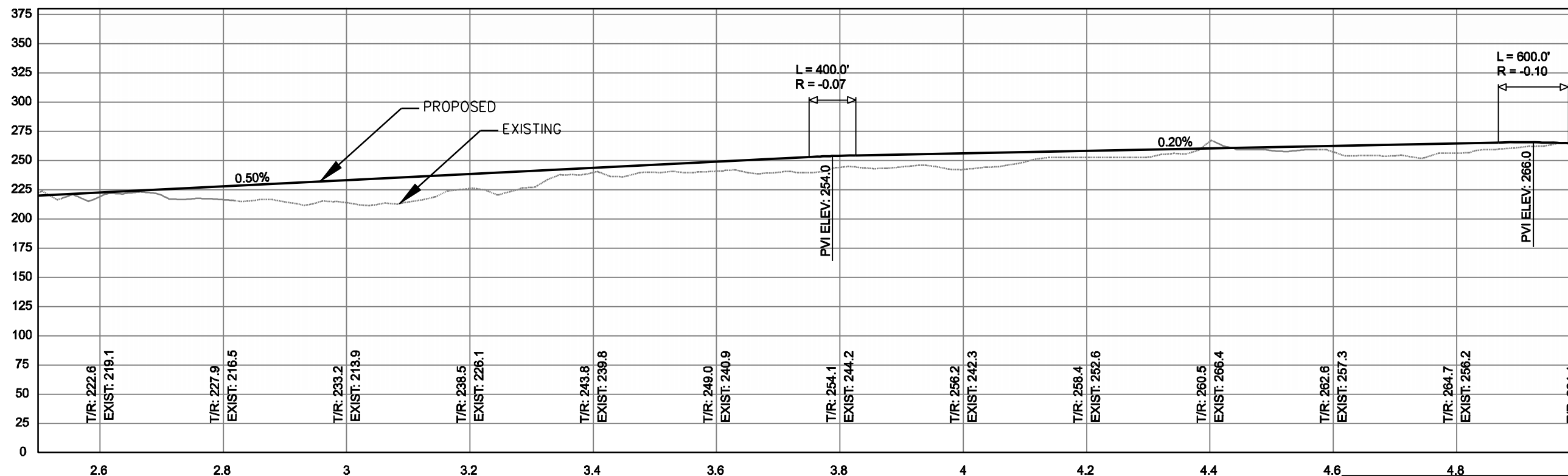
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	HS1
DRAWN BY: DEM			38 OF 48
CHECKED BY: MAP			
APPROVED BY:			



← TO PORT MACKENZIE

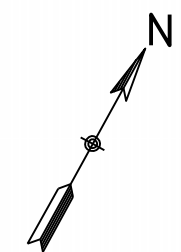
PLAN

TO EXISTING MAINLINE →



PROFILE

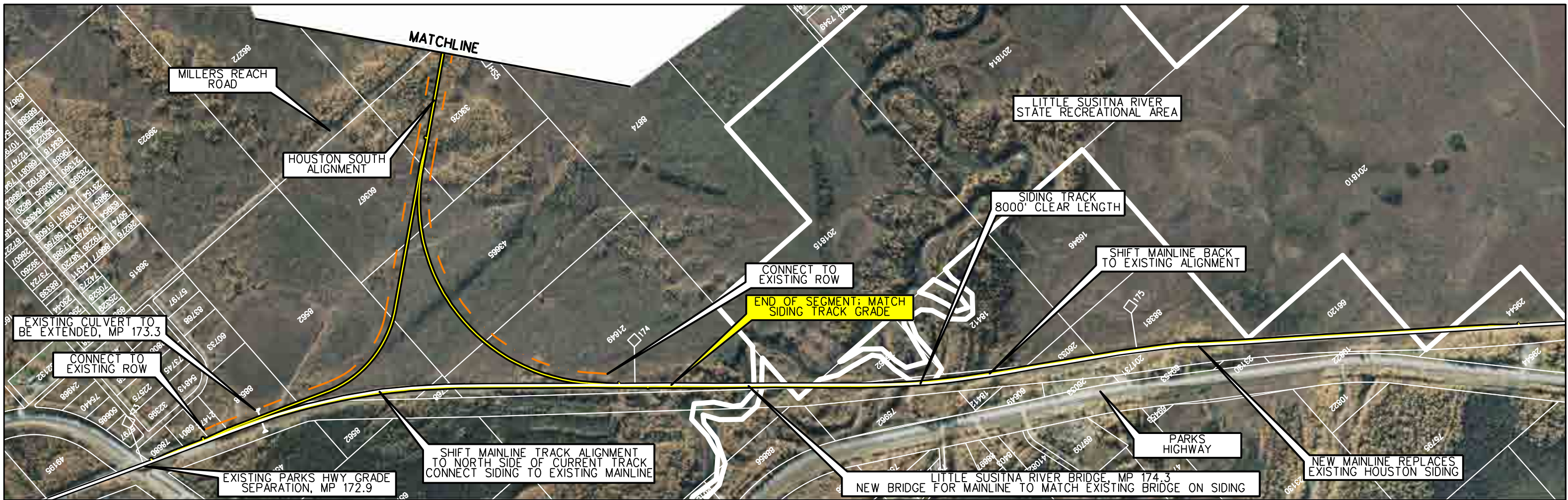
FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

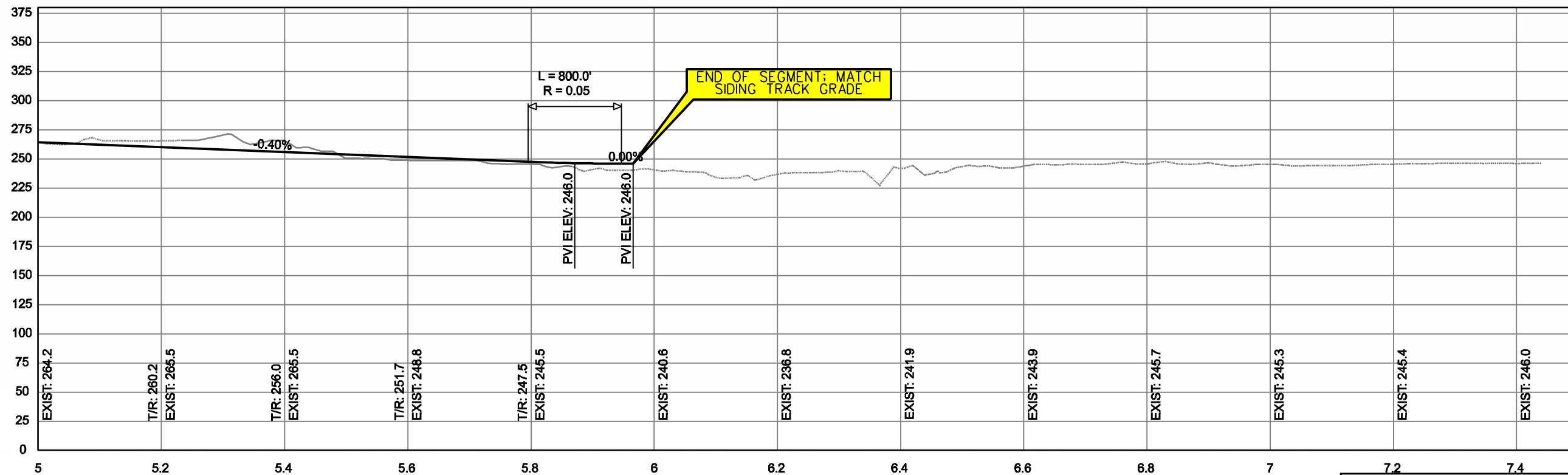
PLAN AND PROFILE HOUSTON SOUTH, MP HS2.5-HS5.0			
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	HS2
DRAWN BY: DEM			39 OF 48
CHECKED BY: MAP			
APPROVED BY:			



← TO ANCHORAGE

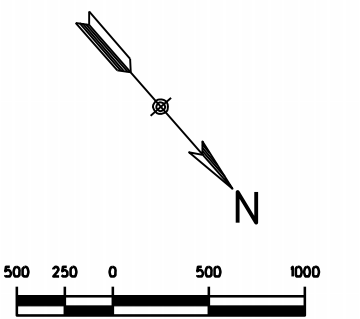
PLAN

TO FAIRBANKS →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



PLAN AND PROFILE  
HOUSTON SOUTH, MP HS5.0-HS7.4

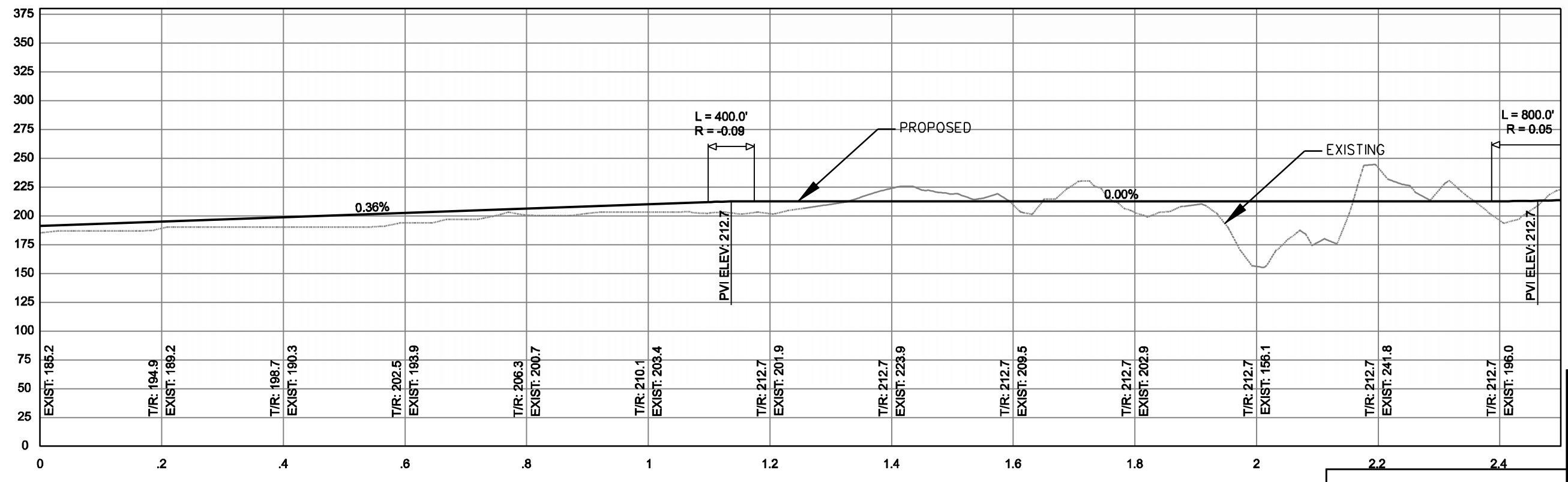
CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	HS3
DRAWN BY: DEM			40 OF 48
CHECKED BY: MAP			
APPROVED BY:			

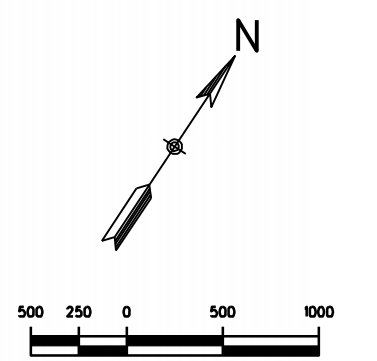


← TO PORT MACKENZIE PLAN TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



**PLAN AND PROFILE  
BIG LAKE, MP B0.0-B2.5**

DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	SHEET NO: <b>B1</b>
DRAWN BY: DEM			41 OF 48
CHECKED BY: MAP			
APPROVED BY:			

AUTHORED BY TNH-HANSON, LLC

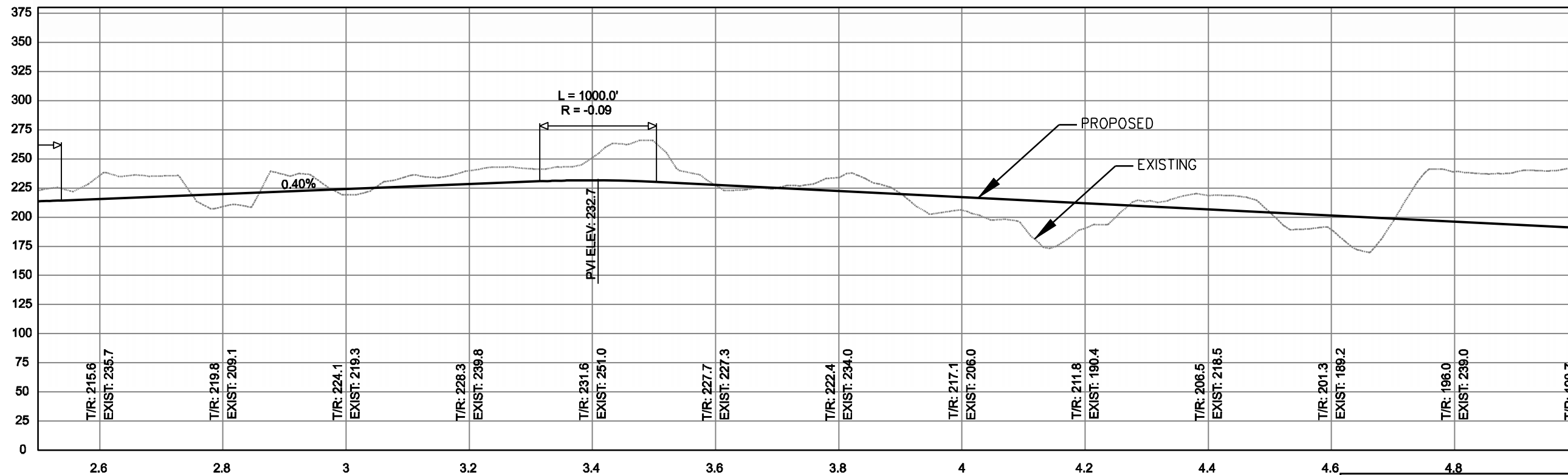
**CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION**



← TO PORT MACKENZIE

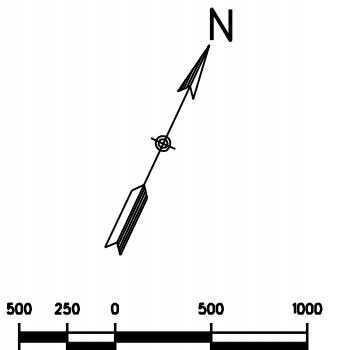
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



PLAN AND PROFILE  
BIG LAKE, MP B2.5-B5.0

CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

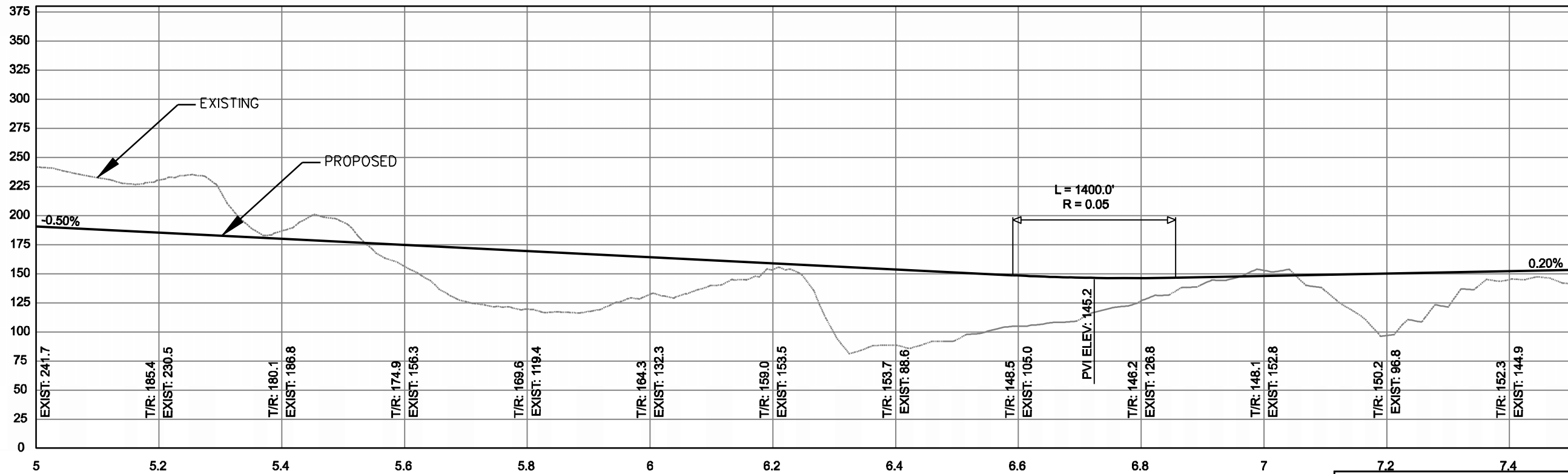
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	APP. NO.:
DRAWN BY: DEM			ACAD FILE:
CHECKED BY: MAP			DWG NO. <b>B2</b>
APPROVED BY:			<b>42</b> OF <b>48</b>



← TO PORT MACKENZIE

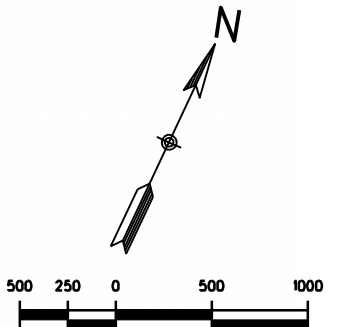
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA  
REFER TO VOLUME 2, APPENDIX G  
PRELIMINARY HYDROLOGY REPORT

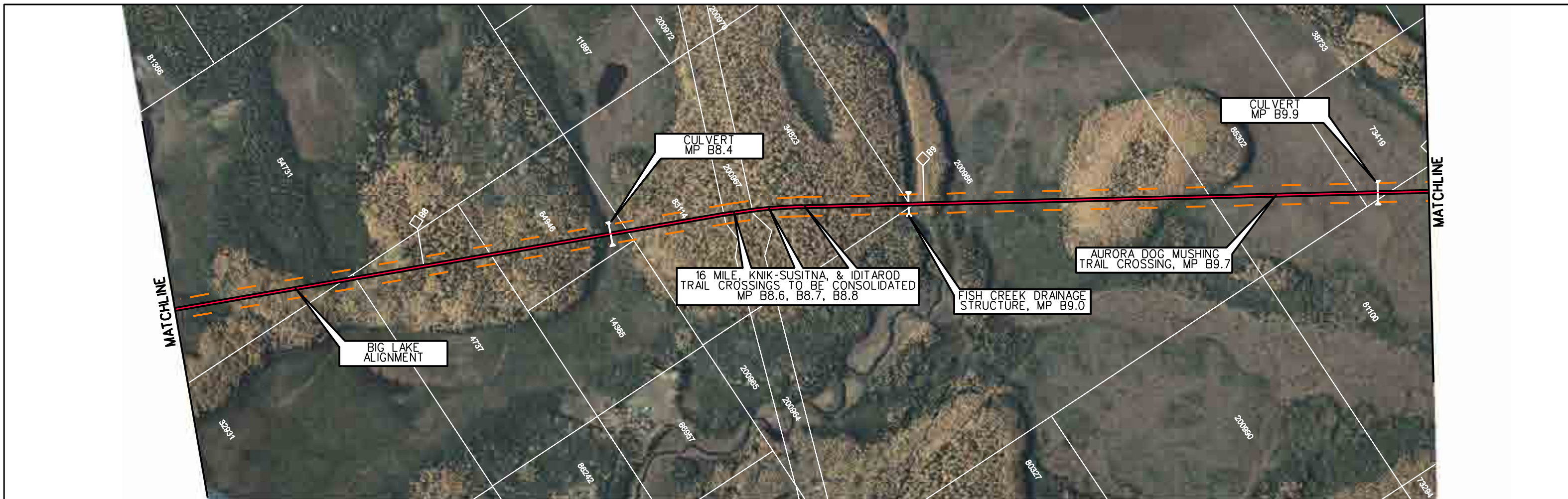


PLAN AND PROFILE  
BIG LAKE, MP B5.0-B7.5

CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

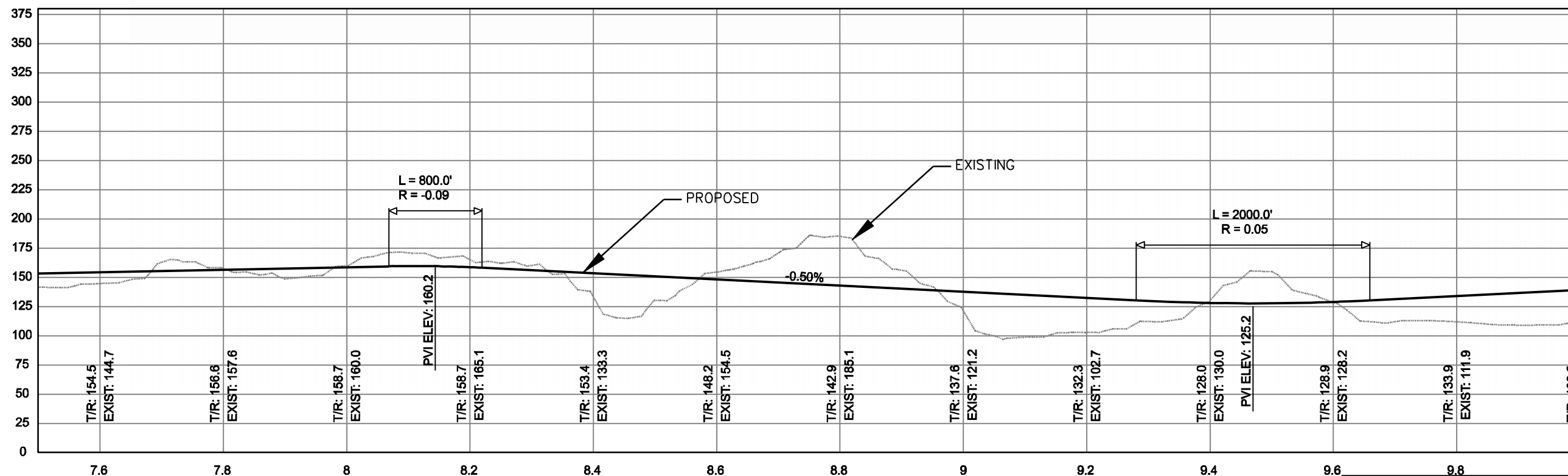
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	DATE: JANUARY 2008
DRAWN BY: DEM			
CHECKED BY: MAP			
APPROVED BY:			



← TO PORT MACKENZIE

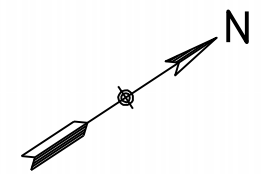
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



CONCEPTUAL PLANS NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

<b>PLAN AND PROFILE</b> <b>BIG LAKE, MP B7.5-B10.0</b>		
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008
DRAWN BY: DEM	CHECKED BY: MAP	APPROVED BY:
PROJECT NO. 44 OF 48		SHEET NO. B4 OF 48

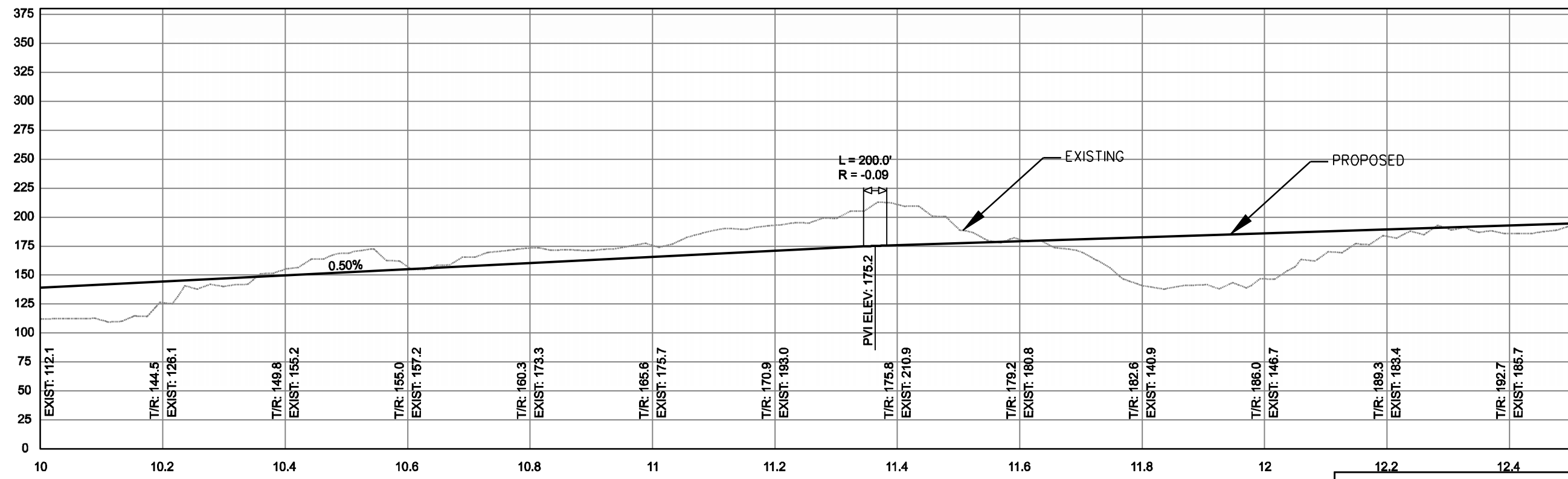




← TO PORT MACKENZIE

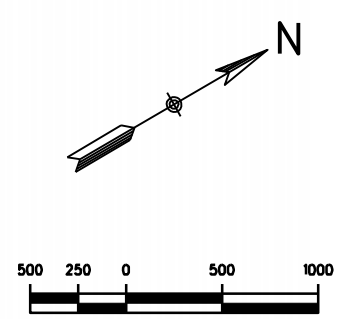
PLAN

TO EXISTING MAINLINE →



PROFILE

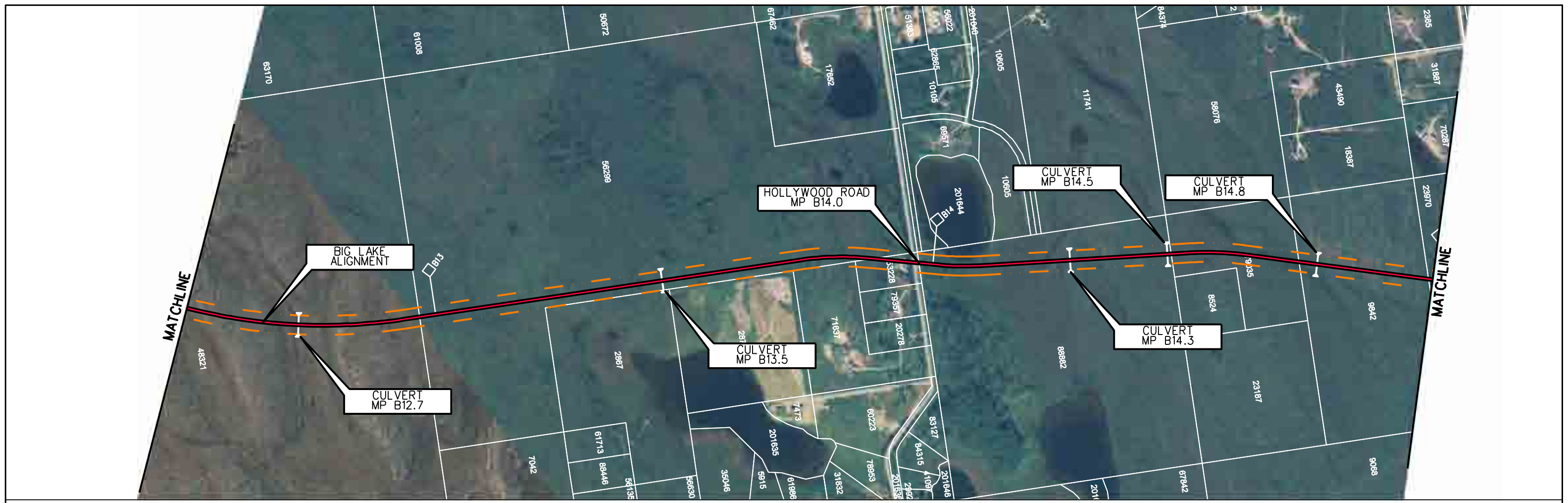
FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



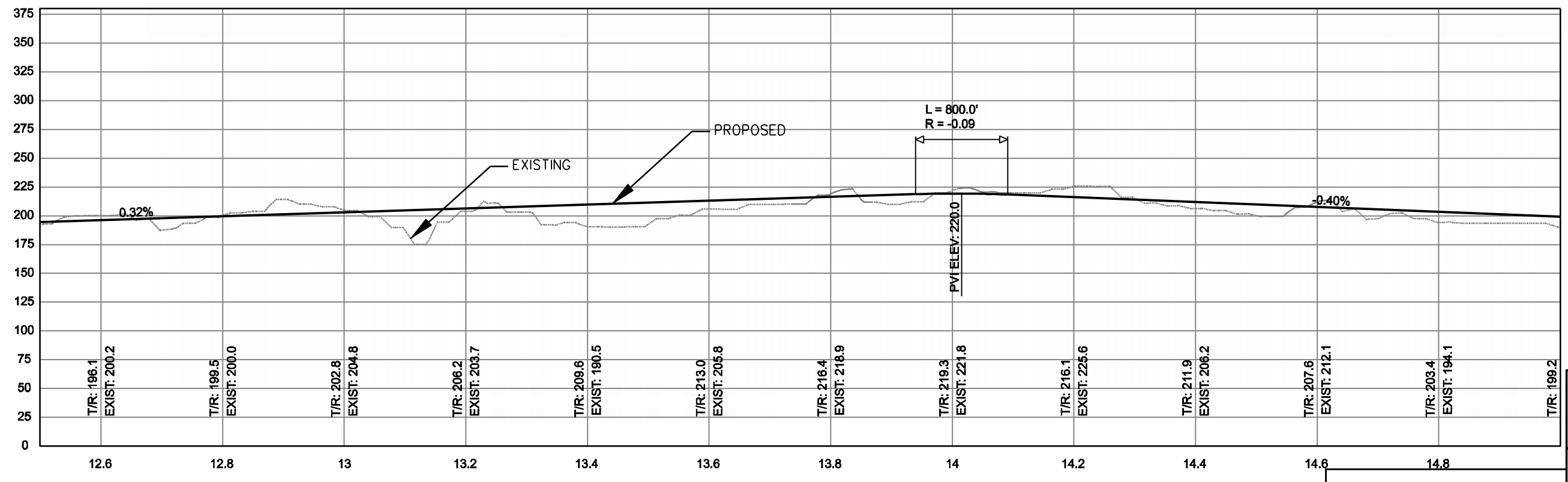
CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

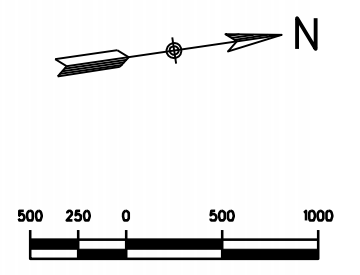
<b>PLAN AND PROFILE BIG LAKE, MP B10.0-B12.5</b>			
DESIGNED BY:	MAP	SCALE: AS SHOWN	DATE: JANUARY 2008
DRAWN BY:	DEM	CHECKED BY:	MAP
APPROVED BY:			
ACAD FILE:		DWG NO.	
B5		45 OF 48	



← TO PORT MACKENZIE PLAN TO EXISTING MAINLINE →



FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT

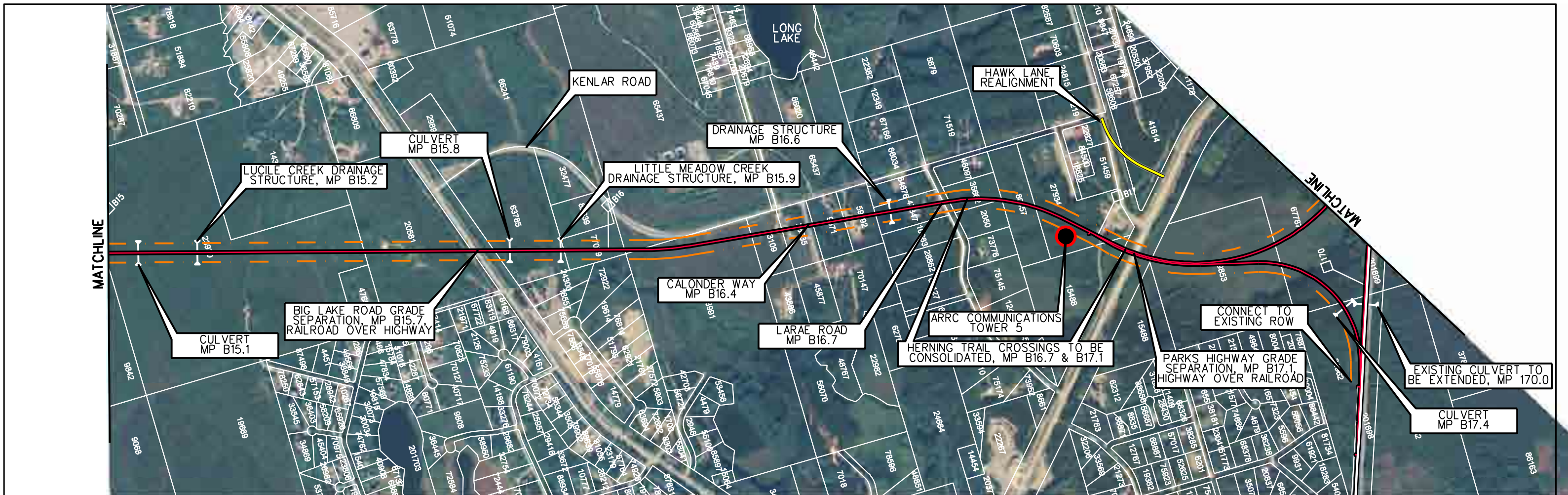


**PLAN AND PROFILE  
BIG LAKE, MP B12.5-B15.0**

**CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION**

AUTHORED BY TNH-HANSON, LLC

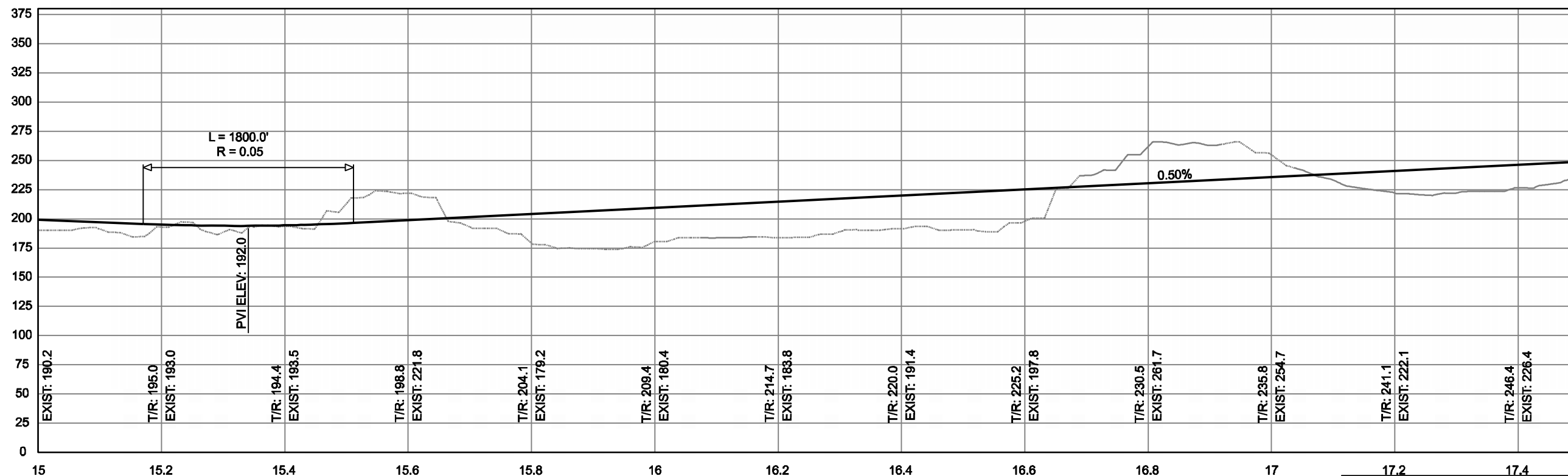
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	APP. NO.: B6
DRAWN BY: DEM			46 OF 48
CHECKED BY: MAP			
APPROVED BY:			



← TO PORT MACKENZIE

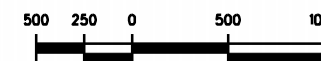
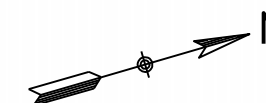
PLAN

TO EXISTING MAINLINE →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



PLAN AND PROFILE  
BIG LAKE, MP B15.0-B17.5

CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

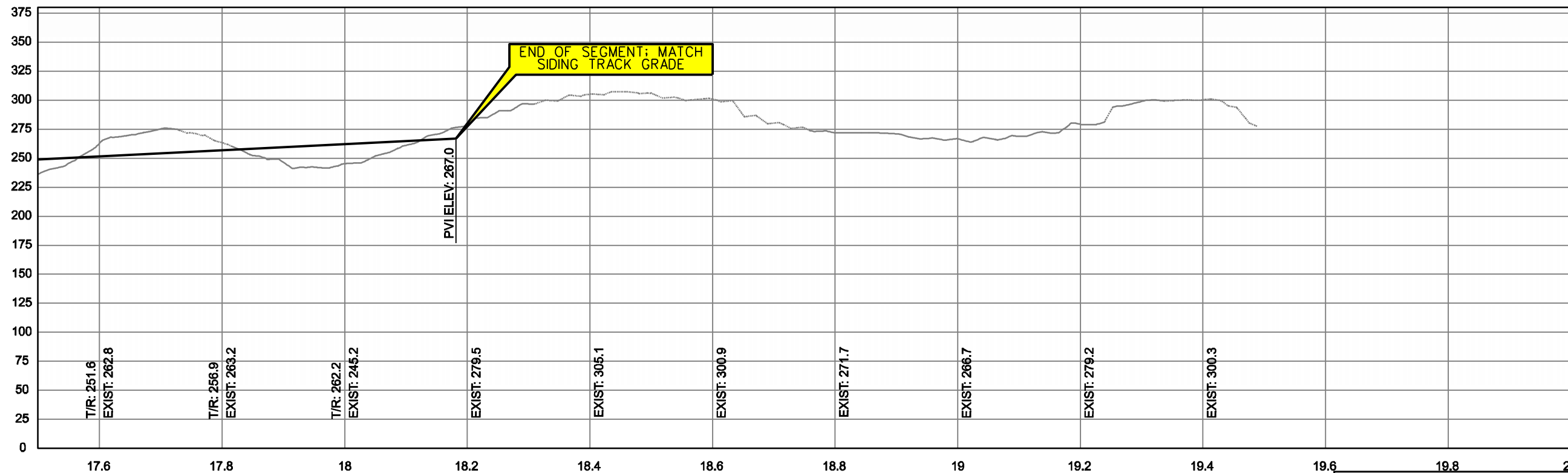
DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	APP. NO. B7
DRAWN BY: DEM			47 OF 48
CHECKED BY: MAP			
APPROVED BY:			



← TO ANCHORAGE

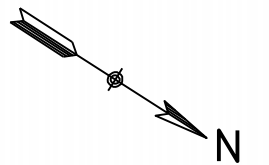
PLAN

TO FAIRBANKS →



PROFILE

FOR ADDITIONAL CULVERT DATA REFER TO VOLUME 2, APPENDIX G PRELIMINARY HYDROLOGY REPORT



PLAN AND PROFILE  
BIG LAKE, MP B17.5-B19.5

CONCEPTUAL PLANS  
NOT FOR CONSTRUCTION

AUTHORED BY TNH-HANSON, LLC

DESIGNED BY: MAP	SCALE: AS SHOWN	DATE: JANUARY 2008	APP. NO.: B8
DRAWN BY: DEM			OF 48
CHECKED BY: MAP			
APPROVED BY:			



# Public Involvement Activities Summary

*Port MacKenzie Rail Extension Project — Preliminary Environmental and Alternatives Report*



Prepared for the Matanuska-Susitna Borough and the Alaska Railroad Corporation

**Table of Contents**

1.0 INTRODUCTION ..... 1-1

    1.1 Overview of Proposed Project ..... 1-1

    1.2 Purpose and Goals..... 1-2

    1.3 Public Involvement Tools Overview ..... 1-2

2.0 STAKEHOLDERS ..... 2-1

    2.1 Stakeholders Involved..... 2-1

    2.2 Small Groups ..... 2-2

    2.3 Landowners..... 2-4

    2.4 Local Government ..... 2-5

3.0 PUBLIC INVOLVEMENT PROCESS..... 3-1

    3.1 Public Involvement Activities and Materials..... 3-1

        3.1.1 Project Mailing List ..... 3-1

        3.1.2 Web Site..... 3-1

        3.1.3 Public Meeting Notification..... 3-1

        3.1.4 Project Press Releases..... 3-2

        3.1.5 Media Outreach & Coverage ..... 3-3

        3.1.6 Public Meetings ..... 3-5

        3.1.7 Postcard..... 3-5

        3.1.8 Newsletter ..... 3-5

        3.1.9 Comments Received ..... 3-6

4.0 ISSUES AND ALTERNATIVES COMMENT SUMMARY ..... 4-1

    4.1 Willow..... 4-1

    4.2 Houston..... 4-1

    4.3 Big Lake..... 4-1

    4.4 Port MacKenzie West and East..... 4-1

    4.5 Other Comments ..... 4-1

        4.5.1 Trails/Crossings ..... 4-1

        4.5.2 Wetlands/Watershed and Anadromous Streams ..... 4-1

        4.5.3 Other Corridors ..... 4-2

        4.5.4 Purpose and Need..... 4-2

5.0 KEY ISSUES ..... 5-1

## Tables

Table 1-1 Public Involvement Tools.....	1-3
Table 2-1 Small Group Meetings/Activities.....	2-3
Table 2-2 Landowner Meetings.....	2-4
Table 2-3 Local Government Meetings/Activities.....	2-6
Table 3-1 Meeting Notification Methods.....	3-2
Table 3-2 Project Press Releases.....	3-3
Table 3-3 Media Coverage.....	3-4
Table 3-4 Meeting Attendance.....	3-5
Table 3-5 Comments Received.....	3-6

## Figures

Figure 1-1 Surface Transportation Board Application process.....	1-1
--	-----

## Appendix

### **Appendix A: Materials Presented**

- Public Meeting Material
- Advertisement Material
- Small Group Meetings
- Landowner Meeting Material
- Media Coverage
- Project Website
- Project Mailing list

### **Appendix B: Comments Received**

- Comment Forms
- Letters Received
- Electronic Comments
- Map Comments
- Court Reporter/Public Testimony Comments



## EXECUTIVE SUMMARY

In August 2007, the Matanuska-Susitna Borough (MSB) and the Alaska Railroad Corporation (ARRC) jointly propose to connect Port MacKenzie to ARRC's rail system by constructing and operating a new rail line. The new rail line would be approximately 30 to 45 miles long, depending on the route selected, extending from Port MacKenzie to a point on the ARRC's existing main line between Wasilla and north of Willow. The Public Involvement Activities Summary Report, Volume 4, documents the public involvement process utilized in the development of the preliminary engineering and environmental data presented in the Preliminary Environmental and Alternatives Report, which will be evaluated by the Surface Transportation Board (STB) as part of the National Environmental Policy Act (NEPA) process required for project construction.

This Public Involvement report summarizes the public involvement activities conducted by the MSB and the ARRC between September and December 2007 as part of the STB application development process and reflects a summary of input provided by the public, local governments, and Tribes. Appendix A provides the materials presented during the process and Appendix B contains all the comments received during the preliminary scoping process.

## ACTIVITIES

Public involvement activities for the proposed Port MacKenzie Rail Extension project took place during a four-month period. A variety of outreach tools were used and more than 361 comments were received. The public outreach conducted during the application development process allowed the proposed project to respond to and evolve as a result of input from the public and various stakeholders. The following list of tools and activities highlights the outreach efforts and opportunities for the public to provide input:

- Comment database (ongoing)
- Local government meetings (10)
- Tribal meetings (1)
- Mailing list (ongoing)
- Media coverage (ongoing)
- Meeting advertisements and other notifications
- Alaska Native Claims Settlement Act (ANCSA)<sup>1</sup> meetings (3)
- Newsletters (1)
- Public open house meetings (5)
- Press releases (7)
- Small group meetings (6)
- Web site (ongoing)

---

<sup>1</sup> ANCSA refers to Native Corporations created under the Alaska Native Claims Settlement Act of 1971. Cook Inlet Region, Incorporated (CIRI), is the Native Regional Corporation owning land in the project area. Knikatu, Incorporated is the Village Corporation owning land in the project area.

## **COMMENTS AND ISSUES**

The goal of the public involvement effort was to assist in the preliminary evaluation of the corridor alternatives by providing locally obtained background information to the MSB and ARRC. Through the public involvement activities, information relating to the three preliminary corridors and to the project in general was gathered. Concerns from the public ranged from private property impacts, to quality of life issues, to local and regional economic impacts. Trails and recreation opportunities were also common themes, as were environmental issues such as wetlands, soils, archeological sites, and wildlife.

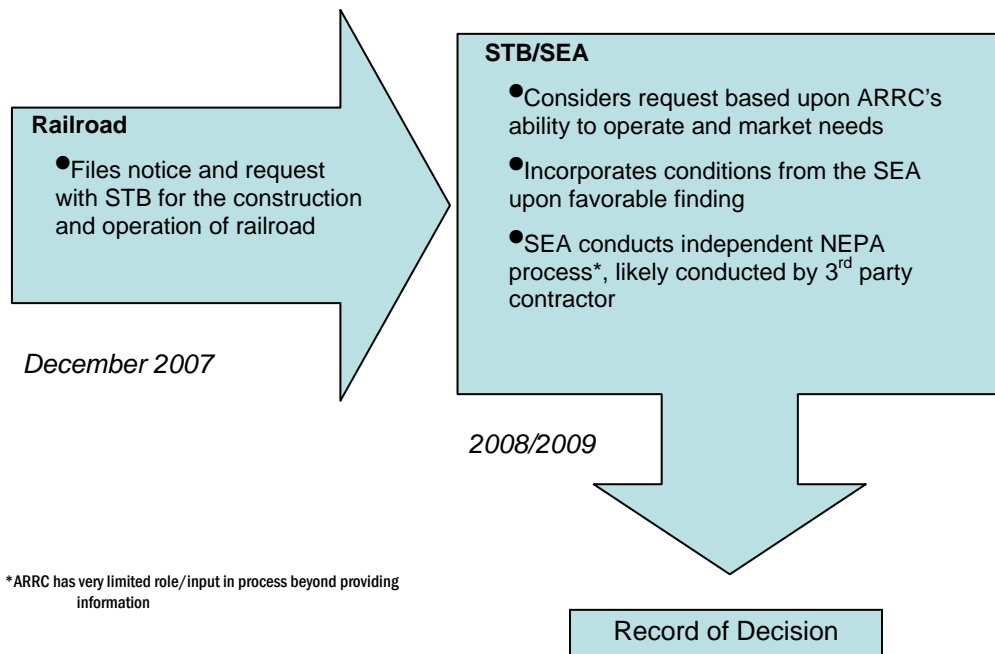
# 1.0 INTRODUCTION

## 1.1 Overview of Proposed Project

The MSB and ARRC propose to construct and operate a new rail line to connect the MSB’s Port MacKenzie to the existing ARRC rail system. The new rail line is anticipated to be approximately 30 to 45 miles long, depending on the route selected. This rail line has been considered for nearly 30 years.

New rail line construction requires the preparation of 1) a request to the Surface Transportation Board (STB) and 2) an environmental document in compliance with the National Environmental Policy Act (NEPA). The STB is the federal agency with jurisdiction over railroad rate and service issues and rail restructuring transactions (such as mergers, line sales, line construction, and line abandonments). A request to the STB to build and operate a new rail line initiates the NEPA process.

The information gathered by the MSB and ARRC through the public involvement activities and incorporated into the Preliminary Environmental and Alternatives Review (PEAR) will be presented to the STB to begin the NEPA process. The PEAR details the identification and analysis of reasonable alignment corridors, completes conceptual engineering design on the corridors, and estimates anticipated construction costs to be fully evaluated during the STB’s NEPA process. Figure 1.1 shows the process and anticipated schedule from the application phase through the STB NEPA process.



**Figure 1-1 Surface Transportation Board Application process**

## **1.2 Purpose and Goals**

The purpose of the Port MacKenzie Rail Extension project is to establish a rail link between Port MacKenzie and the Alaska Railroad's main line, providing Port MacKenzie customers/shippers with efficient rail transportation between the Port and Interior Alaska. The Port MacKenzie rail extension would begin at the existing Port MacKenzie facility and tie into ARRC's existing main line between Mile 167 north of Wasilla and Mile 190 north of Willow.

Specifically, the project proposes to:

- Support Port MacKenzie's continuing development as a bulk material resources export and import facility, and as a multi-modal facility
- Provide an alternative to highway transportation for Port MacKenzie

Chapter 1.1 of the PEAR contains a detailed description of the project's Purpose and Need.

## **1.3 Public Involvement Tools Overview**

Public involvement during the preliminary engineering and environmental data collection phase was led by the MSB's public affairs director. Public involvement was used to obtain information from the public and the various stakeholders to refine the corridor alternatives and provide background information to the MSB and ARRC, leading to the preparation and submission of an application to the STB.

Public involvement activities throughout this initial public outreach stage included:

- Web site creation and maintenance
- Mailing list creation and maintenance
- Timely response to written comments and concerns
- Media coordination and monitoring
- Message development and refinement
- Demonstration that MSB and ARRC value the public process

**Table 1-1 Public Involvement Tools**

<b>Tool</b>	<b>Description</b>
Mailings	Post card mailings were sent to approximately 7,700 recipients to announce public meetings. Contact information was obtained from the MSB property tax lists and from the ARRC's contact list of MSB residents.
Public Open Houses	Five public open house meetings were held on consecutive evenings between October 1 and October 5, 2007. Meetings were held in the potentially affected communities of Wasilla, Big Lake, Willow, Knik, and Houston. A court reporter was present at all open houses to record comments and ideas from people who preferred not to submit written comments.
Press Releases and Media Coverage	Press releases were sent to media outlets before and after each public meeting. These press releases resulted in print media coverage and radio broadcasts.
Informational materials	Maps, fact sheets, Frequently Asked Questions, comment forms and project business cards were produced and distributed at all public meetings.
Web site	A project Web site ( <a href="http://www.portmacrail.com">www.portmacrail.com</a> ) was established to provide project updates, record comments, and distribute documents. The site was updated daily during the week of public meetings and regularly thereafter.
Individual meetings	A letter offering one-on-one or small group meetings with project staff was sent to 26 interest groups potentially affected by the project.
Flyers	Flyers were distributed to libraries and other public places (such as gas stations and post offices) to announce meeting dates and promote the Web site.
Newspaper Display Advertisements	Display advertisements were placed in the Anchorage Daily News, Mat-Su Frontiersman, and the Talkeetna Good Times to announce the public open house meetings and to direct the public to the project Web site.
Project Business Card	A business card provided contact information for the MSB public affairs director and ARRC public information officer. The open house meeting information and project Web address were also provided.
Fact Sheet	ARRC provided a Fact Sheet with basic information regarding the project, project history, and potential benefits.
FAQs	A list of frequently asked questions was provided at the public open house meetings and on the project Web site.

## 2.0 STAKEHOLDERS

### 2.1 Stakeholders Involved

The proposed project has numerous stakeholders—businesses, local government representatives, communities, Alaska Native corporations, Tribal entities, resident and non-resident landowners, local citizens, and recreational users—each with a different level of understanding and perception of the positives and negatives of the project. Communities impacted directly by the proposed project may include Wasilla, Big Lake, Houston, Knik, Point MacKenzie, and Willow. Landowners in the project area also include many weekend or vacation property-owners who live outside the project area but who may have a vested interest in the planning process. The public involvement process, by design, brought out a wide spectrum of voices and interests.

Stakeholders in the project included, but were not limited to:

- Advisory Committees
- ANCSA Corporations
- Alaska Native Tribes and Tribal Entities
- Businesses and Business Councils
- Elected Officials
- Conservation Organizations
- Landowners
- Municipal and Borough Governments
- Community Councils
- Recreation and Trail Groups

The stakeholder groups were further refined into sub-groups to discuss localized concerns such as, recreational use, small business impacts, and historic and cultural issues. The sub-groups are categorized as Small Groups, Landowners and Local Government in sections 2.2, 2.3 and 2.4.

## **2.2 Small Groups**

A number of small groups and organizations were initially identified by the MSB and ARRC as stakeholders that would have an interest in the project from varying perspectives, ranging from historic preservation to recreational opportunities. This list was based on previous experience and local knowledge of the area. The intent of outreach to the small groups and organizations was to share project information and to answer questions. A letter was sent on September 14, 2007 to 26 small organizations.

The letter introduced the project and offered an opportunity for individual meetings with the MSB and ARRC upon request (see Appendix A). Organizations included statewide non-governmental organizations, historic preservation committees, local community councils, recreational user groups and Tribal and Native corporation entities. Of the 26 groups that received the letter, 5 responded requesting a meeting.

Table 2.2-1 provides a summary of the individual meetings, which varied in content and meeting style. Copies of correspondence and meeting materials are included in Appendix A.

**Table 2-1 Small Group Meetings/Activities**

<b>Meeting/Activity</b>	<b>Date (2007)</b>	<b>Purpose</b>
Willow Area Community Organization*	August 6	Introduce the Port MacKenzie Rail Extension project and answer questions from community members
Small group invitation letter mailed	September 14	Introduce the project and invite the organizations to meet with the project team and discuss their concerns. The Willow Dog Musers Association received an emailed copy of the letter.
Area legislators	September 17	Provide an update and information on the project to the local area legislators. A slide presentation was provided to legislators at the Legislative Information Office in Wasilla, Alaska
Willow Dog Musers Association and Alaska Snow machine Association	October 24	Project team meeting in response to request by Willow Dog Musers Association (letter to Patty Sullivan, MSB, dated October 12). The project team provided answers to questions asked by the association and provided a trails map for the participants to comment on. In addition to the map a trails comment form was provided to the participants
Iditarod Trails Committee, Inc.	October 24	Project team members met with the committee's Executive Director to discuss the Iditarod National Historic Trail and concerns from the committee relating to crossings
Houston City Council	October 25	The council invited the project team to discuss the project at the regularly scheduled Houston City Council meeting. A short slide presentation was provided with a brief question and answer period
Knik Tribal Council Historic Preservation Committee	November 5	The project team coordinated a meeting with the Federally recognized Knik Tribal Council. The meeting consisted of members from the Knik Tribal Council, the Knik Tribal Council Historic and Cultural Committee, the MSB Cultural Resource staff, and members of the Project Team. The purpose of the meeting was to provide a project introduction, discuss the project timeline, and to review the archeological probability map generated by the MSB and HDR Alaska.

a. This meeting happened prior to the project invitation letter, but is noted as an outreach effort to the Willow community.



### 2.3 Landowners

There are many landowners in the project area ranging from the State of Alaska and MSB to individual property owners. The project team identified the major property owners as the State of Alaska, MSB, University of Alaska, The Trust (Alaska Mental Health Trust Authority), Cook Inlet Region, Inc. (CIRI), and Knikatnu, Inc.<sup>2</sup>. These property owners were identified as stakeholders with potential commercial and industrial interests. Meetings were held with the landowners between September and November 2007. Table 2.2 provides a summary of the interaction with the landowners.

Meetings with the major land owners were arranged via e-mail and telephone. The project team met with the entities listed in Table 2.2-1 to introduce the project, provide notice that the project would consider alternatives that could impact individual parcels, and to discuss Right-of-Way concerns. Materials presented at the meeting included sign in sheets, an alternatives map, and owner parcel maps showing the alternatives (Appendix A). The maps were used to discuss areas of concern and to identify potential issue areas or areas for future development.<sup>3</sup>

**Table 2-2 Landowner Meetings**

<b>Landowner</b>	<b>Date (2007)</b>	<b>Purpose</b>
University of Alaska Lands Office	September 24	Review project area maps, individual maps with alternatives and provide a project overview
Knikatnu, Inc.	September 25	Review project area maps, individual maps with alternatives and provide a project overview. A follow-up letter requesting additional information regarding future land development was sent to Knikatnu, Inc. by ARRC on October 16
Mental Health Trust, The Trust Land Office	September 25	Review project area maps, individual maps with alternatives and provide a project overview
CIRI	September 28	Review project area maps, individual maps with alternatives and provide a project overview
Knikatnu, Inc.	November 3	Review project area maps, individual maps with alternatives and provide a project overview to the Board of Directors

---

<sup>2</sup> Eklutna, Inc. was also contacted to determine if they owned land in the area. Telephone correspondence on September 9, 2007 indicated Eklutna Inc. does not own land in the vicinity.

<sup>3</sup> Surface and mineral (subsurface) estates were a topic of special interest, particularly regarding lands owned by The Trust (Alaska Mental Health Trust Authority), CIRI, and Knikatnu, Inc. For example, in the circumstance of The Trust, language that is used to reserve the subsurface rights in Trust lands is stated in Alaska Statute 38.05.125. If the lands were received via the Statehood Act of 1959, The Trust has to keep the subsurface rights. If the lands were received through the Mental Health Enabling Act of 1956, there are no restrictions on keeping the subsurface rights. However, The Trust policy is to keep the subsurface, especially in areas with known reserves of gas and coal. The Project Team, through a data exchange with The Trust, was able to establish the lands where The Trust owned the subsurface estate, which will be important as the project is further refined. In addition, CIRI, as a regional Native corporation, owns subsurface rights, and the village corporation, Knikatnu, Inc. owns surface rights. It was explained in general terms by CIRI that the regional entity would be supportive if the village corporation were to develop the surface rights. Coordination between the three entities will be critical if the preferred alternative involves village corporation lands.

## 2.4 Local Government

Local government departments and municipal governments involved during this initial phase included the following entities.

MSB Assembly is the legislative power of the MSB and is comprised of seven members elected from districts for staggered three-year terms for no more than two consecutive full terms. The Assembly approves the budget, sets the mill rate for taxation, and appropriates funds to provide for MSB services. The Mayor presides at meetings and is the ceremonial head of the MSB.

MSB Transportation Advisory Board is responsible for transportation planning, implementing the preliminary design and environmental phase of transportation projects, reviewing development/subdivision applications, collecting and analyzing traffic and traffic calming data throughout the MSB. The advisory board is also responsible for securing federal, state, and local funds for road, bridge, railroad, ferry, transit, and airport projects.

MSB Planning Commission is comprised of seven volunteer community members that work with the Planning Division to develop long range plans, including land use, transportation, and public facilities plans; plans concerning the development and growth of the MSB; and the development of the capital improvement program.

MSB Historic Preservation Commission enables citizens to provide input on issues important for the protection of historic properties and cultural resources. The Historic Preservation Commission is comprised of nine volunteer community members who work with the MSB Cultural Resources Section.

City of Houston is an incorporated second-class city located within the MSB. The City of Houston is governed by a City of Council comprised of five elected community members, Deputy Mayor, and Mayor.

Meetings with these local government entities are summarized in Table 2.3 and materials presented during the meetings are located in Appendix A.

**Table 2-3 Local Government Meetings/Activities**

<b>Meeting &amp; Activity</b>	<b>Date (2007)</b>	<b>Purpose</b>
Mailed letters to the City of Wasilla, City of Palmer, City of Houston	September 14	Introduce the project and offer the opportunity to meet with the Project Team
Port Commission	September 17	Introduce the project and upcoming public outreach efforts
MSB Planning Commission	October 15	Introduce the project and discuss the preliminary corridors
MSB Transportation Advisory Board	October 24	Provide an update on project development and overview of the public meetings
City of Houston	October 25	Provide an overview of the project and answer questions
Joint MSB Assembly/Planning Commission	October 30	Provide an update regarding the project and to give an update on the comments received to date
Port Commission Special Meeting Re: Port MacKenzie rail extension	November 5	Provide update on project activities relating to public involvement and the schedule for the application process
MSB Planning Commission	November 5	Provide project update
MSB Historic Preservation Commission	November 9	Provide project introduction and answer questions from the Commission members
MSB Assembly Regularly Scheduled Meeting	November 20	Provide an update on the project and answer questions. During the meeting the Assembly called for a special work session to discuss the project in greater detail
MSB Assembly Work Session	December 11	Provide the assembly with information relating to how the alternatives were developed and the methods used to measure preliminary impacts
MSB Assembly	December 11	MSB Assembly passed a resolution to send to the STB

## 3.0 PUBLIC INVOLVEMENT PROCESS

### 3.1 Public Involvement Activities and Materials

#### 3.1.1 Project Mailing List

A project mailing list of approximately 7,700 individuals, businesses, and organizations was generated for the project outreach efforts using MSB property tax/landowner information and a MSB address-specific list provided by ARRC. The list also included elected officials, State and Federal agency representatives, and local government officials.

The mailing list was used to send out a project post card and a newsletter, which are included in Appendix A. Updates to the mailing list are on-going. The project mailing list, dated December 21, 2007 is located in Appendix A.

#### 3.1.2 Web Site

A project web site ([www.portmacrail.com](http://www.portmacrail.com)) was created for persons with internet access. The Web site address was referenced on handouts, newspaper advertisements, press releases, postcards, and on the project business card. The Web site, which will be updated continuously throughout the NEPA review process, includes the following pages and is included in Appendix A:

- Home: welcome, alternatives map
- Project Overview: Project history, benefits, area, participants and issues
- FAQs (frequently asked questions)
- Get Involved: Meeting announcements, comments submittal, mailing list, and contact information for the MSB public affairs director and ARRC public involvement officer.
- Project Library: News releases, handouts, presentations, maps, reports, and other related studies
- Useful Links: Links to the MSB, ARRC, Port MacKenzie, and STB

Individuals visiting the Web site are also able to submit comments electronically and to request to be added to the project mailing list. Forty-five individuals used the Web site to provide electronic comments between September 5 and December 14, 2007. Fifty-five electronic comments were received (Appendix B).

#### 3.1.3 Public Meeting Notification

Public meetings were advertised in advance using newspaper display ads; a household postcard mailing; the project, ARRC, and MSB Web sites; press releases; and community flyers. Table 3.1-1 shows both the advertisements used for the notification and the date these notes were published. Materials included in Table 3.1-1 are presented in Appendix A.

**Table 3-1 Meeting Notification Methods**

<b>Meeting Notification Tool</b>	<b>Date (2007)</b>
Project Web site launched	September 5
Postcard	September 14
<ul style="list-style-type: none"> <li>• Project mailing list</li> </ul>	
Press Release: Public invited to five open house events to review and comment on Port MacKenzie Rail Extension Project	September 14
Retail Display Advertisements	September 18, September 21, and September 28
<ul style="list-style-type: none"> <li>• Anchorage Daily News</li> <li>• Frontiersman</li> <li>• Talkeetna Good Times (September 28 only)</li> </ul>	
Community Flyers Posted	September 22
<ul style="list-style-type: none"> <li>• Willow Area Community Center</li> <li>• Townsite Food Mart</li> <li>• Videomart</li> <li>• Willow Creek Grocery</li> <li>• Willow Tesoro</li> <li>• Gold Miners Lodge and Restaurant</li> <li>• Miller's Market</li> <li>• Houston Lodge Café &amp; Lounge</li> <li>• Kathy's Diner</li> <li>• Steve Boy Grocery</li> <li>• Big Lake Library</li> <li>• Big Lake Liquor</li> <li>• Spenard Builders Supply/Big Lake</li> <li>• Bulldog Espresso</li> </ul>	<ul style="list-style-type: none"> <li>• Meadow Lakes Center</li> <li>• Three Bears</li> <li>• Carrs/Safeway, Palmer</li> <li>• HDR Alaska, Palmer</li> <li>• Vagabond Blues, Palmer</li> <li>• Fireside Book Store, Palmer</li> <li>• Mat-Su Borough Office, Palmer</li> <li>• Settler's Bay Post Office, Knik</li> <li>• Body in Balance, Palmer</li> <li>• Daycare, Knik</li> </ul>
Electronic notices	
<ul style="list-style-type: none"> <li>• What's Up (an e-mail list serv provided by the National Wildlife Federation and the Alaska Women's Environmental Network that distributes environmental information statewide). Announcements for the Port MacKenzie rail extension were published three times.</li> </ul>	September 19 September 26 October 3
<ul style="list-style-type: none"> <li>• E-mail broadcast from Senator Charlie Huggins to his constituency.</li> </ul>	September 26
<ul style="list-style-type: none"> <li>• E-mail notice sent to Port MacKenzie e-mail list produced by the Project Team</li> </ul>	September 28

*3.1.4 Project Press Releases*

Seven press and radio releases were written and submitted by the MSB public affairs director to various media outlets. The press releases were also posted on the Project Library page of the project Web site, on the MSB Web site, and on the ARRC Web site. The releases announced the public meetings, recapped previous public meetings, and updated project activities. Table 3.1-2 includes a listing of the topics and the dates of the releases, copies of which are included in Appendix A.

**Table 3-2 Project Press Releases**

<b>Topic</b>	<b>Date (2007)</b>
Public invited to five open house events to review and comment on Port MacKenzie Rail Extension Project (print and radio releases)	September 14
Wasilla open house starts off series of meetings	October 2
Overview of the open house meetings	October 5
Houston to explore rail opportunity	October 10
Assembly calls for work session on Port Mac Rail project	November 20
Assembly passes resolution on Port MacKenzie Rail project	December 11

### 3.1.5 *Media Outreach & Coverage*

Media outreach was conducted by the MSB public affairs director.

The local media outlets and other publication services provided a comprehensive review of the project's progress. Twenty-eight articles and interviews were written and conducted by local media outlets. Print coverage was provided by the Matanuska-Susitna Borough Frontiersman, Alaska Journal of Commerce, Talkeetna Times, and Anchorage Daily News (see Appendix A). Radio interviews and television news stories were also conducted. MP3 versions of the interviews are posted on the project Web site. Table 3.3 provides a summary of media coverage.

**Table 3-3 Media Coverage**

<b>Title</b>		<b>Date (2007)</b>
Legislature nears approval of financing for Agrium coal project	Alaska Journal of Commerce	May 4
Port MacKenzie angles for rail extension for coal	Alaska Journal of Commerce	May 20
Mat-Su, ARRC partner on port project	ARRC All Aboard	July/August
Meetings set to begin on Point MacKenzie rail expansion plan	Frontiersman	September 18
Agrium plan closures in Kenai won't slow Port Mac rail extension	Frontiersman	September 28
Willow hoping it won't be railroaded	Frontiersman	September 28
Patty Sullivan and Dave Hanson on economics of projects	KMBQ	Week of Aug. 29
Patty Sullivan and Brad Sworts on rail process	KMBQ	September 29
Alaska Railroad to host open houses in five communities	Talkeetna Times	October 1
Point MacKenzie railroad debated	Channel 2 KTUU	October 2
Point MacKenzie railroad spur debate begins	MSNBC.com	October 2
Rail link to port explored	Anchorage Daily News	October 3
Willow split in opinions on rail plan	Anchorage Daily News	October 5
Proposed rail routes enough to rile residents	Frontiersman	October 5
Patty Sullivan and John Duffy interview on Mat-Su projects	KSKA	October 10
Houston wants Borough to make tracks	Frontiersman	October 12
Houston invites comment on rail spur	Frontiersman/Letter to Editor	October 20
Rail link proposal stirs interest from Mat-Su residents	Alaska Journal of Commerce	October 21
Rail link proposal stirs interest from Mat-Su residents	Anchorage Daily News	October 27
Houston wants rail guarantees	Anchorage Daily News	October 29
Houston on board early with rail plan	Frontiersman	October 30
Houston needs follow-through with rail desires	Frontiersman	October 30
Valley train could bring economic growth	Channel 2 KTUU	November 21
Houston's fire chief resigns, deputy clerk quits	Frontiersman	November 22
Feds weigh in on impacts of planned rail spur	Frontiersman	November 24
South route for rail line rated highest	Anchorage Daily News	November 28
Houston may be ahead for rail route	Frontiersman	November 29
Rail study off to feds	Frontiersman	December 16

### 3.1.6 Public Meetings

Five large, open house format public meetings were conducted between October 1 and 5, 2007. The meetings were held in Wasilla, Big Lake, Willow, Knik, and Houston. Each meeting was held from 6:00 pm to 8:00 pm. The purpose of the open house meetings was to describe the proposed project, to present the proposed alternatives, to explain how the alternatives were developed, to review the STB process, and to solicit input from the public on issues, concerns, and additional studies needed.

Seven topic “stations” were set up at each meeting: Welcome, General Project Information, Engineering, STB Process, Right-of-Way, Project Area, and Comment. Each station contained informational posters and a project team member familiar with the material who could field questions from the attendees. During each meeting, a slide presentation was given and the audience was requested to ask questions of the project team members at each station. Hand-out materials included comment forms, alternatives maps, and a Frequently Asked Questions sheet. A total of 356 people attended the open house meetings. The meeting materials are included in Appendix A. Table 4.3 summarizes the individual meeting attendance.

Public comment at the open houses was taken by written comments on comment forms, oral comments through a court reporter, and/or by writing on large project area maps provided for the purpose. Fifty-six comments were written on comment forms, 37 verbal comments were recorded with the court reporters, and 137 comments were written on the maps. Materials presented at the open houses are included in Appendix A; all comments are included in Appendix B.

**Table 3-4 Meeting Attendance**

<b>Meeting</b>	<b>Attendees</b>
October 1 – Lake Lucille Inn, Wasilla	68
October 2 – Big Lake Elementary, Big Lake	124
October 3 – Willow Area Community Organization, Willow	86
October 4 – Knik Elementary, Knik	45
October 5 – Houston Middle School, Houston	33

### 3.1.7 Postcard

On September 14, 2007 approximately 7,700 project postcards were mailed via U.S. Postal Service addressed to area landowners, local businesses, organizations, and area government officials. The postcard announced the open houses, project Web site address, and ways to participate in the project. The mailing list was generated by both the MSB and ARRC, with additional individuals added through the project Web site. The complete mailing list and the postcard are located in Appendix A.

### 3.1.8 Newsletter

A newsletter was distributed by mail between December 10 and December 12, 2007 to the project mailing list and posted on the project Web site. The newsletter provided an overview of the matrix used to score each of the project alternatives and provided a recap of the public meetings. Also included in the newsletter was an overview of the project schedule. A copy is included in Appendix A.



3.1.9 *Comments Received*

There were several mechanisms for the public to provide comments during this phase of the project. While the comment deadline for comments to be considered in the development of the Preliminary Environmental and Alternative Report was November 2, 2007, the Public Involvement Report contains all comments received through December 2007. Table 3.5 summarizes the type and number of comments received (see Appendix B for complete comments).

**Table 3-5 Comments Received**

<b>Comment Type</b>	<b>Number Received</b>
Comment forms/letters	102
Electronic comments (website)	55
Court reporter comments/recorded meeting testimony	57
Map comments	147
<b>Total</b>	<b>361</b>

## **4.0 ISSUES AND ALTERNATIVES COMMENT SUMMARY**

Several common themes emerged from the public comments, which are presented by alternative in summary form below. These comments were used in the evaluation of the preliminary environmental impacts of the project and were incorporated into Port MacKenzie Rail Extension project Preliminary Environmental and Alternatives Report. Appendix B contains all comments received during the preliminary scoping process.

### **4.1 Willow**

The comments pertaining to the Willow corridor related to safety, trails, recreation, socio-economic impacts, and environmental impacts. Many comments referenced boaters, snowmachiners, dog mushers, cross country skiers, snowshoers, and small tour business operators as primary groups in the area who would be impacted. Concerns relating to environmental impacts included noise, wildlife, vegetation, stream and river crossings, and historic and cultural sites.

### **4.2 Houston**

Comments relating to the Houston corridor focused on developing the opportunity for an industrial park and potential impacts to existing development. There were concerns relating to private property, noise pollution, wetlands and hydrology, fire hazards, and impacts to wildlife such as calving moose in the spring and the sandhill cranes nesting during the summer.

### **4.3 Big Lake**

Comments regarding the Big Lake corridor ranged from long-term opportunities the extension might provide to support business development to concerns relating to private property such as “expected” decreases in property values and increases to congestion in the area.

### **4.4 Port MacKenzie West and East**

Comments included concerns on impacts to farms, disruption of productive fields, and keeping agricultural parcels intact. There was a preference to follow the Port MacKenzie Road as a suggested alternative.

### **4.5 Other Comments**

#### *4.5.1 Trails/Crossings*

Trails were a common theme in many of the comments and were recognized as important for recreational activities, such as dog mushing training and racing, and snowmachine riding. Trail access was a concern for all alternatives. Concerns focused on maintaining access across the railroad tracks and not consolidating the trails. There was a preference for more frequent rail crossings utilizing both underpasses and overpasses for trails.

#### *4.5.2 Wetlands/Watershed and Anadromous Streams*

Wetland areas were also highlighted in many comments. Some residents expressed concern that the rail embankment would become a barrier that would disrupt natural drainage, especially from Beaver, Horseshoe, and West lakes. Comments suggested the need to review flood hazard areas.

Anadromous streams, such as the outlet streams from Crooked Lake flowing into the Little Susitna River, were also highlighted as areas of concern.

#### *4.5.3 Other Corridors*

A small number of comments suggested other corridors. These suggestions focused on following the existing Knik Goose Bay Road and also following the natural boundary of the Knik Arm.

#### *4.5.4 Purpose and Need*

Purpose and Need comments focused on commodity availability and reliability of the Agrium plant as a potential market. There was concern expressed that not enough information was known about the economic forecast and market availability.

## **5.0 KEY ISSUES**

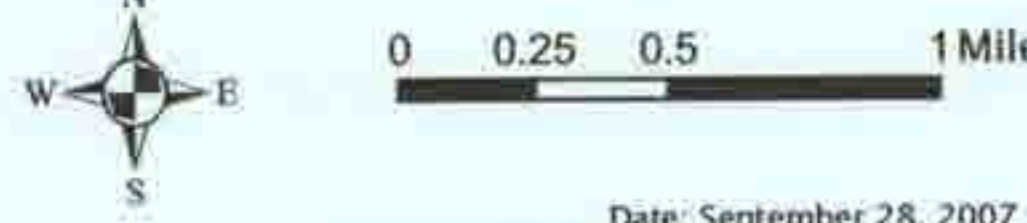
The key issues developed through the initial public involvement activities and to be considered further include:

- Traffic modeling
- Property development
- Conduct environmental analysis
- Conduct financial feasibility study to determine how the funds will be acquired
- Archeological survey of the Red Shirt Lake area
- Economic impact study
- Noise
- Safety
- Habitat (mammal and bird)
- Surface water and drainage impacts
- Social impacts



- Legend**
- Preliminary Alternatives\***
- Mac East
  - Mac West
  - Conn 1
  - Conn 2
  - Conn 3
  - Houston
  - Houston North
  - Houston South
  - Willow
  - Big Lake
  - ARRC Existing Track
  - Point Mackenzie Agricultural Area
  - Port Mackenzie District
  - Park or Refuge
  - Parcel Boundary

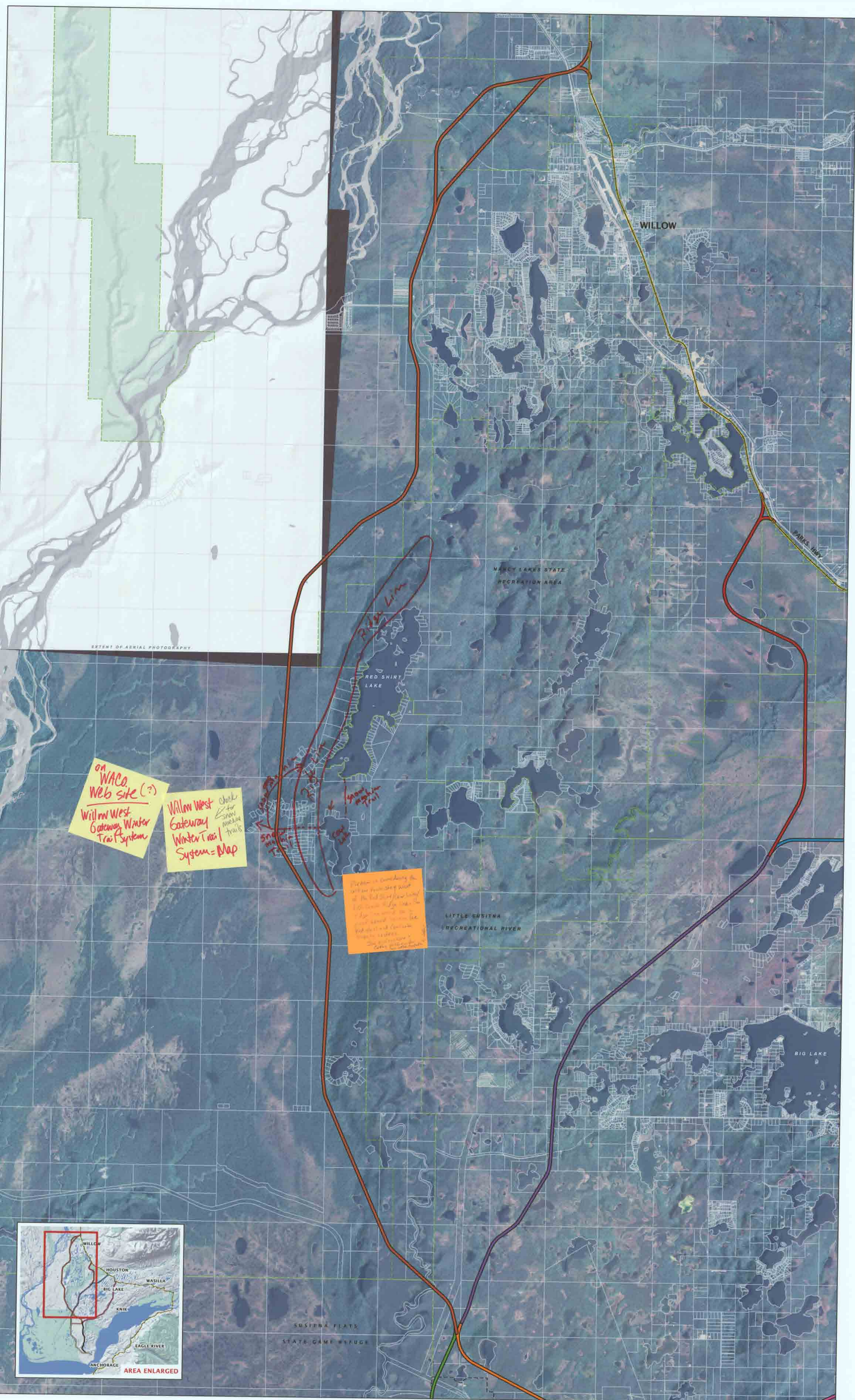
\*These lines generally represent corridors which are subject to future refinement.



Date: September 28, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ARRC, HDR Alaska, Inc., MSB GIS, TNH-Hanson.

WASILLA - A MEETING

MAP 1



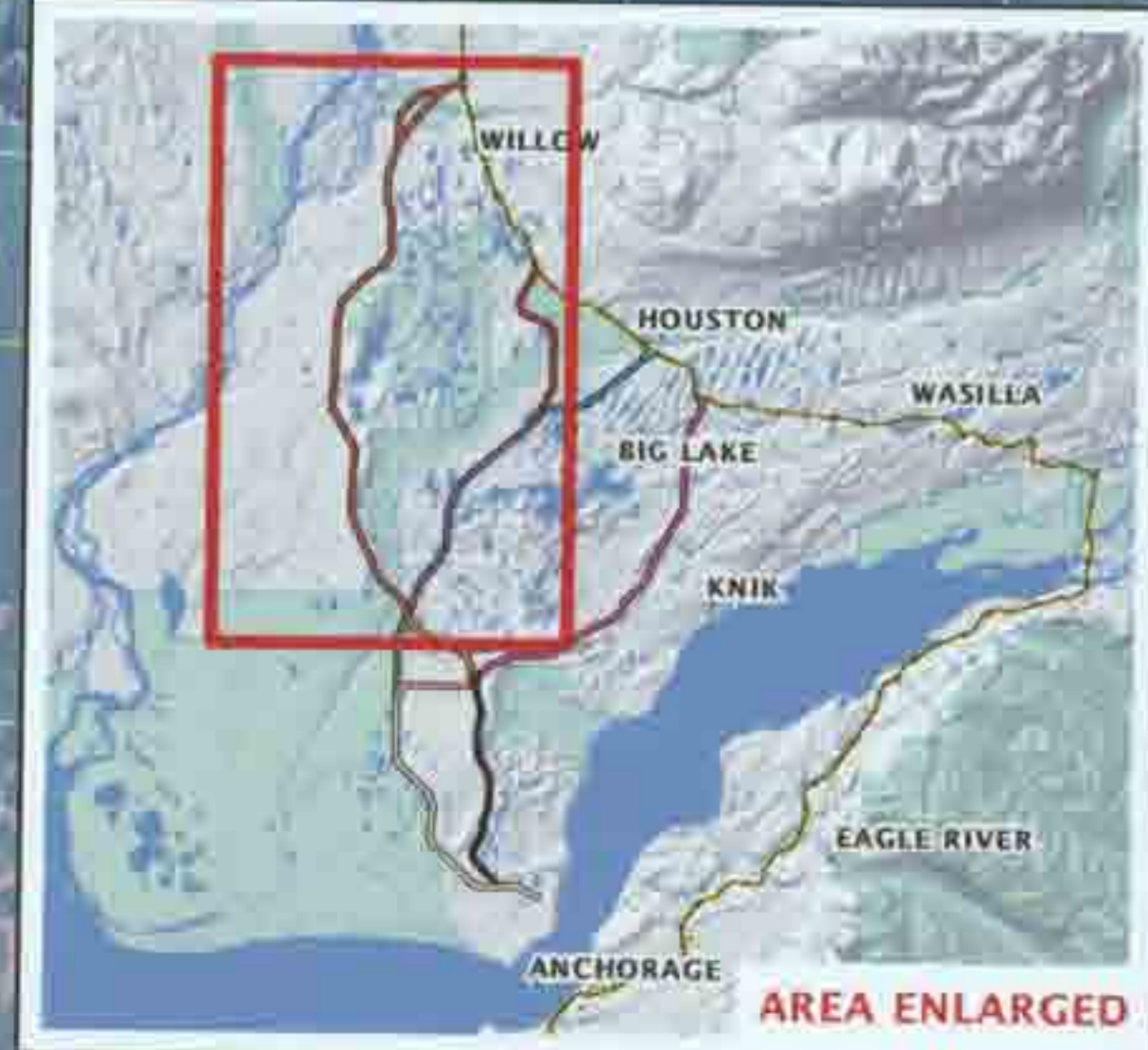
EXTENT OF AERIAL PHOTOGRAPHY

on WACO Web site (?)  
Willow West Gateway Winter Trail System

Willow West Gateway Winter Trail System = Map  
check for snow machine trails

Handwritten notes in red and black ink, including 'Ridge Line', 'Snow Machine Trail', and 'Snow Machine Trail'.

Problem in connecting the Willow West Gateway Winter Trail System to the existing rail line. The solution is to build a new bridge over the Little Susitna River. The bridge will be built on the site of the old bridge. The old bridge was built in 1960 and is now in poor condition. The new bridge will be built on a new site and will be a modern structure. The bridge will be built on the site of the old bridge. The old bridge was built in 1960 and is now in poor condition. The new bridge will be built on a new site and will be a modern structure.

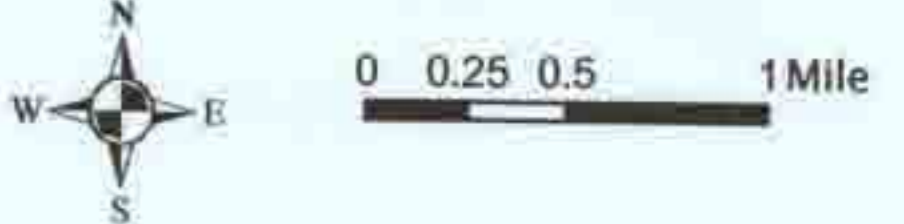


**Legend**

- Preliminary Alternatives\*
  - Mac East
  - Mac West
  - Conn 1
  - Conn 2
  - Conn 3
  - Houston
  - Houston North
  - Houston South
  - Willow
  - Big Lake
  - ARRC Existing Track
  - Point MacKenzie Agricultural Area
  - Port MacKenzie District
  - Park or Refuge
  - Parcel Boundary

\*These lines generally represent corridors which are subject to future refinement.

MAP 2



Date: September 28, 2007  
Projection: Alaska State Plane Zone 4, NAD 83  
Author: HDR Alaska, Inc.  
Sources: ARRC, HDR Alaska, Inc., MSB GIS, TNH-Hanson.



- Legend**
- |                           |               |               |                                   |
|---------------------------|---------------|---------------|-----------------------------------|
| Preliminary Alternatives* | Conn 2        | Houston South | ARRC Existing Track               |
| Mac East                  | Conn 3        | Willow        | Point MacKenzie Agricultural Area |
| Mac West                  | Houston       | Big Lake      | Port MacKenzie District           |
| Conn 1                    | Houston North |               | Park or Refuge                    |
|                           |               |               | Parcel Boundary                   |

\*These lines generally represent corridors which are subject to future refinement.

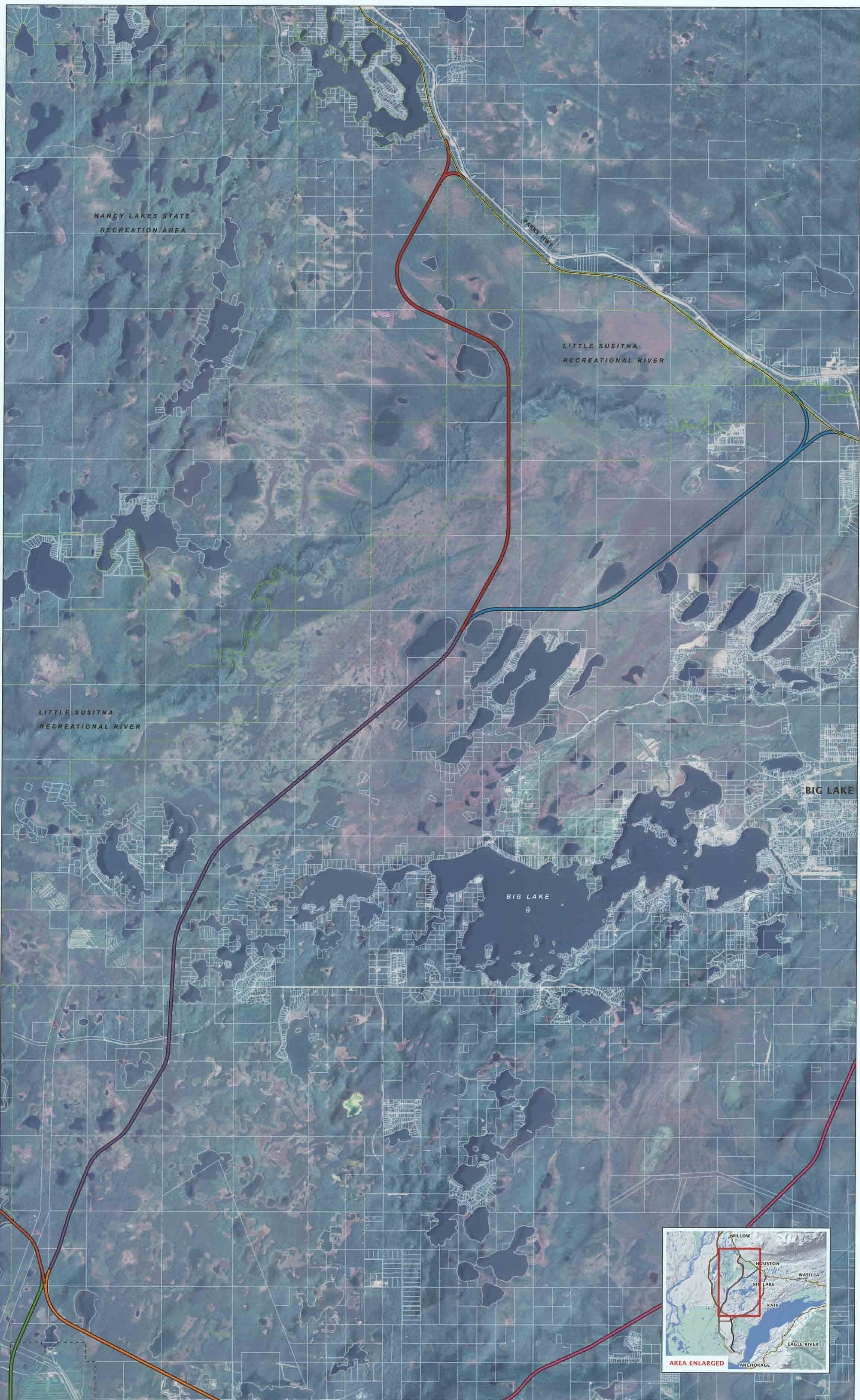


Date: September 28, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ARRC, HDR Alaska, Inc., MSB GIS, TNH-Hanson.

Willow

map 3





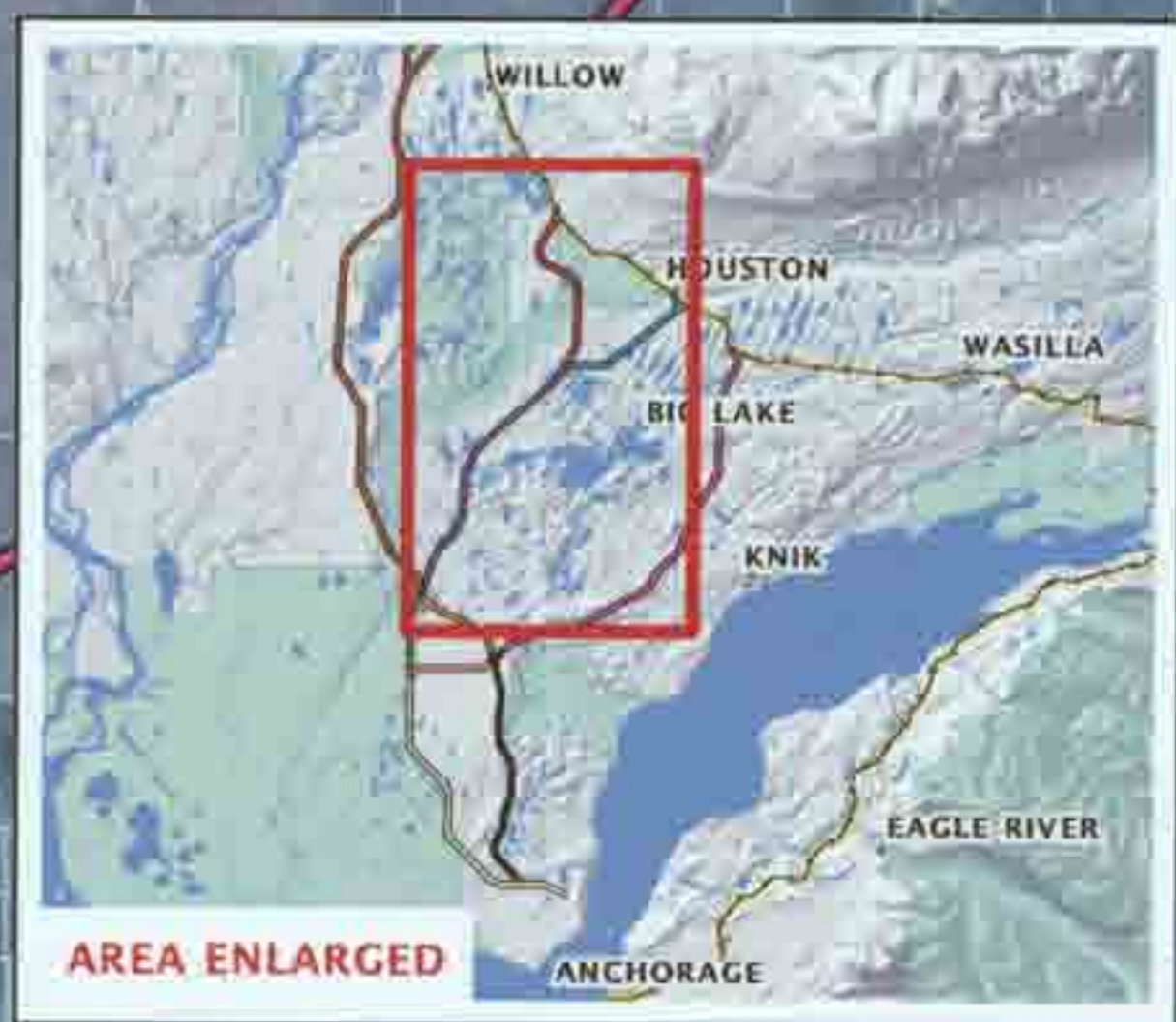
NANCY LAKES STATE RECREATION AREA

LITTLE SUSITNA RECREATIONAL RIVER

LITTLE SUSITNA RECREATIONAL RIVER

BIG LAKE

BIG LAKE



Legend

- Preliminary Alternatives\*
  - Mac East
  - Mac West
  - Conn 1
  - Conn 2
  - Conn 3
  - Houston
  - Houston North
  - Houston South
  - Willow
  - Big Lake
- ARRC Existing Track
- Point MacKenzie Agricultural Area
- Port MacKenzie District
- Park or Refuge
- Parcel Boundary

\*These lines generally represent corridors which are subject to future refinement.



0 0.25 0.5 1 Mile

Date: September 28, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ARRC; HDR Alaska, Inc.; MSB GIS; TNH-Hanson.

PORT MACKENZIE RAIL EXTENSION

WASILLA-C MEETING

MAP 4





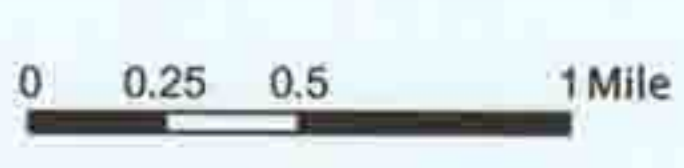
*This is not an open port in the winter.*



**Legend**

- |                           |               |               |                                   |
|---------------------------|---------------|---------------|-----------------------------------|
| Preliminary Alternatives* | Conn 2        | Houston South | ARRC Existing Track               |
| Mac East                  | Conn 3        | Willow        | Point MacKenzie Agricultural Area |
| Mac West                  | Houston       | Big Lake      | Port MacKenzie District           |
| Conn 1                    | Houston North |               | Park or Refuge                    |
|                           |               |               | Parcel Boundary                   |

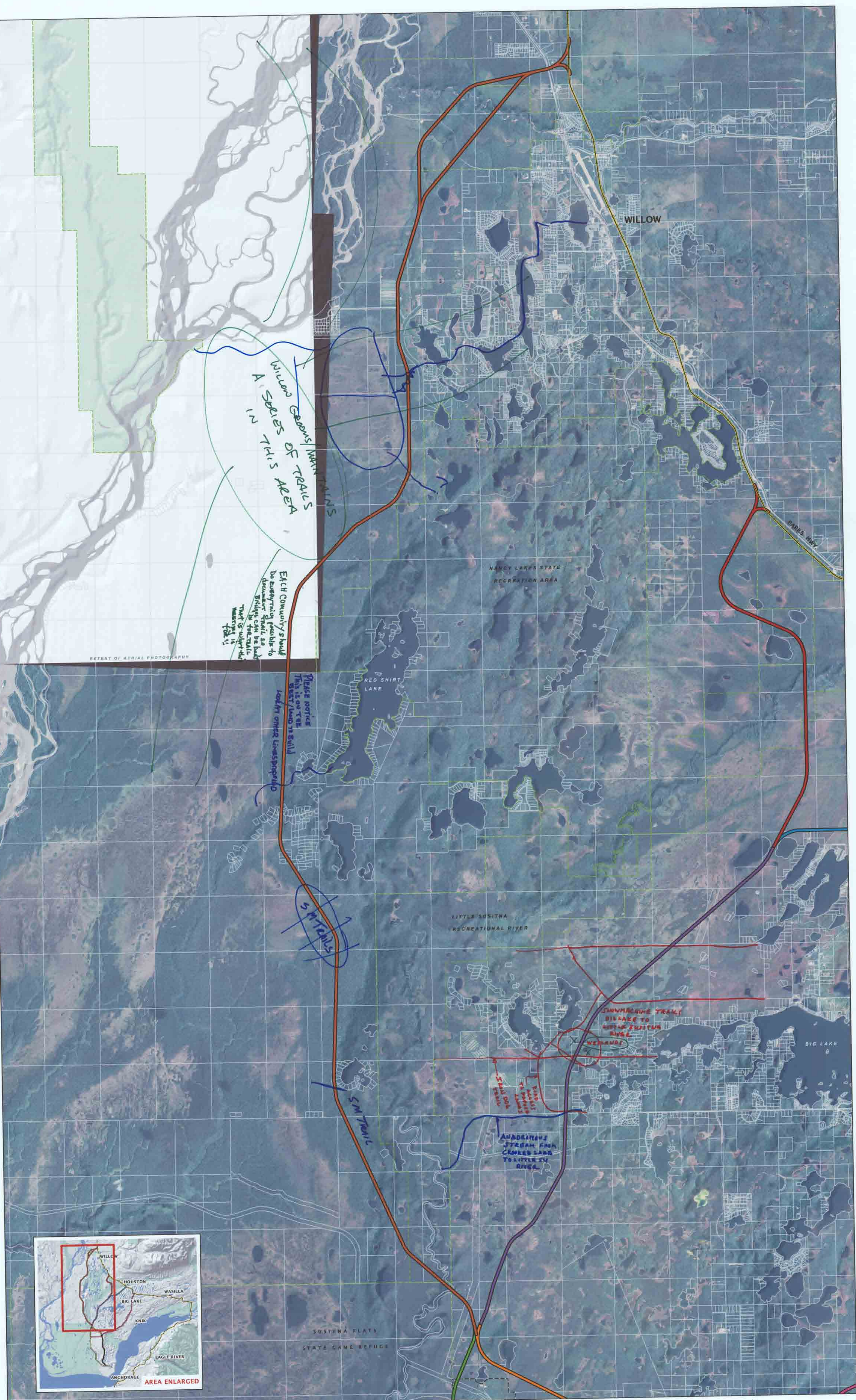
\*These lines generally represent corridors which are subject to future refinement.



Date: September 28, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ARRC, HDR Alaska, Inc., MSB GIS, TNH-Hanson.



*MAP 1*



WILLOW  
 A SERIES OF TRAILS  
 IN THIS AREA

Each Community should  
 be responsible for  
 determining feasible  
 alternatives to  
 the rail line  
 that is subject  
 to the  
 permit

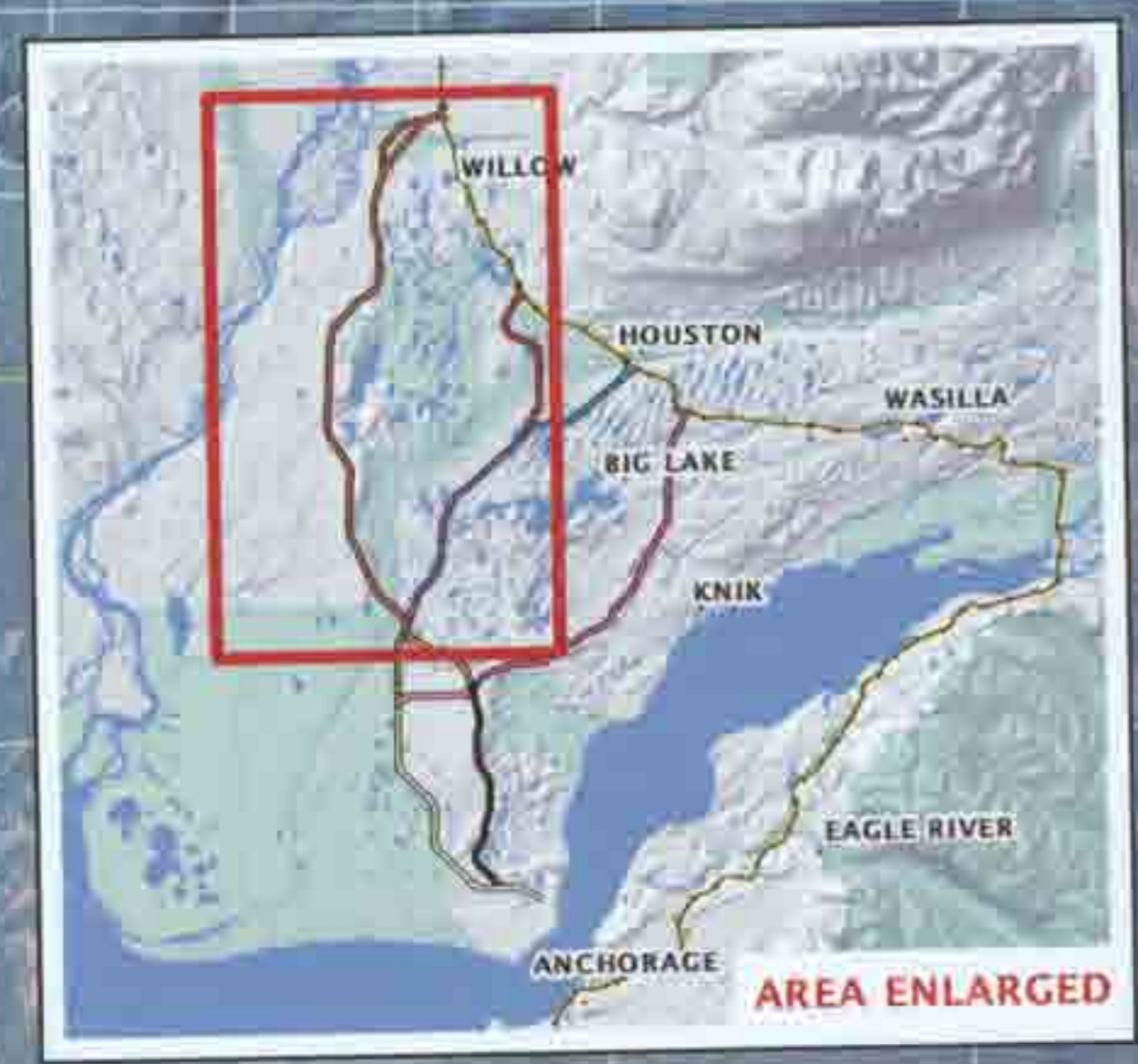
Please notice  
 this is on the  
 west side of  
 the road  
 look for other lines proposed

5-17-08

SM Trail

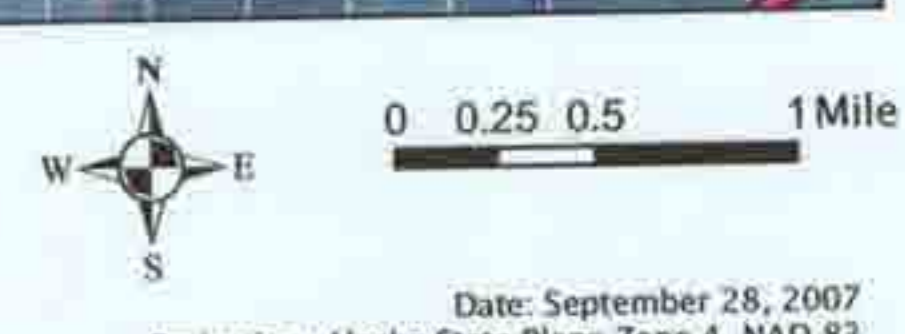
SHANNON TRAIL  
 BRIDGE TO  
 LITTLE SUSITNA  
 RIVER  
 WETLANDS

ANADIRIUM  
 STREAM FROM  
 CROWER LAKE  
 TO LITTLE SV  
 RIVER



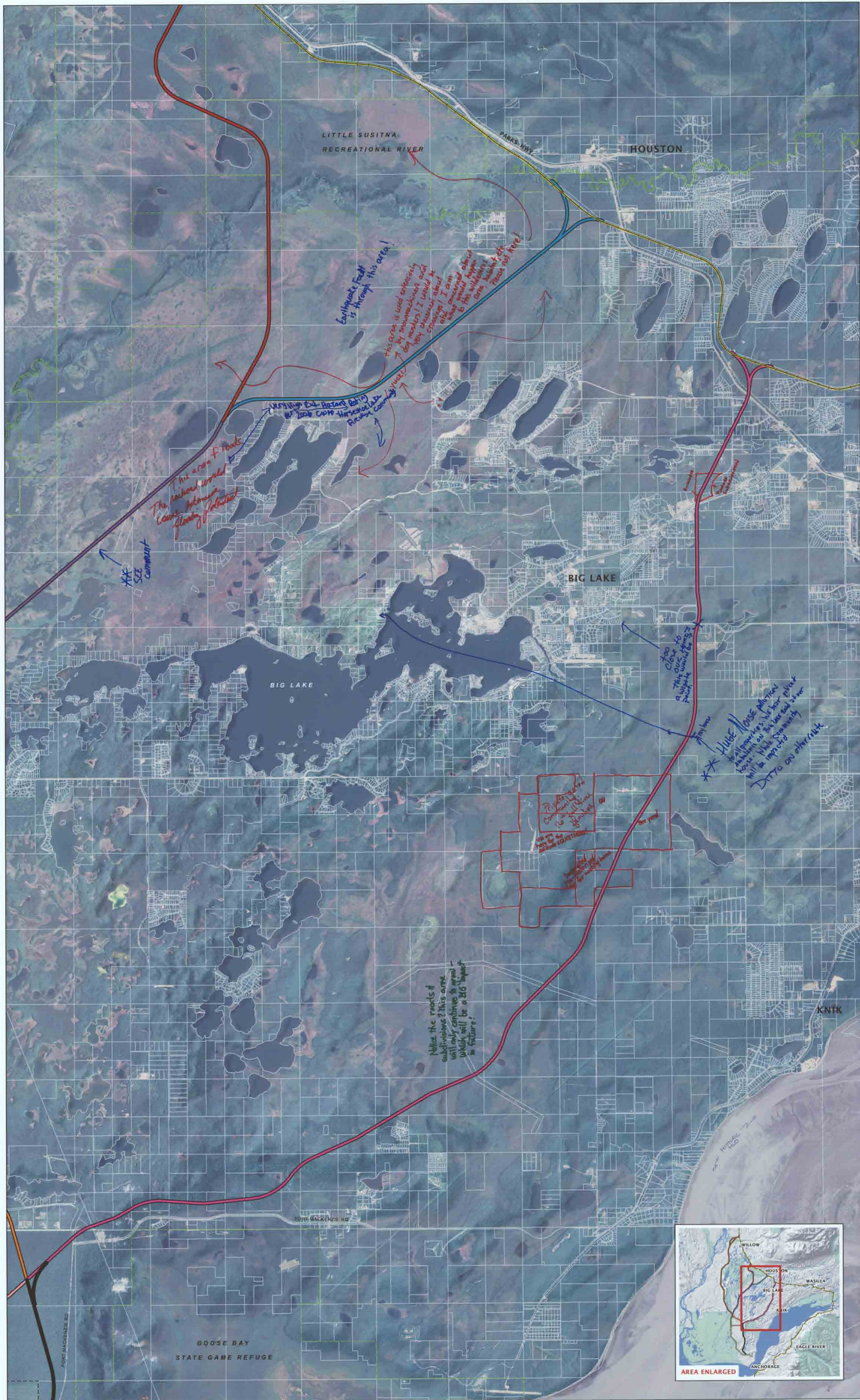
- Legend**
- Preliminary Alternatives\*
    - Conn 2
    - Mac East
    - Mac West
    - Conn 1
    - Houston South
    - Willow
    - Big Lake
    - ARRC Existing Track
    - Point Mackenzie Agricultural Area
    - Port Mackenzie District
    - Park or Refuge
    - Parcel Boundary

\*These lines generally represent corridors which are subject to future refinement.



Date: September 28, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ARRC, HDR Alaska, Inc., MSB GIS, TNR-Hanson.

MAP 2



Legend

- Preliminary Alternatives\***
  - Conn 2
  - Conn 3
  - Mac East
  - Mac West
  - Conn 1
  - Houston
  - Houston North
  - Houston South
  - Willow
  - Big Lake
- ARRC Existing Track
- Point MacKenzie Agricultural Area
- Port MacKenzie District
- Park or Refuge
- Parcel Boundary

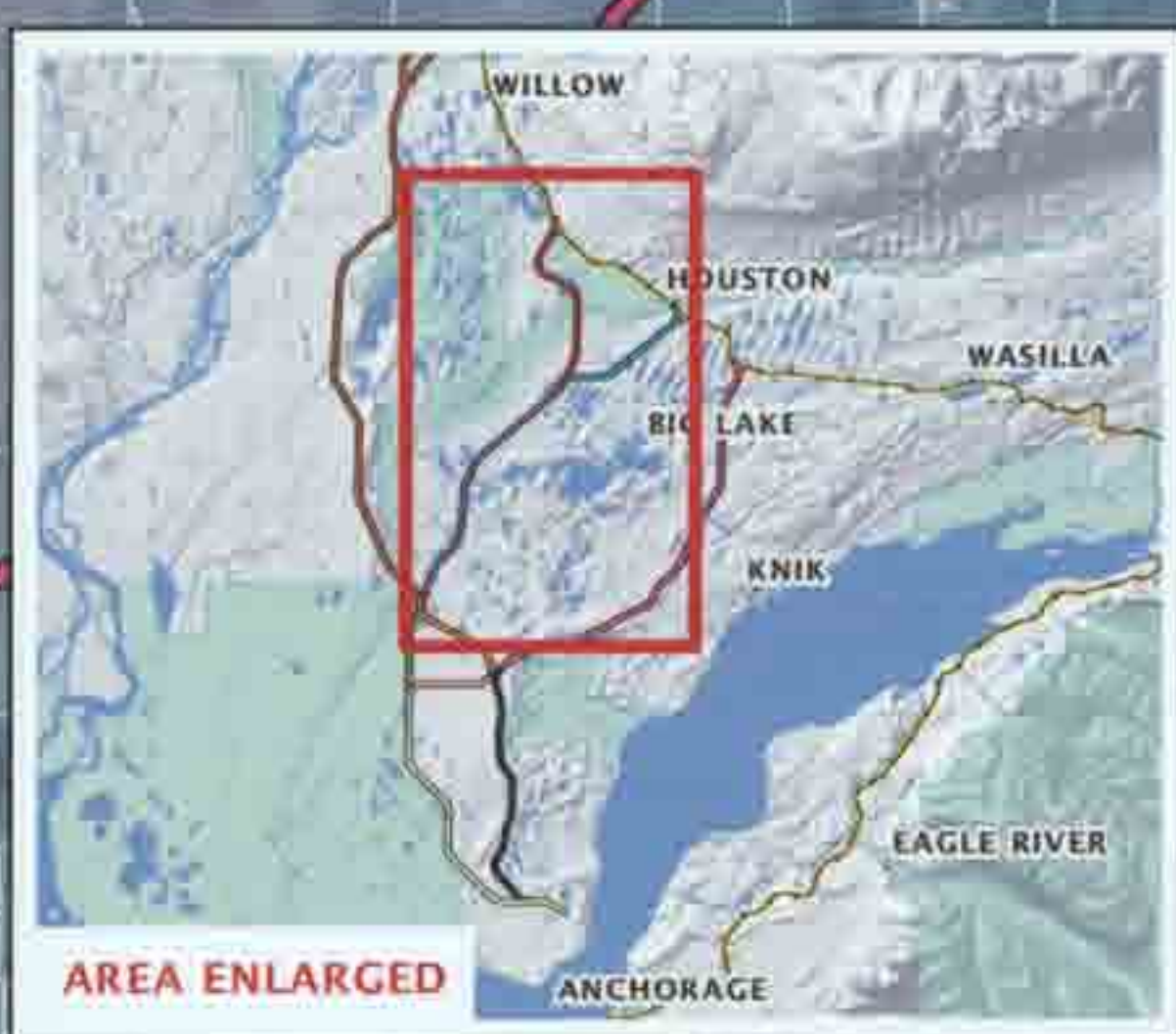
\*These lines generally represent corridors which are subject to future refinement.



MAPP3

Date: September 28, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ARRC, HDR Alaska, Inc., MSB GIS, TNH-Hanson.





- Legend**
- |          |               |                                   |
|----------|---------------|-----------------------------------|
| Conn 2   | Houston South | ARRC Existing Track               |
| Conn 3   | Willow        | Point MacKenzie Agricultural Area |
| Mac East | Big Lake      | Port MacKenzie District           |
| Mac West | Houston North | Park or Refuge                    |
| Conn 1   |               | Parcel Boundary                   |
- \*These lines generally represent corridors which are subject to future refinement.



*MADY*

*BIG LAKE MEETING-C*

Date: September 28, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ARRC, HDR Alaska, Inc., MSB GS, TNH-Hanson.



Handwritten note: snowmachine parking lot

Handwritten note: Why is a route not being considered to run on the opposite side of knik arm from the existing railroad + connecting to the existing railroad just south or around just south of 150. I know this would

Handwritten note: require updating crossings - but isn't that just a cost of doing business? I agree. Stay close to existing roads. Wouldn't that help minimize environmental impact?

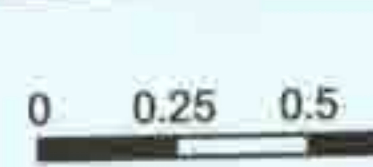


- Legend**
- Preliminary Alternatives\*
    - Mac East
    - Mac West
    - Conn 1
    - Conn 2
    - Conn 3
    - Houston
    - Houston North
    - Houston South
    - Willow
    - Big Lake
    - ARRC Existing Track
    - Point Mackenzie Agricultural Area
    - Port Mackenzie District
    - Park or Refuge
    - Parcel Boundary

\*These lines generally represent corridors which are subject to future refinement.

Willow

MAP 1



Date: September 28  
Projection: Alaska State Plane Zone 4, N  
Author: HDR Alaska  
Sources: ARRC, HDR Alaska, MSB GIS, TNH



What are you going to give us when you take our park?

The route heavily impacts our favorite swamp. We need a better alternative.

Let's let Willow County know we're here. We'll need a better alternative.

Wetlands are a natural feature of the area. We need to preserve them.

Wetlands are a natural feature of the area. We need to preserve them.

Wetlands are a natural feature of the area. We need to preserve them.

Give us trail signs please to cross the trail.

For your consideration, we have several alternatives along the trail. We need to preserve them.

\* Lines indicated in red represent the trail alternatives. \* See Willow-Trail Plan for better representation.

\* Corridor 3 will need to be preserved for trail access as well as roads 1, 2 & 3.

This is Willow, not Houston!

You should put the Nancy Lake State Recreation Area boundary more clearly on all maps to make it obvious how the routes would impact it. That Rec Area is mostly NON-MATURIZED and a RR is at odds with the management intent.

How could it possibly be feasible to build a RR through all those wetlands (Willow corridor) when there are other options? - 2nd time around!!! Willow RR would have more wetlands impact.

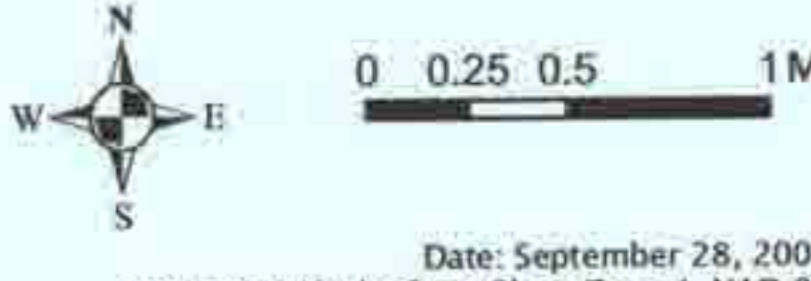
Legend

- Preliminary Alternatives\*
Mac East
Mac West
Conn 1
Conn 3
Houston
Houston North
ARRC Existing Track
Point Mackenzie Agricultural Area
Port Mackenzie District
Park or Refuge
Parcel Boundary

\*These lines generally represent corridors which are subject to future refinement.

Willow

MAP 2





Legend

- Preliminary Alternatives\***
- Mac East
  - Mac West
  - Conn 1

- Conn 2
- Conn 3
- Houston
- Houston North

- Houston South
- Willow
- Big Lake

- ARRC Existing Track
- Point MacKenzie Agricultural Area
- Port MacKenzie District
- Park or Refuge
- Parcel Boundary

\*These lines generally represent corridors which are subject to future refinement.



Date: September 28, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ARRC, HDR Alaska, Inc., MSB GIS, THT-Hanson.

Willow

map 3



- Legend**
- Preliminary Alternatives\***
    - Mac East
    - Mac West
    - Conn 1
    - Conn 2
    - Conn 3
    - Houston
    - Houston North
    - Houston South
    - Willow
    - Big Lake
  - ABRC Existing Track
  - Point MacKenzie Agricultural Area
  - Port MacKenzie District
  - Park or Refuge
  - Parcel Boundary

\*These lines generally represent corridors which are subject to future refinement.

Willow

map 4



0 0.25 0.5 1 Mile

Date: September 28, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ARRC, HDR Alaska, Inc., MSB GIS, TNH-Hanson.

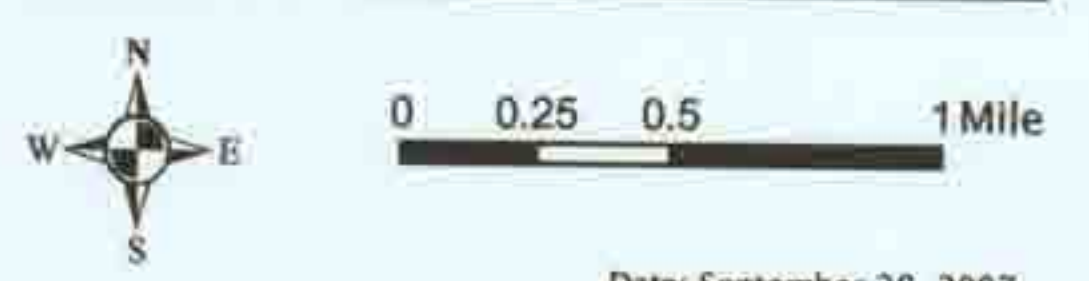




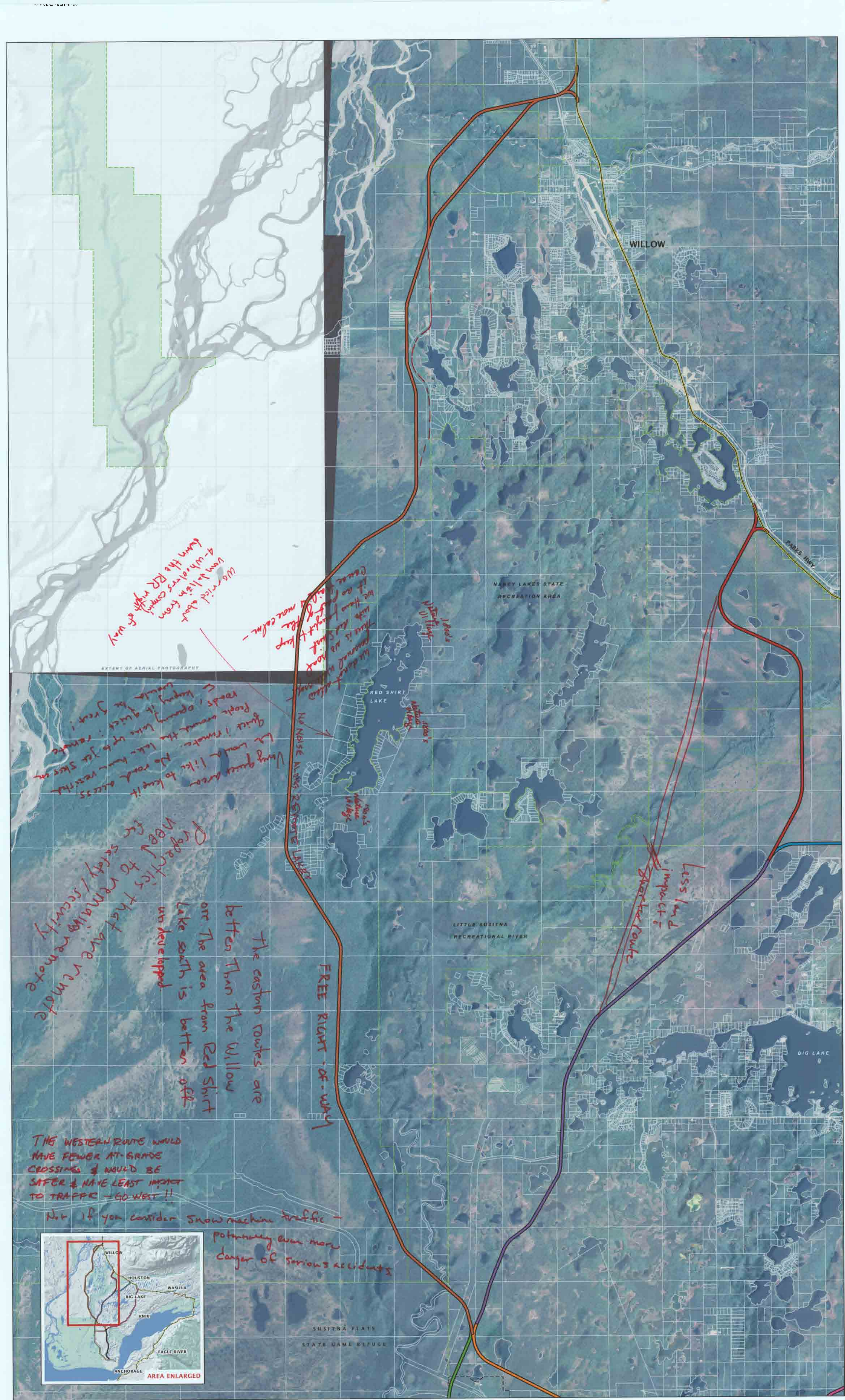
**Legend**

Preliminary Alternatives* Mac East	Conn 2	Houston South	ARRC Existing Track
Mac West	Conn 3	Willow	Point Mackenzie Agricultural Area
Mac Conn 1	Houston North	Big Lake	Port Mackenzie District
			Park or Refuge
			Parcel Boundary

\*These lines generally represent corridors which are subject to future refinement.



Date: September 28, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ARRC, HDR Alaska, Inc., MSB GIS, TNH-Hanson.



Worried about  
4-whole's camp's  
down the RR right of way

There is no noise  
there is no noise  
there is no noise  
there is no noise  
there is no noise  
there is no noise  
there is no noise  
there is no noise  
there is no noise  
there is no noise

Very quiet area  
No road access  
No noise along 2-lane road  
People around the lake up to get  
quiet around the lake up to get  
quiet around the lake up to get  
quiet around the lake up to get  
quiet around the lake up to get  
quiet around the lake up to get  
quiet around the lake up to get  
quiet around the lake up to get  
quiet around the lake up to get  
quiet around the lake up to get

Properties that are remote  
need to remain remote  
for safety / security

The eastern routes are  
better than the Willow  
one. The area from Red Shirt  
Lake south is better off  
undeveloped

THE WESTERN ROUTE WOULD  
HAVE FEWER AT-GRADE  
CROSSINGS & WOULD BE  
SAFER & HAVE LEAST IMPACT  
TO TRAFFIC - GO WEST !!

Not if you consider snow machine traffic -  
potentially even more  
danger of serious accidents



- Legend**
- Preliminary Alternatives\*
    - Mac East
    - Mac West
    - Conn 1
    - Conn 2
    - Conn 3
    - Houston
    - Houston North
    - Houston South
    - Willow
    - Big Lake
  - ARRC Existing Track
  - Point MacKenzie Agricultural Area
  - Port MacKenzie District
  - Park or Refuge
  - Parcel Boundary

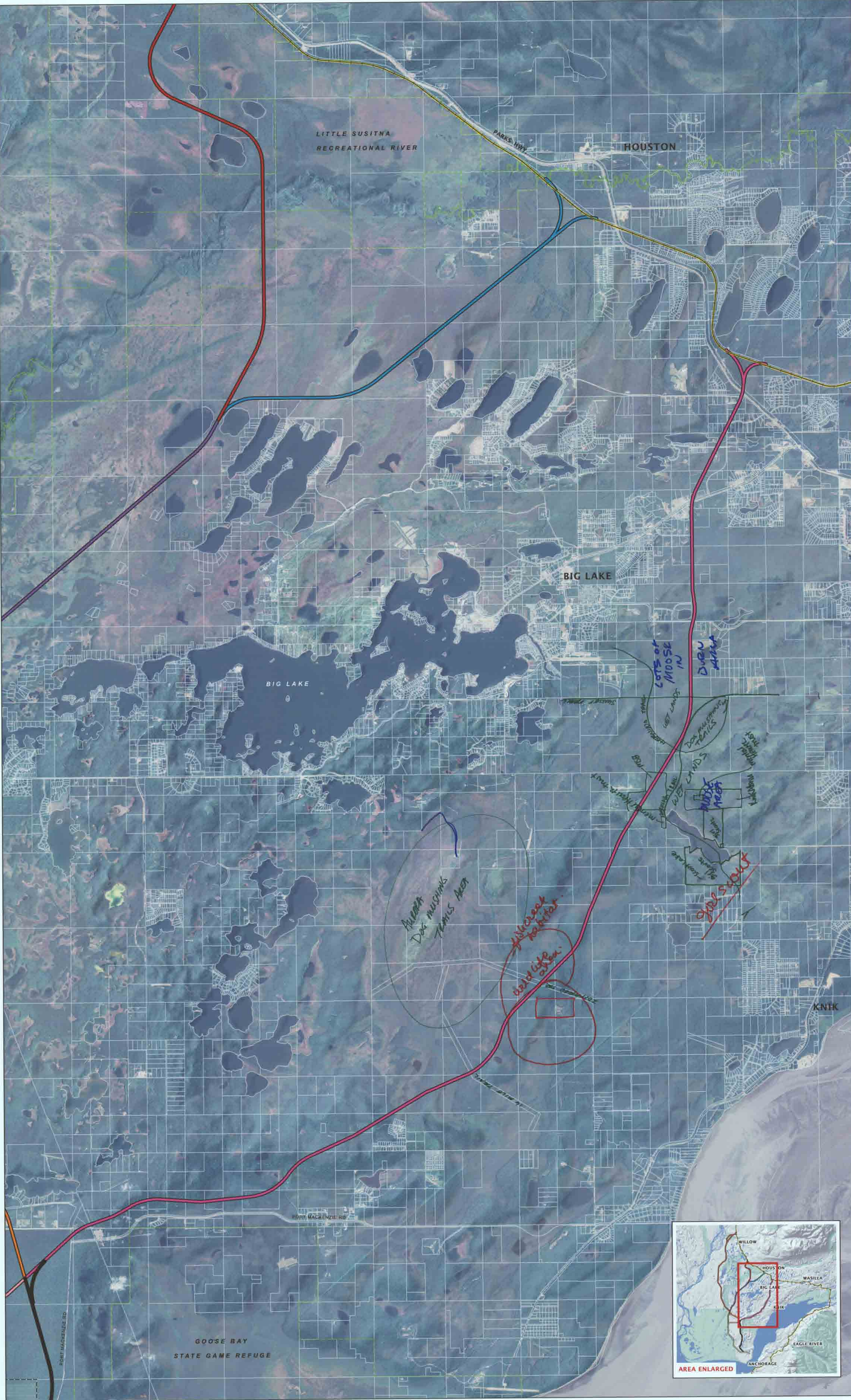
\*These lines generally represent corridors which are subject to future refinement.



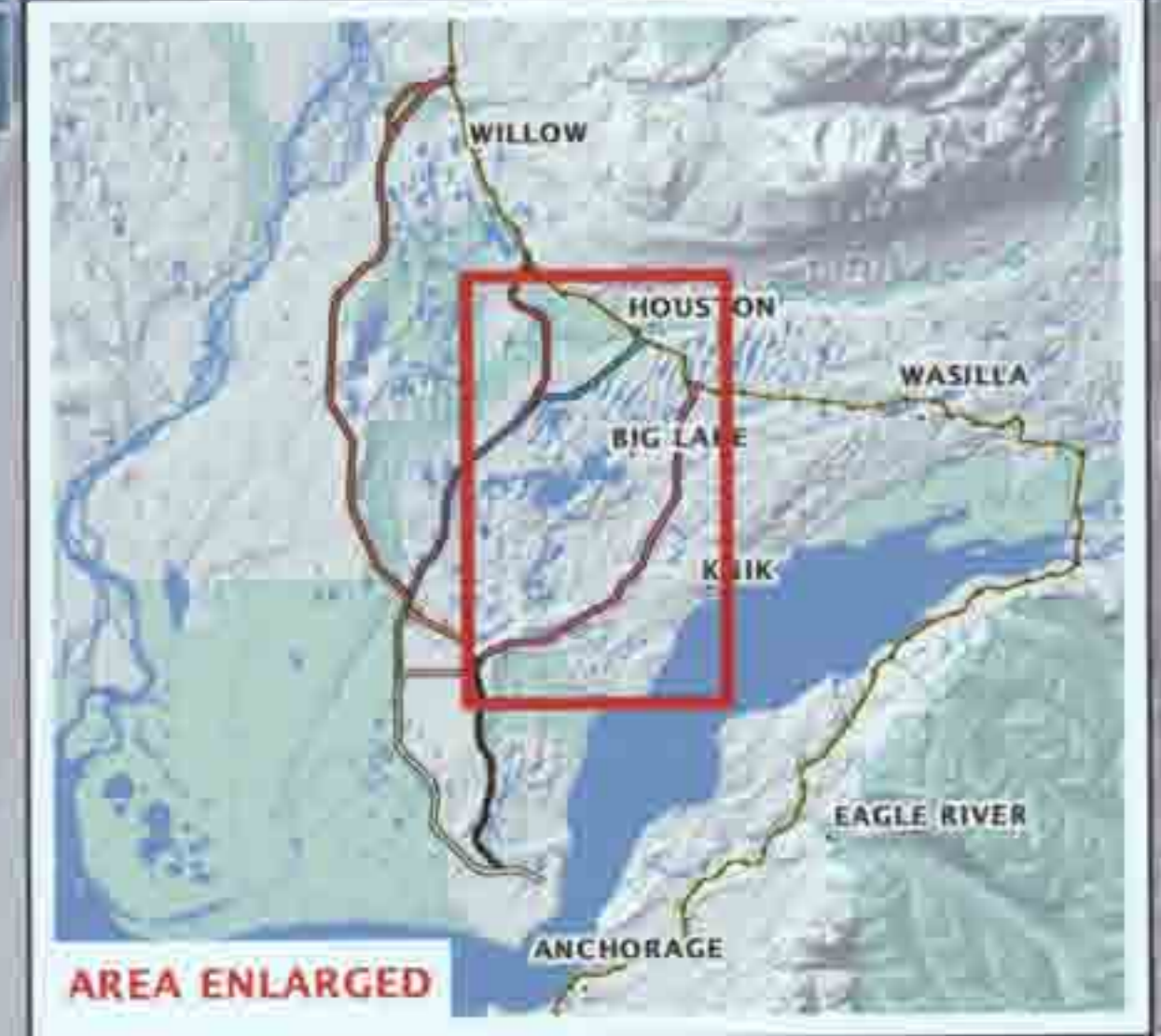
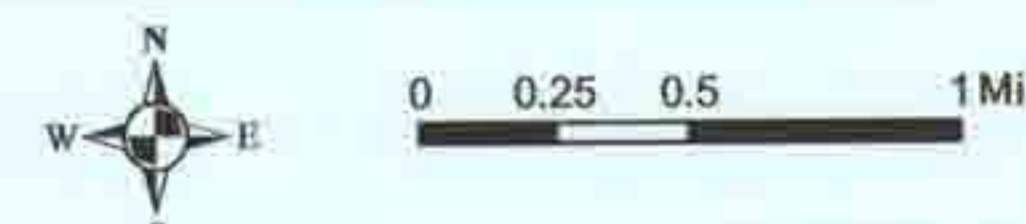
Date: September 28, 2007  
Projection: Alaska State Plane Zone 4, NAD 83  
Author: HDR Alaska, Inc.  
Sources: ARRC, HDR Alaska, Inc., MSB GIS, TMI-Hanson

Oct 4, 2007  
KNIK

MAP 2

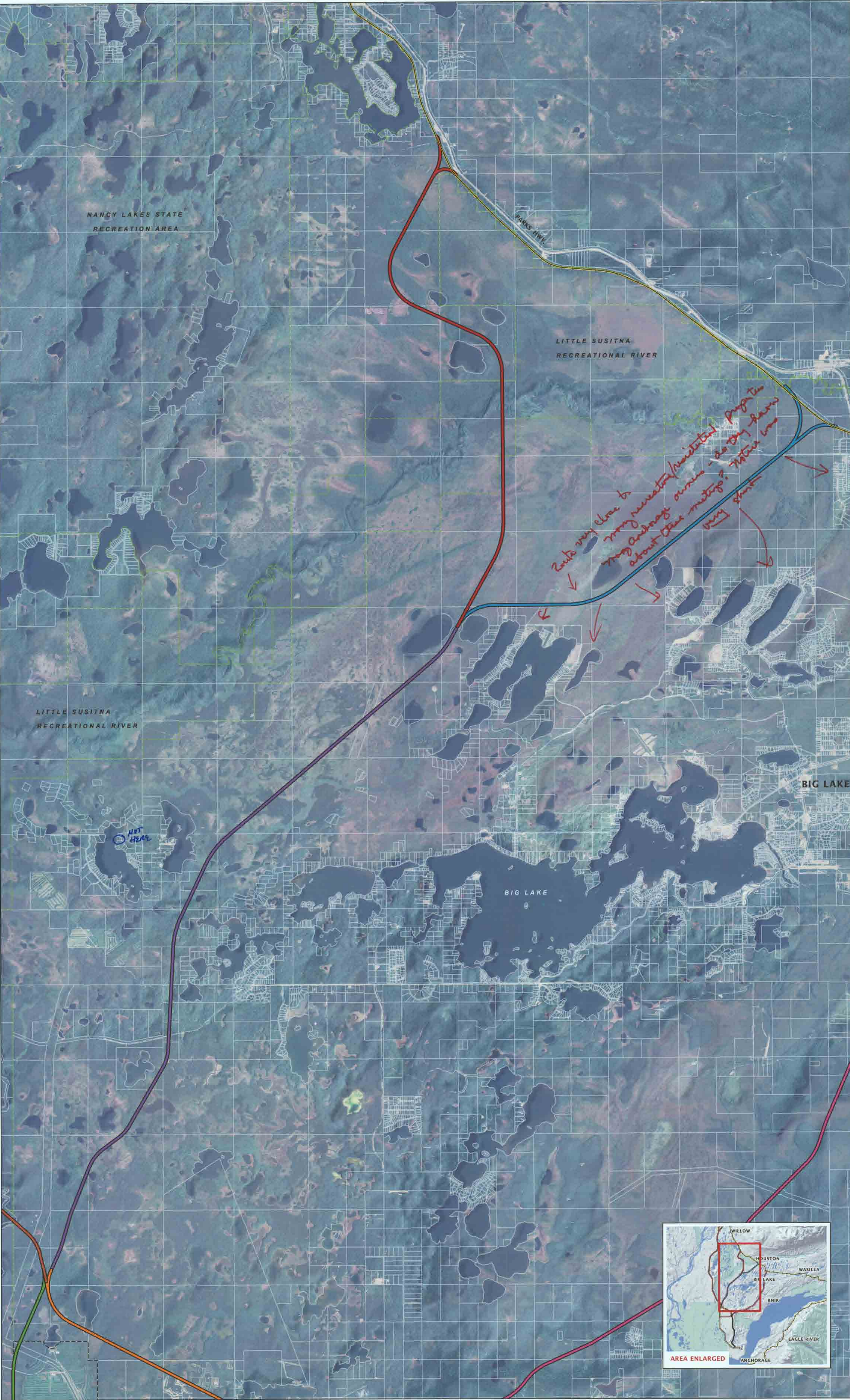


- Legend**
- |                           |               |                |                                   |
|---------------------------|---------------|----------------|-----------------------------------|
| Preliminary Alternatives* | Conn 2        | Houston South  | ARRC Existing Track               |
| Mac East                  | Conn 3        | Willow         | Point MacKenzie Agricultural Area |
| Mac West                  | Houston       | Big Lake       | Port MacKenzie District           |
| Conn 1                    | Houston North | Park or Refuge | Parcel Boundary                   |
- \*These lines generally represent corridors which are subject to future refinement.



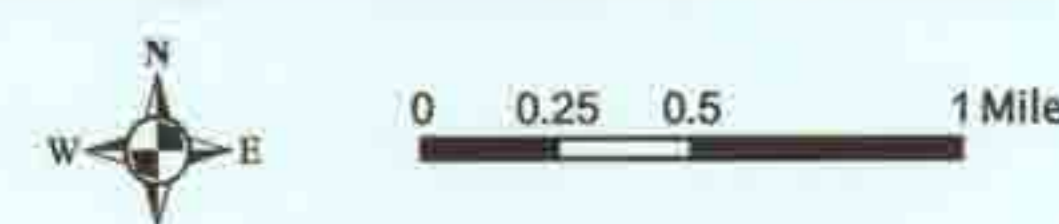
Date: September 28, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ARRC, HDR Alaska, Inc., MSB GIS, TNH-Hanson.

Oct 4, 2007  
 KNIK  
 map 3



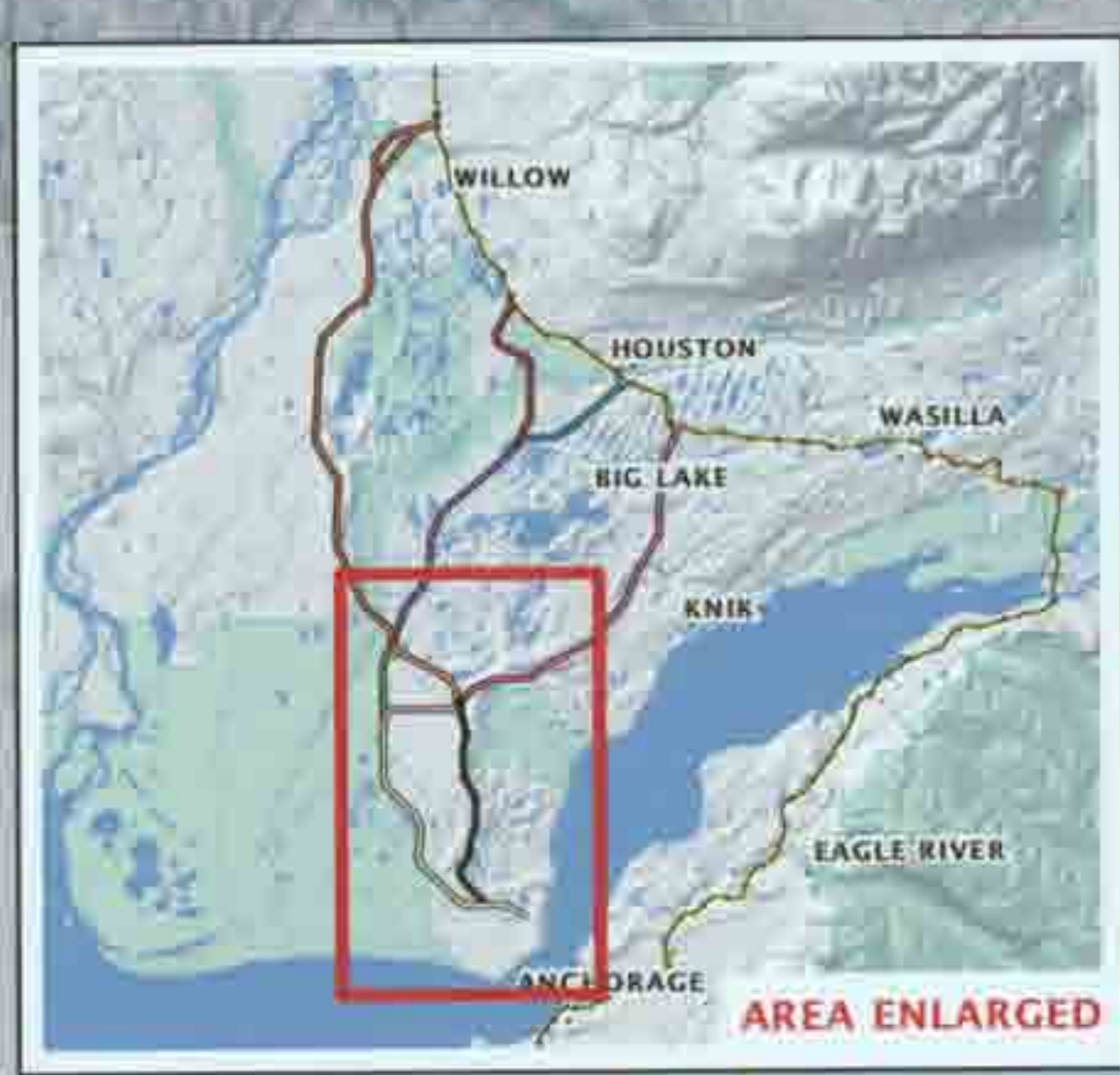
- Legend**
- Preliminary Alternatives\*
    - Mac East
    - Mac West
    - Conn 1
    - Conn 2
    - Conn 3
    - Houston
    - Houston North
  - Houston South
  - Willow
  - Big Lake
  - ARRC Existing Track
  - Point MacKenzie Agricultural Area
  - Port MacKenzie District
  - Park or Refuge
  - Parcel Boundary

\*These lines generally represent corridors which are subject to future refinement.



Date: September 28, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ARRC, HDR Alaska, Inc., MSB GIS, TNH-Hanson.

Oct 4, 2007 knik Map 4



- Legend**
- |                           |               |                |                                   |
|---------------------------|---------------|----------------|-----------------------------------|
| Preliminary Alternatives* | Conn 2        | Houston South  | ARRC Existing Track               |
| Mac East                  | Conn 3        | Willow         | Point Mackenzie Agricultural Area |
| Mac West                  | Houston       | Big Lake       | Port Mackenzie District           |
| Conn 1                    | Houston North | Park or Refuge | Parcel Boundary                   |

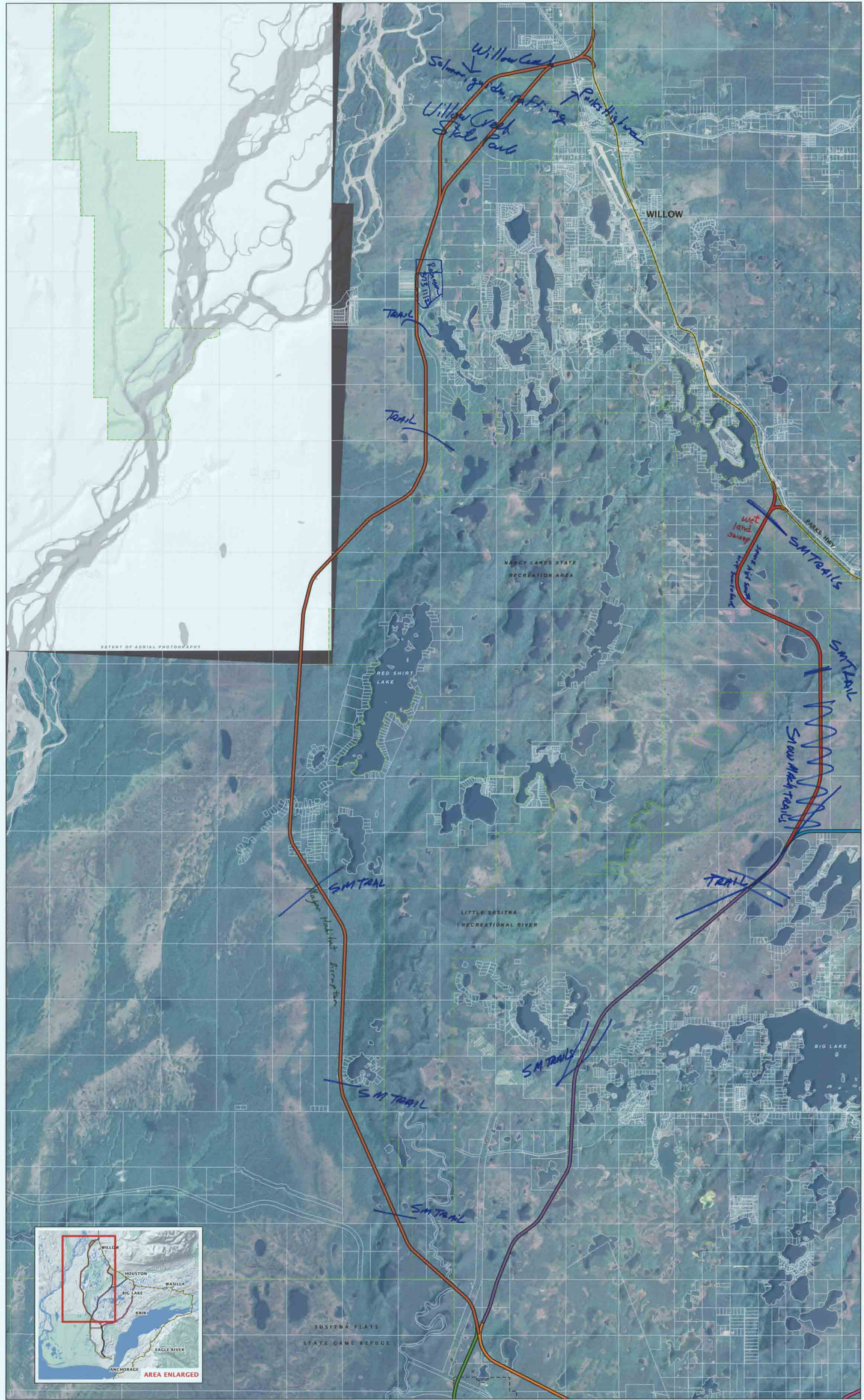
\*These lines generally represent corridors which are subject to future refinement.



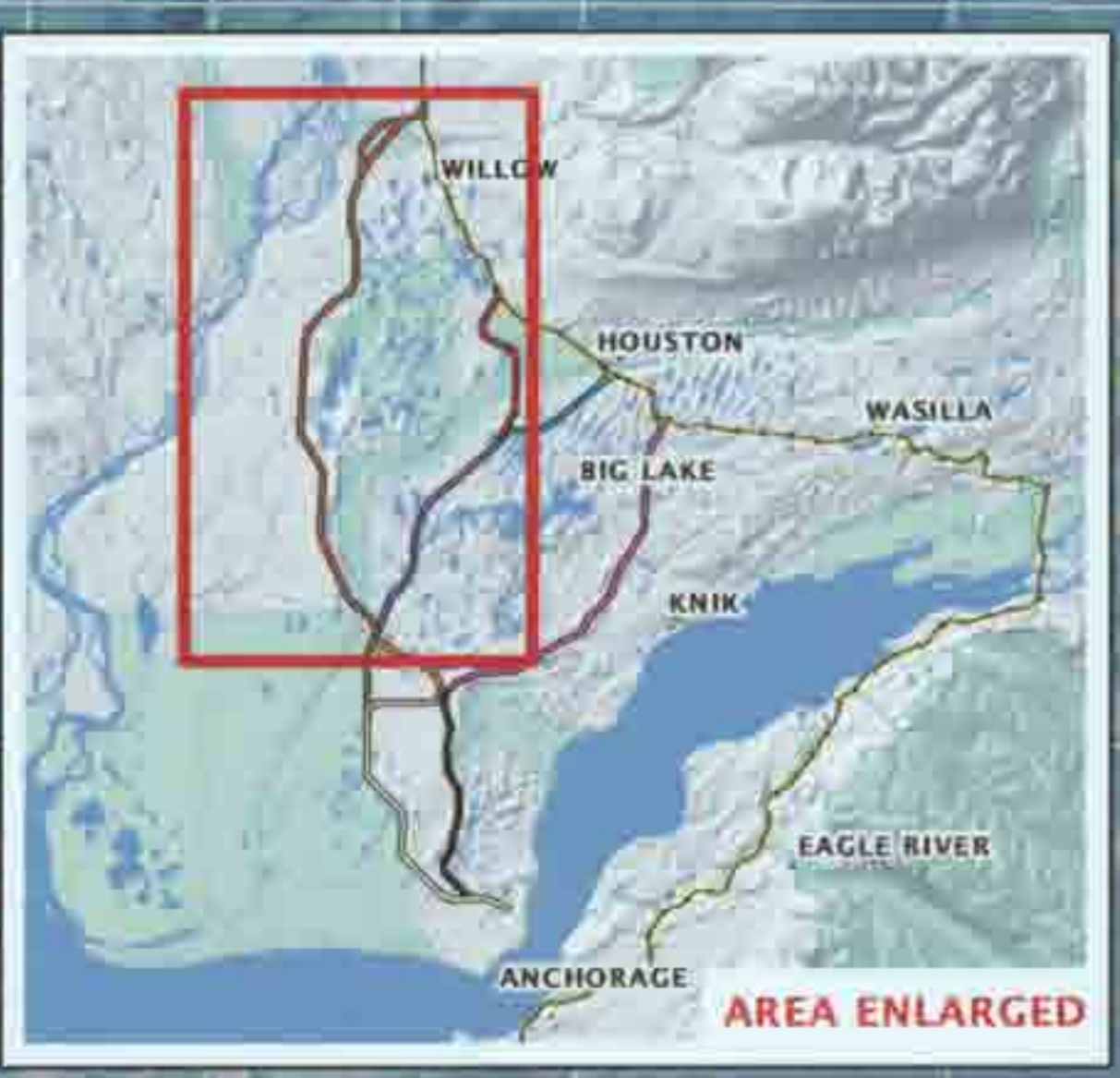
Date: September 28, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ARRC, HDR Alaska, Inc., MSB GIS, TNH-Hanson.

**PORT MACKENZIE**  
RAIL EXTENSION

*Houston A 10/5/07*      *MAP 1*

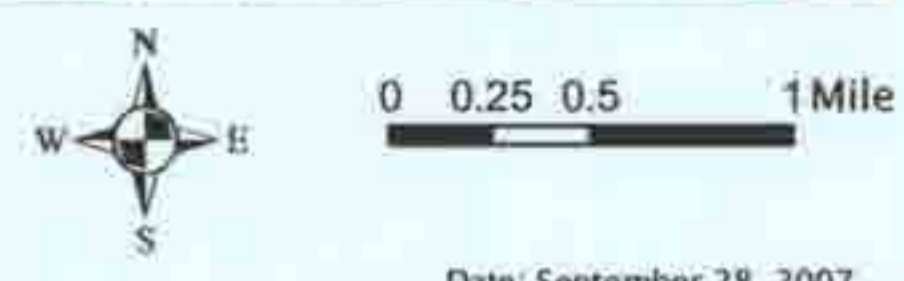


EXTENT OF AERIAL PHOTOGRAPHY



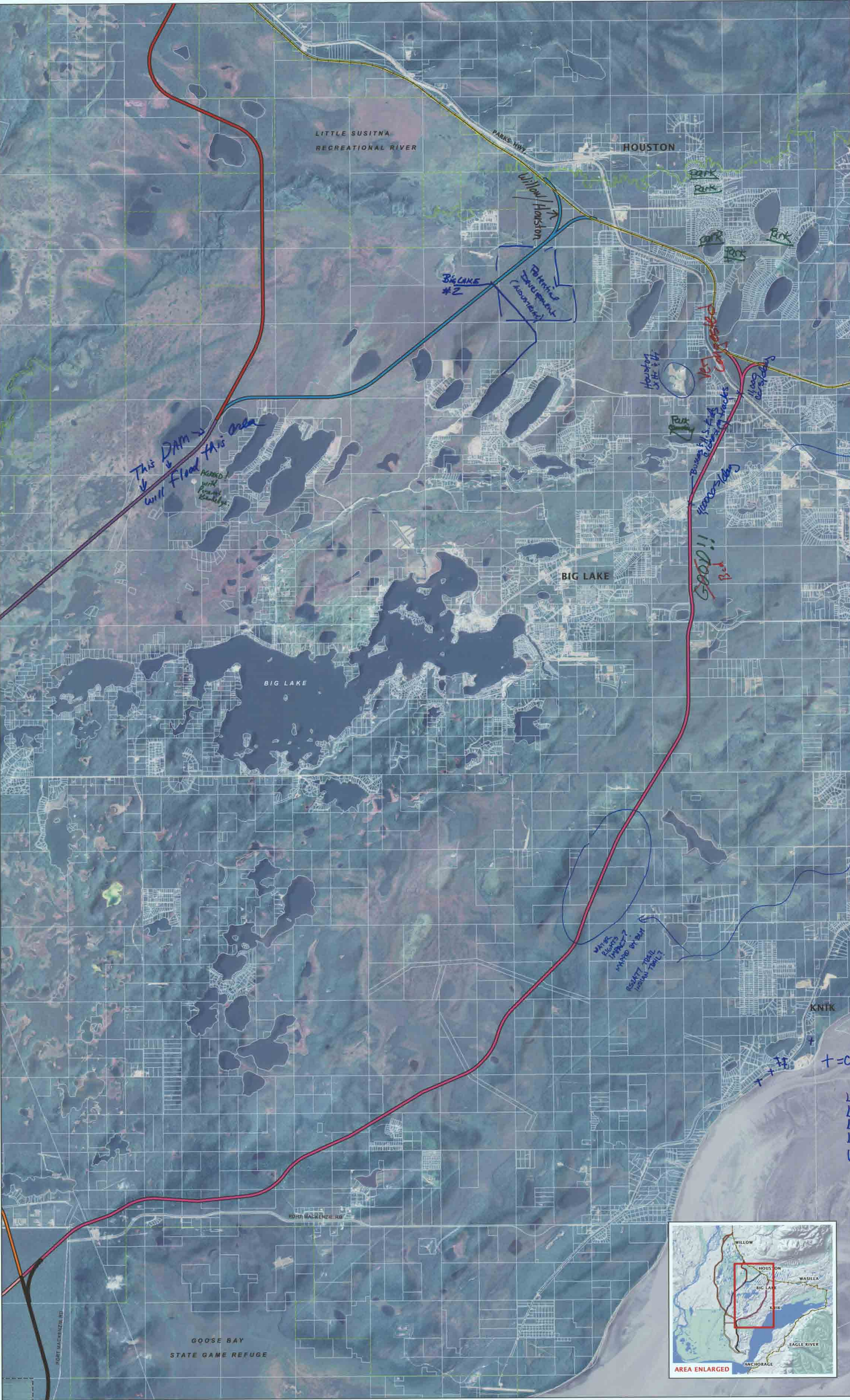
- Legend**
- Preliminary Alternatives\*
    - Mac East
    - Mac West
    - Conn 1
    - Conn 2
    - Conn 3
    - Houston
    - Houston North
    - Houston South
    - Willow
    - Big Lake
    - ARRC Existing Track
    - Point MacKenzie Agricultural Area
    - Port MacKenzie District
    - Park or Refuge
    - Parcel Boundary

\*These lines generally represent corridors which are subject to future refinement.



Date: September 28, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ARRC, HDR Alaska, Inc., MSB GIS, TNH-Hanson.

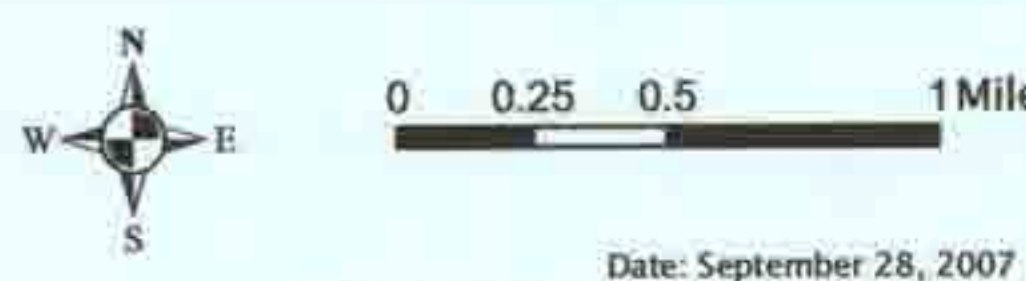
Houston D 10/5/07 MAP 2



- Legend**
- |                                  |               |               |                                   |
|----------------------------------|---------------|---------------|-----------------------------------|
| <b>Preliminary Alternatives*</b> | Conn 2        | Houston South | ARRC Existing Track               |
| Mac East                         | Conn 3        | Willow        | Point Mackenzie Agricultural Area |
| Mac West                         | Houston       | Big Lake      | Port Mackenzie District           |
| Conn 1                           | Houston North |               | Park or Refuge                    |
|                                  |               |               | Parcel Boundary                   |

\*These lines generally represent corridors which are subject to future refinement.

Houston B 10/5/07 MAPS



Date: September 28, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ARRC, HDR Alaska, Inc., MSB GIS, TNH-Hanson.



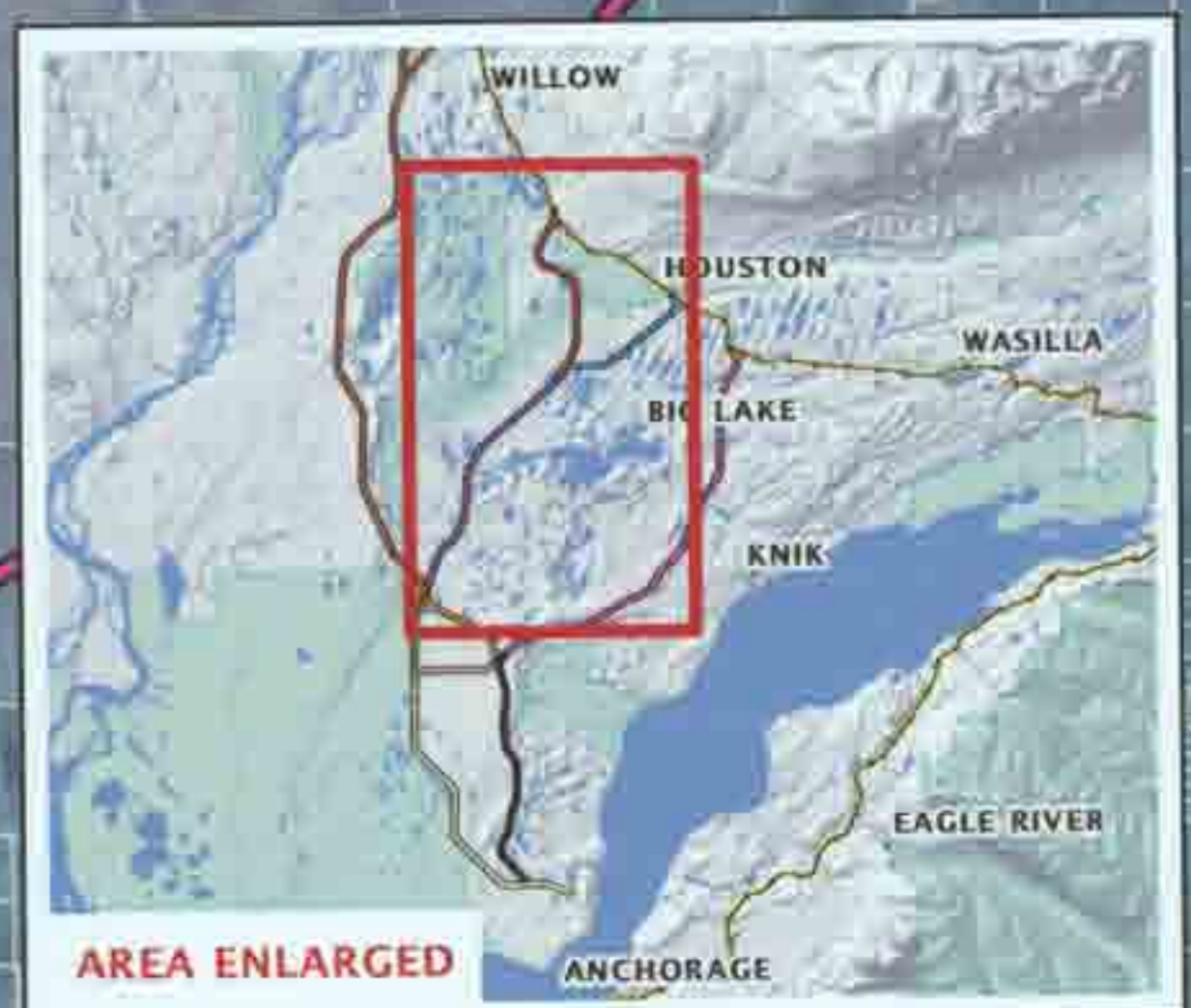
NANCY LAKES STATE RECREATION AREA

LITTLE SUSITNA RECREATIONAL RIVER

LITTLE SUSITNA RECREATIONAL RIVER

BIG LAKE

BIG LAKE



Legend

- Preliminary Alternatives\*
  - Mac East
  - Mac West
  - Conn 1
  - Conn 2
  - Conn 3
  - Houston
  - Houston North
  - Houston South
  - Willow
  - Big Lake
- ARCC Existing Track
- Point Mackenzie Agricultural Area
- Point Mackenzie District
- Park or Refuge
- Parcel Boundary

\*These lines generally represent corridors which are subject to future refinement.



0 0.25 0.5 1 Mi

PORT MACKENZIE RAIL EXTENSION

Houston C

10/5/07 MAP 4

Date: September 28, 2007  
Projection: Alaska State Plane Zone 4, NAD 83  
Author: HDR Alaska, Inc.  
Sources: ARCC, HDR Alaska, Inc., MS&GIS, TMI-Hanson



# PUBLIC MEETING MATERIAL

*(This page intentionally left blank.)*

# Public Open House: October 1, 2007 • Lake Lucille Inn • Wasilla



NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	GENDER Strictly Voluntary	RACE Strictly Voluntary
1. Wes Keller	<input type="checkbox"/>	State House	600 E Railroad Ave Wasilla 99699	wes@wskeller.com	M	W
2. Doreen Mastic	<input type="checkbox"/>	Citizen	P.O. Box 870712 Wasilla, AK 99687	dwm@47@panhandle.com	M	W
3. Terry Sabaly	<input type="checkbox"/>		6833 Elmwood Ave Anchorage 99507		M	W
4. Mike Wineberg Grace Whothee	<input checked="" type="checkbox"/>	Citizen	P.O. Box 231336 Anchorage, AK 99523-1336	mjwhothee@panhandle.com	F	W
5. Stacey Fompson Dodd	<input type="checkbox"/>	San Higgins Sporee	606 E Rainier Blvd Wasilla, AK 99654	stacey-fompson@regis.state.ak.us	F	W
6. SHARV	<input checked="" type="checkbox"/>		1081 W. AOBEN'S SQ WASILLA AK 99654	dodd@sanyensresidency.com	M	W
7. MARY GOTMERAC	<input checked="" type="checkbox"/>		P.O. Box 877387 Wasilla, AK 99654-7387	NA	F	W
8. Tony Turinsky	<input checked="" type="checkbox"/>		Box 24754 Anchorage, AK 99504	TTURINSKY@A1MAIL.COM	F	W

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (AP); Other (O). Category not listed (please see footnote and specify).

# Public Open House: October 1, 2007 • Lake Lucille Inn • Wasilla

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	GENDER Strictly Voluntary	RACE Strictly Voluntary
9. Bruce Paulsen	<input type="checkbox"/>	MSB	330 E. Dublin Ave Palmer, AK 99645			
10. Dana Halls	<input checked="" type="checkbox"/>		Box 1042 W. Hwy. AK 99688			
11. Mary Silveas	<input checked="" type="checkbox"/>		PO Box 1077 Willow, AK 99699			
12. Tim Thompson	<input checked="" type="checkbox"/>	ARRC	327 W. Ship Creek Anchorage, AK 99501	thompson@arrc.com		
13. Mike Pecher	<input checked="" type="checkbox"/>	TNA-Ninson LLC	911 W. 8th Ave Suite 300, ANC 99501	mpecher@hansen-inc.com		
14. Aaron Guletia	<input checked="" type="checkbox"/>	TNA-Ninson	911 W 8th # 300 ANC. 99501	neranger@hansen-inc.com		
15. Mindie Currier	<input checked="" type="checkbox"/>		PO BOX 870103 Wasilla AK 99687	mccormac@mtsonline.net		
16. Fevstiy Family	<input checked="" type="checkbox"/>		POB 872474 Wasilla 99687	fevstiy1@mtsonline.net		
17. Steve Trickett	<input checked="" type="checkbox"/>	Division of Agriculture	1800 Glora Hwy Suite 12, Palmer	Steve.Trickett@alaska.gov		
18. CHA WILSON	<input type="checkbox"/>	State Police	HC 32-Box 6704 Willow, AK 99699			
19. Rosalyn Anderson		HPK	2325 C. Street Anchorage, AK			

RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (AP); Other (O) Category not listed (please self identify and specify)

# Public Open House: October 1, 2007 • Lake Lucille Inn • Wasilla

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	* GENDER Strictly Voluntary	* RACE Strictly Voluntary
20. CARL J. PETER	<input checked="" type="checkbox"/>		POB 577171 WASILLA, AK 99687			
21. Cathi Kramer	<input checked="" type="checkbox"/>	Horticulture Lake Firewise Coordinator	P.O. Box 521703 Big Lake, AK 99652	ca@mtvonline.net		
22. Kevin & Sharon Berg	<input checked="" type="checkbox"/>		PO Box 521165 Big Lake, AK 99652	Kevinberg@geocities.com		
23. JEAN DAVIDSON	<input checked="" type="checkbox"/>		P.O. Box 934 Wasilla, AK 99688	JEAN@MTVONLINE.NET Kenneth Bourgeois Alaska		
24. Ken Bourgeois	<input checked="" type="checkbox"/>	DNR Habitat				
25. Raymond Miska	<input checked="" type="checkbox"/>	Enikoma Inc.				
26. Patrick L. Sharnock	<input checked="" type="checkbox"/>		8731 Solar Dr. Anchorage, AK 99507			
27. LYNN GATTIS	<input checked="" type="checkbox"/>	Gattis Farms			F	
28. Rick Gattis	<input checked="" type="checkbox"/>		1450 W. Shoreline Dr Wasilla, AK 99654	Gattis@mtvonline.net	M	
29. Mike Markler		Carter Burgess		Mike.markler@cc-b.com		
30. SANDY WESSEL-SCHAEFER		Carter Burgess		SANDY.WESSEL-SCHAEFER@CC-B.COM		

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (AP); Other (O). Category not listed (please self identify and specify).

# Public Open House: October 1, 2007 • Lake Lucille Inn • Wasilla

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	* GENDER Strictly Voluntary	* RACE Strictly Voluntary
31. Sharon Spake	X		PO Box 500010 Big Lake, AK 99502	sharke@netcom.com		
32. Steve Brooks	X		600 Wenshire Blvd Suite 100 Los Angeles CA 90017			
33. Pat Tackler	Y		4330 WICKESHAM WAY WASILLA AK 99657			
34. Agnes Finerty	X		21550 E APARADO CIR PALMER AK 99645-2200	WHITECADN.COM		
35. ERINDI WHITE	X	WION	4831 C SUEWALPINE DR WASILLA AK 99654			
36. ERIN ISAHAY	Y		Rt 43 Box 570938 Wasilla AK 99657			
37. M SCHWIZ	Y	IBEW	P.O. Box 20330 Biu Wasilla AK 99657	WASSHEREFEELS.COM NOT WORK	MA	WTR
38. Dean Ernst	Y		P.O. Box 270583 Wasilla AK 99687	edern@geico.net	MA	
39. Marie Brown			480 E. McAdams Way Wasilla AK 99654			
40. Joan's Cathy McCracken	Y		3301 W. 31st AVE Anchorage, AK 99517	joan.mccracken@nara.gov		
41. Tanya R. Ryke	Y		P.O. Box 270311 Wasilla AK 99667	Tanya.Ryke@yale.edu		

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: W White (W); A American Indian (AI); Bl Black (B); H Hispanic (H); As Asian/Pacific Islander (API); O Other (O) Category not listed (please self identify and specify).

# Public Open House: October 1, 2007 • Lake Lucille Inn • Wasilla

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	* GENDER Strictly Voluntary	* RACE Strictly Voluntary
42. NAN LUM	<input checked="" type="checkbox"/>		5735 E. BUCKLED AVE #1 WASILLA, AK 99654	meg-long@hotmail.com		
43. [Signature]	<input checked="" type="checkbox"/>		133 E. SUMNER AVE Wasilla, AK 99654	joyce@stacoinline.net		
44.						
45.						
46.						
47.						
48.						
49.						
50.						
51.						
52.						

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Pitk' (P); Hispanic (H); Asian/Pacific Islander (API); Other (O) Category not listed (please list identity and specify)

# Public Open House: October 1, 2007 • Lake Lucille Inn • Wasilla

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional, Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional, Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	* GENDER Strictly Voluntary	* RACE Strictly Voluntary
JULIANNE HANSON 53 HANSON	<input checked="" type="checkbox"/>	CTB	1130 W. 6th Ave ANCH AK 99501	julianne.hanson@ c-d.com	F	WHT
Gordon Attalides	<input checked="" type="checkbox"/>	PT meet Comm Council	21445 W. PT. MOUNTAIN RD was. 119 AK 99634	faycong@alaska.com	M	WHT
55 Von Gerning RAY JAMES Sutliff	<input checked="" type="checkbox"/>	DOTIF	P.O. Box 1148 Palmer 99645	raycong@alaska.com	F	WHT
57 Sutliff Woff	<input checked="" type="checkbox"/>	DOTIF	P.O. Box 16900 ANCH AK 99516	maryjane.sutliff@ alaska.gov	F	WHT
58 Jack Acorn	<input checked="" type="checkbox"/>	DOTIF	P.O. Box 871565 Wasilla, AK 99687	jack@alaska.gov	M	N/A
59 Matthews Laloux	<input checked="" type="checkbox"/>	EPA	222 W. 7th Ave Box 14 Anchorage	matthews@alaska.gov	M	W
60 Ron Axel	<input checked="" type="checkbox"/>	Alaska Laborers 341	2501 Commercial Dr Anchorage AK 99501	ronaxel@local341.com	M	W
61 Devonia Crockett	<input checked="" type="checkbox"/>	RESURGE Department Council	121 W. FIREWALKER ANCHORAGE AK 99503	devocro@alaska.gov	F	W
62 ANDREW RAJAL	<input checked="" type="checkbox"/>	AK Transp. Priority Project	POB 773454 Eagle River, AK 99577	andrewraj@alaska.gov	M	W
63 Luis Espino	<input checked="" type="checkbox"/>	AK Transp. Priority Project	308 East Suite 221 Anchorage 99501	luis@alaskatransportation.com	M	W

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (API); Other (O). Category not listed: (Please self identify and specify)



# Public Open House: October 1, 2007 • Lake Lucille Inn • Wasilla

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	GENDER Strictly Voluntary	RACE Strictly Voluntary
76. Dary SHOLE	<input checked="" type="checkbox"/>	Self	PO Box 520010 Big Lake 99652	sdshole@comcast.net	M	W
78. JOE CZYZEK	<input type="checkbox"/>			joeczyk@msaonline.net		
77. Paula Erin McLarnon	<input checked="" type="checkbox"/>	WACO Self	POB 1143 Willow, AK 99688	erin-poorpe@alaska.com		
78. BETH SCHEIDT	<input checked="" type="checkbox"/>		600 E BAILEY AVE WASILLA AK 99654	beth.scheidt@legis.state.ak.us		
79. MARL VAN DONGEN	<input checked="" type="checkbox"/>	HSB FOOT DIRECTOR	350 E. DANLIA AVE PALMERA, AK 99645	MARIONDONGEN@MATSIGN.COM	F	
80. IRVING BISSER	<input checked="" type="checkbox"/>	ONLINE				
81. JACK ZERDEL	<input checked="" type="checkbox"/>	SELF	BOX 520904 BIG LAKE AK 99652			
82. B. HITCHIN	<input type="checkbox"/>	Cross Creek	PO BOX 2098 Wasilla		yes	
83. N. Crosby	<input checked="" type="checkbox"/>	Crosby Const.	2155 N. Hanover Palmer AK 99645		M	W
84. K.T Roberts	<input checked="" type="checkbox"/>		PO BOX 1430	KATHKATE@ATT.net	M	W
85.	<input type="checkbox"/>					

RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (AP); Other (O). Category not listed please self identify and specify.

*(This page intentionally left blank.)*

# Public Open House: October 2, 2007 • Big Lake Elementary • Big Lake



= added to our  
communication  
e-mail list  
- 2/23/08, 10/10/307



NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	- GENDER Strictly Voluntary	- RACE Strictly Voluntary
1. Mr. [unclear] Ray Puhli	<input checked="" type="checkbox"/>		P.O. Box 52189 Big Lake			
2. Don Egan	<input type="checkbox"/>		P.O. Box 52189 Big Lake			
3. W.C. Jacobsen	<input checked="" type="checkbox"/>		2221 Duvoy CT ANC. AK 99502			
4. Mrs. Mrs Don Mayfield	<input checked="" type="checkbox"/>		2471 Chandler Dr Anchorage AK 99504	jdmayfield@ak.net		
5. CRANE	<input checked="" type="checkbox"/>		P.O. Box 520201 BIG LAKE, AK 99652			
6. Tim Jonas	<input checked="" type="checkbox"/>		P.O. Box 520201 Big Lake, AK 99652			
7. Steven Divi A. Anderson	<input checked="" type="checkbox"/>		6586 S. Lewis Lane Wasilla AK 99654	Samidon@athletenet		
8. Floyd + Ruth Shilanski	<input checked="" type="checkbox"/>		P.O. Box 520509 Big Lake, AK 99652	Samidon@athletenet		

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (API); Other (O). Category not listed (please self identify and specify).

# Public Open House: October 2, 2007 • Big Lake Elementary • Big Lake

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	GENDER Strictly Voluntary	RACE Strictly Voluntary
9. Jay DeBerkham	<input checked="" type="checkbox"/>		2960 C STREET SUITE 202 AVE1 AK 99503 P.O. Box 988		M	
10. Norman Wiskeman	<input checked="" type="checkbox"/>		Willow, AK 99688		MA	
11. Andy Mickelson	<input checked="" type="checkbox"/>		PO Box 520455 Big Lake, AK 99607		M	
12. Mark Wiskeman	<input checked="" type="checkbox"/>		P.O. Box 520455 Big Lake, AK 99607		M	
13. Elizabeth Wiskeman	<input checked="" type="checkbox"/>		Box 520457 Big Lake, AK 99607	elizabeth.wiskeman@alaska.net	M	W
14. MARTIN SWISER	<input checked="" type="checkbox"/>		ANCHORAGE AK 99520 8731 SHELBOURNE			
15. April Payne	<input checked="" type="checkbox"/>		Anchor age AK 99607		M	
16. Patrick Sherrock	<input checked="" type="checkbox"/>		10261 Ouzel RD Anchorage AK 99507	Jane_Sims@ESAfood.com	M	
17. Jane Sims	<input checked="" type="checkbox"/>		3330 S. Lake Viewway Wasilla AK 99654	slw@slwinc.com	M	
18. Scott Wilson	<input checked="" type="checkbox"/>		4486 Carleton Anchorage AK 99508	scottwilson@comcast.net	M	
19. Bill Wiskeman	<input checked="" type="checkbox"/>			billwiskeman@comcast.net	M	

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories: White (W); Black (B); Hispanic (H); Asian/Pacific Islander (API); Other (O) Category not listed (please self identify and specify).

# Public Open House: October 2, 2007 • Big Lake Elementary • Big Lake

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	* GENDER <u>Strictly</u> <u>Voluntary</u>	* RACE <u>Strictly</u> <u>Voluntary</u>
20 Terry Snyder	<input checked="" type="checkbox"/>		PO Box 521545 Big Lake, AK 99652	terry@mtaonline.net		
21 Dee Loeschke	<input type="checkbox"/>		PO Box 520156 Big Lake, AK 99652	dee@h.e.g.c.net loeschke@mtaonline.net	F	W
22 Bruce Paulson	<input type="checkbox"/>	MSB				
23 Stine Simpson	<input checked="" type="checkbox"/>	Office of Sen. Huggins				
24 Anna Paul-Fridley	<input checked="" type="checkbox"/>		PO Box 520049 Big Lake AK 99652	fridley@mtaonline.net	F	
25 Brian Gillette	<input checked="" type="checkbox"/>		20990 Altery Road Chugiak AK 99567			
26 Steve Simpson	<input checked="" type="checkbox"/>		PO Box 52111 Big Lake AK 99652	ssimpson@mtaonline.net		
27 Cindy Biber	<input checked="" type="checkbox"/>		Box 870008 Wasilla AK 99687	CindyBiber@mtaonline.net	F	W
28 Greg Strong	<input checked="" type="checkbox"/>		PO Box 875169 Wasilla AK 99687	strong@mtaonline.net		
29 Charles Huggins	<input checked="" type="checkbox"/>					
30 SANDY WESUL-SORGE	<input checked="" type="checkbox"/>					

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOI funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: W (White), M (M), A (American Indian), AN (Alaska Native), B (Black), H (Hispanic), N (Native), O (Other). Category not listed (please self identify and specify)

1/1/12

# Public Open House: October 2, 2007 • Big Lake Elementary • Big Lake

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	GENDER Strictly Voluntary	RACE Strictly Voluntary
31. SANDRA BROWN			P.O. BOX 520526 BIG LAKE AK 99553		F	CAU
32. N. J. JONES			P.O. Box 520769 Big Lake AK 99553		F	CAU
33. K. TRODEN	<input checked="" type="checkbox"/>		PO BOX 974594 WASILLA, AK 99687	Kate@bookplans.net	F	CAU
34. CAROLYN HAGEDORN	<input checked="" type="checkbox"/>		HC 3rd BOX 2449 WASILLA, AK 99687	dehagedorn@alaska.net	F	CAU
35. DAVE HANSEN			"	"	M	"
36. NIELLE STONE	<input checked="" type="checkbox"/>		PO BOX 530489 Big Lake, AK 99553	astrom@alaska.net	M	
37. MICHELLE STONE			"			
38. Pamela Darnell	<input checked="" type="checkbox"/>		PO Box 520414 Big Lake AK 99553			
39. L. J. JONES	<input checked="" type="checkbox"/>		P.O. Box 520520 Big Lake AK 99553		F	W
40. Bob Jones	<input checked="" type="checkbox"/>		PO Box 520166 Big Lake 99553	bdjones@alaska.net	M	CAU
41. Richard Garcia	<input checked="" type="checkbox"/>		PO Box 520255 Big Lake		M	

RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DDT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (AP); Other (O) Category not listed (please self identify and specify).

# Public Open House: October 2, 2007 • Big Lake Elementary • Big Lake

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	GENDER Strictly Voluntary	RACE Strictly Voluntary
42. [unclear]	<input checked="" type="checkbox"/>		750 Stewart Street Big Lake AK 99657 PO Box 52116	chris@stewartstreet.com	F	Caucasian
43. Doreen K. Aron	<input checked="" type="checkbox"/>		BIG LAKE AK 99657 PO Box 521473 Big Lake, AK 99652	donell@arons.com	M	
44. Anne Nelson	<input checked="" type="checkbox"/>				F	
45. [unclear]	<input checked="" type="checkbox"/>		PO Box 52116-5 Big Lake, AK 99657 PO Box 52154-5 Big Lake			
46. Houston Snyder	<input checked="" type="checkbox"/>		PO Box 52015-6 Big Lake		M	W
47. [unclear]	<input checked="" type="checkbox"/>		Big Lake		N	W
48. [unclear]	<input checked="" type="checkbox"/>		Big Lake	Community of Big Lake	F	
49. DEAN DANDSON	<input checked="" type="checkbox"/>					
50. JIM PERERA	<input checked="" type="checkbox"/>		PO Box 52116-6 Big Lake, AK 99652	jim@pererak.com	M	W
51. ARL MOFF	<input checked="" type="checkbox"/>		PO Box 52110-7 Big Lake AK 99652 PO Box 52051-8		M	W
52. Shelley Ju	<input checked="" type="checkbox"/>		Big Lake AK 99652		F	W

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (AP); Other (O). Category not listed, please self identify and specify.

# Public Open House: October 2, 2007 • Big Lake Elementary • Big Lake

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	GENDER Strictly Voluntary	RACE Strictly Voluntary
JULIANNE 53. RANSON						
54. Mike MARLER		CEIS				
55. WILGAND COOK	X	Wood County Land Office	P.O. Box 1 E.R. #145710001	AKWA@CCI.NET		
56. WOOTEN THOMAS	X	-	HC 34 Box 2561 Wombula AK 99654	--	4	Mixed
57. Jo Cassidy		Big Lake Library				
58. GAY NORFI	X	-	P.O. Box 526309 BIG LAKE, AK 99652	janette@mtgnet.net	Female	Caucasian
59. JUDY TUMMICK	X		HC 34, Box 2594 Wombula AK 99654	turnick@yahoo.com	F	White
60. Noel WOODS	X		P.O. Box 820 Palmer AK 99645			
61. JERRY WOODS						
62. Steve MORAN						
63. Ewan LOCKYER		Waska Public Library		ewan@prn.org		

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (AP); Other (O) Category not listed (please self identify and specify).



# Public Open House: October 2, 2007 • Big Lake Elementary • Big Lake

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	* GENDER Strictly Voluntary	* RACE Strictly Voluntary
64. Dana Hills			Box 10142 William, AK 99658			
65. Kristi Kardavick	<input checked="" type="checkbox"/>		1300 Setter Dr Anchorage, AK 99522	kristikardavick@ask.com	F	Cauc.
66. Ron Bradis	<input checked="" type="checkbox"/>		2035 Zeile Big Lake, AK 99652		M	"
67. ALLEN BRIGHTON	<input checked="" type="checkbox"/>		7241 STAPPS WAY ANCH. AK 99507	albrighton@biglake.ak.us	M	"
68. Erika Kate SMOUSE	<input checked="" type="checkbox"/>		P.O. Box 520455 Big Lake, AK 99652	erika.kate.smouse@biglake.ak.us	F	"
69. ORIE ORIEN	<input checked="" type="checkbox"/>		P.O. Box 520716 BIG LAKE, AK 99652	orieo@biglake.ak.us		
70. ORIE ORIEN	<input checked="" type="checkbox"/>		P.O. Box 520716 BIG LAKE, AK 99652			
71. Louise KOZISEK	<input checked="" type="checkbox"/>		5006 Kopen Dr Anchorage, AK 99507	louisek@biglake.ak.us	F	C
72. LYNDA TYLER	<input checked="" type="checkbox"/>		3050 S. LAKE VIEW LOOP WASILLA AK 99654	lyndatyler@biglake.ak.us	F	C
73. Maggie O'HARA	<input checked="" type="checkbox"/>		P.O. Box 520510 Big Lake, AK 99652	maggie@biglake.ak.us	F	C
74. Ruth LOOMIS			PC Box 521825 Big Lake, AK 99652	ruthloomis@biglake.ak.us	F	C

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (AP). Other: (O) Category not listed (please self identify and specify).

# Public Open House: October 2, 2007 • Big Lake Elementary • Big Lake

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	* GENDER Strictly Voluntary	* RACE Strictly Voluntary
75. David Kohfeld	<input checked="" type="checkbox"/>		1530 Northwood Dr Unit 1 Anchorage AK 99504	dkohfeld@gmail.com	Male	
76. George Travel					Male	
77. [Signature]						
78. Erin McLarnn						
79. Randi Nelson	<input checked="" type="checkbox"/>		PO Box 50633 Big Lake AK 99504			
80. Loui Choney	<input checked="" type="checkbox"/>		PO BOX 50050 BULLARD 99502			
81. Peg Watkinson	<input checked="" type="checkbox"/>		370 E Semanapitup Wasilla, AK 99654	watkinsonpeg@yellow.com		
82. Kristie Fay	<input checked="" type="checkbox"/>		PO Box 5215014 Big Lake, AK 99502	jkrumpf@starboard.net	female	
83. Cilava E Harris	<input checked="" type="checkbox"/>		3410 Tamar Rd Unit B Wasilla, AK 99654	Alaska-AdultSelfEducation.com	Female	
84. Bill Clark	<input checked="" type="checkbox"/>		Box 50310 Belleme 99502	Belleme@alaska.com	M	
85. [Signature]	<input checked="" type="checkbox"/>		P.O. Box 52070 Big Lake 99502	kenneth@university.net	M	

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (AP); Other (O). Category not listed (please self identify and specify).

# Public Open House: October 2, 2007 • Big Lake Elementary • Big Lake

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	GENDER Strictly Voluntary	RACE Strictly Voluntary
86. Casey Steiman	<input checked="" type="checkbox"/>		HC 34 Box 2428 Wasilla, AK 99554	sellalaska@ yahoo.com	F	
87. NORDEN Amanda	<input checked="" type="checkbox"/>		PO Box 50125 Big Lake, AK 99502		F	
88. VIKI KAS	<input checked="" type="checkbox"/>		517 W. 12th Ave Anchorage AK 99504	vikl@gcl.net		
89. MARY Shivelo	<input type="checkbox"/>					
90. Fido Bowen	<input checked="" type="checkbox"/>		CPD Box 520124		F	
91. Bud Bowen	<input checked="" type="checkbox"/>		Big Lake 99652		M	
92. JAC EISEL	<input type="checkbox"/>					
93. JOHN ROBERTS	<input checked="" type="checkbox"/>		601 WEST TROOD RD ANCHORAGE AK 99503	JEANRO@PA.ALASKA. COM	M	
94. STAN VICTORIS	<input type="checkbox"/>		7444 S B. WILKINSON WASILLA AK		M	
95. CAROLINE SHIVEL	<input checked="" type="checkbox"/>				M	
96.						

\* RACE/GENDER - Strictly voluntary. This purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title V responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (AP); Other (O) Category not listed please self identify and specify.

# Public Open House: October 2, 2007 • Big Lake Elementary • Big Lake

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	GENDER Strictly Voluntary	RACE Strictly Voluntary
107. Family name	<input checked="" type="checkbox"/>		Box 521152 Big Lake, 99652		M	W
108. Linda Oxley	<input checked="" type="checkbox"/>	WACO	Box 1037 W. Ken 99652	<del>Chari</del> Chari@waco.org	F	C
109. Barbara Belman	<input checked="" type="checkbox"/>		P.O. Box 590453 Big Lake, AK 99652	Sub: Mad@qci.net	F	W
110. Robert Belman	<input checked="" type="checkbox"/>		P.O. Box 520453 Big Lake, AK 99652	" "	M	
111. Barbara Miller	<input checked="" type="checkbox"/>		P.O. Box 520296 Big Lake, AK 99652		M F	W
112. Mini Pecked	<input checked="" type="checkbox"/>	FOMS	P.O. Box 116 Palmer, AK 99645 11901 Business Blvd #219 Eagle River AK P.O. Box 521152 Big Lake, AK 99652	<del>Redeem</del> Redeem@muniv.org redem@muniv.org		
113. John Rodda	<input checked="" type="checkbox"/>	Fireworks Manufacturing	2100 S. 21st St Big Lake, AK 99652	redem@muniv.org		
114. Cassi Karon	<input checked="" type="checkbox"/>		P.O. Box 520957 Big Lake, AK 99652	woodbury@qci.net		
115. Kim Woodbury	<input checked="" type="checkbox"/>		Super Alaska			
116. George Woodbury						

**RACE/GENDER** - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W), American Indian (AI), Alaska Native (AN), Black (B), Hispanic (H), Asian/Pacific Islander (AP), Other (O) Category not listed (please self identify and specify).

# Public Open House: October 2, 2007 • Big Lake Elementary • Big Lake

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	* GENDER Strictly Voluntary	* RACE Strictly Voluntary
117. Margie Squidal	<input checked="" type="checkbox"/>		Box 520472 Big Lake, AK 99652			
118. Janet Whitfield	<input checked="" type="checkbox"/>		HC 32 Box 6520 Wasilla, AK 99654			
119. JERRY DICKMAN	<input checked="" type="checkbox"/>		Box 521304 Big Lake AK 99652			
120. Row Eggeston	<input checked="" type="checkbox"/>		1864 S. Lodge Wasilla, AK 99654 PO Box 521675			
121. JIM STRAWNS	<input checked="" type="checkbox"/>		Big Lake AK 99652 11541 H. Henry Road Anchorage, Alaska 99507	jim.strawns@b.e		
122. Brent Hekberg	<input checked="" type="checkbox"/>		P.O. Box 230372 Anchorage, AK 99523			
123. Justin McMullin	<input checked="" type="checkbox"/>		7449 S Babcock Blvd Wasilla, AK 99654			
124. Donna Victors	<input checked="" type="checkbox"/>		15775 - 100th Ave Wasilla, AK			
125. B. ROSTE	<input checked="" type="checkbox"/>					
126						

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we a fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (API); Other (O). Category not listed (please self identify and specify)

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	GENDER Strictly Voluntary	RACE Strictly Voluntary
Dore Langin			5760 S Bradburn Loop Palmer		M	Caucasian
Bob CURRIE			15155 Wilcox Drive Wasilla, AK 99654			
Diane Dusek	<input checked="" type="checkbox"/>		PO Box 21907 Big Lake, AK 99653		M	
Catherine Dusek			PO Box 321602 Big Lake, AK 99653			

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (AP). Other (O) Category not listed (Please self-identify and specify).

Public Open House: October 3, 2007 • Willow Area Community Center • Willow



36



NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	* GENDER Strictly Voluntary	* RACE Strictly Voluntary
1. Dana Hillis	<input type="checkbox"/>		Box 1062 Willow, AK 99688			
2. Jeffrey Fuller	<input type="checkbox"/>		Box 197 Willow, AK 99688			
3. Loreal Factor	<input checked="" type="checkbox"/>		1946 Duffinville Perimeter, 99645	dmthess@dynamicproperties.net		
4. Deane Matlock	<input checked="" type="checkbox"/>		<del>777</del> Willow 99688		M	W
5. Jon Dehgan	<input checked="" type="checkbox"/>		P.O. Box 812 Willow 99688	dehgan@gninet	F	W
6. Sandra Dehgan	<input type="checkbox"/>					
TJIM SPORTEARS	<input checked="" type="checkbox"/>		108th St Willow AK 99688		M	W
8. BRYAN NORSE ROSS	<input type="checkbox"/>		108th St Willow AK 99688		F	W

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (AP); Other (O) Category not listed (please self identify and specify).

Public Open House: October 3, 2007 • Willow Area Community Center • Willow

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	GENDER Strictly Voluntary	RACE Strictly Voluntary
9. DERR Davidson	<input type="checkbox"/>					
10. Vernon Wakemeyer	<input type="checkbox"/>					
11. Whitham	<input type="checkbox"/>					
12. Steven Hoard	<input checked="" type="checkbox"/>		P.O. Box 243 Willow - 98688	primary@MTAonline.com J. Hoff		
13. Thomas Ducek	<input checked="" type="checkbox"/>		P.O. Box 1156 Willow, 99688	transid@center.org		
14. Floyd Brooks	<input checked="" type="checkbox"/>		P.O. Box 1070 Willow, AK 99698	F		
15. Bruce Paulsen	<input type="checkbox"/>	MSB				
16. Joan Bird	<input checked="" type="checkbox"/>		P.O. Box 742 Willow AK 99688	JMBIRD@CCI.NET	M	Cau
17. Pat Barber	<input type="checkbox"/>		2500 Belmont Dr. Anch 99517		F	White
18. Audrey Simpson	<input type="checkbox"/>	name of Co. Higgins				
19. Pat Steig	<input checked="" type="checkbox"/>		P.O. Box 928 Willow AK 99688		F	Cauc

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (API); Other (O) Category not listed (please self-identify and specify).



Public Open House: October 3, 2007 • Willow Area Community Center • Willow

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	GENDER Strictly Voluntary	RACE Strictly Voluntary
20. Lawrence Jones	<input checked="" type="checkbox"/>		PO Box 343 Willow AK 99688			
21. Charlie Higgins	<input checked="" type="checkbox"/>		493 Independence Barrow 99605			
22. Lloyd Malone	<input checked="" type="checkbox"/>		PO Box 522 Willow AK 99688			
23. Dawn Gravel	<input checked="" type="checkbox"/>		PO Box 810 Willow AK 99688	hastlev@vopinion.net		
24. Christian Hark	<input checked="" type="checkbox"/>					
25. Erin McLanner	<input checked="" type="checkbox"/>		PO Box 143 Willow 99688			
26. Paul Hargan	<input checked="" type="checkbox"/>		PO Box 27 Willow, AK 99688			
27. Edward McCain	<input checked="" type="checkbox"/>		PO Box 2571 Willow, AK 99688	crowley10m@place@yahoo.com		
28. Mary Crowley	<input checked="" type="checkbox"/>					
29. Tila Palmer	<input checked="" type="checkbox"/>		PO Box 1040 Willow	ladynorth@webtv.net	F	Cauc.
30. Dick Palmer	<input checked="" type="checkbox"/>		"	"	M	Cauc.

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (API); Other (O). Category not listed (please specify gender and specialty).

Public Open House: October 3, 2007 • Willow Area Community Center • Willow

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	• GENDER Strictly Voluntary	• RACE Strictly Voluntary
31. Bob Chisholm	X	Alaska Environmental Coalition Willow Area Chapter	P.O. Box 931	rich.pesch@mtaonline.net		Kusmoo
32. Lynda Barone	X	Willow Dogmatics	PO Box 202			
33. Jean Berry	X	-	PO Box 49 Willow			
34. Ted Barry		Barry-Govette Tribesman Council	Taraestra PO Box 371 99676	ellenmwo@c Yahoo.com		
35. Ellen Wolf						
36. Vic Frazzarello	✓		PO Box 732 Willow, AK 99688			
37. Brock Brattle						
38. Lawrence Beckwith				monday@prairieksu.net		
39. James Lane	✓					
40. Stephen + Allison Dean	✓		P.O. Box 85 Willow, AK 99688			
41.						

• RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (API); Other (O) Category not listed (please self identify and specify).

Public Open House: October 3, 2007 • Willow Area Community Center • Willow

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	* GENDER Strictly Voluntary	* RACE Strictly Voluntary
Yvonne McIntyre	<input checked="" type="checkbox"/>		P.O. Box 515 Willow AK 99688	yvonne.mcintyre@willow.com		
76 Bonnie Lewis	<input checked="" type="checkbox"/>		P.O. Box 661 Willow AK 99688			
77 [Signature]	<input checked="" type="checkbox"/>		1111 Willow AK 99688			
78 [Signature]	<input checked="" type="checkbox"/>		HC 89, Box 56 Willow, AK 99688	Depek@yahoo.com		
79. Dee Marie	<input checked="" type="checkbox"/>		P.O. Box 247 Willow AK 99688			
80. Joan Bryner	<input checked="" type="checkbox"/>		P.O. Box 9 Willow AK 99688			
81. Joyce Holmes	<input checked="" type="checkbox"/>		210 Boland Dr Anchorage, AK 99517			
82. Ed Barber	<input checked="" type="checkbox"/>		P.O. Box 1 Willow AK 99688			
83. Vic Frankel	<input checked="" type="checkbox"/>		P.O. Box 7 Willow AK 99688	Rail line in Willow is a S&D I.D. EA		<input checked="" type="checkbox"/>
84. [Signature]	<input checked="" type="checkbox"/>		Box 249 Willow AK 99688			
85. Walter Soroken	<input checked="" type="checkbox"/>		Willow AK 99688	Soroken@mtaonline.net		

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (AP). Other (O). Category not listed please self identify and specify.

Public Open House: October 3, 2007 • Willow Area Community Center • Willow

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	GENDER Strictly Voluntary	RACE Strictly Voluntary
86. Donna Kats Brainard			P.O. Box 1183 Willow, AK 99688			
87. Heather Johnson	<input checked="" type="checkbox"/>		Box 966 79688	hooqeyent@earthlink.net		
88. Gordon Boeve	<input checked="" type="checkbox"/>		HC 89 Box 124 Willow AK 99688	gotboeve@hotmail.com		
89. Brenda Williams			Box 27 Willow, AK 70 Box 759 Phillips AK 99645	young_bre@earthlink.net belen@ms.taonline.com		
89. Brenda Crabbe	<input checked="" type="checkbox"/>		P.O. Box 4566 Wasilla, AK 99657	bram@alaska.net		
89. Claxton John Straight	<input checked="" type="checkbox"/>		873571 Wasilla Alaska 99687	WRCR@CCLARRA.NET		
95. LeRoy Frank	<input checked="" type="checkbox"/>		PO Box 198 Willow 99688			
94. Clara P. Harris			21128 Banning Lane Chugiak, AK	bjager@mtaonline.net		
95. Brenda Jager	<input checked="" type="checkbox"/>		PO Box 752 Willow, AK 99688			
96. Carol Bora	<input checked="" type="checkbox"/>					

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (API); Other (O) Category not listed (please self identify and specify).

Public Open House: October 3, 2007 • Willow Area Community Center • Willow

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	- GENDER Strictly Voluntary	- RACE Strictly Voluntary
97. Jim Houston Kathy Houston		Willow Chamber of Commerce	P.O. Box 38 Willow, AK 99688	AK.HOST@MIA.NET ONLINE	M F	
98. LINDI JATE	X	ADN		lindijate@adn.com		
99. Kuba Kelly	X	WIKO	Box 1027 Willow	chance.wiki@wik.org	F	C.
100. <del>Wendy</del> <del>Wendy</del>	X	Chamber	Box 303 Willow 99688	chamberonline.net	F	W
101. <del>Wendy</del> Wendy	X		Box 689 Willow 99688			
102. Heidi Jenkins	X		P.O. Box 171 Willow, 99688	hjad@adn.com	F	
103. MARIE SOROKA	X		P.O. Box 349 Willow, AK 99688	SOROKA.MARIE@ADN.COM	F	
104. <del>James</del> James	X		P.O. Box 534 Willow AK 99688	jamiespublic@alaska.net	F	
105. Harry Backs	X		P.O. Box 534 Willow, AK 99688	os@alaska.net	M	
106. <del>Wendy</del> Wendy						

- RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (API); Other (O); Category not listed (please self identify and specify).

Public Open House: October 3, 2007 • Willow Area Community Center • Willow

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	* GENDER Strictly Voluntary	* RACE Strictly Voluntary
107. Tamara B...						
108. BRUCE H MATTSON	<input checked="" type="checkbox"/>		POB 253 W. Willow, AK 99688			
109. Andrea Susan Hartmann	<input checked="" type="checkbox"/>		PO Box 88L 10 Willow AK 99688	streddig@interwire.net		
110. Bob McGinn	<input checked="" type="checkbox"/>		POB 212 W. Willow AK 99688	<del>streddig@interwire.net</del> Kronline@Alaska.com		
111. Randy Bedford	<input checked="" type="checkbox"/>			bedford@interwire.net		
112. CAPT Stromberg	<input checked="" type="checkbox"/>		PO Box 11686 Anchorage Alaska 99511	ststromberg@interwire.net		
113. Donna Quante	<input checked="" type="checkbox"/>		PO Box 1055 Willow 99688	dquante@interwire.net		
114. Helen	<input checked="" type="checkbox"/>		915 E. BOWEN ST, 4N CHUGALUA AK 99567	helen.1949@interwire.net		
115. Keith Ryberg	<input checked="" type="checkbox"/>					
116. Doreen Smith	<input checked="" type="checkbox"/>		PO Box 571 Tulacewa AK 99676			

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we a fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (API); Other (O). DISPATCH REGISTER (please self identify and specify).

# Public Open House: October 3, 2007 • Willow Area Community Center • Willow

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	* GENDER Strictly Voluntary	* RACE Strictly Voluntary
117. <i>Fantley Dan</i>	<input checked="" type="checkbox"/>	<i>Willow Area Transit</i>	<i>Box 85 Willow Ark</i>		<i>Indefinite</i>	
118.						
119.						
120.						
121.						
122.						
123.						
124.						
125.						
126.						

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we a fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (AP). Other (O) Category not listed. (please self identify and specify).

*(This page intentionally left blank.)*



458

# Public Open House: October 4, 2007 • Knik Elementary • Knik



NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	* GENDER Strictly Voluntary	* RACE Strictly Voluntary
1. ART/BOYS SCOUTS	<input checked="" type="checkbox"/>	—	15916 W PT MACKENZIE RD #10 WILKINSON AV 99754 P.O. BOX	brt@ccc.net	MA	W
2. TARA GOSTMERAK	<input checked="" type="checkbox"/>	—	12503 S. FARMERS RD. WASILLA, AK 997654 Box 871361 99767	SUSITNA EXPEDITIONS @YATTOO.COM	MA	W
3. TOSY RIDDELL	<input checked="" type="checkbox"/>	—	Box 871361 99767	PARVINE NIPALASKA.COM	MA	AI
4. ERIC WILSON	<input checked="" type="checkbox"/>	—	Box 871361 99767	PARVINE NIPALASKA.COM	MA	AI
5. RON ARVIN	<input checked="" type="checkbox"/>	—	Box 871361 99767	PARVINE NIPALASKA.COM	MA	AI
6. Sally Selzer	<input type="checkbox"/>	Knik Shuttle Sondheween Gulch	4330 Bayview Ct. Frishton Lakes 99758	SEELEYE-GULCH.NET	F	W
7. Lynn DeCarville	<input checked="" type="checkbox"/>	—	PO BOX 87298 WASILLA, AK 99767	LYN-DECARVILLE@HOTMAIL.COM	M	W

\* RACE/ENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding via a fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the community. Race Categories are: W (W) American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (AP); Other (O) Category not listed (please self-identify and specify)

# Public Open House: October 4, 2007 • Knik Elementary • Knik

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	GENDER Strictly Voluntary	RACE Strictly Voluntary
9. <i>Rosario Vazquez</i>	<input checked="" type="checkbox"/>	Nursing Home	PO Box 971574 Wash DC 20009		F	
10. <i>Sister Place</i>	<input type="checkbox"/>	Private	PO Box 2133 Palmer AK 99645		F	
11. <i>Beverly Chasnat</i>	<input checked="" type="checkbox"/>	"	POB 2526 Palmer AK 99645	chesnat@mtaonline.net		
12. <i>Pat Diamond</i>	<input checked="" type="checkbox"/>	"	4923 N. Kipling Dr Wasilla AK 99654	sewtain@mtaonline.net		
13. <i>GREEN AND S. BONDCHAL</i>	<input checked="" type="checkbox"/>	"	20300 Wagon LAWSONVILLE AK 99700			
14. <i>MILIKY S. KIPPUS</i>	<input type="checkbox"/>	"				
15. <i>Ed Ruskong</i>	<input checked="" type="checkbox"/>	Private	6985 Big Mountain Anchorage, AK 99506	Ed.ed.ruskong@alaskaenergy.com		
16. <i>Nathan MacLennan</i>	<input checked="" type="checkbox"/>	"				
17. <i>MILAS MILASSIO</i>	<input type="checkbox"/>	"			M	
18. <i>MILICEVIC MILASSIO</i>	<input checked="" type="checkbox"/>	Private	PO Box 880512 Wasilla AK 99681	anna-sil@mtaonline.net	F	
19. <i>KELLEY GRIFFIN</i>	X	MAT-SU SLED DOG COUNCIL INC	HC 35 Box 53557 Wasilla AK 99654			

RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (AP); Other (O). Category not listed (please self identify and specify).

OCTOBER 4, 2007  
Krik

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	* GENDER Strictly Voluntary	* RACE Strictly Voluntary
Tom SUPPENA	<input checked="" type="checkbox"/>	FRUIT Suntogs Office	500 857 5710 30 WASHA			WM
Tracy Sumpster		ASFE	Danner			
Timothy Mikobik						
MARLENE BROWN			MC Box 5113 Wasilla 99654 2150 Claridge Dr Wasilla AK 99654			
Helga Margaret Brown			3700 W. WARD STREET WASILLA AK 99654	tkroy@goinet		
Elly Knapp						
TIM KRIK	<input checked="" type="checkbox"/>	SELF Publication Inc.	901 W. LAKWOOD AVE Wasilla 99654	lockwood@mtsonline.net		
Ringsdorf, Sheree	<input checked="" type="checkbox"/>		Box 520530 Bug Lake, Ak 99652	knapp@mtsonline.net		
Phil Lockwood	<input checked="" type="checkbox"/>	Sales	Box F75107 WASILLA, AK			
Wynne Knapp	<input checked="" type="checkbox"/>					
S. Alder	<input checked="" type="checkbox"/>					

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (API); Other (O). Category not listed (please self identify and specify).

OCTOBER 4, 2007  
Knik

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	* GENDER Strictly Voluntary	* RACE Strictly Voluntary
Jerry Eschby McDONALD	<input checked="" type="checkbox"/>		PO Box 940215 HOUSTON, AK 99504			
JILL PARSON	<input checked="" type="checkbox"/>		1113 N ROSINS RD WASILLA AK 99654			
Kent Hall	<input checked="" type="checkbox"/>		6113 N RIVER RD WASILLA 99654			
Debbie Anderson			824 DeLaney St Anchorage AK 99501			
Mimi Brothers	<input checked="" type="checkbox"/>		1751 W. STURGE MANE DR WASILLA, AK 99654			
June Parker	<input checked="" type="checkbox"/>			June Parker@comcast.net		
Kim Sinnott	<input checked="" type="checkbox"/>		PO Box 520178 BIG LAKE AK 99652		F	W
ERIC SINNETT					M	L

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DOT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); Asian/Pacific Islander (AP); Other (O) Category not listed (please specify identity and specify)

# Public Open House: October 4, 2007 • Knik Elementary • Knik

47

NAME (First and Last) Please Print	Check box if you'd like to receive project mailings	ORGANIZATION (if applicable)	MAILING ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	E-MAIL ADDRESS Optional. Please provide if you checked the box to receive project mailings. This information may be subject to public disclosure.	* GENDER Strictly Voluntary	* RACE Strictly Voluntary
20. EVA DETRAEUS	<input checked="" type="checkbox"/>		PO BOX 241843 ANCHORAGE 99504			
21. EARTHED BLUNT	<input checked="" type="checkbox"/>		Same as above			
22. ROB HUNTER CHAMBERS	<input checked="" type="checkbox"/>		4330 SINGER DR WILSON, AK 99654	mmcmunisse@gmail.com		
23. RANDY DENWEL LYONS	<input checked="" type="checkbox"/>		PO Box 672356 Chugiak, AK 99667			
24.						
25.						
26.						
27.						
28.						
29.						
30.						

\* RACE/GENDER - Strictly voluntary. The purpose of collecting this information is to ensure as a recipient of Federal DDT funding we are fulfilling our Title VI responsibilities by ensuring fair and equitable representation by the public. Race Categories are: White (W); American Indian (AI); Alaska Native (AN); Black (B); Hispanic (H); As-ao-Pacific Islander (AP); Other (O) Category not listed (please self identify and specify)

*(This page intentionally left blank.)*

NAME

ADDRESS

E-MAIL

INFO?

DON PICKETT 2065 SONSON W. D. GUNN 99187 den@dpickett.net

kluberton@ntzonline.net

Box 200 Talkeetna 99676

GLEN SWAN 1611 MEYER EARL RIVE 99577 H8greences2@gci.net

ROBINSON, DAVE 5061 W. HOLLOWBROOK WATSONA, AK 99654

RICHARD GAFFEY

KATHLEEN HARMS PO Box 521362 ~~Scappato Drive~~ Big Lake 99652 kpharms@hotmail.com

Carla Hendrix carla@alaskawebdesigns.com

Nancy Sult houstonakchamber@hotmail.com

Rich Dillingham Alc Maggie Matelski@AlcEmail.com

Houston

Houston

NAME	ADDRESS	E-MAIL	MS?
Wendy Buzack		claudia.b@mtaonline	MS?
Val Duceomb			
Erin Mitchell	2146 Solway Circle Anch AK 99508		
LANCE WILSON	Houston PO Box 988, Willow	wilson.e@mtaonline.net	
DELL ALSOP	3221 South Circle Anch 99507		
ERIC MITCHELL	12000 Gillette Dr. Anch, AK 99516	emik@acsalaska.net	
RICH THILSTE	15687 W Sun DR Wasilla AK 99614		
Norman Wakeman			
BEN WATNEY	3221 South Circle Anch 99507	benwatney@celoak.com	Yes
Lily Tetterton	P.O. Box 220622, Anchorage, AK 99522		
Laurie Smith	PO Box 1767 Big Lake AK 99582		
DANA L. OLSON	HC-35 Box 5138 Wasilla		
KATHERINE BULBER	P.O. Box 336 Willow		
STUART RYDHEK	P.O. Box 336 Willow		
MORAN PASCHE	P.O. Box 336 Willow		
Jane Peterson	3531 Beaubien Dr Anchorage	Schulthick@ak.net	
MALLOWS	1800 Sherman Dr	cmis@stet.net	
SANDY McDONALD			
ROGER PURCELL	Hous-ton-city council		
UEN HALFER	Willow Aly	whalter@mtaonline.net	
ALICE RICHLEY	2420 Pick A Pilly		
ANITA OLSON	PO Box 908 Willow		
JOHN SCUDDER	8421 STONEY CR Anch AK	AK-MAILMAN@GCI.NET	

Houston 10/15/02





*(This page intentionally left blank.)*



*Welcome!*  
Port MacKenzie Rail Extension Project  
Public Open House



[www.portmacrail.com](http://www.portmacrail.com)



# Project Participants

Alaska Railroad Corporation \_\_\_\_\_ Applicant

Matanuska-Susitna Borough \_\_\_\_\_ Project sponsor

Surface Transportation Board \_\_\_\_\_ Lead federal agency

HDR Alaska, Inc. \_\_\_\_\_ Consultant

TNH-Hanson, LLC \_\_\_\_\_ Consultant

STB Section of Environmental Analysis \_\_\_\_\_ NEPA Compliance

To be determined \_\_\_\_\_ 3<sup>rd</sup> Party NEPA Contractor



[www.portmacrail.com](http://www.portmacrail.com)

# STB Process

## Railroad

- Files notice with STB
- Files petition for the construction and operation of railroad

## STB

- Considers application based upon Railroad's ability to operate/be stable and market needs
- Incorporates conditions from the SEA upon favorable finding

## SEA

- Conducts NEPA process independently of railroad\*
- Most likely utilizes 3rd party contractor

\*Railroad has very limited role/input in process beyond providing information



# What's Happening Now and Why



- 2003 Study was a reconnaissance study
- Re-evaluating and refining most promising corridors
- Incorporating latest environmental and planning information



[www.portmacrail.com](http://www.portmacrail.com)

# Constraints Mapping



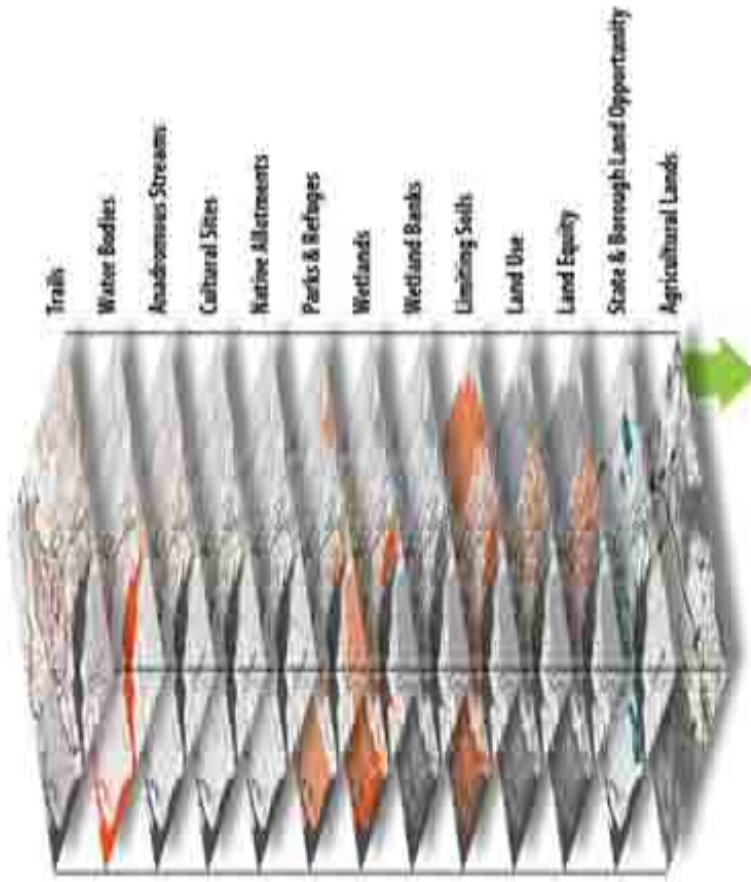
## Topics considered:

- Water bodies
- Anadromous streams
- Habitat
- Cultural sites
- Native allotments
- Parks and refuges
- Wetlands and wetland banks
- Limiting soils
- Land use
- Land value
- Land availability
- Agricultural lands
- Trails



[www.portmacrail.com](http://www.portmacrail.com)

# Corridor development process



2007 Corridors Under Consideration



[www.portmacrail.com](http://www.portmacrail.com)



# Working with the Public



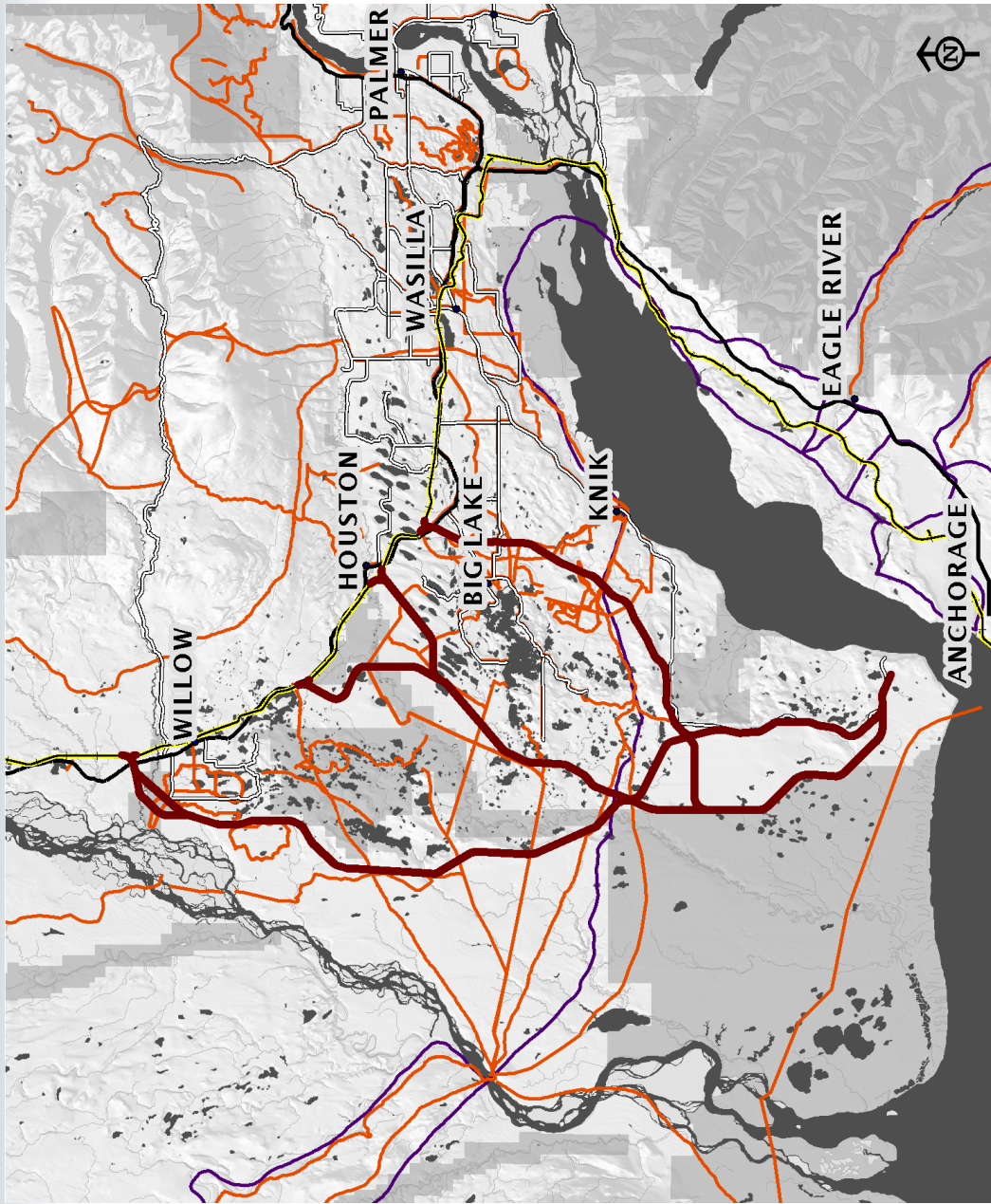
- Hosting series of public open house meetings
- Web site – informative and interactive
- Newsletter (forthcoming)
- Requesting public comment and using the comments to help determine corridors
- Meeting with small stakeholder groups
- Identification of mitigation ideas early in the process – Do you have solutions – We want to hear them
- Developing a comprehensive list of criteria to be considered and evaluated in the corridor development
- Conducting a more detailed study of the proposed corridors

**Your comments will help us find the best corridor.**



[www.portmacrail.com](http://www.portmacrail.com)

# Trails

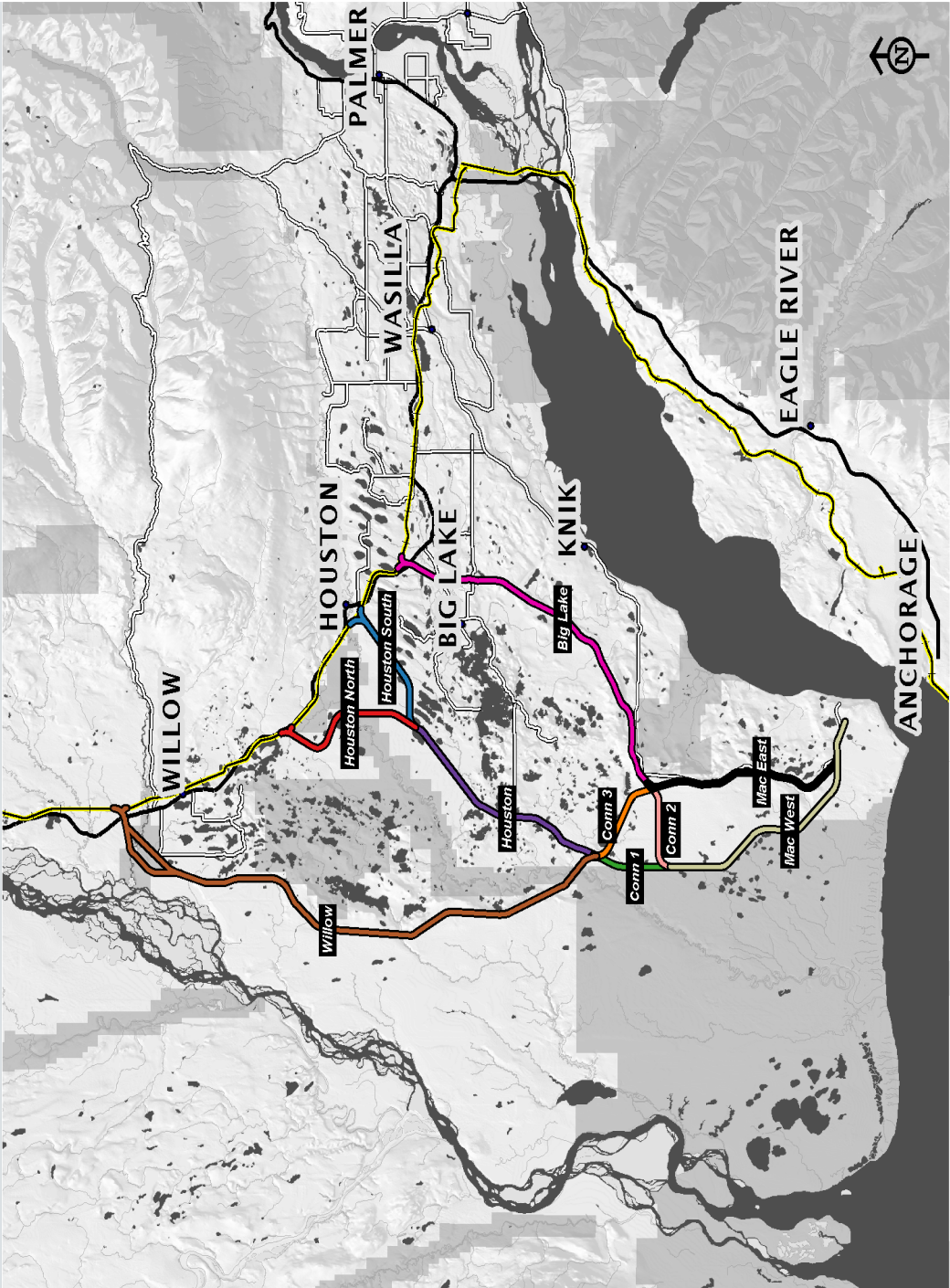


*Trails are an important consideration, especially as they relate to the quality of life concerns and recreational opportunities in the area.*



[www.portmacrail.com](http://www.portmacrail.com)

# Corridors Under Consideration



**Note: These lines represent possible corridors and are subject to change.**

[www.portmacrail.com](http://www.portmacrail.com)



# Anticipated Project Schedule

## October 2007

- Solicit input from public and regulatory agencies
- Consider possible impacts and mitigation associated with route alternatives
- File notice with STB end of October

## November 2007

- Finalize route considerations
- Refine possible mitigation based upon potential impacts

## December 2007

- Complete Route Alternatives document and submit to STB
- Develop possible MOA's/MOU's as appropriate
- Anticipate the STB to select 3<sup>rd</sup> Party NEPA contractor by end of 2007

## 2008 - 2009

- File petition in early spring 2008
- Anticipate the STB 3<sup>rd</sup> Party Contractor to complete environmental document by mid-2009

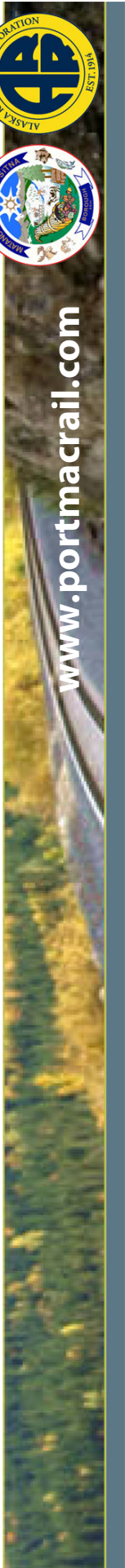


[www.portmacrail.com](http://www.portmacrail.com)



*Thank You for coming!*

Please visit the stations in the room and provide input and ask questions.



*(This page intentionally left blank.)*



# PROJECT FAQs

## Why do we need an extension to Port MacKenzie?

The extension will provide bulk transport of goods and commodities between Port MacKenzie and Interior Alaska. This will achieve the Borough's vision for the Port as a multi-modal economic engine for both the Matanuska-Susitna Borough and the State of Alaska and support the Alaska Railroad Corporation's mission of fostering the development of Alaska's economy by integrating railroad development with rail belt community development plans.

## Who is responsible for the project?

The Port MacKenzie Rail Extension Project will be owned and operated by the Alaska Railroad Corporation (ARRC). The Borough, as the owner and operator of Port MacKenzie, is the project sponsor. A Memorandum of Agreement has been signed between the two entities, establishing "co-management" in the study of this project. Because it involves construction of a new rail line, the project will require federal approval. The Surface Transportation Board (STB) is the federal agency responsible for completing an environmental document and for ultimately approving the new route.

## This project has been considered since the 1970s. What has changed?

Over the past 30 years, several entities have considered construction of a rail connection to a port in the Mat-Su Borough. Port MacKenzie is now a port with ocean-going commerce and a road, soon-to-be-paved, but still requires rail to expand its development. Marine ports don't reach their full potential unless they are multi-modal.

The most recent and detailed of these studies was completed in 2003 (Matanuska-Susitna Borough Rail Corridor Study <http://www.matsugov.us/port/railCorridorStudy.cfm>) which culminated in the recommendation of a route that would ultimately connect to the Alaska Railroad near Willow.

The Mat-Su Borough has been the fastest

growing area of the State for the past 16 years. Conditions have changed since the project was first considered. This growth along with new technical data make it critical to take a fresh look at the various alignments. The Borough and the Alaska Railroad are partners in this project. The Borough has asked the Alaska Railroad to prepare and submit an application to construct and operate a railroad between their rail system and Port MacKenzie. This is beyond a planning action and represents a specific request to construct the rail line. Attached to it will likely be explicit conditions under which such a railroad would be constructed. An application of this type requires an environmental document (either an Environmental Assessment or Environmental Impact Statement) as required by the National Environmental Policy Act or NEPA. The environmental document will be started sometime after January of 2008 and will be another way for you to comment. The environmental document is carried out by the federal Surface Transportation Board. Right now, we're gathering as much information as possible in preparation for the application before the STB and the environmental document, which will follow.

Because of the finality associated with this action, the Borough and the ARRC felt it prudent to take a final review of all the studies and information previously assembled to ensure that the route proposed to the STB be the best possible.

## Where and when will there be public involvement opportunities?

There will be many opportunities to participate in the Port MacKenzie Rail Extension Project. ARRC and the Borough are currently gathering public comment, which will be considered prior to the formal application to the STB. The STB will also hold formal public scoping meetings to gather additional information on community concerns and issues regarding this project. The public's involvement will be critical in identifying



possible project impacts. Additional opportunities will include a Web site and submission of comments to the project team.

### **What are the benefits to the Borough?**

The rail extension could potentially open new markets for minerals and stimulate the creation of new jobs. The rail could also provide long-term benefits by lowering property taxes (through the infusion of statewide and local economic opportunities).

### **Was the rail extension project included in the Borough's Long Range Transportation Plan?**

The rail extension from Port MacKenzie to the mainline was discussed at many of the public meetings held as part of the Borough's Long Range Transportation Plan (LRTP) project. As a consequence it was added to the Official Streets and Highways Plan Map in the LRTP showing it as a future project. Communities undergoing local comprehensive planning processes may consider including discussion of the rail extension project as part of the planning effort.

### **What happens to the existing rail line?**

There will be no changes to the existing line between Anchorage and Fairbanks. The proposed extension will provide access to Port MacKenzie.

### **What are some of the issues that will be considered?**

The Surface Transportation Board, the lead federal agency responsible for conducting an environmental review of the Port MacKenzie Rail Extension Project, will study a number of issues and concerns including:

- Natural environmental impacts
- Noise
- Crossing locations
- Wetlands
- Potential impacts to private property
- Socio-economic impacts
- Historic and cultural resource impacts
- Construction and operational costs
- Engineering and construction feasibility
- Local and regional trails

What other concerns or issues do YOU have about the project? Go to [www.portmacrail.com](http://www.portmacrail.com) to provide comments.

### **How will the project be funded?**

The State of Alaska appropriated \$10 million during the 2007 legislative session to studies that will give a more thorough picture to the Surface Transportation Board. The studies will accompany an application for a license for constructing and operating the rail line. This includes alternative analysis, preliminary engineering, National Environmental Policy Act (NEPA) environmental documentation, and a financial feasibility study.

The final design and construction of a rail extension to Port MacKenzie is estimated to cost \$300 million (2007 dollars). The Matanuska-Susitna Borough and the Alaska Railroad will jointly conduct a financing study to assess options for funding the project. User fees could play a role.

### **When will the project be completed?**

The project sponsor (MSB) and applicant (ARRC) plan for rail extension operation by the year 2012. This will require a fast-paced timeline that assumes submission of the application to the STB by the beginning of 2008, STB completion of an environmental document by the end of 2008, engineering and design in 2009, and construction between 2010 and 2012. This is only an estimate, however, with the actual design and completion relying on the pace of the STB's environmental process, as well as funding availability, and the land acquisition process and actual construction conditions.

### **How will land be acquired?**

Once a route has been defined and accepted, and funding identified, the railroad will obtain third-party assessments of land that is needed for the new right-of-way. The railroad will then negotiate with landowners based primarily on fair market value. While the railroad, as a public transportation entity, can evoke eminent domain to obtain land from an unwilling landowner, it has never had to do so. Rather, the railroad has strived for fair and comprehensive negotiations, or in some cases modifying design when no resolution can be obtained with a landowner. If all avenues were exhausted and it was necessary to evoke eminent domain and acquire land without landowner consent, ARRC would comply with the Uniform Relocation Act (URA), and the landowner would be compensated under the URA for the property loss.





[www.portmacrail.com](http://www.portmacrail.com)

**Patty Sullivan**  
Public Affairs Director,  
Matanuska-Susitna Borough  
350 E. Dahlia Ave.  
Palmer, AK 99645  
907-745-9577 tel  
psullivan@matsugov.us

**Stephenie Wheeler**  
Public Involvement Officer,  
Alaska Railroad Corporation  
327 W. Ship Creek Ave.  
Anchorage, AK 99501  
907-265-2671 tel  
wheelers@akrr.com



[www.portmacrail.com](http://www.portmacrail.com)

**Patty Sullivan**  
Public Affairs Director,  
Matanuska-Susitna Borough  
350 E. Dahlia Ave.  
Palmer, AK 99645  
907-745-9577 tel  
psullivan@matsugov.us

**Stephenie Wheeler**  
Public Involvement Officer,  
Alaska Railroad Corporation  
327 W. Ship Creek Ave.  
Anchorage, AK 99501  
907-265-2671 tel  
wheelers@akrr.com



[www.portmacrail.com](http://www.portmacrail.com)

**Patty Sullivan**  
Public Affairs Director,  
Matanuska-Susitna Borough  
350 E. Dahlia Ave.  
Palmer, AK 99645  
907-745-9577 tel  
psullivan@matsugov.us

**Stephenie Wheeler**  
Public Involvement Officer,  
Alaska Railroad Corporation  
327 W. Ship Creek Ave.  
Anchorage, AK 99501  
907-265-2671 tel  
wheelers@akrr.com



[www.portmacrail.com](http://www.portmacrail.com)

**Patty Sullivan**  
Public Affairs Director,  
Matanuska-Susitna Borough  
350 E. Dahlia Ave.  
Palmer, AK 99645  
907-745-9577 tel  
psullivan@matsugov.us

**Stephenie Wheeler**  
Public Involvement Officer,  
Alaska Railroad Corporation  
327 W. Ship Creek Ave.  
Anchorage, AK 99501  
907-265-2671 tel  
wheelers@akrr.com



[www.portmacrail.com](http://www.portmacrail.com)

**Patty Sullivan**  
Public Affairs Director,  
Matanuska-Susitna Borough  
350 E. Dahlia Ave.  
Palmer, AK 99645  
907-745-9577 tel  
psullivan@matsugov.us

**Stephenie Wheeler**  
Public Involvement Officer,  
Alaska Railroad Corporation  
327 W. Ship Creek Ave.  
Anchorage, AK 99501  
907-265-2671 tel  
wheelers@akrr.com



[www.portmacrail.com](http://www.portmacrail.com)

**Patty Sullivan**  
Public Affairs Director,  
Matanuska-Susitna Borough  
350 E. Dahlia Ave.  
Palmer, AK 99645  
907-745-9577 tel  
psullivan@matsugov.us

**Stephenie Wheeler**  
Public Involvement Officer,  
Alaska Railroad Corporation  
327 W. Ship Creek Ave.  
Anchorage, AK 99501  
907-265-2671 tel  
wheelers@akrr.com



[www.portmacrail.com](http://www.portmacrail.com)

**Patty Sullivan**  
Public Affairs Director,  
Matanuska-Susitna Borough  
350 E. Dahlia Ave.  
Palmer, AK 99645  
907-745-9577 tel  
psullivan@matsugov.us

**Stephenie Wheeler**  
Public Involvement Officer,  
Alaska Railroad Corporation  
327 W. Ship Creek Ave.  
Anchorage, AK 99501  
907-265-2671 tel  
wheelers@akrr.com



[www.portmacrail.com](http://www.portmacrail.com)

**Patty Sullivan**  
Public Affairs Director,  
Matanuska-Susitna Borough  
350 E. Dahlia Ave.  
Palmer, AK 99645  
907-745-9577 tel  
psullivan@matsugov.us

**Stephenie Wheeler**  
Public Involvement Officer,  
Alaska Railroad Corporation  
327 W. Ship Creek Ave.  
Anchorage, AK 99501  
907-265-2671 tel  
wheelers@akrr.com



[www.portmacrail.com](http://www.portmacrail.com)

**Patty Sullivan**  
Public Affairs Director,  
Matanuska-Susitna Borough  
350 E. Dahlia Ave.  
Palmer, AK 99645  
907-745-9577 tel  
psullivan@matsugov.us

**Stephenie Wheeler**  
Public Involvement Officer,  
Alaska Railroad Corporation  
327 W. Ship Creek Ave.  
Anchorage, AK 99501  
907-265-2671 tel  
wheelers@akrr.com



[www.portmacrail.com](http://www.portmacrail.com)

**Patty Sullivan**  
Public Affairs Director,  
Matanuska-Susitna Borough  
350 E. Dahlia Ave.  
Palmer, AK 99645  
907-745-9577 tel  
psullivan@matsugov.us

**Stephenie Wheeler**  
Public Involvement Officer,  
Alaska Railroad Corporation  
327 W. Ship Creek Ave.  
Anchorage, AK 99501  
907-265-2671 tel  
wheelers@akrr.com





# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Phone




Address

Email

Your Comments:

Large empty area for writing comments.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:

	HDR Alaska Inc. Port MacKenzie Rail Extension Comment 836 S. Colony Way, Suite 2 Palmer, AK 99645		<a href="mailto:info@portmacrail.com">info@portmacrail.com</a>
			Fax (907) 644-2022


*(This page intentionally left blank.)*



*Welcome! Please Sign In*

Port MacKenzie Rail Extension Project  
Public Open House Meeting


[www.portmacrail.com](http://www.portmacrail.com)




## Project History & Area


**HISTORY**

- Port planning began in the early 1970s
- Port facilities developed in 1990s
- 2003 Railroad Corridor Study is the most recent analysis
- 2007 Legislature provided \$10 million to Mat-Su Borough
- June 2007 MSB and ARRC develop MOA to jointly pursue STB approval



[www.portmacrail.com](http://www.portmacrail.com)







## Project Purpose

**Elements of the project:**

- 30 to 45 miles of new railroad
- 200 foot right of way
- Railroad only (with maintenance road)
- Final design and construction estimated to cost \$300 million (2007 dollars)
- Could be operational by 2012

Establish a rail link between Port MacKenzie and the Alaska Railroad, providing Port MacKenzie customers/ shippers efficient rail transportation between the port and interior Alaska.

[www.portmacrail.com](http://www.portmacrail.com)



## Project Participants

Alaska Railroad Corporation.....Applicant

Matanuska-Susitna Borough.....Project Sponsor

Surface Transportation Board...Lead Federal Agency

HDR Alaska, Inc. ....Environmental Consultant

TNH-Hanson, LLC .....Engineering Consultant

[www.portmacrail.com](http://www.portmacrail.com)






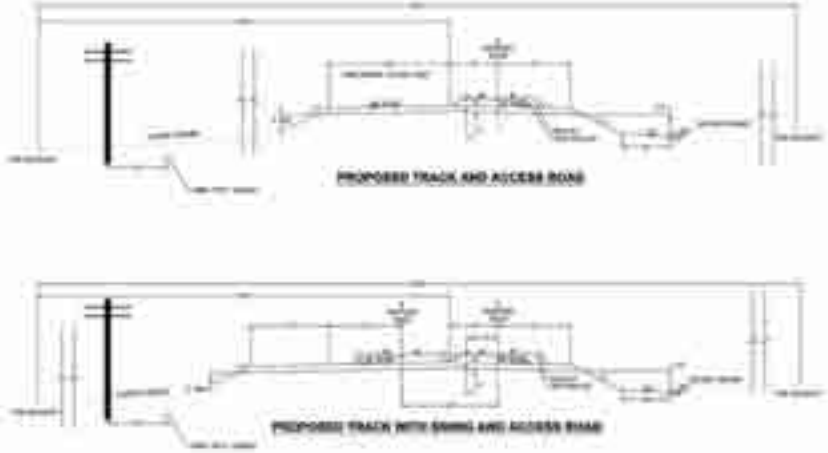
## Project Schedule

- October 2007 – Public Open Houses
  - o October 1 – Lake Lucille Inn, Wasilla
  - o October 2 – Big Lake Elementary, Big Lake
  - o October 3 – Willow Area Community Organization, Willow
  - o October 4 – Knik Elementary, Knik
  - o October 5 – Houston Middle School, Houston
- October 2007 – MSB Transportation Advisory Board
- November 5, 2007 – MSB Planning Commission Public Hearing
- November 20, 2007 – Presentation to MSB Assembly
- Mid-December 2007 – Submit Alignment Alternatives Document to STB


[www.portmacrail.com](http://www.portmacrail.com)

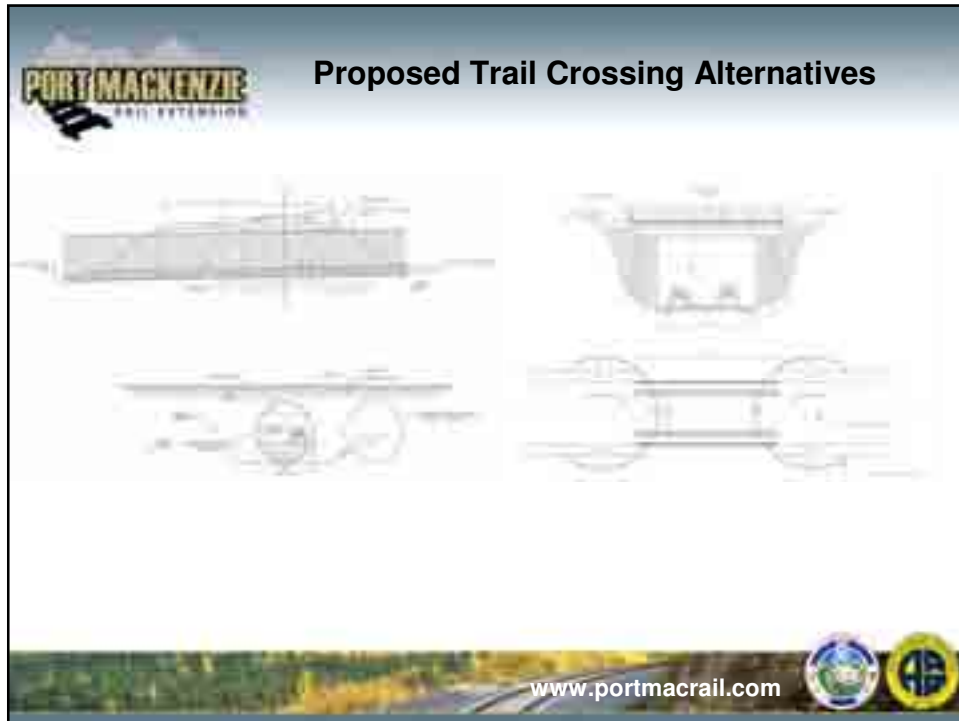


## Proposed Track and Maintenance Road



[www.portmacrail.com](http://www.portmacrail.com)






**PORT MACKENZIE RAIL EXTENSION**

## Proposed Trail Crossing Alternatives

The slide displays four technical diagrams illustrating different proposed trail crossing alternatives. The diagrams show various configurations of tracks and crossing structures, including what appears to be a bridge or overpass structure and different track alignments.

[www.portmacrail.com](http://www.portmacrail.com)



**PORT MACKENZIE RAIL EXTENSION**

## Developing Alternatives

The slide features a 3D wireframe model of a rail corridor. To the right of the model is a list of factors being considered in the development of alternatives:

- Water Quality
- Wilderness Character
- Conservation
- Water Management
- Wildlife Habitat
- Wildlife
- Wildland Health
- Energy Use
- Climate
- Land Use
- Energy Management/Requirements
- Aggregated Costs
- Cost

2007 Corridor Study Considerations

[www.portmacrail.com](http://www.portmacrail.com)





**PORT MACKENZIE RAIL EXTENSION** **Corridors Under Consideration**

\*\*\* These lines represent possible corridors and are subject to change.

[www.portmacrail.com](http://www.portmacrail.com)

**PORT MACKENZIE RAIL EXTENSION** **STB Process**

Because a new line with common carrier status triggers STB jurisdiction, the project requires a review under the National Environmental Policy Act.

**Alaska Railroad Corporation—Project Applicant**

- Files requests with the STB for the construction and operation of a new rail line
- NEPA role limited to information provider


**Surface Transportation Board—Lead Federal Agency**

- Final decision-making authority
- Considers application based upon market needs and the ARRC's ability to operate the new line

**Section of Environmental Analysis—a department of the STB**

- Conducts NEPA process independently of ARRC
- Most likely utilizes 3rd party contractor to prepare NEPA document
- Recommends conditions for construction and operation to the STB upon favorable finding

[www.portmacrail.com](http://www.portmacrail.com)






## What will the STB Consider?

The Surface Transportation Board, at a minimum, will consider the following topics during the NEPA process and development of the environmental document:

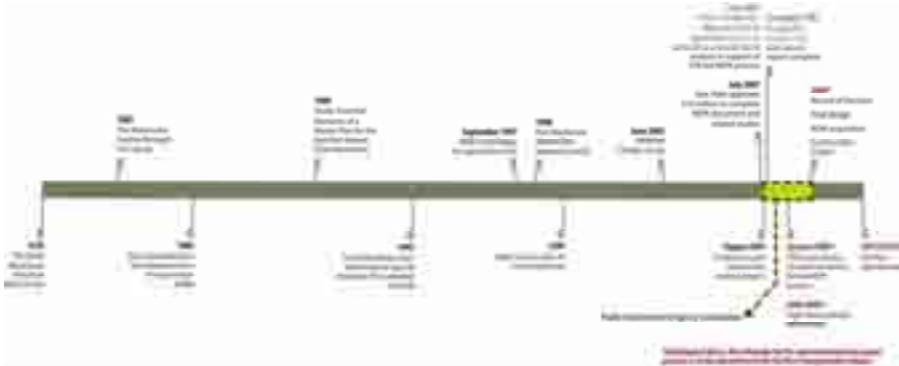
---



<ul style="list-style-type: none"> <li>✓ Proposed action and alternatives</li> <li>✓ Regional and local transportation systems and patterns</li> <li>✓ Land use</li> <li>✓ Energy</li> <li>✓ Noise</li> </ul>	<ul style="list-style-type: none"> <li>✓ Cultural &amp; Historic Resources</li> <li>✓ Safety</li> <li>✓ Biological Resources (wildlife sanctuaries, refuges, National or State parks and forests)</li> <li>✓ Air</li> <li>✓ Water (wetlands and floodplains)</li> <li>✓ Proposed Mitigation</li> </ul>
---	--

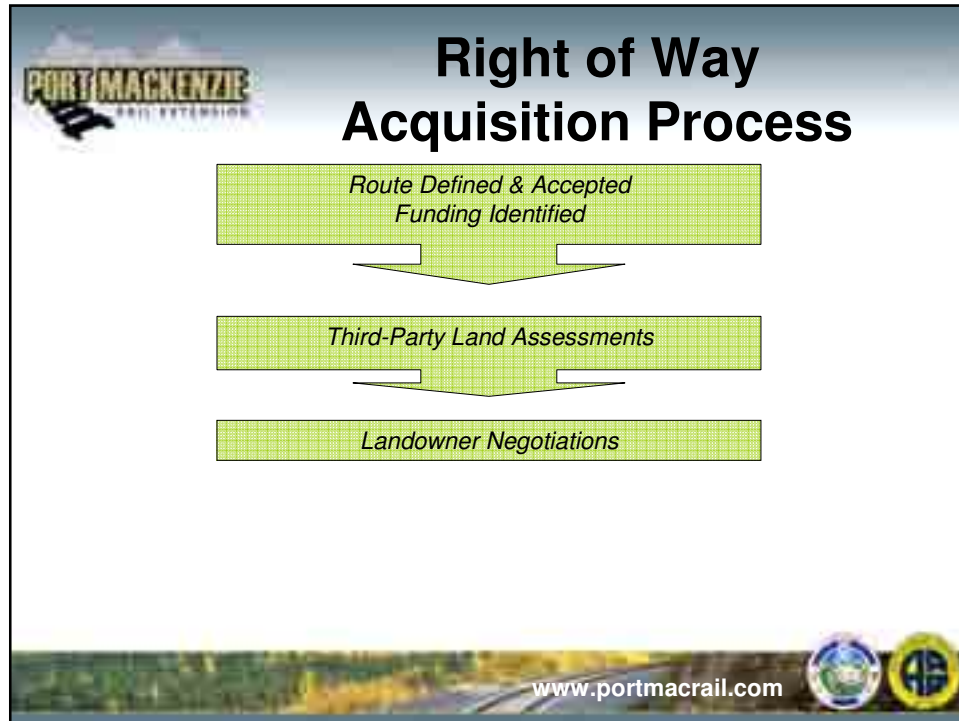
www.portmacrail.com  



## Historic Milestones & Anticipated STB Schedule



www.portmacrail.com  



**Why Comments are important NOW!**

- Provide early input as part of the alignment evaluation
- Help to identify areas of potential conflict early in the process

[www.portmacrail.com](http://www.portmacrail.com)

The slide has a white background with a blue gradient header. The title is in bold black text. Below the title are two bullet points. The background of the slide features a landscape with a road and a sky, with the website URL and logos at the bottom right.



## For more information

**Please contact:**

<i>Patty Sullivan, Public Affairs Director Matanuska-Susitna Borough Office of Public Affairs 350 East Dahlia Avenue Palmer, Alaska 99645 Phone: (907)745-9577 E-mail: <a href="mailto:patty.sullivan@matsugov.us">patty.sullivan@matsugov.us</a></i>	<i>Stephenie Wheeler, Public Involvement Officer Alaska Railroad Corporation 327 West Ship Creek Avenue Anchorage, Alaska 99501 Phone: (907) 265-2671 E-mail: <a href="mailto:wheelers@akrr.com">wheelers@akrr.com</a></i>
---	--

**Visit: [www.portmacrail.com](http://www.portmacrail.com)**



# ADVERTISEMENT MATERIAL

*(This page intentionally left blank.)*

## FRONT OF POSTCARD MAILER



Notice of Project  
**OPEN HOUSE**  
 and Opportunity to Comment

The Alaska Railroad (ARRC) and the Matanuska-Susitna Borough (MSB) invite the public to an open house to learn about a joint effort to pursue planning, engineering, design and construction of a rail connection between Port MacKenzie and the existing railroad system. Project team members will present project plans and proposals, and will answer questions from interested citizens. For more information on the Port Mackenzie Rail Extension, visit

[www.portmacrail.com](http://www.portmacrail.com)

---

**All Open House events are 6:00-8:00 p.m.  
 with a presentation at 7:00 p.m**

- **WASILLA, Oct. 1:** Lake Lucille Inn, 1300 W. Lake Lucille Drive
- **BIG LAKE, Oct. 2:** Big Lake Elementary, 3808 S. Big Lake Road
- **WILLOW, Oct. 3:** Willow Area Community Ctr, Mile 69.5 Parks Hwy
- **KNIK, Oct. 4:** Knik Elementary, 6350 W. Hollywood Drive
- **HOUSTON, Oct. 5:** Houston Middle School, 12801 W. Hawk Lane

---

Meeting facilities are accessible to individuals with disabilities. If you have special needs, please contact the Railroad by September 28, at TTY/TDD 265-2620, voice 265-2494, or via the Alaska Relay Service (ARS) TTY 1-800-770-8973 or voice 1-800-770-8255.

# BACK OF POSTCARD MAILER

## Ways to Participate....

There are several ways to participate during this information-gathering stage:

- Attend any of the 5 Public Open House events
- Visit the project web site for more project information — [www.portmacrail.com](http://www.portmacrail.com)
- Email comments to [info@portmacrail.com](mailto:info@portmacrail.com)
- Mail written comments to:  
HDR Alaska, Inc. (Port Mackenzie Rail Extension)  
836 S. Colony Way, Suite 2, Palmer, AK 99645
- Borough Contact: MSB Public Affairs Director  
Patty Sullivan, (907) 745-9577  
Railroad Contact: ARRC Public Involvement Officer  
Stephenie Wheeler, (907) 265-2671

More opportunity to participate will take place during the Environmental Impact Study (EIS) phase later on.



Alaska Railroad Corporation  
P.O. Box 107500  
Anchorage, AK 99510-7500

### CONTACT CORRECTIONS?

Please make corrections beside the label and fax to (907) 265-2365.

- Name misspelled
- Address Wrong
- Contact Person Changed
- Remove from Mailing List

PRSRT STD  
U.S. POSTAGE

**PAID**

ANCHORAGE, AK  
PERMIT NO. 500





# PORT-MACKENZIE RAIL EXTENSION

## Opportunity to Comment & **OPEN HOUSE**

The Matanuska-Susitna Borough (MSB) and the Alaska Railroad (ARRC) invite the public to an open house to learn about a joint effort to pursue planning, engineering, design and construction of a rail connection between Port MacKenzie and the existing railroad system. Project team members will present project plans and proposals, and will answer questions from interested citizens. For project information, visit [www.portmacrail.com](http://www.portmacrail.com)

**All Open House events are 6:00-8:00 p.m.  
with a presentation at 7:00 p.m**

- **WASILLA, Oct. 1:** Lake Lucille Inn, 1300 W. Lake Lucille Drive
- **BIG LAKE, Oct. 2:** Big Lake Elementary, 3808 S. Big Lake Road
- **WILLOW, Oct. 3:** Willow Area Community Ctr, Mile 69.5 Parks Hwy
- **KNIK, Oct. 4:** Knik Elementary, 6350 W. Hollywood Drive
- **HOUSTON, Oct. 5:** Houston Middle School, 12801 W. Hawk Lane



- MSB Public Affairs Director  
Patty Sullivan, (907) 745-9577
- ARRC Public Involvement Officer  
Stephenie Wheeler, (907) 265-2671



Meeting facilities are accessible to individuals with disabilities. If you have special needs, please contact the Railroad by September 28, at TTY/TDD 265-2620, voice 265-2494, or via the Alaska Relay Service (ARS) TTY 1-800-770-8973 or voice 1-800-770-8255.



# PORT-MACKENZIE

RAIL EXTENSION

## Opportunity to Comment & **OPEN HOUSE**

The Alaska Railroad (ARRC) and the Matanuska-Susitna Borough (MSB) invite the public to an open house to learn about a joint effort to pursue planning, engineering, design and construction of a rail connection between Port MacKenzie and the existing railroad system. Project team members will present project plans and proposals, and will answer questions from interested citizens. For project information, visit [www.portmacrail.com](http://www.portmacrail.com)

**All Open House events are 6:00-8:00 p.m.  
with a presentation at 7:00 p.m**

- **WASILLA, Oct. 1:** Lake Lucille Inn, 1300 W. Lake Lucille Drive
- **BIG LAKE, Oct. 2:** Big Lake Elementary, 3808 S. Big Lake Road
- **WILLOW, Oct. 3:** Willow Area Community Ctr, Mile 69.5 Parks Hwy
- **KNIK, Oct. 4:** Knik Elementary, 6350 W. Hollywood Drive
- **HOUSTON, Oct. 5:** Houston Middle School, 12801 W. Hawk Lane



- MSB Public Affairs Director  
Patty Sullivan, (907) 745-9577
- ARRC Public Involvement Officer  
Stephenie Wheeler, (907) 265-2671



Meeting facilities are accessible to individuals with disabilities. If you have special needs, please contact the Railroad by September 28, at TTY/TDD 265-2620, voice 265-2494, or via the Alaska Relay Service (ARS) TTY 1-800-770-8973 or voice 1-800-770-8255.

**Alcantra, Rosetta M.**

---

**From:** Peg Tileston [pegt@gci.net]  
**Sent:** Wednesday, September 19, 2007 2:59 PM  
**To:** What's Up  
**Subject:** [Allowed by White List] [whatsup] What's Up 9/19/07

**Attachments:** What's Up.doc



What's Up.doc

==== A message from the whatsup mailing list =====

-  
 \*\*mark those items that are new in this What's Up.

WHAT'S UP - September 19, 2007 - Compiled Weekly by Peg Tileston  
 On behalf of the Alaska Women's Environmental Network (AWEN), Alaska  
 Center for the Environment (ACE), and Alaska Conservation Alliance (ACA)

WORKSHOPS, SEMINARS, TRAINING & STATEWIDE TELECONFERENCED EVENTS

September 22

HOMER - 20 YEARS OF SAILING FOR SCIENCE will be presented from 1 to 4pm  
 at the Homer Harbor where the Tiglax is moored. Help celebrate the 20th  
 birthday of the Alaska Maritime National Wildlife Refuge's ship, the M/V  
 Tiglax. Try your hand at shipboard research, visit a virtual field camp,  
 learn about seabirds, otters, sea lions, and archaeology from the field  
 biologists and staff that study them! For more information, contact  
 Marianne Aplin, Manager, Alaska Islands & Ocean Visitor Center, at (907)  
 226-4619, Fax: (907) 235-7469, <http://IslandsAndOcean.org> or  
<http://alaskamaritime.fws.gov>.

September 21 & 22

SEWARD - FIRE IN ALASKA (ED 580 R) course to be offered again in Seward.  
 UAA tuition free (sponsored by Western States Fire Managers). This hands  
 on, interactive, and fun course features instruction and activities for  
 teachers, home-schooling parents, and others to use with students. Over  
 700 participants have learned about Alaskan fire ecology, fire behavior,  
 and fire mitigation with Fire in Alaska. First come, first served. For  
 more info or to register contact Matt Weaver [Matt.weaver@alaska.gov](mailto:Matt.weaver@alaska.gov) or  
 907-269-8481.

September 24 - 27

ANCHORAGE - 2007 ARCTIC SCIENCE CONFERENCE will be held at the Hotel  
 Captain Cook. This year's theme for the annual Arctic AAAS Division  
 meeting's theme is "PARTNERING FOR NORTHERN FUTURES: SCIENCE • POLICY •  
 EDUCATION • LEGACY • IN THE INTERNATIONAL POLAR YEAR." The featured  
 Banquet Speaker will be DAVID HIK, Canadian IPY Secretariat, and the  
 Master of Ceremonies will be GARY LAURSEN, Dept. of Biology and  
 Wildlife, UAF. For more information, contact John Kelley at  
[ffjkk@uaf.edu](mailto:ffjkk@uaf.edu) or 907-474-5585, or Arthur Roraff at  
[info@thecoordinatorsinc.com](mailto:info@thecoordinatorsinc.com) or 907-646-9000. For more information about  
 the AAAS Arctic Division and division activities contact Larry Duffy,  
 Secretary of the AAAS Arctic Division at [fychem@uaf.edu](mailto:fychem@uaf.edu) or 907-474-7525.

September 25 -27

FAIRBANKS - "2007 ALASKA COMMUNITY TRANSPORTATION TRANSIT CONFERENCE  
 will be held at the Westmark Fairbanks Hotel. This is an annual  
 conference hosted by the AK Dept. of Transportation State Transit Office  
 and is geared toward public transportation and human service

*(This page intentionally left blank.)*

**Alcantra, Rosetta M.**

---

**From:** Peg Tileston [pegt@gci.net]  
**Sent:** Wednesday, September 26, 2007 7:35 PM  
**To:** What's Up  
**Subject:** [Allowed by White List] [whatsup] What's Up 9/26/07

**Attachments:** What's Up.doc



What's Up.doc

==== A message from the whatsup mailing list =====

\*\*mark those items that are new in this What's Up.

WHAT'S UP - September 26, 2007 - Compiled Weekly by Peg Tileston  
 On behalf of the Alaska Women's Environmental Network (AWEN), Alaska  
 Center for the Environment (ACE), and Alaska Conservation Alliance (ACA)

WORKSHOPS, SEMINARS, TRAINING & STATEWIDE TELECONFERENCED EVENTS

September 25 -27

FAIRBANKS - "2007 ALASKA COMMUNITY TRANSPORTATION TRANSIT CONFERENCE  
 will be held at the Westmark Fairbanks Hotel. This is an annual  
 conference hosted by the AK Dept. of Transportation State Transit Office  
 and is geared toward public transportation and human service  
 transportation agencies in Alaska. The conference provides important,  
 up-to-date training and information for operating a public or human  
 service transportation agency. For more information, registration,  
 agenda, etc, please visit the conference website at  
[www.signup4.com/DOT2007](http://www.signup4.com/DOT2007) or the State Transit Office website at  
<http://www.dot.alaska.gov/stwdplng/transit/>. You may also email any  
 questions to the events coordinator, Robyn Langlie at  
[robyn.langlie@visionsus.com](mailto:robyn.langlie@visionsus.com).

September 28

ANCHORAGE - ALASKA CONSERVATION FOUNDATION 2007 CONSERVATION ACHIEVEMENT  
 AWARDS will be presented the Anchorage Marriott Downtown, 820 W. Seventh  
 Ave. Doors open at 6pm, Awards program begins at 7pm. Price \$25 per  
 ticket; \$250 for a table of 10. Please RSVP by calling ACF at  
 907-276-1917 or email [acfinfo@akcf.org](mailto:acfinfo@akcf.org). Please specify if you need a  
 vegetarian dish. For more information about the Awards, go to  
<http://www.akcf.org/>.

September 29 & 30

ANCHORAGE - ALASKA NATURAL HISTORY WORKSHOP: PART 1 will be held on  
 Saturday from 10am to 4pm at the Eagle Nature Center and ALASKA NATURAL  
 HISTORY WORKSHOP: PART 2 will be held at the same times on Sunday at the  
 Eagle River Nature Center. Naturalists UTE OLSSON and GISELLE BERGERON  
 will introduce nature interpretation, geology, ecology and plants of  
 Southcentral Alaska. The program is free to candidates enrolled in the  
 Interpretive Certification Program of Alaska (INCPA). Non-INCPA attendee  
 fee: \$25 for one day; \$40 for both days. Limited to 15 participants ages  
 16 and up. Call 694-2108 for more information or to register.

October 5, 6 & November 2

PALMER - "DISCOVERING WETLANDS" WORKSHOP for educators will be held at  
 Spring Creek Farm. This fun-filled workshop will be taught by wildlife  
 educators LILLY GOODMAN and ELIZABETH MANNING and will introduce formal  
 and non-formal educators to hands-on activities that teach kids about

outdoor exploration. This event will be followed by an Estuaries Discovery Lab for all ages from 2 - 4 pm. Contact Carmen at 226-4659 or carmen\_field@alaska.gov for more information.

September 30

KODIAK - Celebrate the 100 anniversary of the National Wildlife Refuges at 7pm in the Biliken Theater for a performance of A SENSE OF WONDER, written and performed by Broadway actress KAIULANI LEE. It is the story of one woman's love for the natural world and her fight to defend it. It is the account of the extremely private RACHEL CARSON thrust into the role of controversial public figure. Call (907) 487-2600 for more information.

October 1

TYONEK - The TYONEK FISH & GAME ADVISORY COMMITTEE will meet at the Tyonek Tribal Office, Robert Standifer Conference room at 1 pm to prepare Joint Board proposal comments, prepare GMU 15 small game proposal comments, prepare Board of Fisheries Lower Cook Inlet proposal comments and discuss drafting Board of Game Statewide Cycle "B" proposals. Public is encouraged to attend. For more information contact Cornell Constantine at 583-2227 or Sherry Wright at 267-2354.

October 1

WASILLA - Public hearing will be held before the Mat-Su Planning Commission in Central Public Safety Building Station 6-1, 101 W. Swanson Ave. on the conditional used permit application for the proposed COALBED METHANE PILOT PROJECT on the KIRCHER FARM on Trunk Road. The project proposes a single well on 840 acres, underground directional drilling, a 10'x20'x10' equipment building and a gravel access road. For more information, go to <http://www.fowleroilandgas.com> and <http://fowleroilandgasalaska.com/>.

October 1

WASILLA - Public hearing will be held before the Mat-Su Planning Commission in Central Public Safety Building Station 6-1, 101 W. Swanson Ave. on the conditional used permit application for the proposed COALBED METHANE PILOT PROJECT on the KIRCHER FARM on Trunk Road. The project proposes a single well on 840 acres, underground directional drilling, a 10'x20'x10' equipment building and a gravel access road. For more information, go to <http://www.fowleroilandgas.com> and <http://fowleroilandgasalaska.com/>.

**FIVE OPEN HOUSES for PORT MACKENZIE RAIL EXTENSION PROJECT will be held on the following dates and locations:**

**OCTOBER 1 - WASILLA from 6 to 8pm at Lake Lucille Inn, 1300 W. Lake Lucille Drive**

**October 2 - BIG LAKE from 6 to 8pm at Big Lake Elementary School, 3808 S. Big Lake Rd.**

**October 3 - WILLOW from 6 to 8pm at Willow Area Community Center, Mile 69.5 Parks Hwy.**

**October 4 - KNIK from 6 to 8pm at Knik Elementary School, 6350 W. Hollywood Dr.**

**October 5 - HOUSTON from 6 to 8pm at Houston Middle School, 12801 W. Hawk Lane**

**Purpose of the meetings is to inform citizens about the joint effort to PLAN, ENGINEER, DESIGN and CONSTRUCT A 30- to 45-MILE RAIL LINE FROM PORT MACKENZIE, connecting to the EXISTING RAIL SYSTEM at a point between Meadow Lakes and Willow. Project team members will be on-hand to discuss route proposals and to answer questions from interested citizens. Each event will also include a project overview presentation beginning at 7:00 p.m. For more information, go to [www.portmacrail.com](http://www.portmacrail.com) or contact Borough Public Affairs Director Patty Sullivan at 745-9577 and [psullivan@matsugov.us](mailto:psullivan@matsugov.us) or contact Railroad Public Involvement Officer Stephanie Wheeler at 265-2671 and [wheelers@akrr.com](mailto:wheelers@akrr.com).**

\*\*October 2

**Alcantra, Rosetta M.**

---

**From:** Alcantra, Rosetta M.  
**Sent:** Thursday, October 04, 2007 9:51 AM  
**To:** Alcantra, Rosetta M.  
**Subject:** 10-03-07 What's Up

**Attachments:** What's Up.doc



What's Up.doc

-----Original Message-----

From: Peg Tileston [mailto:pegt@gci.net]  
 Sent: Wednesday, October 03, 2007 10:54 PM  
 To: What's Up  
 Subject: [White List] [whatsup] Wha't Up 10/3/07

-

===== A message from the whatsup mailing list =====

-

\*\*mark those items that are new in this What's Up.

WHAT'S UP - October 3, 2007 - Compiled Weekly by Peg Tileston  
 On behalf of the Alaska Women's Environmental Network (AWEN), Alaska  
 Center for the Environment (ACE), and Alaska Conservation Alliance (ACA)

WORKSHOPS, SEMINARS, TRAINING & STATEWIDE TELECONFERENCED EVENTS

October 5, 6 & November 2

PALMER - "DISCOVERING WETLANDS" WORKSHOP for educators will be held at Spring Creek Farm. This fun-filled workshop will be taught by wildlife educators LILLY GOODMAN and ELIZABETH MANNING and will introduce formal and non-formal educators to hands-on activities that teach kids about wetlands ecosystems from the new Alaska Wildlife Curriculum, "Wetlands and Wildlife." The course will also familiarize participants with water and wetlands activities from the national curricula, Project WILD and Project Learning Tree. Come have fun, and learn how to teach science, math, language arts and other subjects through wetlands-focused activities that get kids outside and foster a sense of place. To register, contact Elizabeth Manning at ADF&G at 267-2168 or by email at elizabeth.manning@alaska.gov. Cost is \$20 payable to ANROE for materials. Students may get one graduate credit (UAA course number 580) for taking the course. If you wish to get credit, there is additional fee of \$69. The times of the course are as follows: Oct. 5, 4:30 to 8:30 pm, Oct. 6, 9am to 4:30pm with a Follow-up on Nov. 2, 5 to 8pm. (This date may be flexible depending on the needs of the group.)

October 9 - 12

HOMER - CONSERVING NATURAL RESOURCES THROUGH INTERPRETIVE PANELS and EXHIBITS Training will be held beginning at noon on Tuesday and run to 5pm on Friday at the Alaska Islands & Ocean Visitor Center. Participants design tools to spark visitor interest, encourage compliance with site rules, and meet other objectives. The course content covers the research, theme development, graphics, and presentation of interpretive exhibits. Who should attend: Outdoor recreation planners, interpretive specialists, public use specialists, landscape architects, maintenance personnel, or any conservation professional responsible for designing and producing interpretive panels and exhibits. Cost: No charge for FWS personnel for tuition. For participants from other agencies and

Port MacKenzie Rail Extension

To RECEIVE What's Up, or to ADD meetings, events, publications,  
deadlines, websites, or CHANGE EMAIL ADDRESS OR UNSUBSCRIBE, contact Peg  
Tileston at 907-561-0540, FAX 907-563-2747 or [pegt@gci.net](mailto:pegt@gci.net)..

-----  
To unsubscribe, send a blank email to:  
[whatsup-unsubscribe@lists.onenw.org](mailto:whatsup-unsubscribe@lists.onenw.org)

To change your subscription options, view list  
information and archives, visit this list's Web page:  
<http://lists.onenw.org/lists/info/whatsup>



**Alcantra, Rosetta M.**

---

**From:** Patty Sullivan [patty.sullivan@matsugov.us]  
**Sent:** Wednesday, September 26, 2007 4:36 PM  
**To:** Alcantra, Rosetta M.  
**Subject:** Fwd: FYI: Open Houses for AK Railroad Extension to Port MacKenzie  
**Importance:** High

Patty Sullivan

Public Affairs Director  
 Mat-Su Borough  
[psullivan@matsugov.us](mailto:psullivan@matsugov.us)  
 (907)745-9577

Begin forwarded message:

**From:** "Sen. Charlie Huggins" <[Senator\\_Charlie\\_Huggins@legis.state.ak.us](mailto:Senator_Charlie_Huggins@legis.state.ak.us)>  
**Date:** September 26, 2007 4:05:40 PM AKDT  
**To:** Undisclosed recipients::  
**Subject:** **FYI: Open Houses for AK Railroad Extension to Port MacKenzie**

Dear Friends,

A series of open houses will stretch from Wasilla to Willow as the **Mat-Su Borough invites the public** to review and discuss routes for a 30- to 45-mile extension of the Alaska Railroad to Port MacKenzie. If you did not read the article in the Frontiersman, here is the link to the story: [http://frontiersman.com/articles/2007/09/18/local\\_news/doc46ef85df43590341651013.txt](http://frontiersman.com/articles/2007/09/18/local_news/doc46ef85df43590341651013.txt) There is also a website to get up-to-date information on the project. Project website: [www.portmacrail.com](http://www.portmacrail.com)

Here are the details on the open houses:

**The meetings are all from 6-8 p.m., with a project overview at 7 p.m.**

**Wasilla, Oct. 1, Lake Lucille Inn ,1300 W. Lake Lucille Dr.**

**Big Lake, Oct. 2, Big Lake Elementary School, 3808 S. Big Lake Road.**

**Willow, Oct. 3, Willow Area Community Center, Mile 69.5 Parks Highway.**

**Knik, Oct. 4, Knik Elementary School, 6350 W. Hollywood Dr.**

**Houston, Oct. 5, Houston Middle School, 12801 W. Hawk Lane.**

If you have any questions about the project, please contact:

**Patty Sullivan, Public Affairs Director**  
 Matanuska-Susitna Borough  
 Office of Public Affairs

350 East Dahlia Avenue  
Palmer, Alaska 99645  
Phone: (907)745-9577  
E-mail: [patty.sullivan@matsugov.us](mailto:patty.sullivan@matsugov.us)

or

**Stephenie Wheeler, Public Involvement Officer**  
Alaska Railroad Corporation  
327 West Ship Creek Avenue  
Anchorage, Alaska 99501  
Phone: (907) 265-2671  
E-mail: [wheelers@akrr.com](mailto:wheelers@akrr.com)

---

Best Regards,

Charlie Huggins

# SMALL GROUP MEETINGS



Sept. 14, 2007

Dear Willow Dog Musers Association

**SUBJECT:** Port MacKenzie Rail Extension Project

The Matanuska-Susitna Borough and the Alaska Railroad Corporation (ARRC) will feature the Port MacKenzie Rail Extension Project during five open houses held the first week of October in Wasilla, Big Lake, Houston, Willow and Knik. The purpose of these meetings is to bring you up to date on the project and to listen to your comments.

The idea of a rail connection to Port MacKenzie has been considered for some 30 years. In June 2007, the Borough and ARRC entered into a Memorandum of Agreement to pursue construction of the Port MacKenzie Rail Extension Project. This proposed rail line would connect Port MacKenzie to the existing ARRC rail system. Depending on the route selected, the proposed rail line would involve approximately 30 to 45 miles of new track, connecting to a point somewhere between Meadow Lakes and Willow.

At the direction of the Borough, the ARRC is preparing an application to construct and operate a railroad to Port MacKenzie. The application will be submitted to the Surface Transportation Board (STB), the federal agency with final approval authority for new rail lines. This application process represents the first step to actually prepare for construction.

Immediately following the application submittal, the STB will oversee an environmental study as required under the National Environmental Policy Act (NEPA). The STB may modify and/or eliminate routes and could possibly select a new route not yet considered. The STB will host public scoping meetings throughout the environmental document process providing additional opportunity to provide comments. Ultimately, however, the final route licensed will be at the discretion of the STB and will be determined at the completion of the environmental document process.



Given the ongoing development in the project area since the last study in 2003, along with new available technical data, it is prudent to take a fresh look at the strongest alignment options prior to beginning the STB process. This review will potentially consider new field information and draw from new planning documents. We feel it is important to gather this new information and present it to the STB before the formal environmental document process begins in order to ensure that local voices are heard. The information will assist the Borough, the ARRC, and ultimately the STB in identifying measures that will help to avoid, minimize, or mitigate potential impacts.

The open houses for the Port MacKenzie Rail Extension Project update are scheduled for 6:00 – 8:00 p.m. each night, with a project presentation at 7:00 p.m.:

- October 1 – Lake Lucille Inn, Wasilla**
- October 2 – Big Lake Elementary, Big Lake**
- October 3 – Willow Community Center, Willow**
- October 4 – Knik Elementary, Wasilla**
- October 5 – Houston Middle School, Houston**

If your organization would like a separate presentation about the project, please contact me at 745-9577 or by e-mail at [psullivan@matsugov.us](mailto:psullivan@matsugov.us)

Please visit our Web site at [www.portmacrail.com](http://www.portmacrail.com) or find a link to it on the Borough and Railroad Web sites.

On behalf of the Borough and the Alaska Railroad, we appreciate your interest and involvement in this important project.

Sincerely,

*Patty Sullivan*

Patty Sullivan, Public Affairs Director  
Matanuska-Susitna Borough  
350 E. Dahlia Avenue  
Palmer, AK 99645  
(907) 745-9755  
[psullivan@matsugov.us](mailto:psullivan@matsugov.us)

President Michael Tucker  
 KNIK TRIBAL COUNCIL  
 P.O. BOX 871565  
 WASILLA, AK 99687

President and 1st Chief Dorothy Cook  
 Native Village of Eklutna  
 26339 Eklutna Village Road  
 Chugiak, AK 99567

President / CEO Margaret L. Brown  
 Cook Inlet Regional Corporation  
 P.O. Box 93330  
 Anchorage, AK 99509-3330

President Raymond Theodore  
 Knikatnu, Inc.  
 P.O. Box 872130  
 Wasilla, AK 99687

KNIK-FAIRVIEW COMM COUNCIL  
 PO BOX 877291  
 WASILLA, AK 99687

MEADOW LAKES COMM COUNCIL  
 P O BOX 878666  
 WASILLA, AK 99687

POINT MACKENZIE CC  
 PO BOX 877393  
 WASILLA, AK 99687

Chair Linda Oxley  
 WILLOW AREA COMMUNITY ORG  
 P O BOX 1027  
 WILLOW, AK 99688

Big Lake Community Council  
 PO BOX 520931  
 BIG LAKE, AK 99652

Executive Director Kathy Wells  
 Friends of Mat-Su  
 P.O. Box 116  
 Palmer, AK 99645

Executive Director Lisa Eyler  
 The Great Land Trust, Inc.  
 619 E. Ship Creek Avenue, Ste 321  
 Anchorage, AK 99501

Chair Paul Foreman  
 Sierra Club - Alaska Chapter  
 333 W. 4th Avenue, Ste 307  
 Anchorage, AK 99510

AK Regional Director Eleanor Huffines  
 The Wilderness Society  
 430 W. 7th Avenue, Ste 210  
 Anchorage, AK 99501

Executive Director Randy Virgin  
 Alaska Center for the Environment  
 807 G Street, Suite 100  
 Anchorage, AK 99501

Executive Director Lois Epstein  
 Alaska Transportation Priorities Project  
 308 G Street, Suite 211  
 Anchorage, AK 99501

Executive Director Steve Cleary  
 Alaska Public Interest Research Group  
 P.O. Box 101093  
 Anchorage, AK 99510-1093

Chamber President Chas St. George  
 Iditarod Trail Committee, Inc.  
 PO Box 870800  
 Wasilla, AK 99687

Cultural Resources Fran Seager-Boss  
 MSB Cultural Resources Department  
 350 East Dahlia  
 Palmer, AK 99645

Wasilla Historical Society  
 300 Boundary Street  
 Wasilla, AK 99654

Steve Gallagher  
 Alaska Farmlands Trust Corp.  
 248 E. Dahlia Avenue  
 Palmer, AK 99645

Mayor Dianne Keller  
 City of Wasilla  
 PO BOX 872611  
 WASILLA, AK 99687

Mayor Dale Adams  
 City of Houston  
 P.O. Box 940027  
 Houston, AK 99694

Mayor John Combs  
 City of Palmer  
 231 W. Evergreen Ave.  
 Palmer, AK 99645

F. LeRoi Heaven  
 Wasilla, Knik, Willow Historic Society  
 300 N. Boundary, Suite B  
 Palmer, AK 99645

Willow Trails Committee  
 Steve Charles  
 PO Box 845  
 Willow, AK 99688

Willow Dog Musers Association  
 emailed

*(This page intentionally left blank.)*



# Telephone Record

Project: Port MacKenzie Rail Extension	Project No: 09585-67879
Date: 09-05-07	Subject: Eklutna, Inc. Land ownership
Call to: Curtis McQueen, Shareholder Relations	Phone No: 696-2828
Call from: Rosetta Alcantra	Phone No: 644-2074

**Discussion, Agreement and/or Action:**

Mr. McQueen, Communication and Shareholder Relations Manager returned a call to Rosetta Alcantra, Public Involvement Specialist to indicate that Eklutna, Inc. did not have land in the Port MacKenzie area. He did suggest contacting Cook Inlet Regional Incorporated and talk to Kim Cunningham.



*(This page intentionally left blank.)*

**Alcantra, Rosetta M.**

---

**From:** Patty Sullivan [patty.sullivan@matsugov.us]  
**Sent:** Friday, August 31, 2007 7:24 PM  
**To:** Alcantra, Rosetta M.  
**Subject:** sent to 7 legislators


Dear Legislators,  
The project team that is working on the Port MacKenzie Rail Extension Project would like to brief you on the project before its latest details reach the public. We are considering giving a presentation Monday, Sept. 17, in the morning, perhaps 9:30 a.m. at the LIO in Wasilla.

Will this fit with your schedules? Presenters could be Borough Manager John Duffy, Railroad CEO Pat Gamble, Borough Consultant Joe Perkins, A Vice President at the Railroad, Eileen Reilly, among others.

I am leading the public involvement campaign on that project. There will be several public meetings. The Web site will soon be up:  
[portmacrail.com](http://portmacrail.com)



Please let me know if this is a workable date soon.  
Many thanks,  
Patty Sullivan

*(This page intentionally left blank.)*



*Welcome!*

Port MacKenzie Rail Extension Project

September 18, 2007 [www.portmacrail.com](http://www.portmacrail.com)  



## Project Participants

Alaska Railroad Corporation \_\_\_\_\_ Applicant

Matanuska-Susitna Borough \_\_\_\_\_ Project sponsor

Surface Transportation Board \_\_\_\_\_ Lead federal agency


HDR Alaska, Inc. \_\_\_\_\_ Consultant

TNH-Hanson, LLC \_\_\_\_\_ Consultant

STB Section of Environmental Analysis\_ NEPA Compliance

To be determined \_\_\_\_\_ 3<sup>rd</sup> Party NEPA Contractor

September 18, 2007 [www.portmacrail.com](http://www.portmacrail.com)  



## STB NEPA Process

**Railroad**

- Files notice with STB
- Files petition for the construction and operation of railroad



**STB**


- Considers application based upon Railroad's ability to operate/be stable and market needs
- Incorporates conditions from the SEA upon favorable finding

**SEA**

- Conducts NEPA process independently of railroad\*
- Most likely utilizes 3<sup>rd</sup> party contractor



\*Railroad has very limited role/input in process beyond providing information


September 18, 2007 [www.portmacrail.com](http://www.portmacrail.com)  



## What's Happening Now



- Re-evaluating and refining most promising corridors
- Incorporating latest environmental and planning information

September 18, 2007 [www.portmacrail.com](http://www.portmacrail.com)  



## Anticipated STB Schedule

- File notice with STB end of October 2007
- Produce Alignment Alternatives Document December 2007
- Select 3<sup>rd</sup> Party NEPA contractor by end of 2007
- File petition to operate and construct in early spring 2008
- Complete NEPA mid 2009

September 18, 2007 [www.portmacrail.com](http://www.portmacrail.com)  



## Next Steps

**October**

- Solicit input from public and regulatory agencies
- Consider possible impacts and mitigation associated with route alternatives

**November**

- Finalize route considerations
- Refine possible mitigation based upon potential impacts

**December**

- Complete Route Alternatives Document and submit to STB
- Develop possible MOA's/MOU's as appropriate

September 18, 2007 [www.portmacrail.com](http://www.portmacrail.com)  

**PORT MACKENZIE**  
RAIL EXTENSION

## Proposed Corridors



September 18, 2007

[www.portmacrail.com](http://www.portmacrail.com)



**PORT MACKENZIE**  
RAIL EXTENSION


## Constraints Mapping

**Sample topics considered:**

- Water bodies, anadromous streams
- Habitat
- Cultural sites
- Native allotments
- Parks and refuges
- Wetlands and wetland banks
- Soils
- Land use
- Land value

September 18, 2007

[www.portmacrail.com](http://www.portmacrail.com)





**PORT MACKENZIE**  
RAIL EXTENSION

*Thank You!*

Overview of the Port MacKenzie Rail  
Extension Project

September 18, 2007

[www.portmacrail.com](http://www.portmacrail.com)





*(This page intentionally left blank.)*



Port MacKenzie Rail Extension Project  
 University of Alaska Meeting  
 Tuesday, September 24, 2007

1:30 pm

HDR Alaska, Inc., 2525 C Street, Suite 305, Anchorage, Alaska

First Name	Last Name	Address	City	Zip	Email
BRAND	LINDA WOOD	SHIP CREEK AVE	ANCHORAGE	99501	lindawood6@akrrc.com
MARI	MONTPOMERY	3890 UNIVERSITY LAKE DR. #103	ANCHORAGE	99508	marl.montgomery@alaska.edu
DOUG	CAMPBELL	"	"	"	DOUGLAS.CAMPBELL@alaska.edu
BRAD	SWARTS	350 E DALHIA	PAIPUS AK	99645	brad.swarts@matsugov.us
ROSSETTA	ALCENTRA	2525 C STREET, STE. 305	ANCHORAGE AK	99503	rosetta.alcentra@hdrinc.com



# Meeting Notes

Subject: Port MacKenzie Rail Extension project overview	
Client: Alaska Railroad Corporation (ARRC)	
Project: Port MacKenzie Rail Extension	Project No: 09585-67879
Meeting Date: September 25, 2007	Meeting Location: HDR Alaska, Anchorage
Notes by: HDR Alaska – Rosetta Alcantra	

**Attendees:**

- University Lands: Mari Montgomery  
Doug Campbell
- HDR: Rosetta Alcantra, Public Involvement
- ARRC: Brian Lindamood, Project Manager
- MSB: Brad Sworts, Project Manager

**Topics Discussed:**

The meeting began at 1:30 pm. Brian Lindamood (ARRC) opened the meeting with introductions and an overview of the Port MacKenzie Rail Extension project.

**Highlights include:**

- Review of 2003 Corridor Study
- \$10 million appropriated by the Alaska Legislature to conduct study and prepare a license to operate a new rail extension
- ARRC would own and operate.
- Review of Surface Transportation Board process.
- Review of schedule
  - Gathering existing studies
  - Soliciting public input
  - Analysis done by end of October, with a matrix scoring system to look at the corridors
    - Meetings with Mat-Su Borough Transportation Advisory Board, Planning Commission and Assembly.
    - Anticipating the STB process to take about 18 months
    - Operational by 2011/2012 to meet the coal gasification project for Agrium
- Joint agreement between the ARRC and the MSB
- Could be looking at Right of Way and engineering as early as next year.
- ARRC is looking for 200 ft. "fee simple"

After the brief overview, Mr. Lindamood explained that the parcels (referred to maps) currently under review are at a high level and the final route would not be completed until more evaluation. There was a question regarding eminent domain. Both the ARRC and MSB have the ability to do eminent domain, and they have different processes.

There was also a discussion regarding the plans for the Port MacKenzie district and if the environmental document would include the port proper. Mr. Sworts indicated the MSB is still in the process of developing the Port Plan, Ms. Montgomery requested a copy of the plan.

Before adjourning a brief discussion regarding the project schedule took place. Mr. Lindamood went over the anticipated schedule for the Surface Transportation Board and also indicated there is not a preferred route at this time, but there is anticipation a more refined route would be determined by next spring.

Meeting ended at 2:20 pm.

###

*(This page intentionally left blank.)*

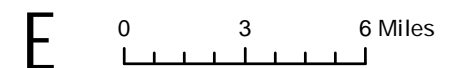


**LEGEND**

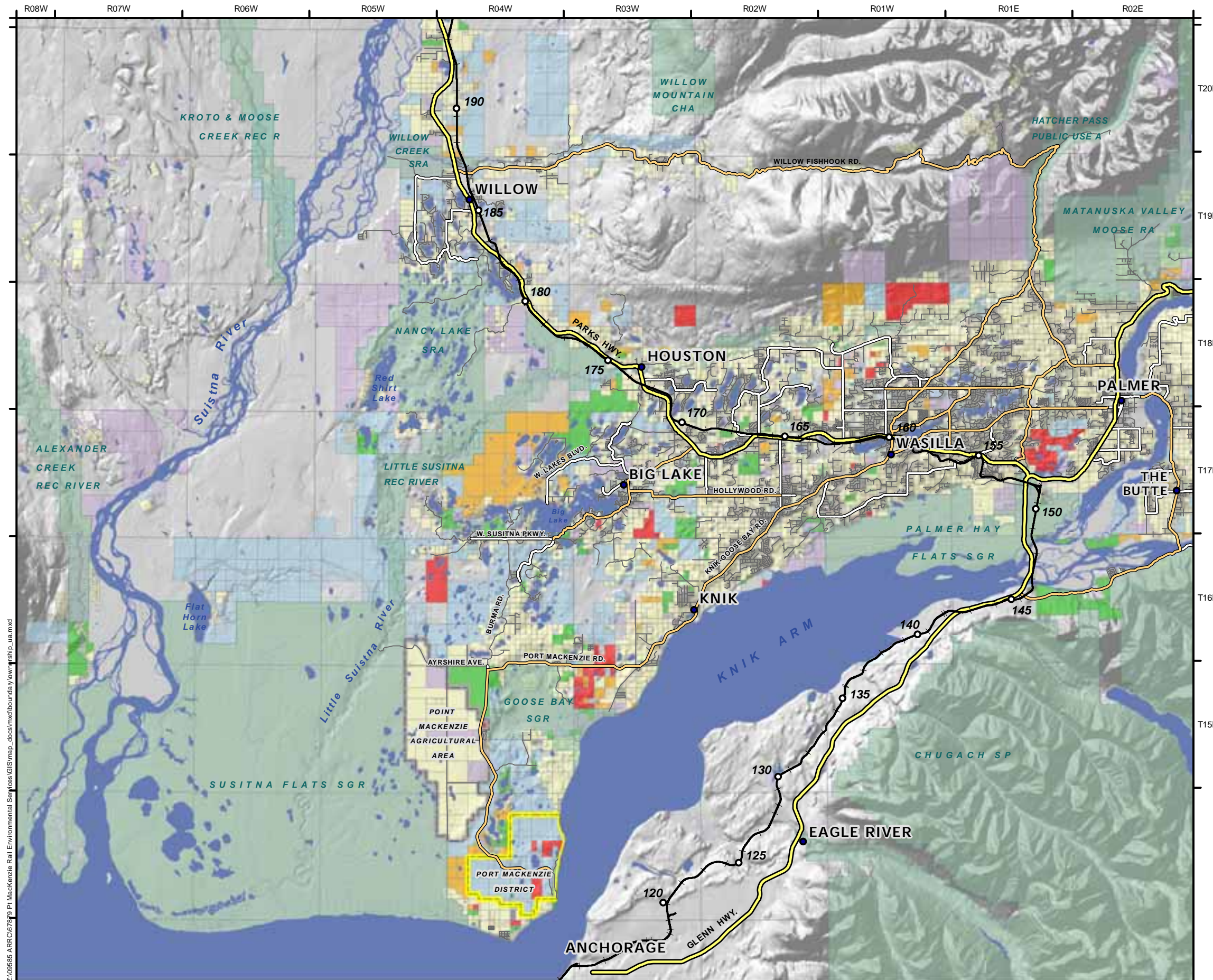
- General Land Ownership
- Public University
  - Borough
  - Mental Health
  - Native Corporation
  - Private
  - State

- ARRC Milepost
- ARRC Track
- Highway
- Major Rd.
- Medium Rd.
- Minor Rd.
- Park or Refuge
- Port MacKenzie District
- Point MacKenzie Ag Area

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: August 29, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc.,  
 MSB GIS, TNH- Hanson, USGS.



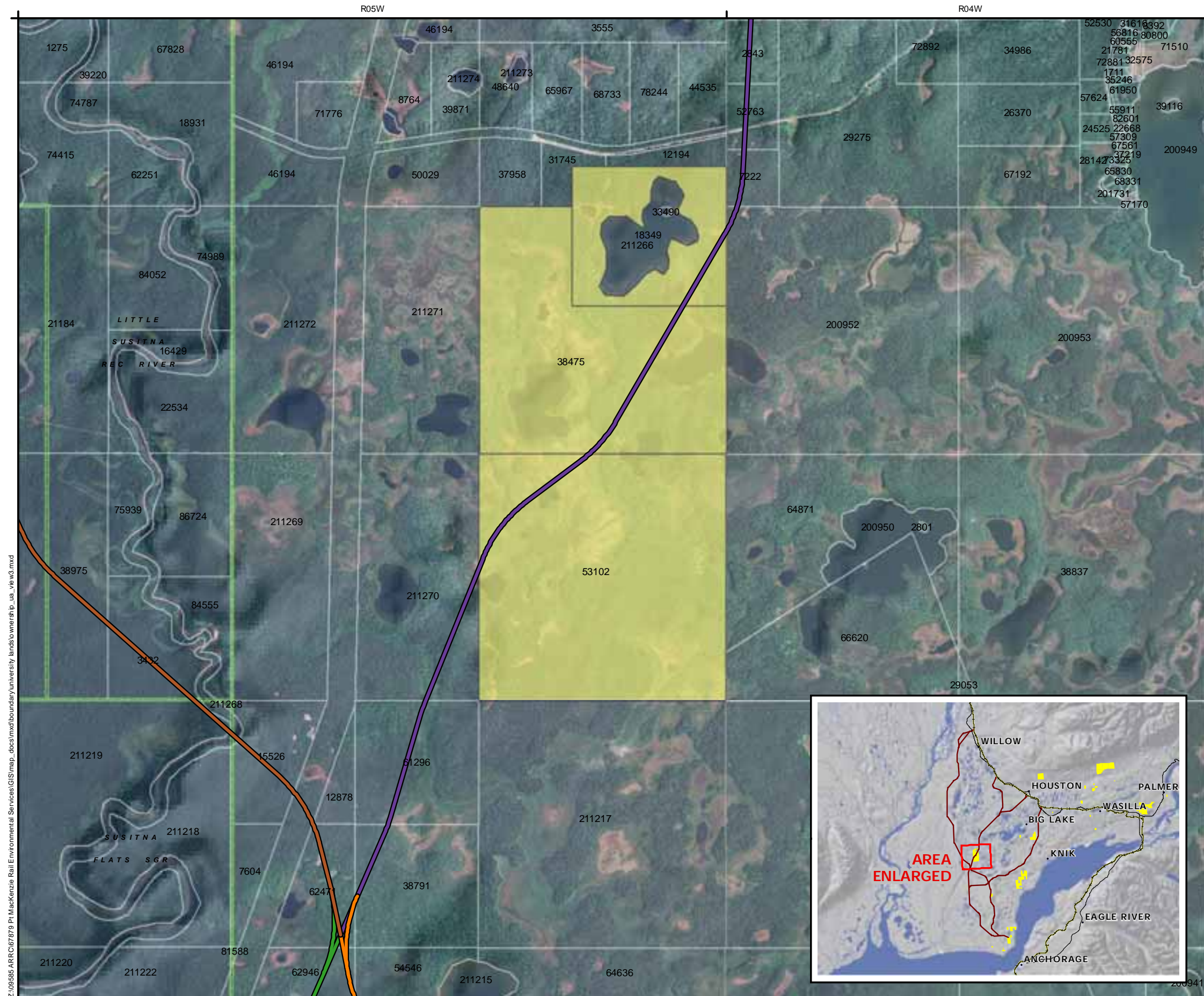
Z:\09585 ARRC\67879 P1 MacKenzie Rail Environmental Services\GIS\map\_docs\mxd\boundary\low\ship\_usa.mxd

*(This page intentionally left blank.)*

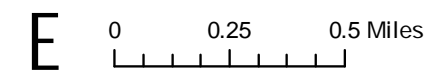


**LEGEND**

- Parcel Boundary
- Public University Lands
- Preliminary Alternatives**
- Mac East
- Mac West
- Conn 1
- Conn 2
- Conn 3
- Houston
- Houston North
- Houston South
- Willow
- Big Lake
- ARRC Track
- Point MacKenzie Ag Area
- Port MacKenzie District
- Park or Refuge



This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: September 24, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.



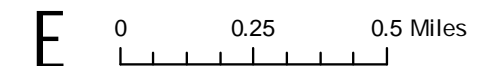
*(This page intentionally left blank.)*



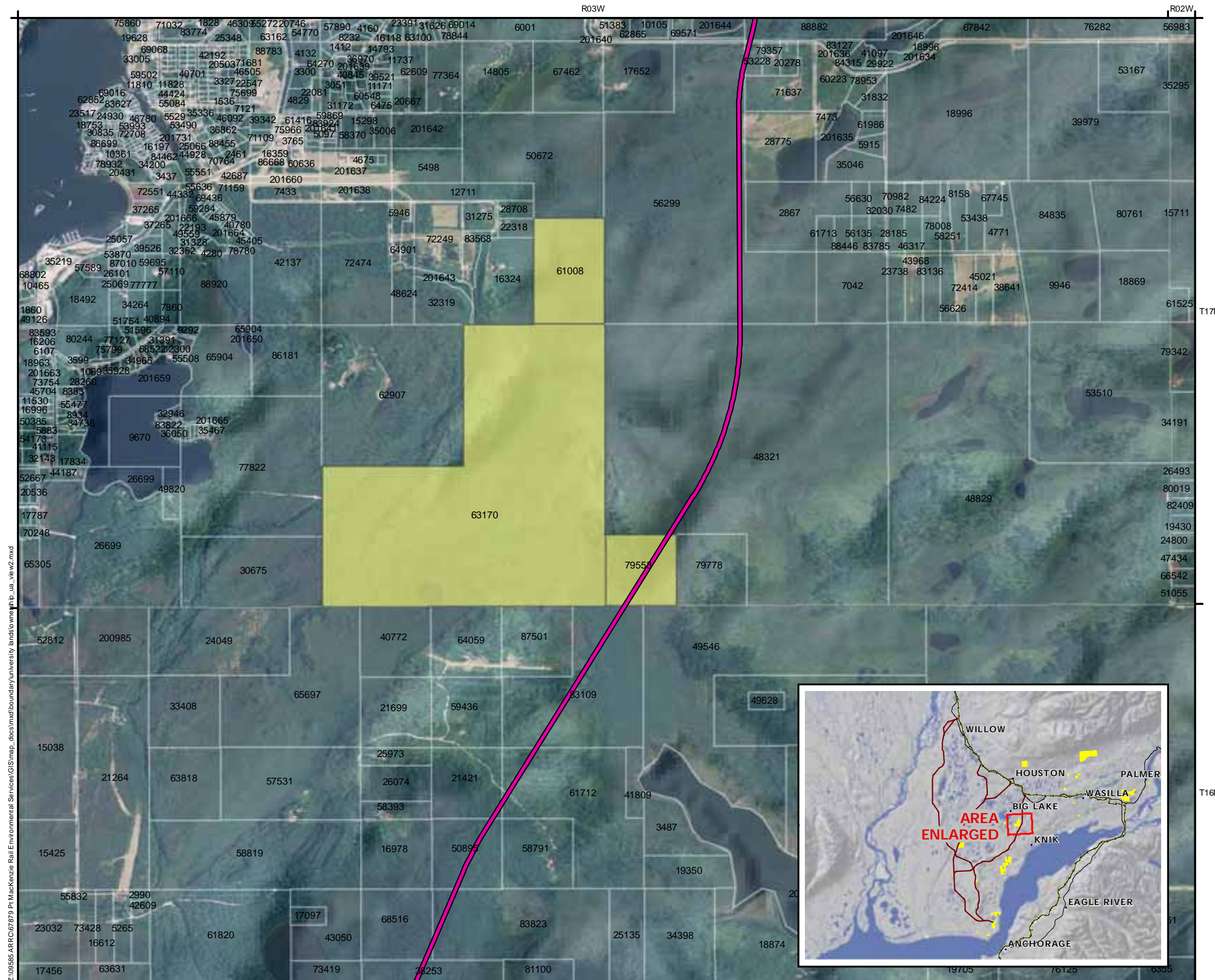
**LEGEND**

- Parcel Boundary
- Public University Lands
- Preliminary Alternatives**
- Mac East
- Mac West
- Conn 1
- Conn 2
- Conn 3
- Houston
- Houston North
- Houston South
- Willow
- Big Lake
- ARRC Track
- Point MacKenzie Ag Area
- Port MacKenzie District
- Park or Refuge

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: September 24, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc.,  
 MSB GIS, TNH- Hanson, USGS.



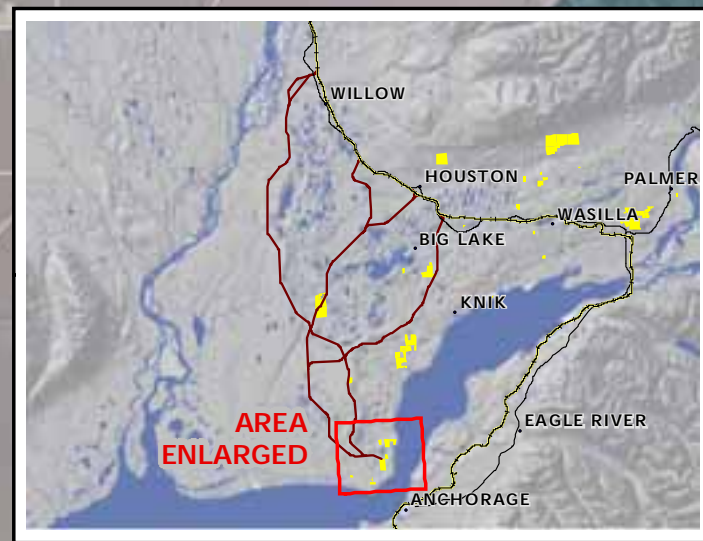
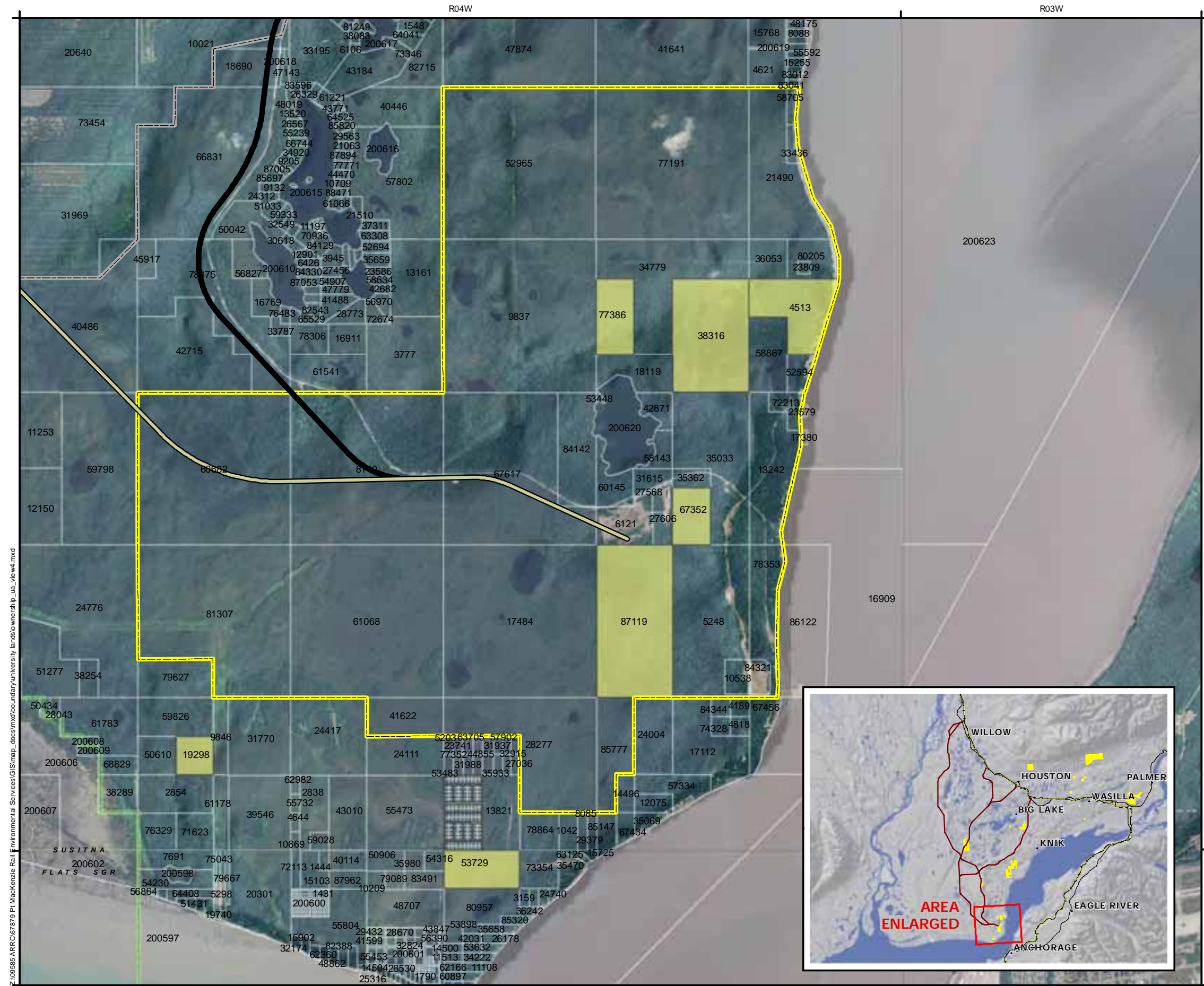
Z:\09585-ARRC\07879-Port MacKenzie Rail Environmental Services\GIS\map\_docs\mxd\boundary\university lands\cmr\fig\_1a\_v1\fig2.mxd

*(This page intentionally left blank.)*

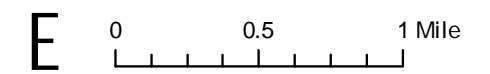


**LEGEND**

- Parcel Boundary
- Public University Lands
- Preliminary Alternatives**
- Mac East
- Mac West
- Conn 1
- Conn 2
- Conn 3
- Houston
- Houston North
- Houston South
- Willow
- Big Lake
- ARRC Track
- Point MacKenzie Ag Area
- Port MacKenzie District
- Park or Refuge



This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: September 24, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

*(This page intentionally left blank.)*



Port MacKenzie Rail Extension Project  
 Knikatu Inc. Meeting  
 Wednesday, September 25, 2007  
 10:00 am  
 HDR Alaska, Inc. 836 S. Colony Way, Palmer, Alaska

First Name	Last Name	Address	City	Zip	Email
TRACY	MCDANIEL	350 E. DANAIA	PALMER	99652	Tracy.McDaniel@mtcsugol.us
Raymond	Theodore	P.O. Box 872130 WASILLA, AK. 99654	WASILLA	99654	raymond.theodore@hotmail.com
Dawn	Lindawood		Arc	99804	lindawood@aker.com
Rosetta Alcega	Alcega	836 S. Colony Way #2 Palmer, AK	Palmer	99645	rosetta.alcega@hdkrac.com

*(This page intentionally left blank.)*

# Meeting Notes

Subject: The Port MacKenzie Rail Extension Project Overview	
Client: Alaska Railroad Corporation (ARRC)	
Project: Port MacKenzie Rail Extension	Project No: 09585-67879
Meeting Date: September 25, 2006	Meeting Location: HDR Alaska, Palmer Office
Notes by: Rosetta Alcantra	

## Attendees:

Brian Lindamood, Project Manager, ARRC  
 Tracy McDaniel, Right of Way, MSB  
 Rosetta Alcantra, Public Involvement, HDR Alaska, Inc.  
 Raymond Theodore, President, Knikatu Inc.

## Topics Discussed:

Meeting began at 10:00 am

Brian Lindamood (ARRC) opened the meeting with introductions and an overview of the Port MacKenzie Rail Extension project.

Highlights include:

- Review of 2003 Corridor Study
- \$10 million appropriated by the Alaska Legislature to conduct study and prepare a license to operate a new rail extension.
- ARRC would own and operate.
- Review of Surface Transportation Board process.
- Review of schedule
  - Gathering existing studies
  - Soliciting public input
  - Analysis done by end of October, with a matrix scoring system to look at the corridors
  - Meetings with Mat-Su Borough Transportation Advisory Board, Planning Commission and Assembly.
  - Anticipating the STB process to take about 18 months
  - Operational by 2011/2012 to meet the coal gasification project for Agrium
- Joint agreement between the ARRC and the MSB
- Could be looking at Right of Way and engineering as early as net year.
- ARRC is looking for 200 ft. "fee simple"

Mr. Theodore indicated there were land files prepared by Arc Toast in the late 1070s for the land parcel on map #2. The project could request this information in the form of a letter. Mr. Theodore also requested that a formal presentation be given to the Knikatu, Inc. Board of Directors; there are four other board members.

Before adjourning, a brief discussion subsurface rights and the coordination with CIRI (Regional Corporation) took place.

Meeting ended at 11:00 am.



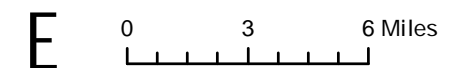
*(This page intentionally left blank.)*



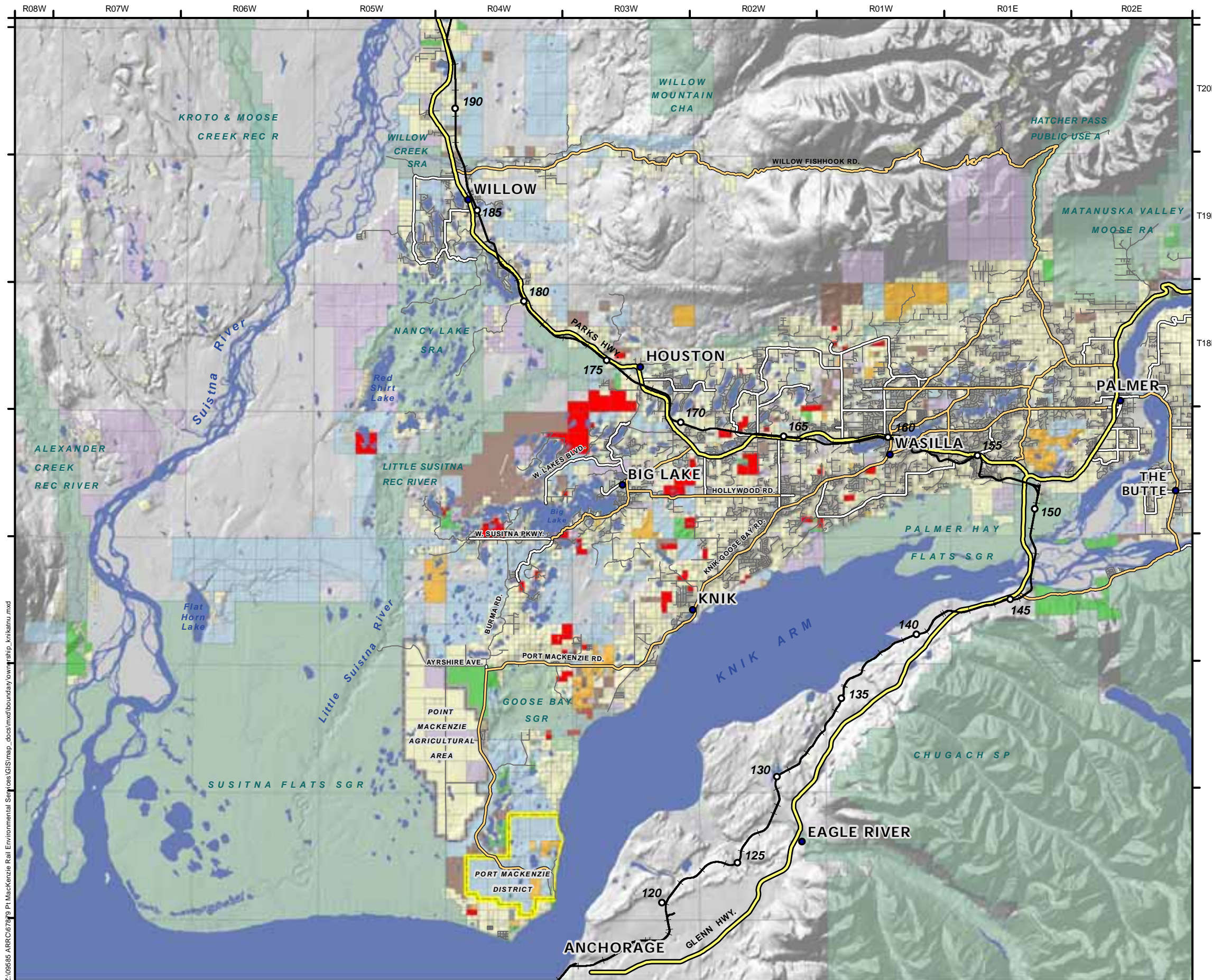
**LEGEND**

- General Land Ownership
- Knikatu, Inc.
  - Native Corporation (other)
  - Mental Health
  - Public University
  - Borough
  - Private
  - State
- 
- ARRC Milepost
  - ARRC Track
  - Highway
  - Major Rd.
  - Medium Rd.
  - Minor Rd.
  - Point MacKenzie Ag Area
  - Port MacKenzie District
  - Park or Refuge

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: August 29, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc.,  
 MSB GIS, TNH- Hanson, USGS.



Z:\09585 ARRC\67879 P1 MacKenzie Rail Environmental Services\GIS\map\_docs\mxd\boundry\low\shp\_knikatu.mxd

*(This page intentionally left blank.)*



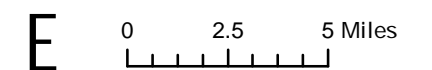
**LEGEND**

- General Land Ownership
- Knikatnu, Inc.
  - Native Corp. (other)
  - Mental Health
  - Public University
  - Borough
  - Private
  - State

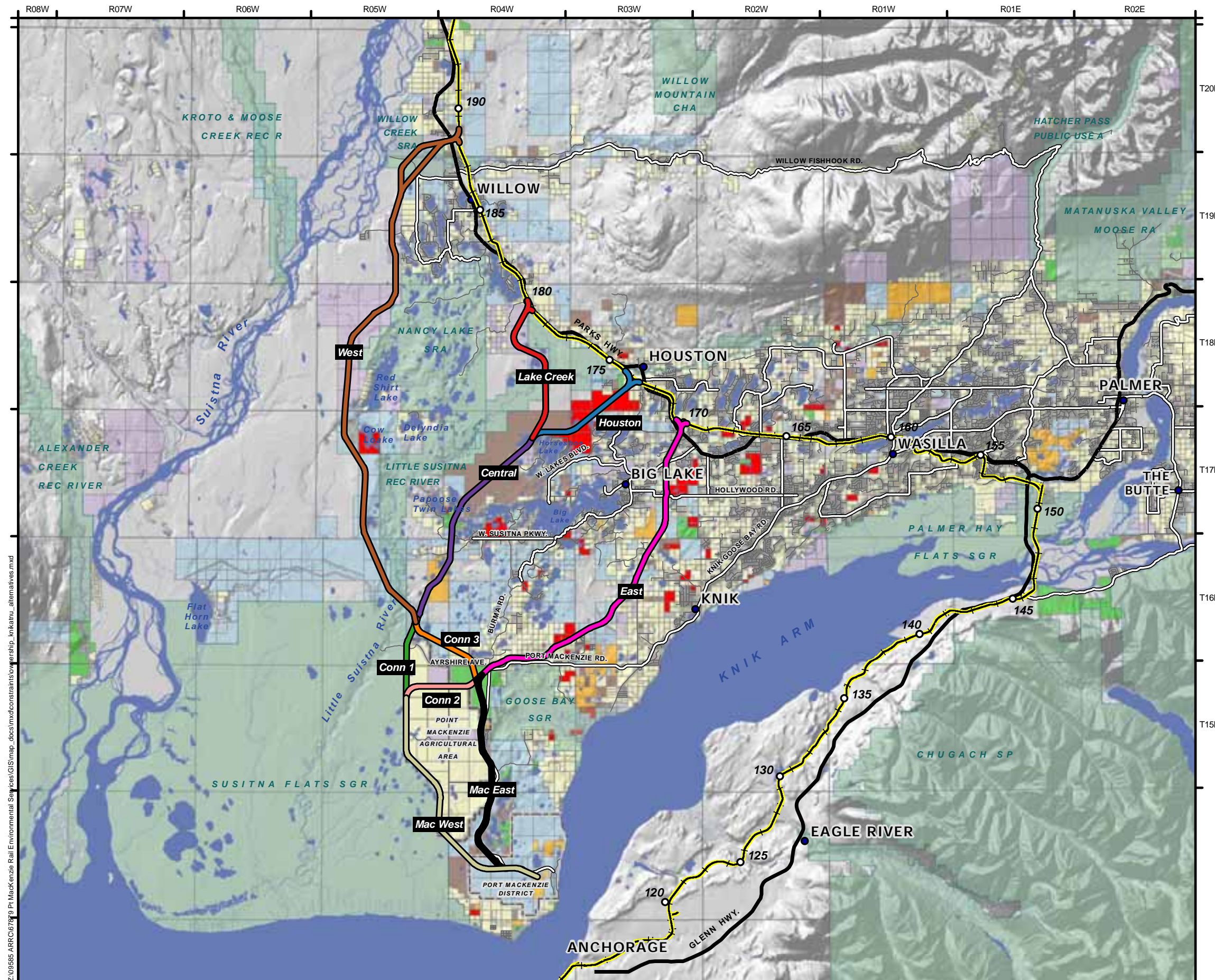
Preliminary Alternatives

- Mac East
- Mac West
- East
- West
- Conn 1
- Conn 2
- Conn 3
- Central
- Lake Creek
- Houston
- ARRC Milepost
- ARRC Track

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: August 29, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc.,  
 MSB GIS, TNH- Hanson, USGS.



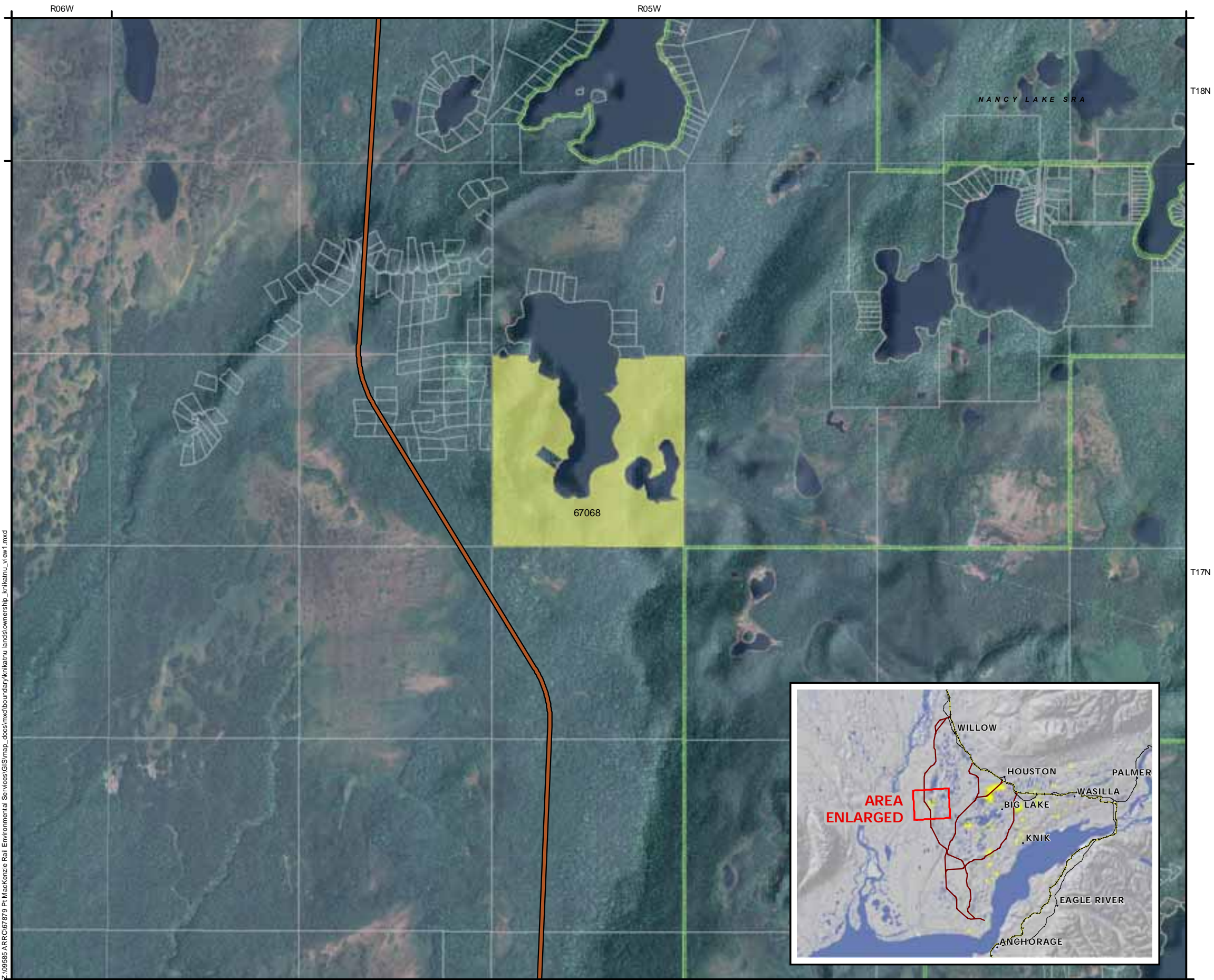
F:\09585 ARRC\67879 P1 MacKenzie Rail Environmental Services\GIS\map\_docs\mxd\constrnts\ownership\_knikatnu\_alternatives.mxd

*(This page intentionally left blank.)*

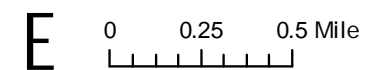


**LEGEND**

- Knikatnu, Inc.
- Parcel Boundary
- Preliminary Alternatives**
- Mac East
- Mac West
- Conn 1
- Conn 2
- Conn 3
- Houston
- Houston North
- Houston South
- Willow
- Big Lake
- ARRC Track
- Point MacKenzie Ag Area
- Port MacKenzie District
- Park or Refuge



This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: September 25, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc.,  
 MSB GIS, TNH- Hanson, USGS.

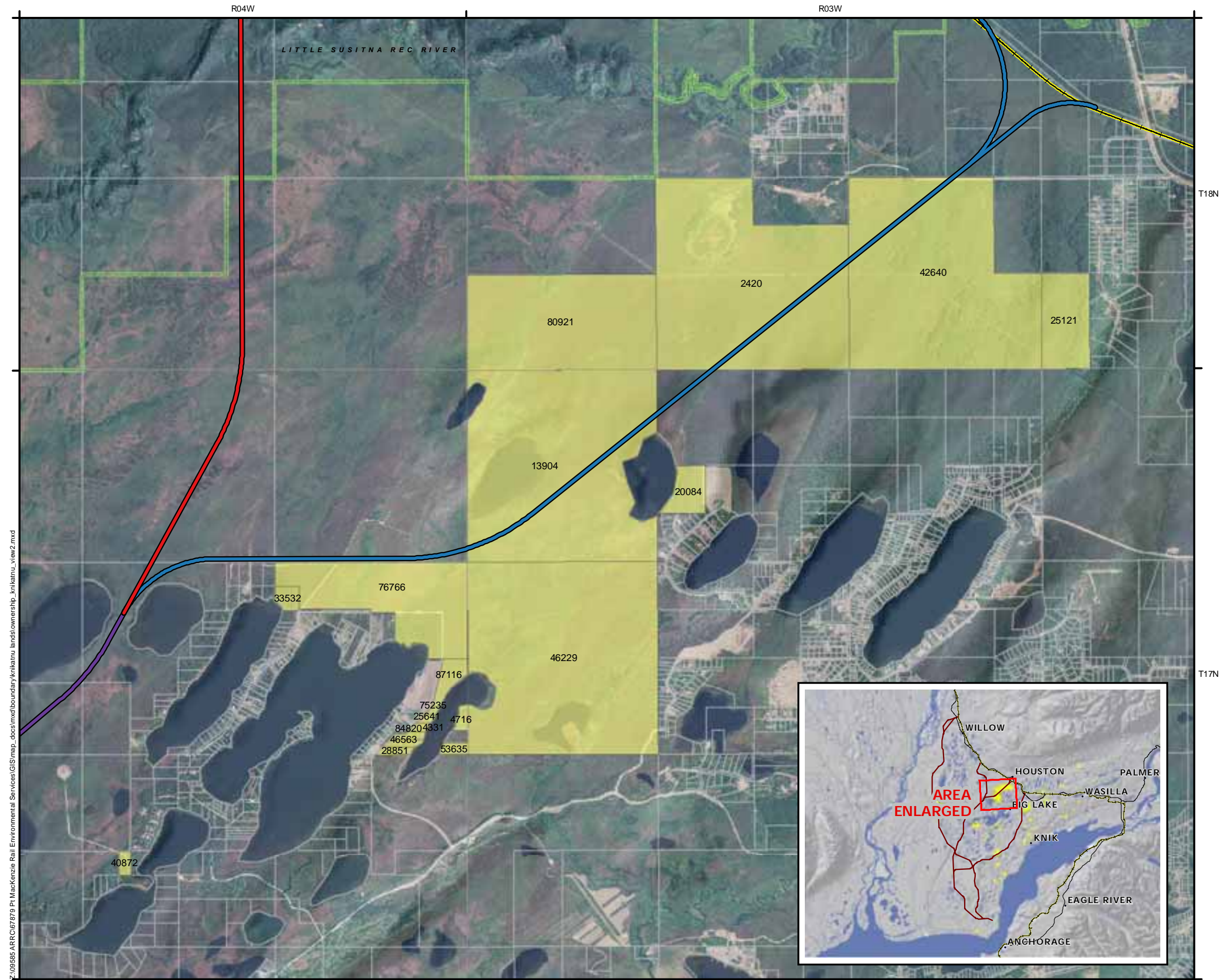
F:\09585 ARRC\67879 P1 MacKenzie Rail Environmental Services\GIS\map\_docs\mxd\boundary\Knikatnu lands\ownership\_knikatnu\_view1.mxd

*(This page intentionally left blank.)*



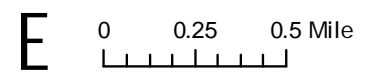
**LEGEND**

- Knikatnu, Inc.
- Parcel Boundary
- Preliminary Alternatives**
- Mac East
- Mac West
- Conn 1
- Conn 2
- Conn 3
- Houston
- Houston North
- Houston South
- Willow
- Big Lake
- ARRC Track
- Point MacKenzie Ag Area
- Port MacKenzie District
- Park or Refuge



F:\09585 ARRC\67879 P1 MacKenzie Rail Environmental Services\GIS\map\_docs\mxd\boundary\knikatnu\_lands\ownership\_knikatnu\_view2.mxd

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: September 25, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.



*(This page intentionally left blank.)*



**LEGEND**

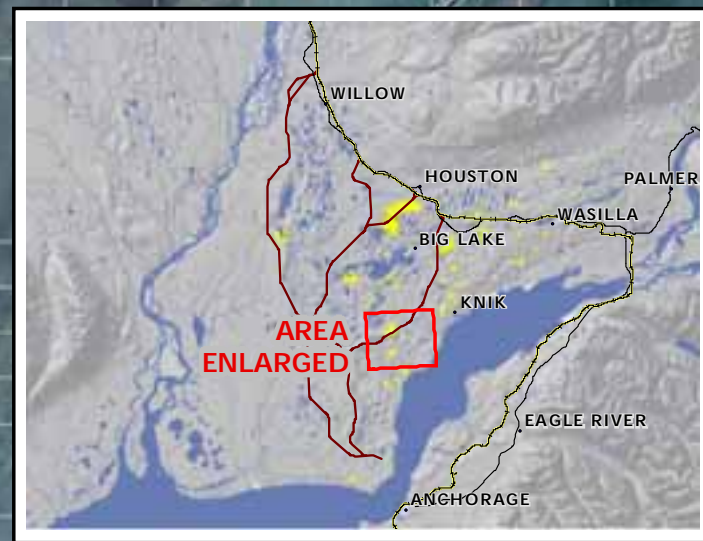
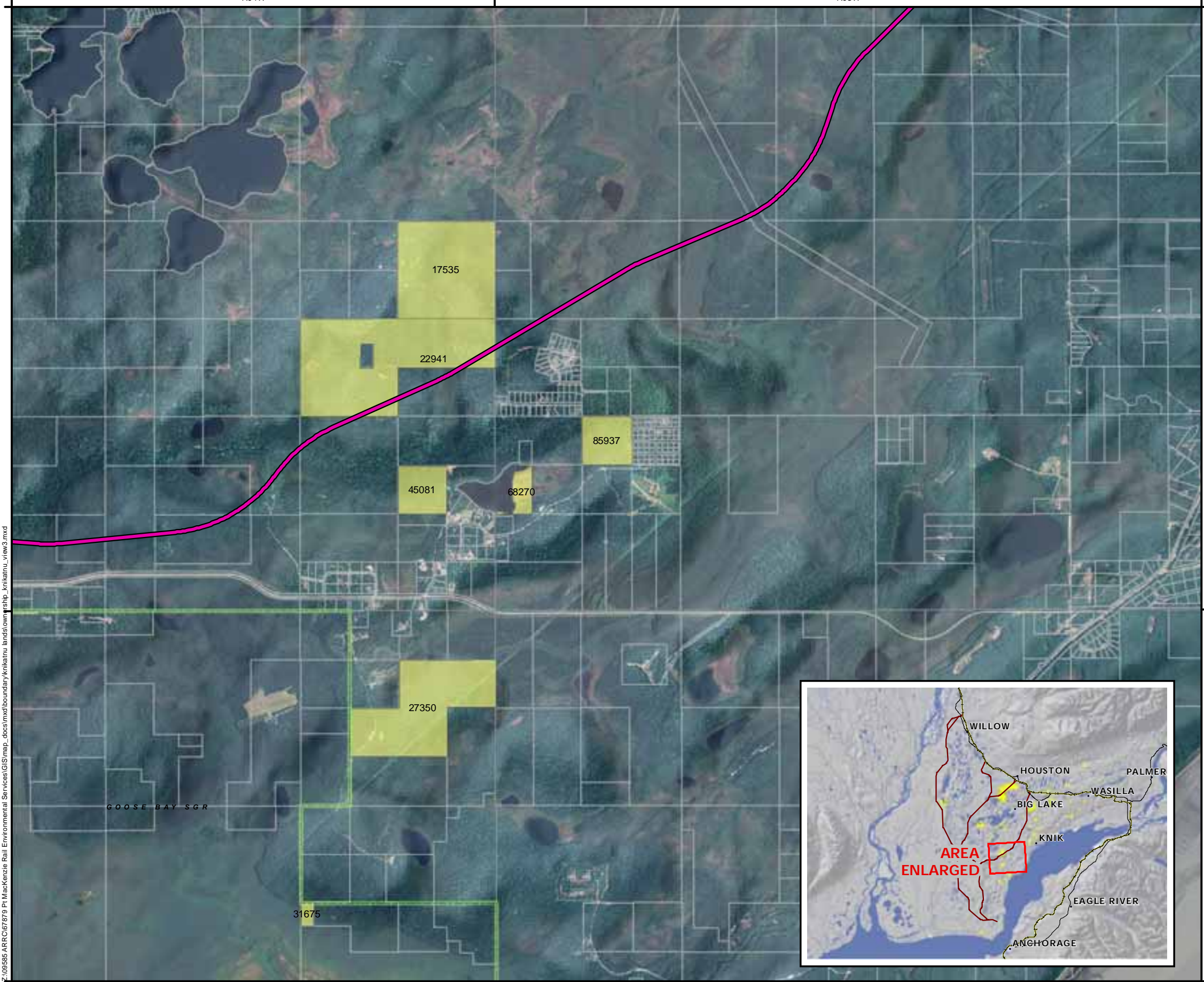
- Knikatnu, Inc.
- Parcel Boundary
- Preliminary Alternatives**
- Mac East
- Mac West
- Conn 1
- Conn 2
- Conn 3
- Houston
- Houston North
- Houston South
- Willow
- Big Lake
- ARRC Track
- Point MacKenzie Ag Area
- Port MacKenzie District
- Park or Refuge

T16N

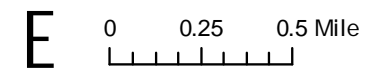
T15N

R04W

R03W



This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



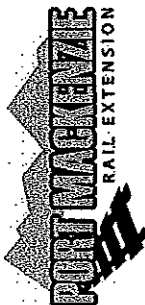
Date: September 25, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

F:\09585 ARRC\7879 P1 MacKenzie Rail Environmental Services\GIS\map\_docs\mxd\boundary\Knikatnu lands\down\ship\_knikatnu\_views.mxd

*(This page intentionally left blank.)*

Port MacKenzie Rail Extension Project  
 The Trust Land Office Meeting  
 Wednesday, September 25, 2007  
 2:30 pm

HDR Alaska, Inc., 2525 C Street, Suite 305, Anchorage, Alaska



First Name	Last Name	Address	City	Zip	Email
Wendy	Wolf	718 L St. Suite 202 <del>Anchorage AK 99501</del>	Anchorage	99501	wendy.wolf@alaska.gov
MIKE	FRANZER	" "	"	"	MIKE.FRANZER@alaska.gov
TRACY	MCDANIEL	350 E. DAKOTA AVE	PRIMER	AK	Tracy.McDaniel@mat.supnet.us
Brian	LINDAMOOD		ANC	99504	lindamoodb@atrr.com
Rosetta	Alcuntra	2525 C Street #305	Anch.	99503	rosetta.alcuntra@hadrinc.com

*(This page intentionally left blank.)*

# Meeting Notes

Subject: The Trust Land Office Project Overview	
Client: Alaska Railroad Corporation (ARRC)	
Project: Port MacKenzie Rail Extension	Project No: 09585-67879
Meeting Date: September 25, 2006	Meeting Location: HDR Alaska Office
Notes by: Rosetta Alcantra	

**Attendees:**

Brian Lindamood, Project Manager, ARRC  
 Tracy McDaniel, Right of Way, MSB  
 Rosetta Alcantra, Public Involvement, HDR Alaska, Inc.  
 Wendy Woolf, Deputy Director, The Trust Land Office  
 Mike Franger, Senior Resource Manager, The Trust Land Office

**Topics Discussed:**

Meeting began at 2:30 pm.

Brian Lindamood (ARRC) opened the meeting with introductions and an overview of the Port MacKenzie Rail Extension project.

Highlights include:

- Review of 2003 Corridor Study
- \$10 million appropriated by the Alaska Legislature to conduct study and prepare a license to operate a new rail extension.
- ARRC would own and operate.
- Review of Surface Transportation Board process.
- Review of schedule
  - Gathering existing studies
  - Soliciting public input
  - Analysis done by end of October, with a matrix scoring system to look at the corridors
  - Meetings with Mat-Su Borough Transportation Advisory Board, Planning Commission and Assembly.
  - Anticipating the STB process to take about 18 months
  - Operational by 2011/2012 to meet the coal gasification project for Agrium
- Joint agreement between the ARRC and the MSB
- Could be looking at Right of Way and engineering as early as next year.
- ARRC is looking for 200 ft. "fee simple"

There was discussion on dealing with surface and mineral estate. Brian indicated the project would need utility corridor access. The Trust Office indicated there cannot be any restrictions on the mineral estates. There could potentially be minimum extraction; however any extraction would not impact the surface estate. Brian also mentioned the ARRC and State conveyance, a relationship established with 2004 legislation that would give fee title to the ARRC.

Wendy Woolf indicated that under federal law, the Trust Land Office cannot convey the mineral estate. Under federal entitlement, if the subsurface estate is designated for another use, this weakens the position of the Trust Land Office.

The meeting also included a brief discussion on the corridors developed to date. The Alternatives map was available at the meeting along with the Trust Land Office parcels identified (see attachment).

Highlights include:

- Mac East – access issues when following the road.
- Houston route – issues include, Little Susitna River, Castle Mountain Fault and the property held by The Trust Land Office and the Knikatnu, Inc.
- Ms. Woolf indicted the refuge area, The Trust Land Office owns the mineral estate (oil and gas), they would not convey subsurface, but this should not impact the project.
- Ms. Woolf asked if the parcels identified were surface or subsurface. She said there is a difference and the project team should look at their website or she could send over the data. Brian requested the data be sent to Rosetta Alcantra.
- The Trust Land Office is not opposed at this point to the project; the rail extension would open their land for development. They are in the process of testing for oil and gas, and it would be enticing to have the ARRC there. In the Big Lake area they are looking at commercial and residential, the other corridors they do not have any plans right now.
- Does the railroad need a larger siding area? Brian indicated they are looking at a queuing area at the Y with 8000 foot siding to the north of it. Will also need communication terminals, 1-2 maintenance facilities (Y-area and other end), rail sidings are expected every 10-miles.

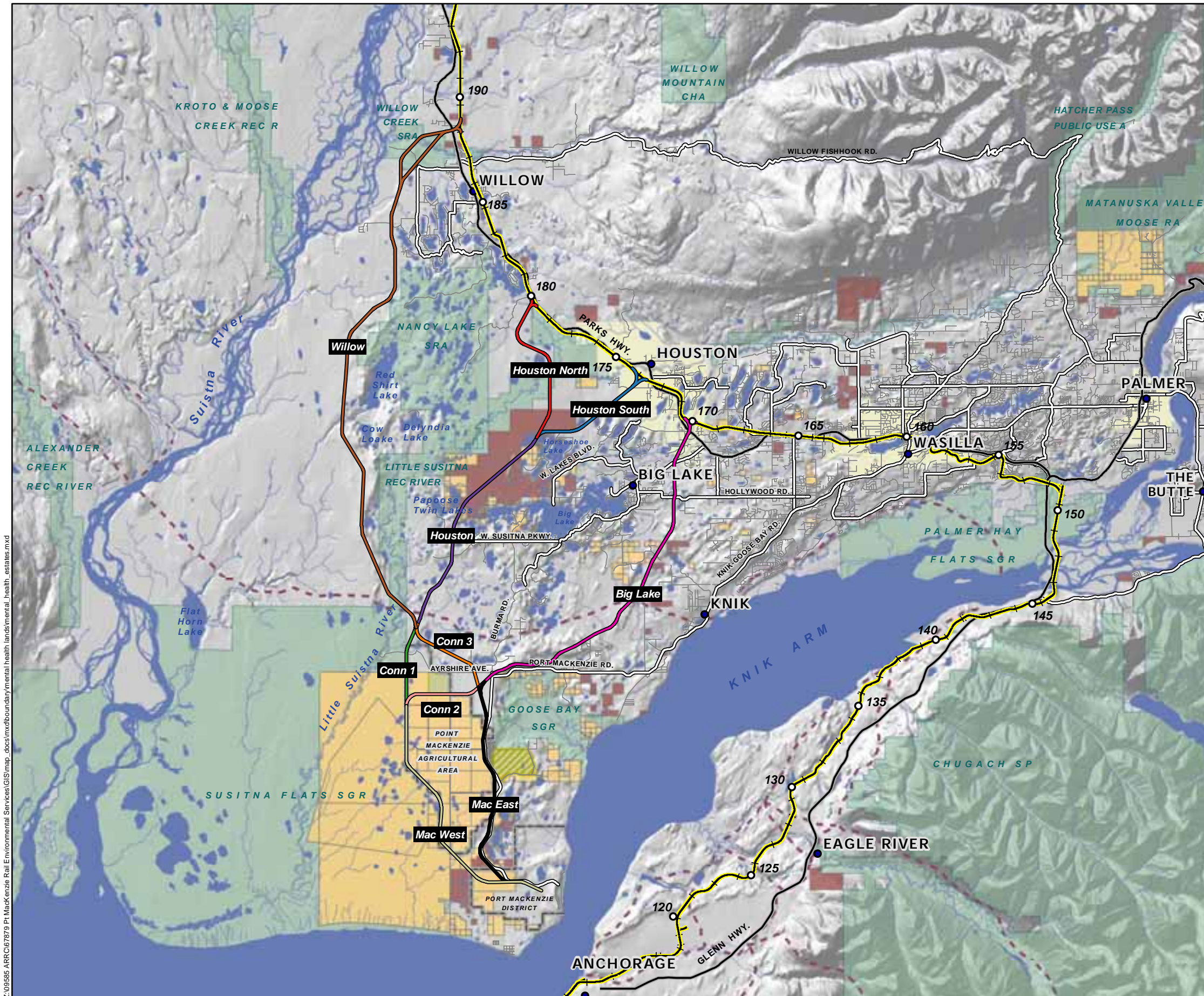
In closing the meeting, Brian indicated that the project corridors are at the conceptual stages. As more information is available the corridors will be refined and project team will likely come back to meet with The Trust Land Office.

Meeting ended at 3:05 pm.

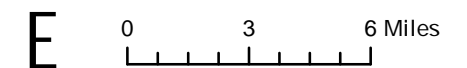


**LEGEND**

- Mental Health Estates**
- Land and Mineral Estate
  - Mineral Estate
  - Land Estate
- 
- ARRC Milepost
  - ARRC Track
  - Highway
  - Medium Rd.
  - Minor Rd.
  - Iditarod Trail
  - City Boundary
  - Proposed Prison
  - Point MacKenzie Correctional Farm
  - Park or Refuge



*This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.*



Date: October 2, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc.,  
 Mental Health Land Trust Office,  
 MSB GIS, TNH- Hanson, USGS.

Z:\05585 ARRC\67679 P1 MacKenzie Rail Environmental Services\GIS\map\_docs\mxd\boundary\mental health\_land\mental\_health\_estates.mxd

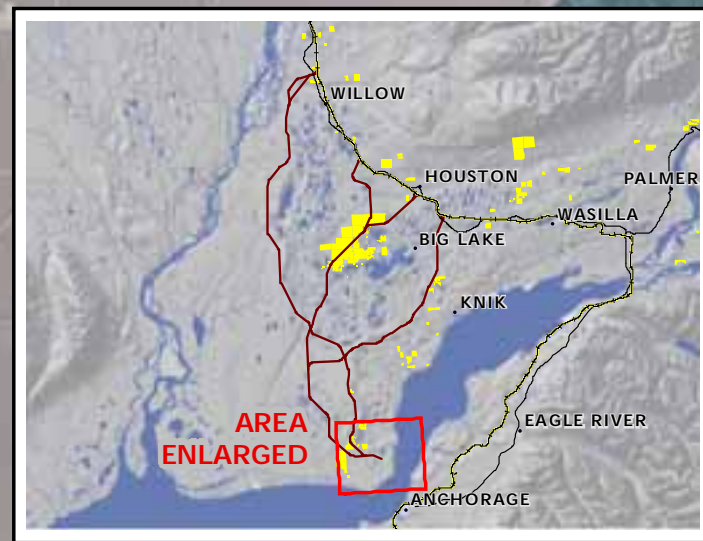
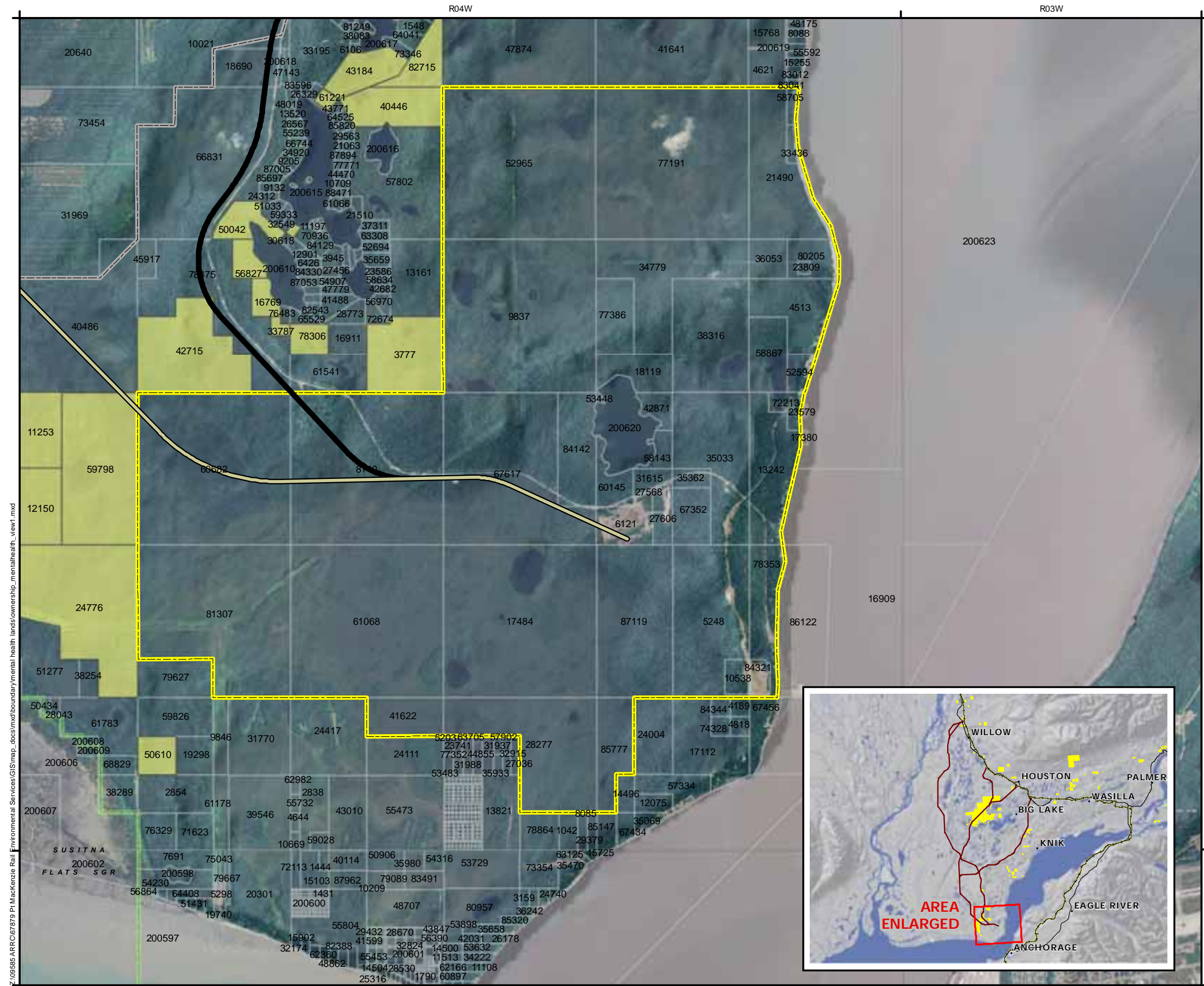


*(This page intentionally left blank.)*

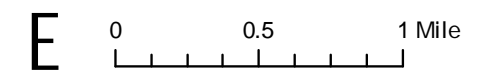


**LEGEND**

- Parcel Boundary
- Mental Health
- Preliminary Alternatives**
- Mac East
- Mac West
- Conn 1
- Conn 2
- Conn 3
- Houston
- Houston North
- Houston South
- Willow
- Big Lake
- ARRC Track
- Point MacKenzie Ag Area
- Port MacKenzie District
- Park or Refuge



*This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.*



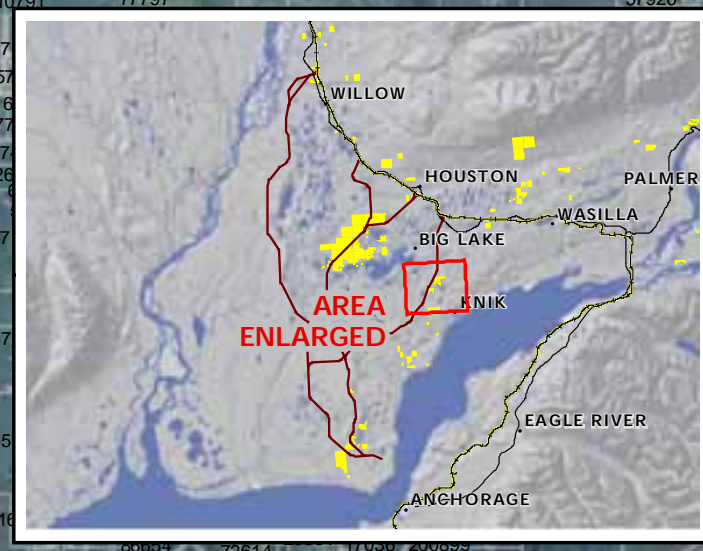
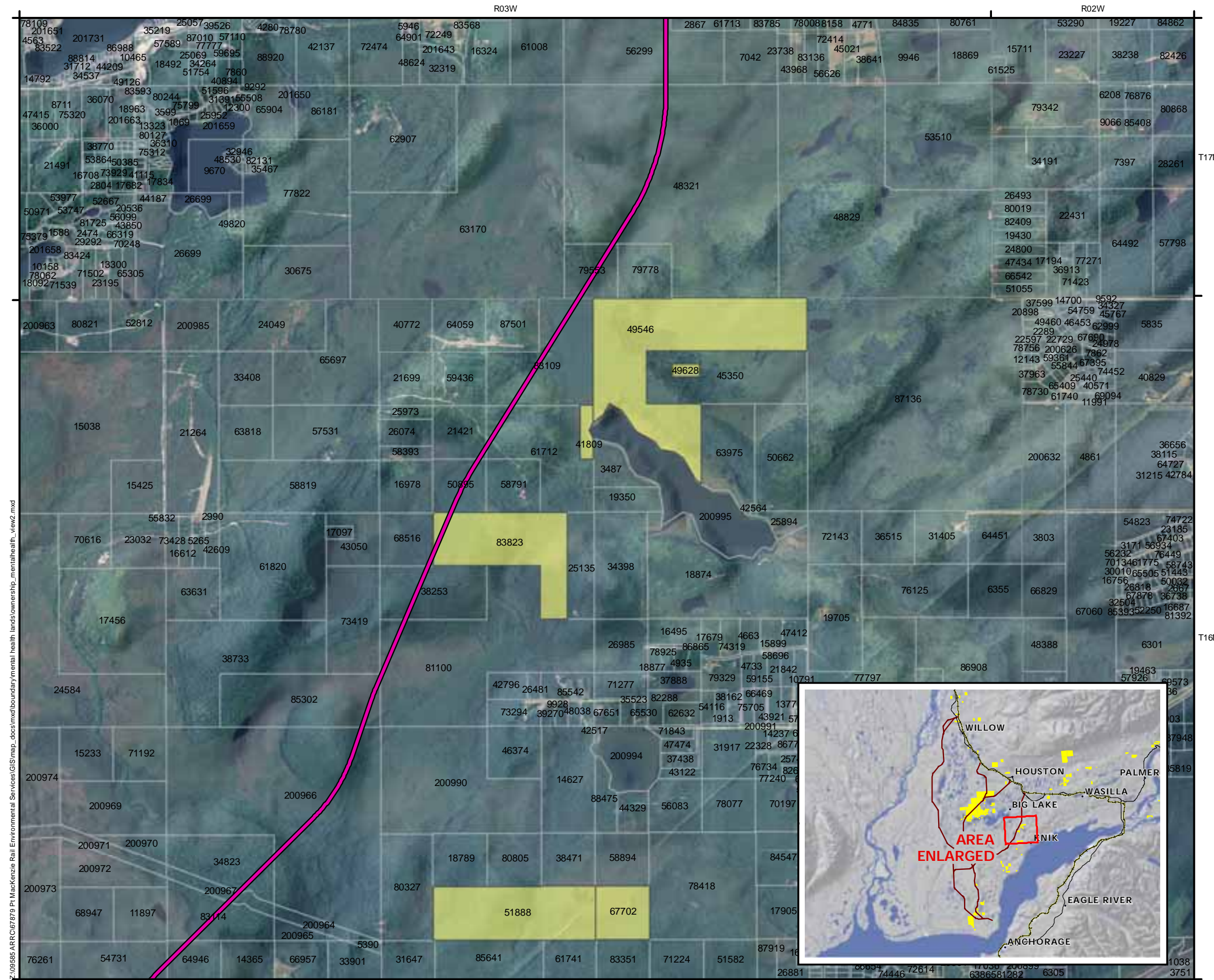
Date: September 24, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

*(This page intentionally left blank.)*

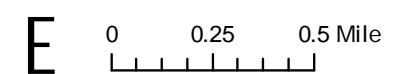


**LEGEND**

- Parcel Boundary
- Mental Health Lands
- Preliminary Alternatives**
- Mac East
- Mac West
- Conn 1
- Conn 2
- Conn 3
- Houston
- Houston North
- Houston South
- Willow
- Big Lake
- ARRC Track
- Point MacKenzie Ag Area
- Port MacKenzie District
- Park or Refuge



This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: September 24, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

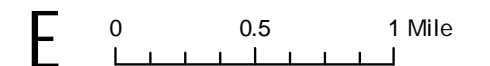
*(This page intentionally left blank.)*



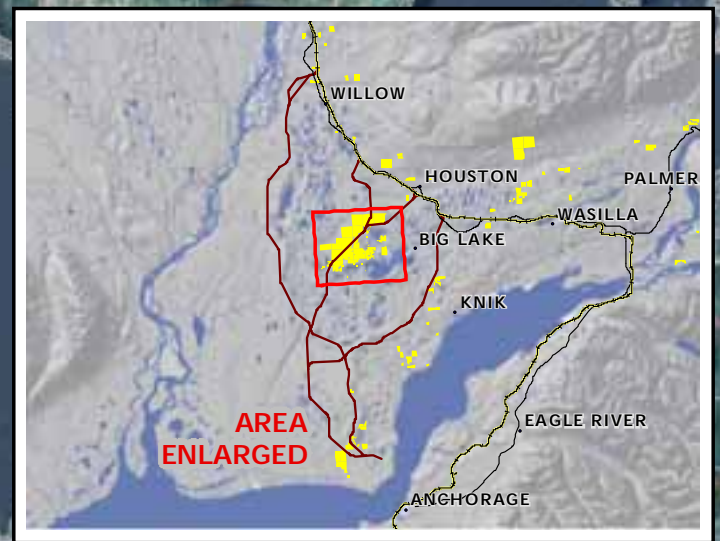
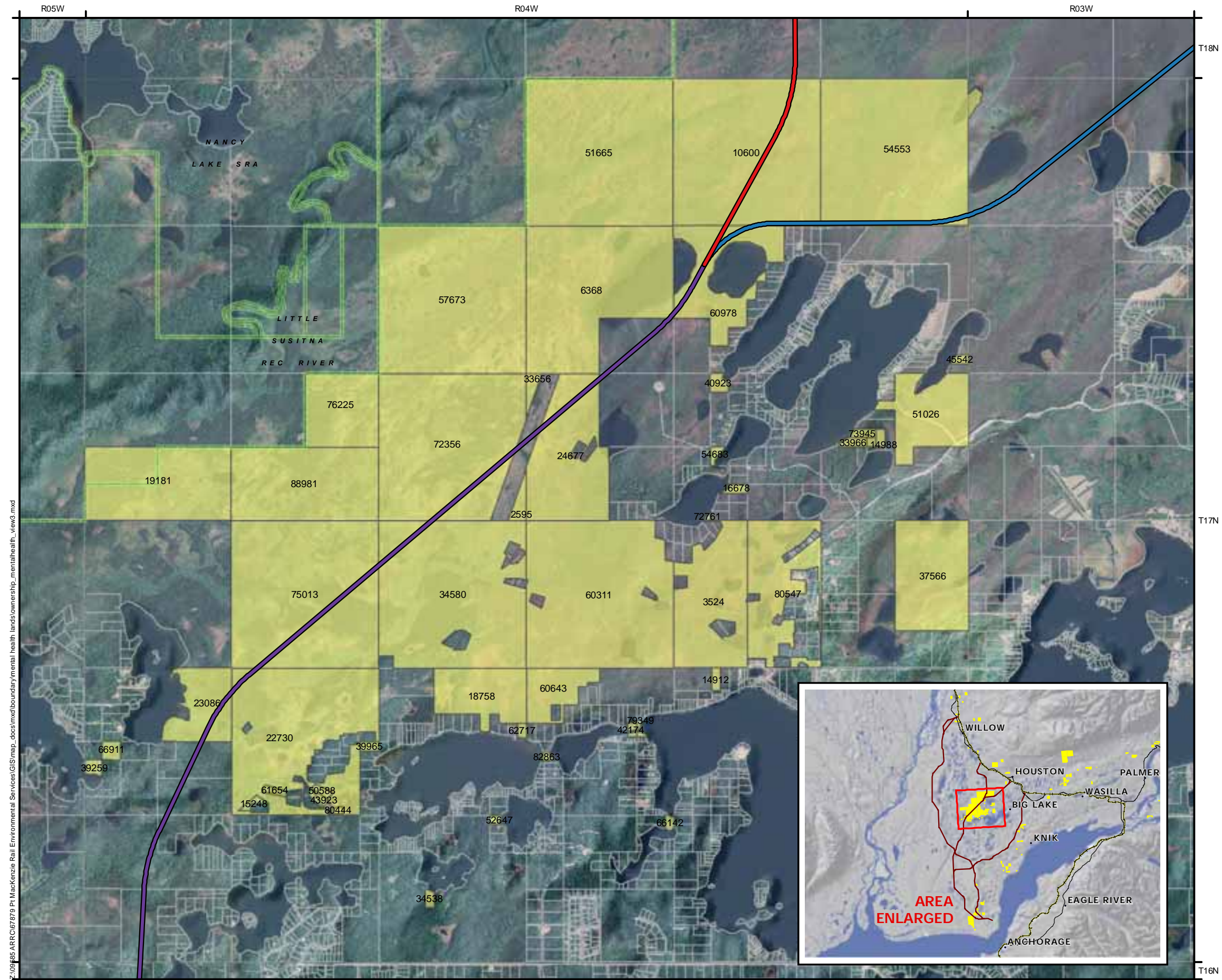
**LEGEND**

- Parcel Boundary
- Mental Health
- Preliminary Alternatives**
- Mac East
- Mac West
- Conn 1
- Conn 2
- Conn 3
- Houston
- Houston North
- Houston South
- Willow
- Big Lake
- ARRC Track
- Point MacKenzie Ag Area
- Port MacKenzie District
- Park or Refuge

*This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.*



Date: September 24, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc.,  
 MSB GIS, TNH- Hanson, USGS.



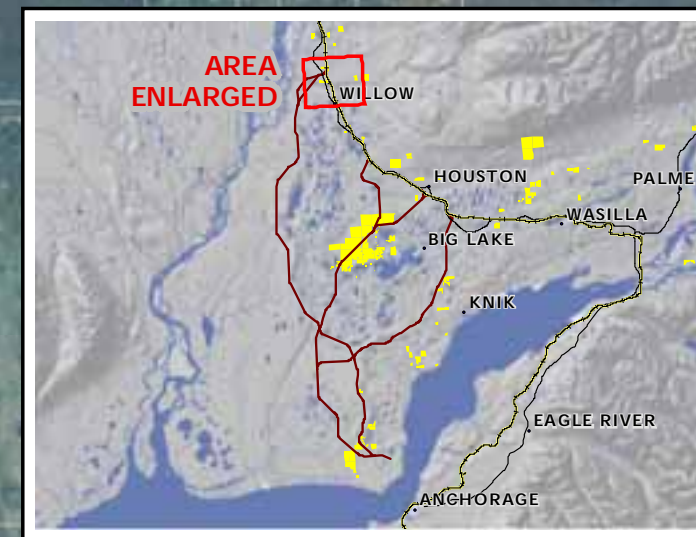
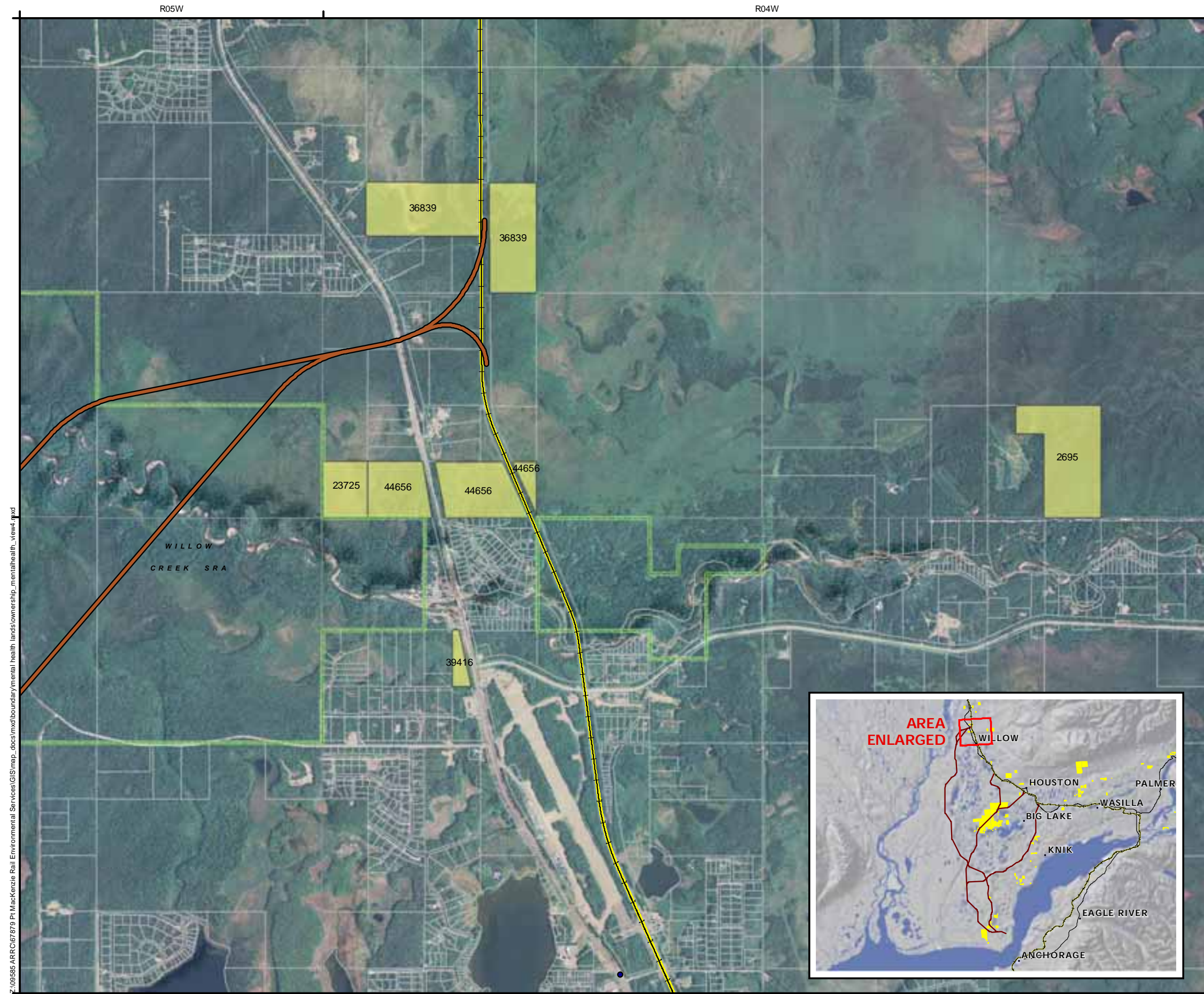
Z:\09\B5-ARRC\67879-PMackenzie Rail Environmental Services\GIS\map\_docs\mxd\boundary\mental health lands\ownership\_mentalhealth\_views.mxd

*(This page intentionally left blank.)*

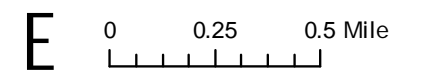


**LEGEND**

- Parcel Boundary
- Mental Health
- Preliminary Alternatives**
- Mac East
- Mac West
- Conn 1
- Conn 2
- Conn 3
- Houston
- Houston North
- Houston South
- Willow
- Big Lake
- ARRC Track
- Point MacKenzie Ag Area
- Port MacKenzie District
- Park or Refuge



This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: September 24, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

Z:\09585 ARRC\67879 P1 MacKenzie Rail Environmental Services\GIS\map\_docs\mxd\boundary\mental health lands\ownership\_mentalhealth\_views.mxd



*(This page intentionally left blank.)*

Port MacKenzie Rail Extension Project

CIRI Meeting

Friday, September 28, 2007  
10:00 am

HDR Alaska, Inc. 2525 C Street, Suite 305, Anchorage, Alaska



First Name	Last Name	Address	City	Zip	Email
Brian	Kiml Anood		AWC	99504	lindemadbb@alaska.com
Kim	Canningham	C Street	AWC	99507	kcanning@alaska.com
Ariel	FELTON	2525 C ST #500	AJC	99501	hfelton@alaska.com
Cindi B.	BETTIN	11	AJC	"	cbettin@alaska.com
Roscoe Ak	ALCANTARA	2525 C ST #305	HDR	99501	rosco_a.alcantara@alaska.com

*(This page intentionally left blank.)*



# Meeting Notes

Subject: Port MacKenzie Rail Extension project overview	
Client: Alaska Railroad Corporation (ARRC)	
Project: Port MacKenzie Rail Extension	Project No: 09585-67879
Meeting Date: September 28, 2007	Meeting Location: CIRI Conference Room, Anchorage
Notes by: HDR Alaska – Rosetta Alcantra	

**Attendees:**

- CIRI:** Kim Cunningham, Director of Land & Resources  
 Hazel Felton, Special Projects Manager (Land and Resources)  
 Cindi Bettin, Land Administrator
- HDR:** Rosetta Alcantra, Public Involvement
- ARRC:** Brian Lindamood, Project Manager

**Topics Discussed:**

The meeting began at 10:00 am. Brian Lindamood (ARRC) opened the meeting with introductions and an overview of the Port MacKenzie Rail Extension project.

Highlights include:

- Review of 2003 Corridor Study
- \$10 million appropriated by the Alaska Legislature to conduct study and prepare a license to operate a new rail extension.
- ARRC would own and operate.
- Review of Surface Transportation Board process.
- Review of schedule
  - Gathering existing studies
  - Soliciting public input
  - Analysis done by end of October, with a matrix scoring system to look at the corridors
  - Meetings with Mat-Su Borough Transportation Advisory Board, Planning Commission and Assembly.
  - Anticipating the STB process to take about 18 months
  - Operational by 2011/2012 to meet the coal gasification project for Agrium
- Joint agreement between the ARRC and the MSB
- Could be looking at Right of Way and engineering as early as next year.
- ARRC is looking for 200 ft. "fee simple"

After the brief overview, Mr. Lindamood explained the purpose of meeting with the various landowners. The project is looking for information regarding the parcels (referred to maps provided). This information could relate to if there are development plans for the parcels and if there are issues the project should be aware of, for example if a parcel was chosen that was owned by CIRI.

Mr. Lindamood indicated at this point it is not known which route will be chosen, but he anticipated having more information as soon as next spring.

As far as development in the area that CIRI is aware of is private home development in the Twin Lakes area. There was also mention of unexploded ordinances in the area (Susitna Gunner project).

Before adjourning, a brief discussion subsurface rights and the coordination with Knikatu, Inc. (Village Corporation) took place. CIRI owns Knikatu Inc. subsurface rights, however if the village corporation is interested in developing the surface rights, the regional will support what they want to do. CIRI referred to this as a non-development covenant (NDC). CIRI indicated that they would have internal discussions about any other potential issues on the parcels and get back to Mr. Lindamood or Ms. Alcantra if they have anything more.

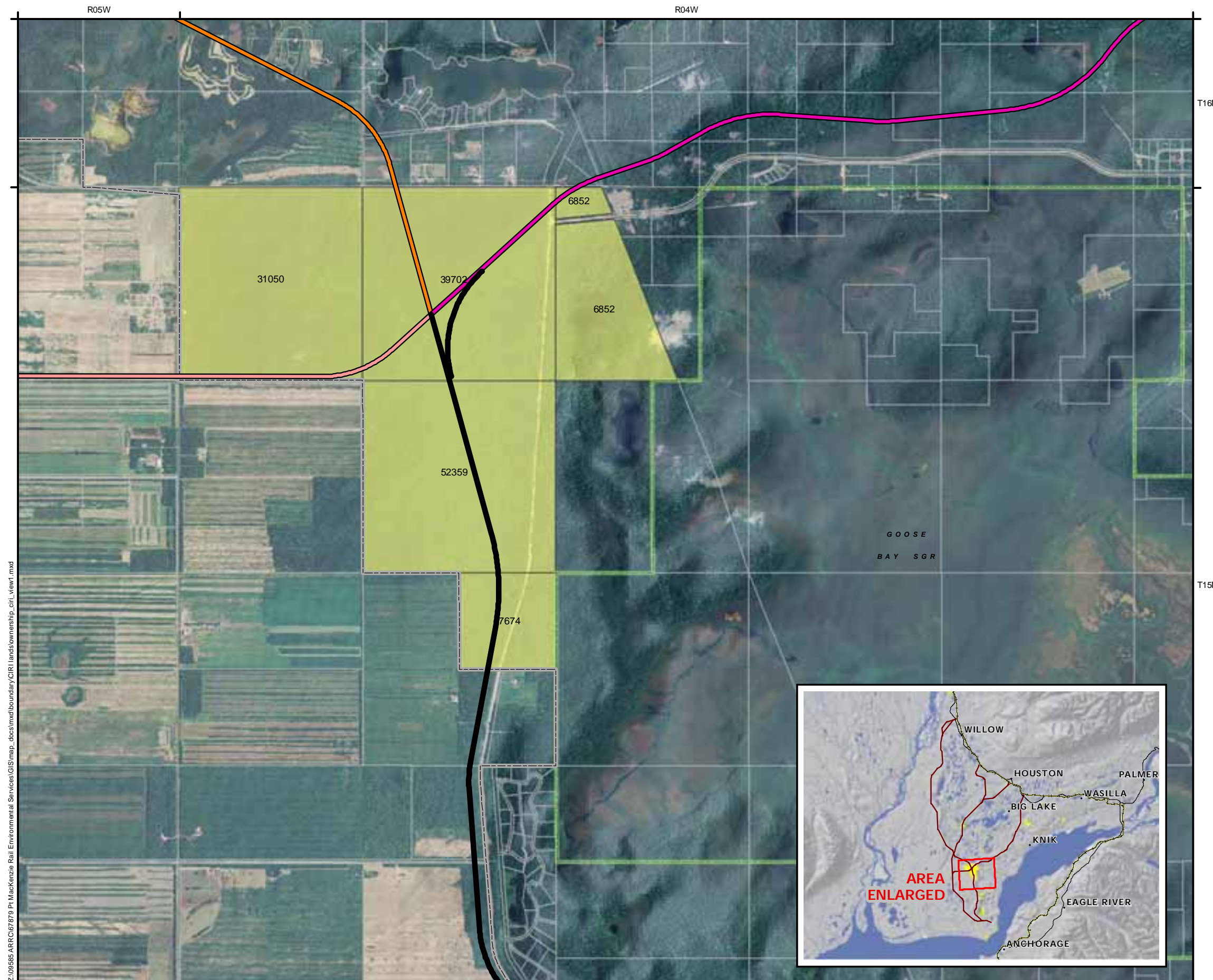
Meeting ended at 10:45 am.

###

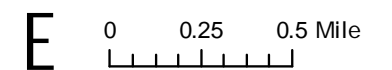


**LEGEND**

- CIRI, Inc.
- Parcel Boundary
- Preliminary Alternatives**
- Mac East
- Mac West
- Conn 1
- Conn 2
- Conn 3
- Houston
- Houston North
- Houston South
- Willow
- Big Lake
- ARRC Track
- Point MacKenzie Ag Area
- Port MacKenzie District
- Park or Refuge



This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: September 25, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

Z:\09585 ARRC\7879 P1 MacKenzie Rail Environmental Services\GIS\map\_docs\mapdoc\boundary\CIRI Lands\ownership\_ciri\_view1.mxd

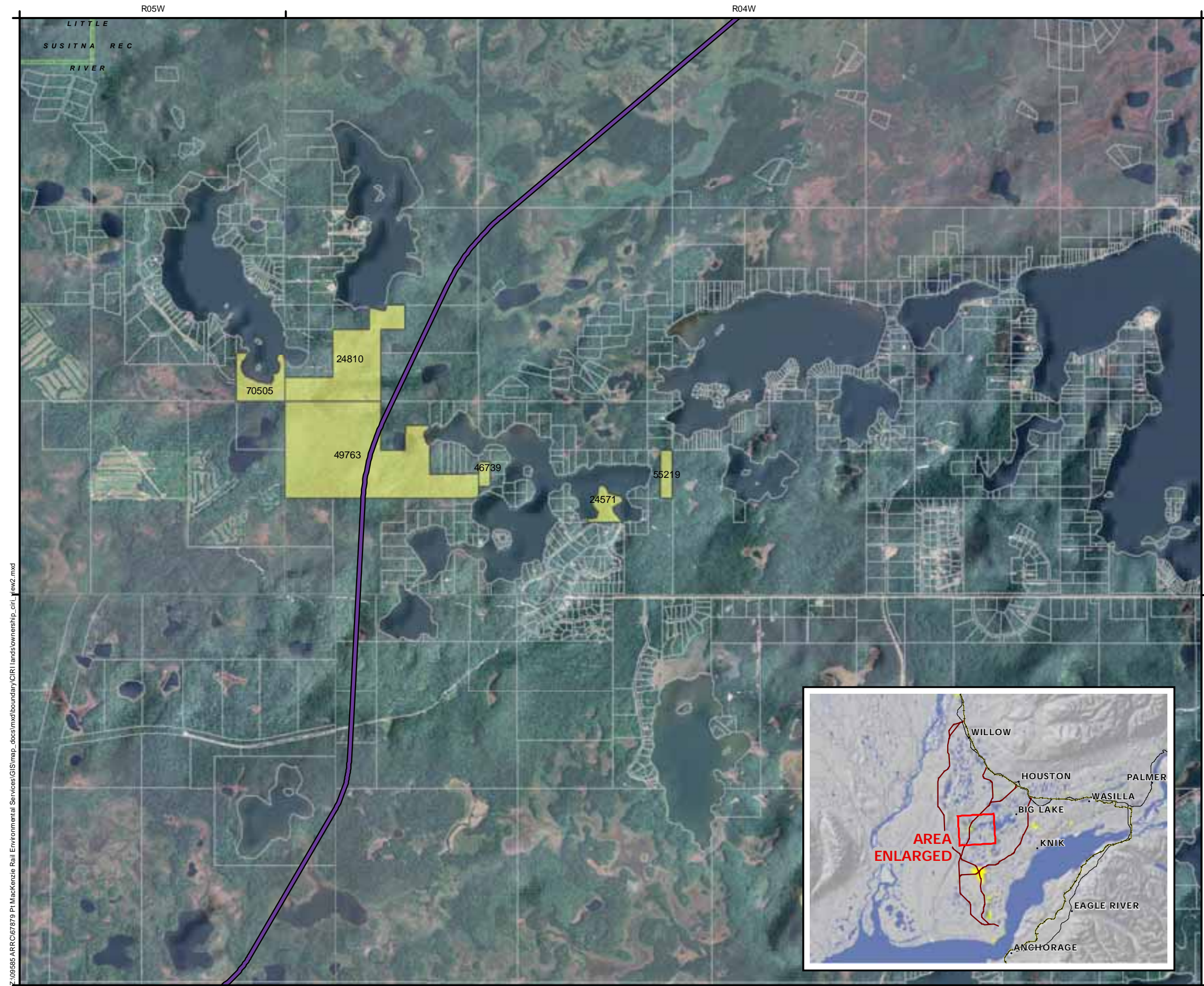
*(This page intentionally left blank.)*



**LEGEND**

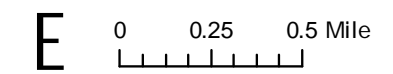
- CIRI, Inc.
- Parcel Boundary
- Preliminary Alternatives**
- Mac East
- Mac West
- Conn 1
- Conn 2
- Conn 3
- Houston
- Houston North
- Houston South
- Willow
- Big Lake
- ARRC Track
- Point MacKenzie Ag Area
- Port MacKenzie District
- Park or Refuge

T17N  
T16N



F:\09585 ARRC\7879 P1 MacKenzie Rail Environmental Services\GIS\map\_docs\map\boundary\CIRI Lands\ownership\_ciri\_view2.mxd

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: September 25, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

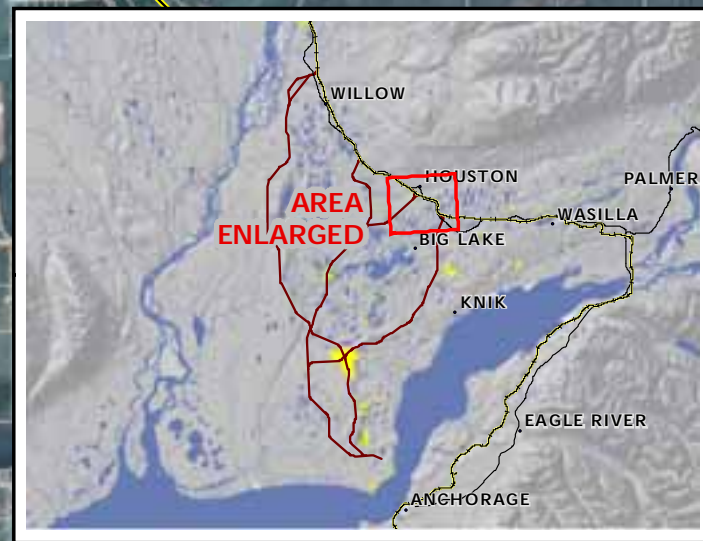
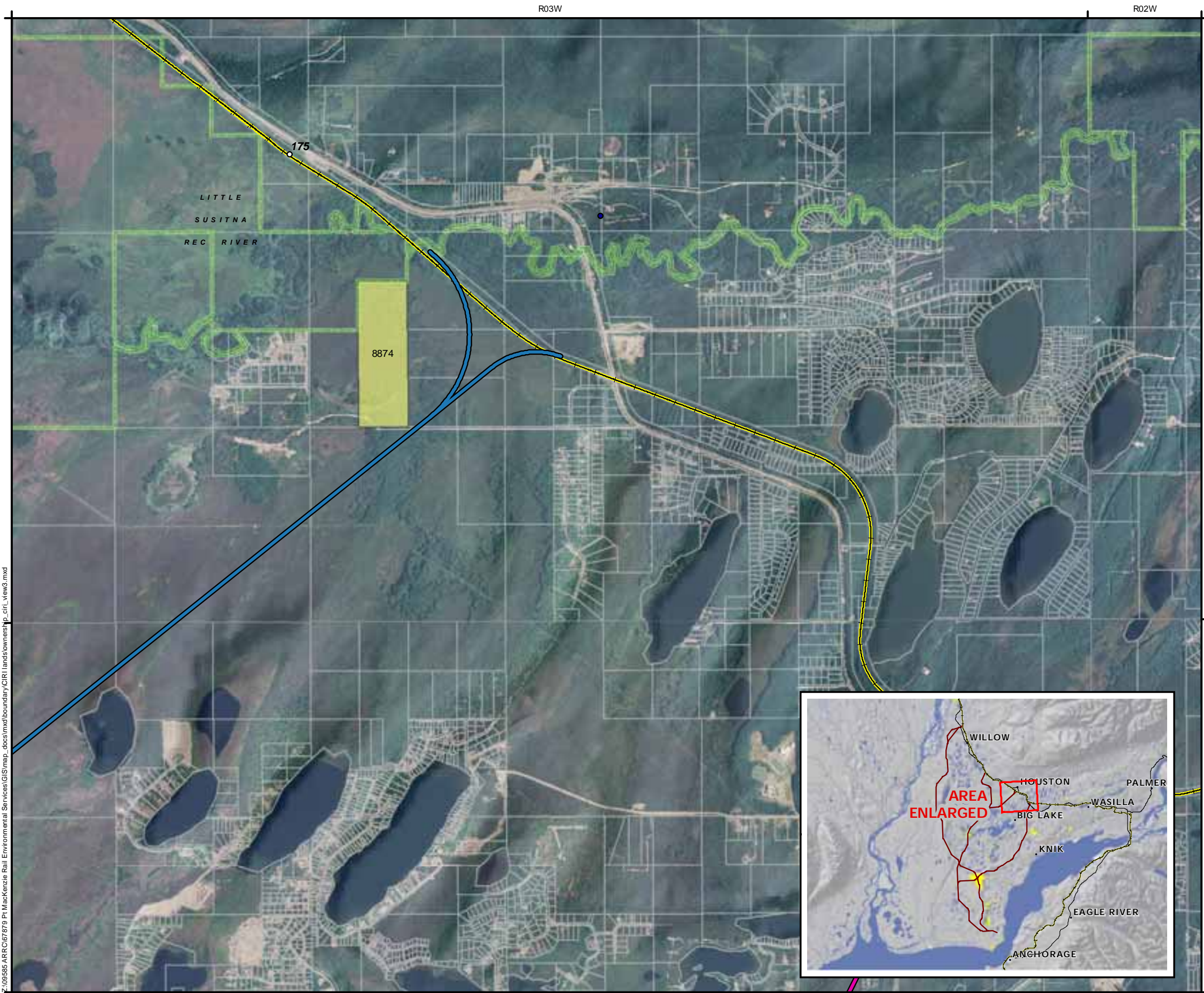


*(This page intentionally left blank.)*

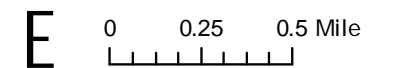


**LEGEND**

- CIRI, Inc.
- Parcel Boundary
- Preliminary Alternatives**
- Mac East
- Mac West
- Conn 1
- Conn 2
- Conn 3
- Houston
- Houston North
- Houston South
- Willow
- Big Lake
- ARRC Track
- Point MacKenzie Ag Area
- Port MacKenzie District
- Park or Refuge



This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: September 25, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

Z:\09585 ARRC\67879 P1 MacKenzie Rail Environmental Services\GIS\map\_docs\mapdoc\boundary\CIRI lands\ownersh\_p\_ciri\_view3.mxd

*(This page intentionally left blank.)*

**Alcantra, Rosetta M.**

---

**From:** Alcantra, Rosetta M.  
**Sent:** Monday, October 08, 2007 1:08 PM  
**To:** Alcantra, Rosetta M.  
**Subject:** Willow Dog Musers Assoc. - Public Forum with Alaska Railroad and Mat-Su Borough Concerning Port Mackenzie Rail Extension  
**Importance:** High

---

**From:** Patty Sullivan [mailto:patty.sullivan@matsugov.us]  
**Sent:** Friday, October 05, 2007 11:05 AM  
**To:** Brian Lindamood; Brad Sworts; John Duffy; Alcantra, Rosetta M.; Stephenie Wheeler  
**Subject:** Fwd: Public Forum with Alaska Railroad and Mat-Su Borough Concerning Port Mackenzie Rail Extension  
**Importance:** High

Patty Sullivan  
Public Affairs Director  
Mat-Su Borough  
psullivan@matsugov.us  
(907)745-9577

Begin forwarded message:

**From:** "Erin McLarnon" <erin-pcorp@alaska.com>  
**Date:** October 5, 2007 9:40:00 AM AKDT  
**To:** <erin-pcorp@alaska.com>  
**Subject:** Public Forum with Alaska Railroad and Mat-Su Borough Concerning Port Mackenzie Rail Extension

Good day! The Willow Dog Musers Association is sponsoring a public forum with the Alaska Railroad and the Mat-Su Borough concerning the Port Mackenzie rail extension.

The forum is going to follow the same format as the Candidates Forum we sponsored last year. WDMA will submit 5-6 questions next week, in which the railroad and borough will respond to in writing before the meeting. Those answers will be circulated via email before the forum. At the forum the pre-submitted questions will be asked and answered, then we will give individuals a maximum of 2 minutes to do follow-ups with these questions as well as ask/comment on other items regarding the railroad extension.

The forum will last a maximum of one hour.

At this time WDMA is putting out a call for questions. Five to six questions will be chosen to be submitted to both the borough and railroad. I need your proposed questions by Monday, Oct. 8th.

I am also looking for volunteers to be the MC (John Wood did a great job for the Candidates Forum) and a time keeper (Joan Bryner was a winner at that one last time too). If you are interested, please let me know.

Once the questions are chosen and we know exactly who is going to attend the forum I will make flyers for folks to post around Willow. Please forward this email to others who may be interested.

After the forum the Willow Dog Mushers will proceed with their regularly scheduled monthly meeting.

Thanks and have a great weekend,  
Erin



## Willow Dog Musher's Association

P.O. Box 858  
Willow, Alaska 99688

October 12, 2007

Ms. Patty Sullivan  
Public Affairs Director  
Mat-Su Borough  
350 E. Dahilla Avenue  
Palmer, Alaska 99645

Re: Invitation to Willow Dog Musher's Association 2007 Mat-Su Borough/Alaska Railroad Public Forum Concerning the Port Mackenzie Railroad Extension

Dear Ms. Sullivan:

Good day! The Willow Dog Musher's Association (WDMA) would like to cordially invite you and anyone from the Mat-Su Borough and/or Alaska Railroad Corporation to attend our next scheduled general membership meeting on October 24<sup>th</sup> at 7:00 PM in the Willow Community Center, Parks Highway Mile 70. The *Public Forum* will be the first and most important item on our agenda.

The Willow Dog Musher's Association was created in order for mushers to have a larger voice on issues concerning dog mushing in our area. WDMA's mission is to protect and preserve the dog mushers' historical way of life today, tomorrow and for future generations. One approach in accomplishing this mission is being pro-active on matters that affect dog mushers and their unique lifestyle; and this includes hearing from local government and others on matters.

Therefore, we would like to invite anyone involved in the Port Mackenzie Rail Extension Project to address a few of the issues that are most important to us.

These issues/questions are:

- 1) When the Mat-Su Borough noticed that the 2003 T-N-H report chose Willow as the preferred alternative and they didn't conduct a public meeting in the community of Willow, why didn't the Borough insist T-N-H go back and conduct a public meeting?
- 2) What other alternatives were developed and why were they eliminated?
- 3) There are obvious, unavoidable human and environmental impacts with the Willow Corridor, therefore how is the railroad planning to offset and/or mitigate those impacts?
- 4) If the Willow Comprehensive Plan meets all the Mat-Su Borough's planning requirements, but doesn't support the Willow Railroad Corridor will the Borough accept Willow's plan?
- 5) What exact economic benefits will the railroad bring to Willow?

Page 2

6) Will the railroad honor our 10+ trail crossings?

Those attending from the Mat-Su Borough and Alaska Railroad Corporation will be afforded two (2) minutes per question to address the members. Following these presentations, the floor will be opened up for one hour or less, so that our members can give you up to two (2) minutes of feedback on what is important to them individually or ask follow-up questions.

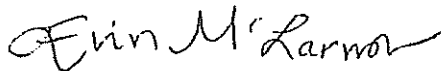
We fully understand that you have a very busy schedule and are attempting to make this commitment time constrained so that you can plan other events.

If you submit your answers to the above questions in writing by October 22<sup>nd</sup> we will make copies available to all attending the meeting as well as distribute through our email list which numbers over seventy five addresses.

If you could RSVP or have any questions, comments and/or concerns, please don't hesitate to contact me directly at home - 495-0671, cell - 907-230-4032 or via email - [erin-pcorp@alaska.com](mailto:erin-pcorp@alaska.com).

Thank you for your time and consideration.

Sincerely,



Erin McLarnon  
President



Oct. 18, 2007

(The following are questions submitted by the Willow Dog Musers Association.)

1) When the Mat-Su Borough noticed that the 2003 T-N-H report chose Willow as the preferred alternative and they didn't conduct a public meeting in the community of Willow, why didn't the Borough insist T-N-H go back and conduct a public meeting?

A: The failure to have a meeting in Willow for the 2003 TNH study was an oversight. The Borough has apologized for it. Please consider that the meetings four years ago did not travel. The budget was smaller. All three were held at the same location: Houston. Additionally, Willow is no longer the preferred route. Three routes are under consideration. During this fact-finding process we have met with Willow residents five times in less than a year, ensuring that the oversight does not happen again.

2) What other alternatives were developed and why were they eliminated?

A: Many alternatives from previous studies were reviewed, including several new ones. Multiple layers of physical and environmental constraints were then overlaid upon the proposed alignments. Some examples of these constraints include wetlands, soil conditions, and land use. The remaining alignment corridors represent those areas that show the least constraints.

3) There are obvious, unavoidable human and environmental impacts with the Willow Corridor, therefore how is the railroad planning to offset and/or mitigate those impacts?

A: There are unavoidable human and environmental impacts on any route we choose. The Railroad will continue to work with the Surface Transportation Board and the regulatory agencies to identify potential impacts and develop mitigation measures where appropriate. The mitigation requirements will be laid out as the project seeks permits from the federal STB. The project team has held five open houses and smaller meetings to help lessen the impacts as much as possible. Much of the public comment is helping refine the proposed corridors.

4) If the Willow Comprehensive Plan meets all the Mat-Su Borough's planning requirements, but doesn't support the Willow Railroad Corridor will the Borough accept Willow's plan?

A: The Willow Comprehensive Plan may make community-based recommendations. However, the plan should also be consistent with Borough-wide planning documents, goals, objectives and policies. If the Willow Comp. Plan does not support a particular corridor, the Borough may accept the plan, but not all of its recommendations, particularly those that conflict with Borough-wide planning documents, goals, objectives and policies.

-more-



5) What exact economic benefits will the railroad bring to Willow?

A: Willow has not been selected at this stage of the process. The Willow route is one of three proposed corridors. At this time we do not know the “exact” benefits the Railroad would bring to Willow. Some of the benefits may include commercial opportunities due to the multimodal characteristics of the area, for example, airport, rail, highway. A rail station may be built providing for passenger and tourism opportunities. Moreover, we do not know what the future might bring in terms of other opportunities, such as jobs that could come with a possible rail maintenance facility.

6) Will the railroad honor our 10+ trail crossings?

A: The Railroad is working with the administrative agencies (the Mat-Su Borough and the State Dept. of Natural Resources) to ensure that the connectivity of existing legally dedicated trails remains intact. The Borough is also tracking trails where easements are being worked on. Tonight we bring a map of trails. We want you to review them and give us your comments. Please send in your comments by Nov. 2.

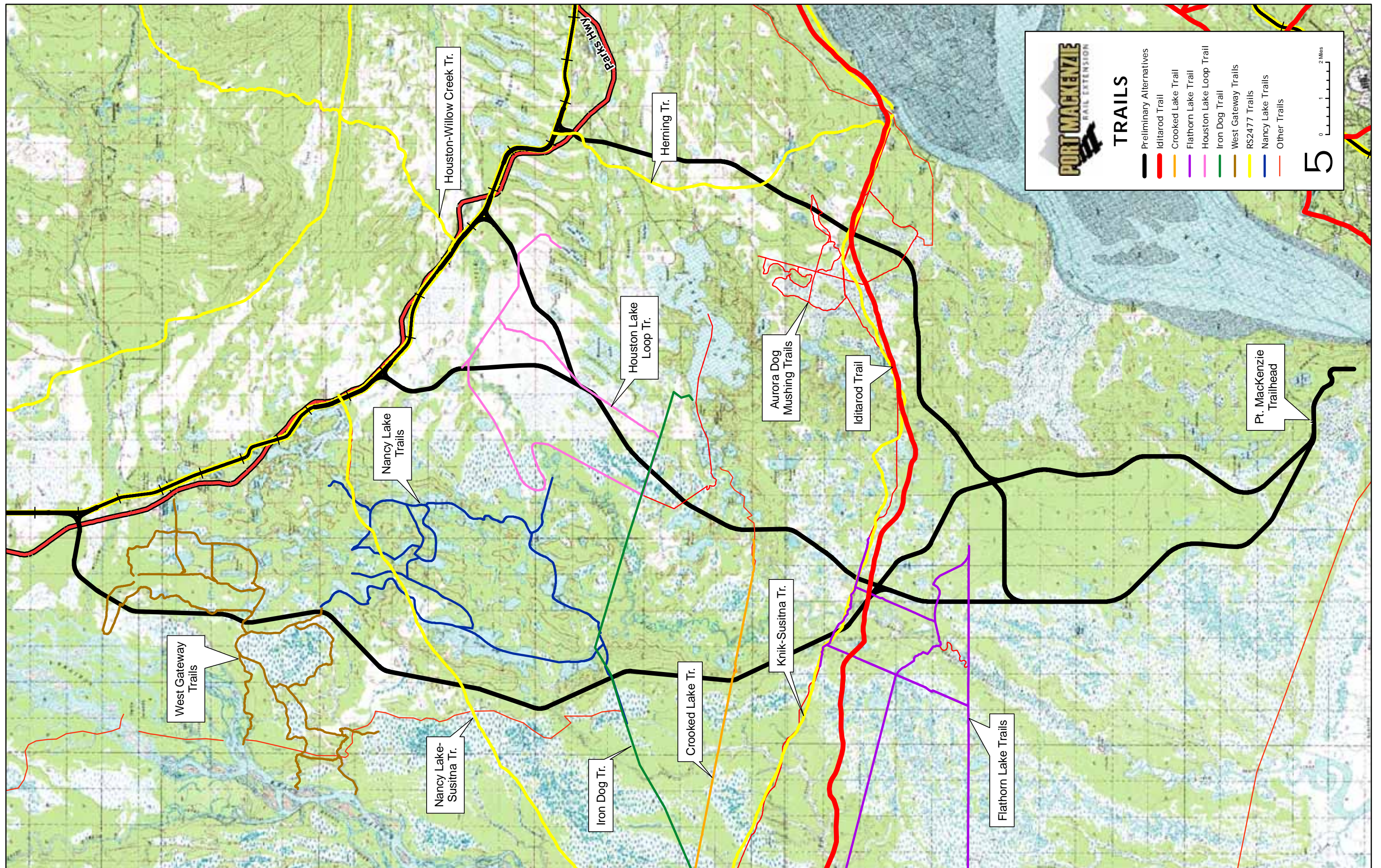
7) It has been argued that there are no direct benefits to Willow in this rail extension project. Would both the Borough and the Railroad be agreeable to dedicating 10' cleared leveled "trail" on each side of the corridor along with access underpasses spaced at reasonable intervals? One of those trails would be dedicated to motorized recreational use while the other side would be dedicated to non-motorized recreational use and each should be buffered from the rails by growth of natural vegetation or other barrier. It would open up large new areas for recreational use and, most likely, result in new cottage businesses starting up to service those folks.

A: In the process of identifying impacts there may be a wide range of mitigation, including trails.

Thank you for your questions.

For more information visit the project site at [www.portmacrail.com](http://www.portmacrail.com)

-end-



*(This page intentionally left blank.)*



**Comments for this phase must be received by FRIDAY, NOV. 2, 2007**

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we jointly pursue the Port MacKenzie Rail Extension project. Please write your comments legibly (printing is appreciated).

Name	<input type="text"/>	Phone	<input type="text"/>
Address	<input type="text"/>	Email	<input type="text"/>

Your Comments:

**COMMENT FORM**

HDR Alaska, Inc.  
 Attn: Rosetta Alcantra  
 2525 C Street, Suite 305  
 Anchorage, AK 99503-2632

FIRST-CLASS  
 POSTAGE  
 REQUIRED

---



---



---

..... 1. FOLD HERE .....

----- 2. FOLD HERE, TAPE OPEN EDGES CLOSED (DO NOT STAPLE), AND AFFIX POSTAGE BEFORE MAILING -----

**DURING THIS RESEARCH PHASE** of the Port MacKenzie Rail Extension project, we are asking for your help to identify concerns related to trails. The map shows trails with dedicated easements and also trails that the State or Borough may plan to acquire easements for in the project area. Please use the map to identify your concerns and write comments (there is also space for your comments on the Comment Form). When you're done, follow the folding and sealing instructions, affix a stamp, and mail your comments to us so they are **received by Friday, November 2, 2007.**

*Thank you for participating.*

[www.portmacrail.com](http://www.portmacrail.com)

*(This page intentionally left blank.)*

Frontiersman

Page A12

CITY OF HOUSTON  
CITY COUNCIL CHAMBERS  
13965 ARMSTRONG ROAD  
HOUSTON, TX  
WWW.HOUSTON.TX.GOV

MAYOR STEVE FROST  
DEPUTY MAYOR SASHY McDONALD  
COUNCIL MEMBER ROSEMARY BURKETT  
COUNCIL MEMBER ROGER PURCELL  
COUNCIL MEMBER CARLA HENRIKX  
COUNCIL MEMBER LEE HINES  
COUNCIL MEMBER GLEN TELGEMAN

**PUBLIC MEETING NOTICE**

The Houston City Council will host a Public Meeting on the Port Mackenzie Rail Extension, Thursday October 25th at 7 p.m. at Houston City Hall.

Representatives from the Railroad and the Mat-Su Borough will be giving a presentation on the Rail Extension. There will be a Question and Answer period for the public following the presentation.

The public are encouraged to attend.

For more information call the City Clerk's office (907)892-6869  
Office hours are 8a.m. - 3p.m. Monday thru Friday.

*(This page intentionally left blank.)*



*Welcome!*

Port MacKenzie Rail Extension Project

City of Houston

Thursday, October 25, 2007

[www.portmacrail.com](http://www.portmacrail.com)



## Project Participants

Alaska Railroad Corporation \_\_\_\_\_ Applicant

Matanuska-Susitna Borough \_\_\_\_\_ Project sponsor

Surface Transportation Board \_\_\_\_\_ Lead federal agency


HDR Alaska, Inc. \_\_\_\_\_ Consultant

TNH-Hanson, LLC \_\_\_\_\_ Consultant


STB Section of Environmental Analysis \_\_\_\_\_ NEPA Compliance

To be determined \_\_\_\_\_ 3<sup>rd</sup> Party NEPA Contractor

[www.portmacrail.com](http://www.portmacrail.com)







## STB Process

**Railroad**

- Files notice with STB
- Files petition for the construction and operation of railroad

**STB**



- Considers application based upon Railroad's ability to operate/be stable and market needs
- Incorporates conditions from the SEA upon favorable finding

**SEA**

- Conducts NEPA process independently of railroad\*
- Most likely utilizes 3rd party contractor

\*Railroad has very limited role/input in process beyond providing information


[www.portmacrail.com](http://www.portmacrail.com)




## What's Happening Now and Why

- 2003 Study was a reconnaissance study
- Re-evaluating and refining most promising corridors
- Incorporating latest environmental and planning information

[www.portmacrail.com](http://www.portmacrail.com)







# Constraints Mapping

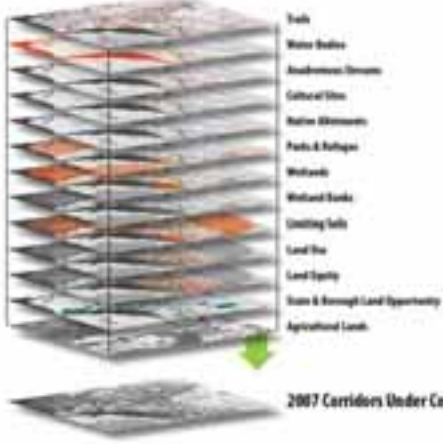
**Topics considered:**

- Water bodies
- Anadromous streams
- Habitat
- Cultural sites
- Native allotments
- Parks and refuges
- Wetlands and wetland banks
- Limiting soils
- Land use
- Land value
- Land availability
- Agricultural lands
- Trails

[www.portmacrail.com](http://www.portmacrail.com)




# Corridor development process



- Trails
- Water Bodies
- Anadromous Streams
- Cultural Sites
- Native Allotments
- Parks & Refuges
- Wetlands
- Wetland Banks
- Limiting Soils
- Land Use
- Land Value
- Land Availability
- State & Borough Land Ownership
- Agricultural Lands

2007 Corridors Under Consideration

[www.portmacrail.com](http://www.portmacrail.com)




**PORT MACKENZIE**  
RAIL EXTENSION

## Working with the Public

- Hosting series of public open house meetings
- Web site – informative and interactive
- Newsletter (forthcoming)
- Requesting public comment and using the comments to help determine corridors
- Meeting with small stakeholder groups
- Identification of mitigation ideas early in the process – Do you have solutions –We want to hear them
- Developing a comprehensive list of criteria to be considered and evaluated in the corridor development
- Conducting a more detailed study of the proposed corridors

**Your comments will help us find the best corridor.**

[www.portmacrail.com](http://www.portmacrail.com)



**PORT MACKENZIE**  
RAIL EXTENSION

## Trails


*Trails are an important consideration, especially as they relate to the quality of life concerns and recreational opportunities in the area.*



[www.portmacrail.com](http://www.portmacrail.com)




### Corridors Under Consideration



*Note: These lines represent possible corridors and are subject to change.*

[www.portmacrail.com](http://www.portmacrail.com)




### Anticipated Project Schedule

- October 2007**
  - Solicit input from public and regulatory agencies
  - Consider possible impacts and mitigation associated with route alternatives
  - File notice with STB end of October
- November 2007**
  - Finalize route considerations
  - Refine possible mitigation based upon potential impacts
  - November 2 – Comment deadline
- December 2007**
  - Complete Route Alternatives document and submit to STB
  - Develop possible MOA's/MOU's as appropriate
  - Anticipate the STB to select 3<sup>rd</sup> Party NEPA contractor by end of 2007
- 2008 - 2009**
  - File petition in early spring 2008
  - Anticipate the STB 3<sup>rd</sup> Party Contractor to complete environmental document by mid-2009

[www.portmacrail.com](http://www.portmacrail.com)






*Thank You for coming!*

Please ask questions!

Comment deadline is November 2nd

[www.portmacrail.com](http://www.portmacrail.com)





# Opportunity to Comment & INFORMATIONAL MEETING

To learn more about a joint effort to plan, engineer, design, and construct a rail connection between Port MacKenzie and the existing railroad system, the Matanuska-Susitna Borough (MSB) and the Alaska Railroad (ARRC) will be available to answer questions and provide information at the Houston City Council meeting on October 25, 2007 at 7:00 p.m. For additional project information, visit [www.portmacrail.com](http://www.portmacrail.com)

Contacts: MSB Public Affairs Director Patty Sullivan, 745-9577  
ARRC Public Involvement Officer Stephenie Wheeler, 265-2671



*(This page intentionally left blank.)*

Port MacKenzie Rail Extension Project  
 Knikatu, Inc.  
 Saturday, November 3, 2007  
 10:30 am  
 Wasilla, Alaska



First Name	Last Name	Address	City	Zip	Email
Alfred	Tellman	190 Aspen Ave	Wasilla	99654	aptel@mtaonline.net
Dale	Theodore				ASPENAVE@mtsonline.net
THEO	GARCIA	11506 Kenby Ct Apt 6	Engle Cluse	99577	tgarcia@gei.net
Raymond	Theodore		WASILLA	99654	r-theodore@CCI.net
Pitney	Garcia	P.O. Box 42794	Chinchaga	99574	gardener@mtaonline.net



*(This page intentionally left blank.)*

Subject: The Port MacKenzie Rail Extension Overview	
Client: Alaska Railroad Corporation (ARRC)	
Project: Port MacKenzie Rail Extension	Project No: 09585-67879
Meeting Date: November 3, 2007	Meeting Location: Knikatu, Inc.
Notes by: Rosetta Alcantra	

### Attendees:

Brian Lindamood, Project Manager, ARRC  
 Rosetta Alcantra, Public Involvement, HDR Alaska, Inc.  
 Tom Brooks, ARRC  
 Raymond Theodore, President, Knikatu, Inc.  
 Al Tellman, Board of Directors, Knikatu, Inc.  
 Patsy Garcia, Board of Directors, Knikatu, Inc.  
 Theo Garcia, Board of Directors, Knikatu, Inc.  
 Dale Theodore, Board of Directors, Knikatu, Inc.

### Topics Discussed:

The meeting began at 10:30 AM at the Knikatu, Inc. office in Wasilla. Raymond Theodore opened the meeting by providing introductions of the members; he also introduced the project team members, Brian Lindamood, Project Manager for the Alaska Railroad Corporation, Tom Brooks, ARRC and Rosetta Alcantra, Public Involvement Specialist for HDR, Alaska. Following introductions, Mr. Lindamood thanked the Knikatu, Inc. Board of Directors for inviting the project team to provide an overview of the Port MacKenzie Rail Extension project. In the project overview, Mr. Lindamood, explained the current use of Port MacKenzie and the future intensions of the Matanuska Susitna Borough (MSB). In 2003 the MSB and the Alaska Department of Transportation & Public Facilities commissioned a study of a rail extension between the port district and the rail road. The 2003 study recommended Willow. Last year the Alaska State Legislature appropriated \$10 Million to conduct further analysis of the area. This was motivated by the desire to meet the goals of the Agrium plant in Nikiski and their interest in coal gasification. The ARRC and MSB are now in the process of conducting the environmental analysis and engineering to prepare an application to the Surface Transportation Board (STB) for the license to construct and operate a rail extension.

Mr. Lindamood then explained the role of the Surface Transportation Board and their process for developing an independent study from the railroad.

- They will conduct an independent study from the railroad.
- There will be an additional opportunity to provide input
- Discussed the corridors and how they were developed
  - Trails
  - Wetlands
  - Constructability
  - Took this information and layered it. Now in the process of developing a matrix, which would score each of the corridors. There will be no final corridors recommended.
  - The STB will have the final say on the corridor selected.
- Houston route
  - Interested in talking about plans and development, what have you tried before? What would you like to do? Would like to work with you to come to a right of way agreement or develop mitigation for impacts.

Questions:

- Is the railroad looking at 200' ROW? And is there opportunity for wiggle room?
- What would the ROW accommodate?

Answer:

- Railroad is looking at 200' ROW and that would provide for the utilities that the railroad is obligated to provide for development.
- The utilities include gas, sewer and electric.

Meeting with agencies, they are weighing in.

STB will be meeting with the Knik Tribal Council to fulfill their government-to-government responsibility. Tom Brooks; said the ROW was for gas and sewer. The comment regarding public roads was the ARRC will try and keep public access away from the rail line. There is opportunity to discuss this further when the corridor is selected, and it is possible to negotiate.

Al Tellman asked about other rail sightings on other Knikatu, Inc. properties?

Discussion about CIRI owning the subsurface rights and how that would work. The project team had met with CIRI and the indication from them was they would be following the village corporations lead. Theo indicated that was inline with previous development.

Question about how long it would take for the STB process. Brian indicated the approximate timeline is 1 and a half years.

Al Tellman asked how Knikatu, Inc. could support the project. Brian said the corporation could give us input now or later.

There was a question about the feasibility of the project happening. Brian indicated there were some questions regarding the funding for the project. The ARRC has been given the authority to sell bonds, but this has not been fully discussed between the project partners.

Question regarding trails. Brian indicated trails have become an issue. The bottom line is if the trail is legally recognized with an easement the project will maintain the connectivity.

There was general discussion on development opportunity and how much it would cost to get rail service to provide shipping for those opportunities.

To conclude the meeting the group talked about eminent domain. To the recollection of Brian and Tom, the railroad has not exercised that right in Alaska. Tom explained the process of a ROW acquisition, if a property was to be acquired there would be an independent appraiser that both parties would agree to, this would establish the fair market value and a place to begin negotiations. The goal of the railroad is to work with the party(s) to minimize the impact, and provide the fair market value.

The meeting adjourned at 11:45 am.



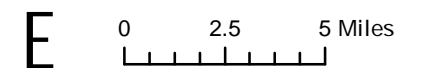
**LEGEND**

- General Land Ownership
- Knikatnu, Inc.
  - Native Corp. (other)
  - Mental Health
  - Public University
  - Borough
  - Private
  - State

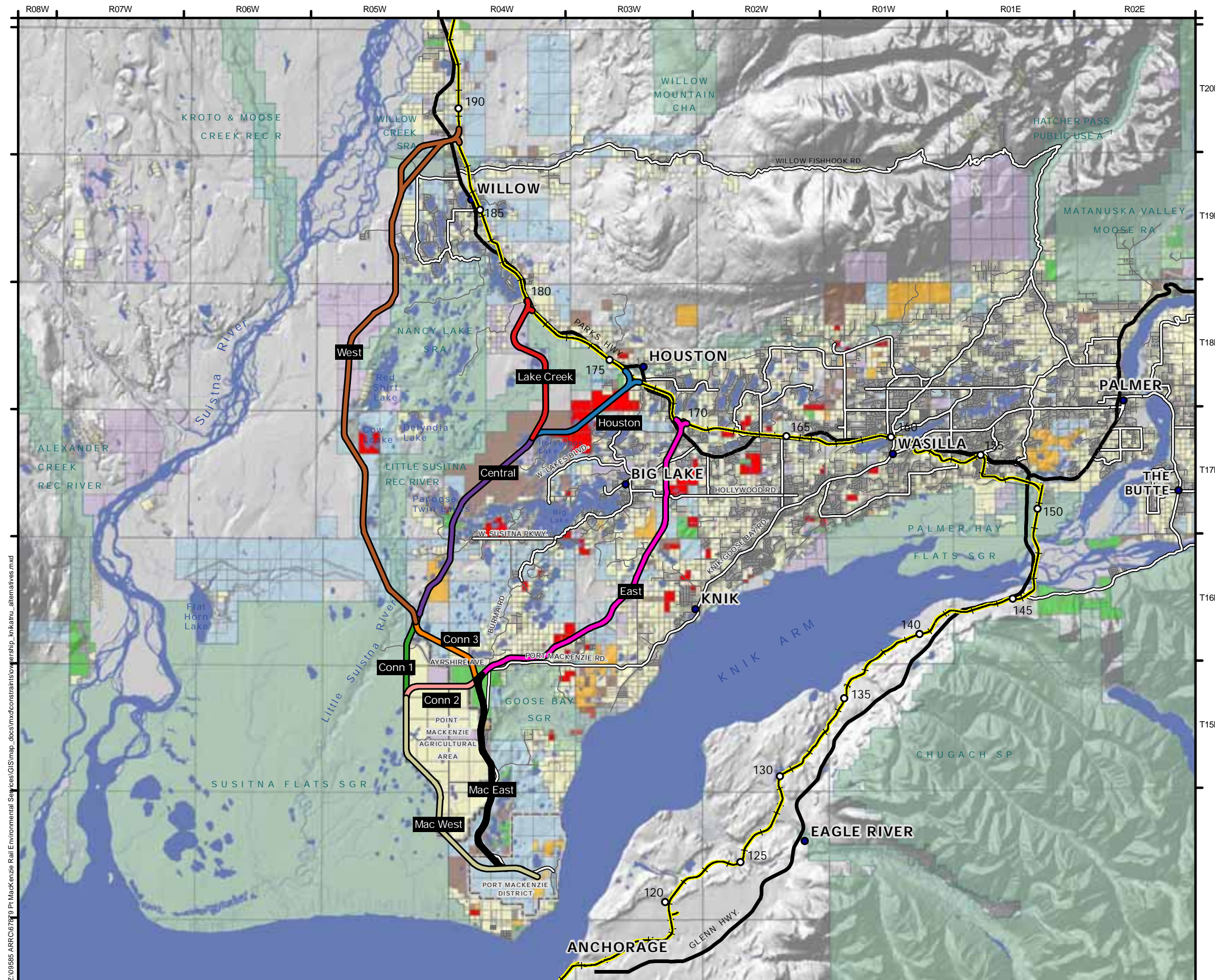
Preliminary Alternatives

- Mac East
- Mac West
- East
- West
- Conn 1
- Conn 2
- Conn 3
- Central
- Lake Creek
- Houston
- ARRC Milepost
- ARRC Track

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: August 29, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.



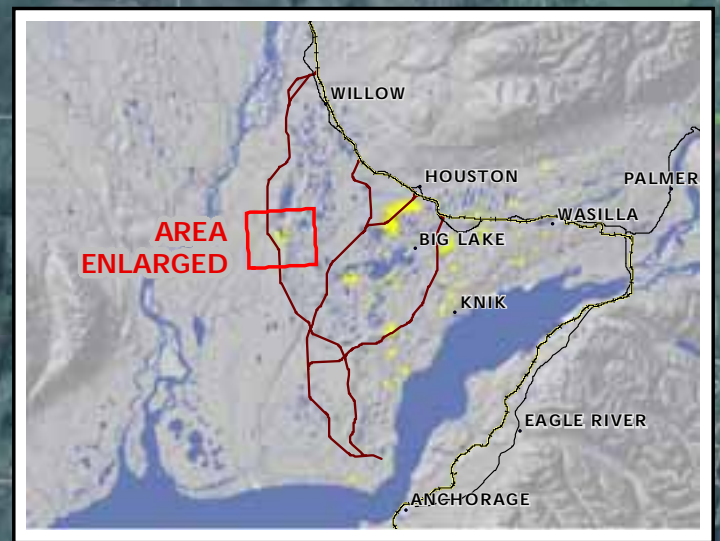
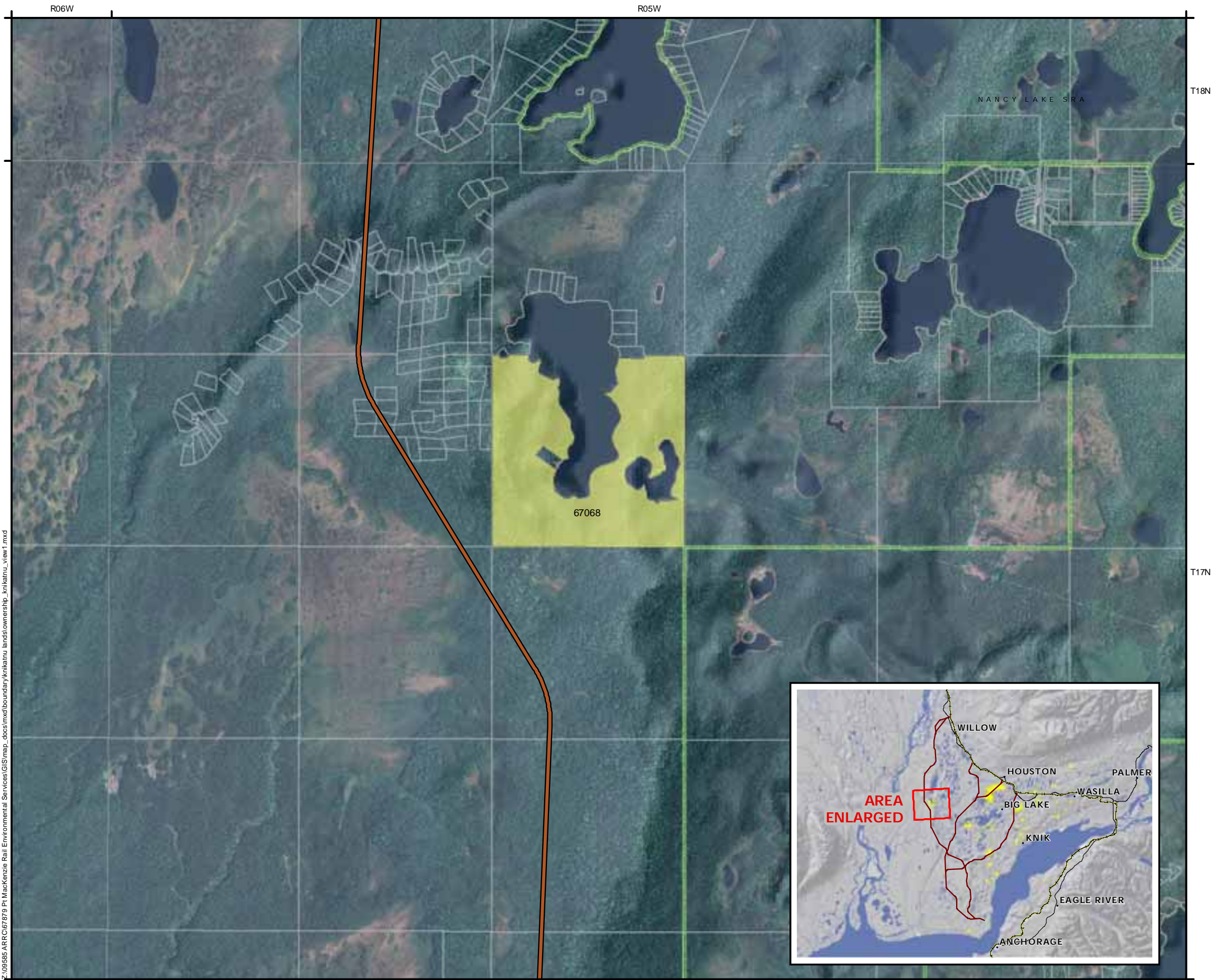
F:\09585 ARRC\67879 P1 MacKenzie Rail Environmental Services\GIS\map\_docs\mxd\construct\ownership\_knikatnu\_alternatives.mxd

*(This page intentionally left blank.)*

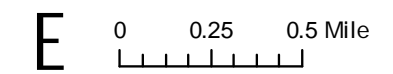


**LEGEND**

- Knikatnu, Inc.
- Parcel Boundary
- Preliminary Alternatives**
- Mac East
- Mac West
- Conn 1
- Conn 2
- Conn 3
- Houston
- Houston North
- Houston South
- Willow
- Big Lake
- ARRC Track
- Point MacKenzie Ag Area
- Port MacKenzie District
- Park or Refuge



This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: September 25, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

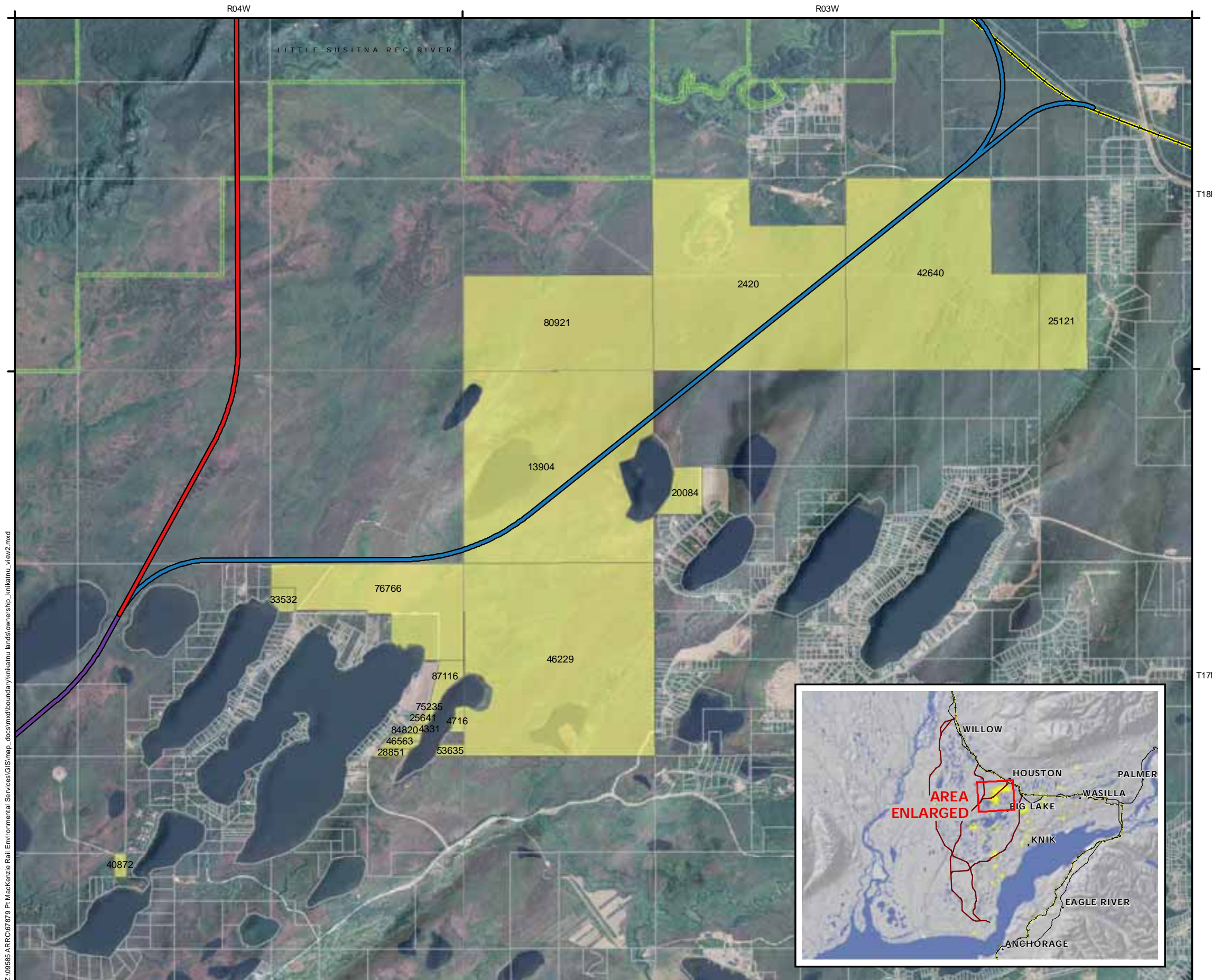
F:\09585 ARRC\67879 P1 MacKenzie Rail Environmental Services\GIS\map\_docs\mxd\boundary\Knikatnu lands\ownership\_knikatnu\_view1.mxd

*(This page intentionally left blank.)*

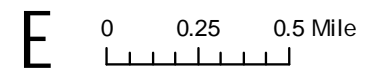


**LEGEND**

- Knikatnu, Inc.
- Parcel Boundary
- Preliminary Alternatives**
- Mac East
- Mac West
- Conn 1
- Conn 2
- Conn 3
- Houston
- Houston North
- Houston South
- Willow
- Big Lake
- ARRC Track
- Point MacKenzie Ag Area
- Port MacKenzie District
- Park or Refuge



This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: September 25, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc.,  
 MSB GIS, TNH- Hanson, USGS.

F:\09585 ARRC\7879 P1 MacKenzie Rail Environmental Services\GIS\map\_docs\mxd\boundary\knikatnu\_landownership\_knikatnu\_view2.mxd

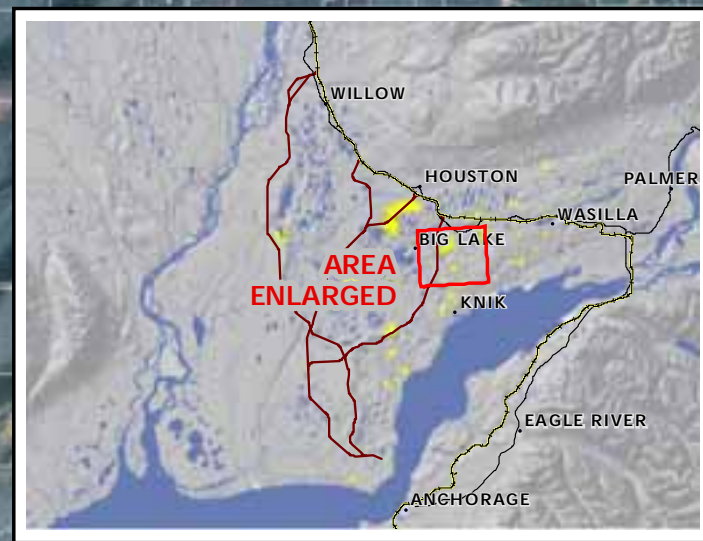
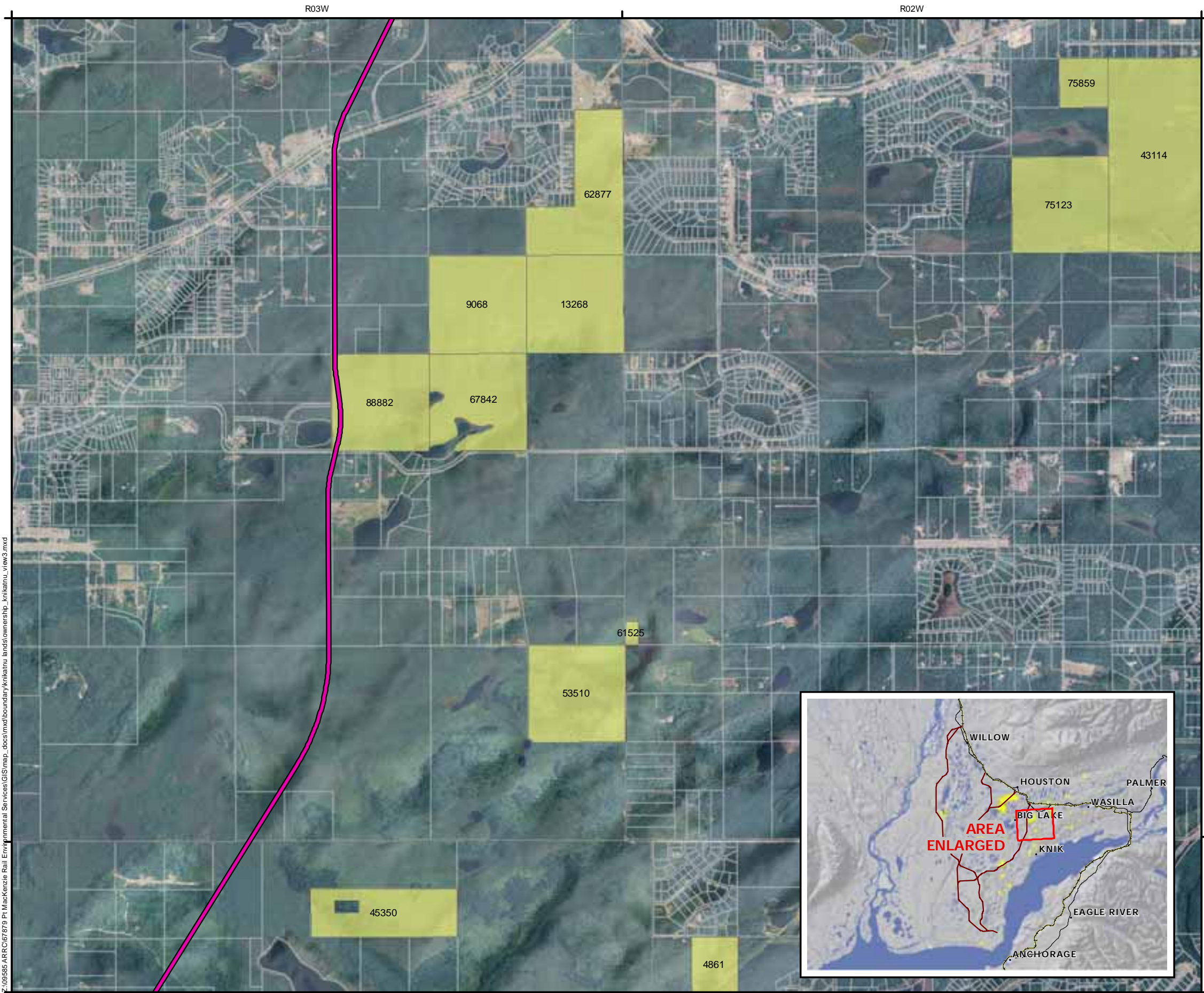


*(This page intentionally left blank.)*

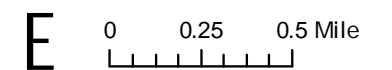


**LEGEND**

- Knikatnu, Inc.
- Parcel Boundary
- Preliminary Alternatives**
- Mac East
- Mac West
- Conn 1
- Conn 2
- Conn 3
- Houston
- Houston North
- Houston South
- Willow
- Big Lake
- ARRC Track
- Point MacKenzie Ag Area
- Port MacKenzie District
- Park or Refuge



This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: September 25, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

F:\09585 ARRC\7879 P1 MacKenzie Rail Environmental Services\GIS\map\_docs\mxd\boundary\Knikatnu lands\ownership\_knikatnu\_view3.mxd

*(This page intentionally left blank.)*

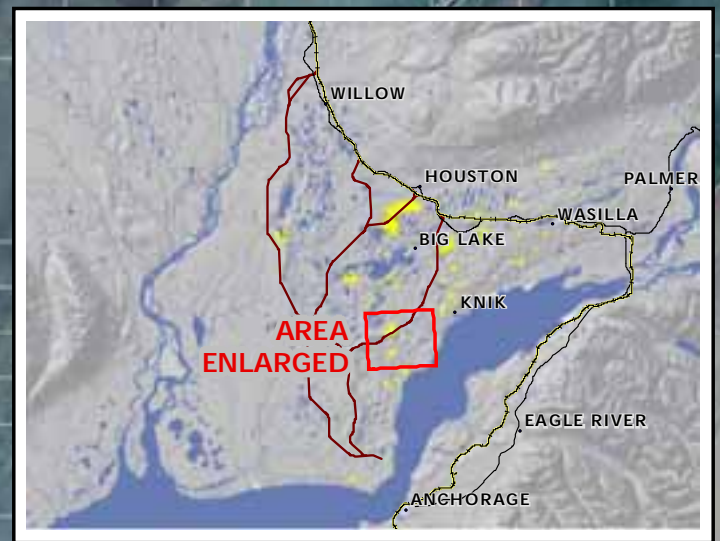
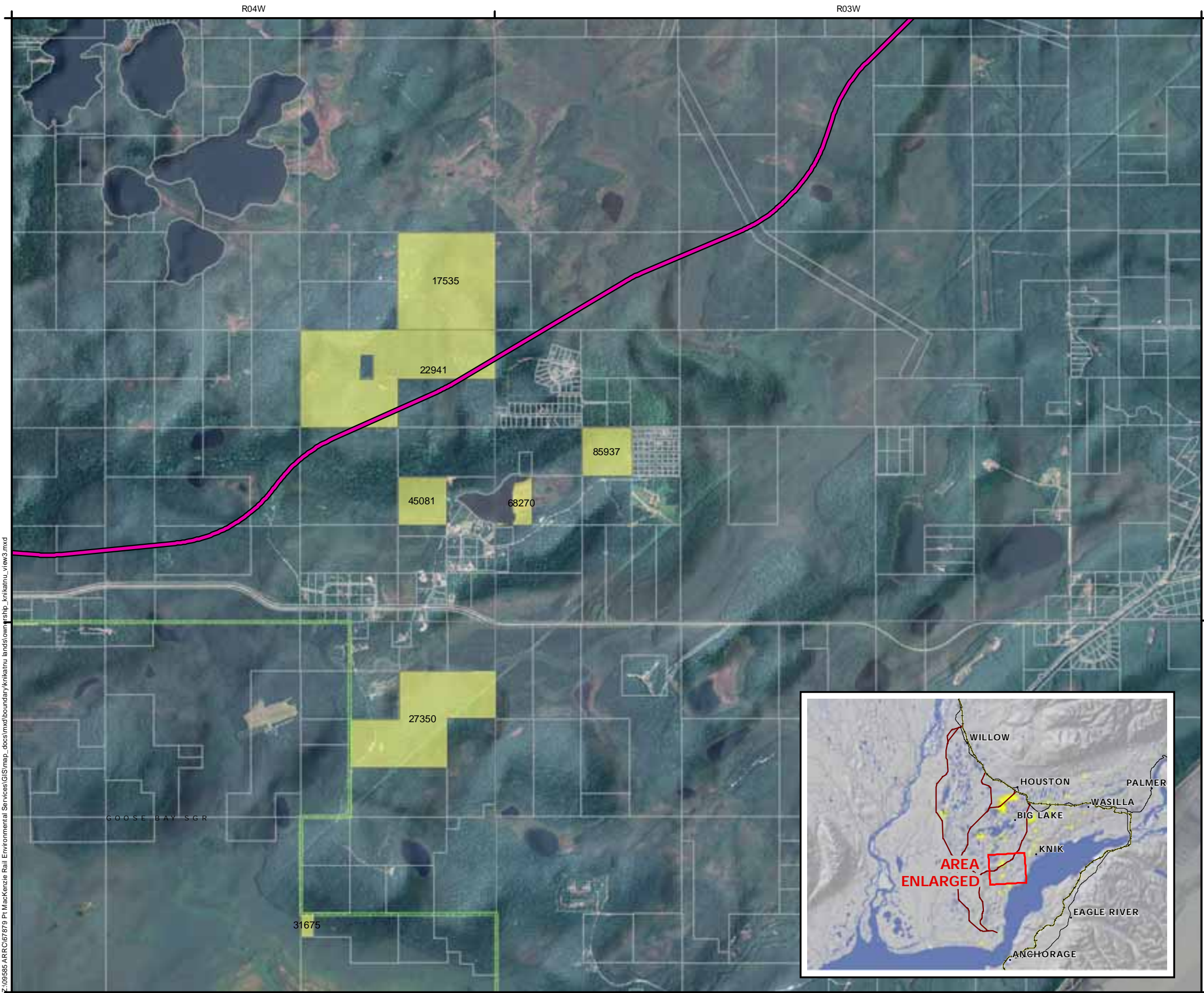


**LEGEND**

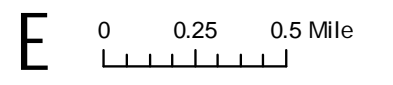
- Knikatnu, Inc.
- Parcel Boundary
- Preliminary Alternatives**
- Mac East
- Mac West
- Conn 1
- Conn 2
- Conn 3
- Houston
- Houston North
- Houston South
- Willow
- Big Lake
- ARRC Track
- Point MacKenzie Ag Area
- Port MacKenzie District
- Park or Refuge

T16N

T15N



This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: September 25, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.

F:\09585 ARRC\67879 P1 MacKenzie Rail Environmental Services\GIS\map\_docs\mxd\boundary\Knikatnu lands\down\ship\_knikatnu\_views.mxd

*(This page intentionally left blank.)*



Fax

Date: 10-19-07

Number of pages (include cover): 2

**Message To**

Name: Raymond Theodore, President

Fax No: 376-2847

Firm: Knikatnu, Inc.

Phone No: 376-2845

City: Wasilla

State: AK

Re: 10/16/07 Letter

Project: ARRC

**Message From:**

Name: Rosetta Alcantra

Fax No: 745-4139

CC:

Phone No: 745-4129

**Notes:**

Raymond,

Here is the letter requesting a meeting with Knikatnu, Inc. and the additional lands information. I will send it in the mail as well with the maps we provided.

Hope all is well and have a wonderful weekend.

Regards,

Rosetta

*(This page intentionally left blank.)*



October 16, 2007

Mr. Raymond Theodore, President  
Knikatnu, Inc.  
P.O. Box 872130  
Wasilla, AK 99654

Dear Mr. Theodore:

Thank you for taking the time to meet and discuss the Port MacKenzie Rail Extension Project and the Knikatnu, Inc. land parcels identified in the initial corridor development process. Your interest in assisting us with compiling additional information on Knikatnu, Inc. lands and setting up a meeting with the corporation Board of Directors is greatly appreciated.

Per your request, I would like to take this opportunity to request additional information on the land parcels pertaining to existing use and future development. As you are aware from our meeting and from the Public Open House meetings last week, this project is moving quickly. Any additional information you have regarding the land parcels would be useful in comparing the identified corridors.

I have also enclosed six copies of project information for the Board to review. Please coordinate with Rosetta Alcantra at 745-4129 or via email at [rosetta.alcantra@hdrinc.com](mailto:rosetta.alcantra@hdrinc.com) to set up a meeting. I would encourage you to set up the meeting during the week of October 22, 2007 if possible. I am also available if you have any questions at 265-3095 or via email at [LindamoodB@akrr.com](mailto:LindamoodB@akrr.com).

Again, your interest in this project is greatly appreciated and I look forward to receiving additional information on the land parcels as well as meeting with the Knikatnu, Inc. Board of Directors.

Regards,

Brian Lindamood, Project Manager  
Alaska Railroad Corporation

CC: Joe Perkins, Mat-Su Borough  
Eileen Riley, Alaska Railroad Corporation  
Brad Sworts, Project Manager, Mat-Su Borough  
Donna Robertson, Project Manager, HDR Alaska, Inc.



**Alcantra, Rosetta M.**


---

**From:** Alcantra, Rosetta M.  
**Sent:** Thursday, October 18, 2007 8:39 AM  
**To:** Alcantra, Rosetta M.  
**Subject:** 10-17-07 Response from Knik Tribal Council

---

**From:** telefuture . [mailto:telefuture@gci.net]  
**Sent:** Wednesday, October 17, 2007 10:41 AM  
**To:** Alcantra, Rosetta M.  
**Cc:** Debra Call; Robertson, Donna  
**Subject:** Re: Port MacKenzie Rail Extension Project

Rosetta,

We have set up a project description / comment area in the main office.

We posted the map and comment form distributed at the Lake Lucille community meeting, developed an explanation flyer, and then requested comments with a folder to catch them. Let me know when we should cut that off and forward the comments to you.

Debra will be able to advise on a meeting with the HP committee. I know that 2 members will be at AFN that week.

Thanks for involving KTC.

Jack Alcorn

----- Original Message -----

**From:** "Alcantra, Rosetta M." <Rosetta.Alcantra@hdrinc.com>  
**Date:** Wednesday, October 17, 2007 9:45 am  
**Subject:** Port MacKenzie Rail Extension Project  
**To:** JACK ALCORN <telefuture@gci.net>, Debra Call <calld73@yahoo.com>  
**Cc:** "Robertson, Donna" <Donna.Robertson@hdrinc.com>

> Jack,

>

> I have been asked to set up a meeting with the Knik Tribal Council  
 > regarding the Port MacKenzie Rail Extension project. As you know this  
 > project is moving quickly and we would like to get as much information  
 > as possible regarding concerns or interests of the various  
 > stakeholders. The Knik Tribal Council is very important, especially  
 > as we assess the  
 > cultural significance of the area and information pertaining to  
 > historic and cultural sites.

>

> The project team members would like to meet with you and the Knik  
 > Tribal Council Historic Committee next week, possibly Tuesday

- > Tribal Council Historic Committee next week, possibly Tuesday
- > morning. The
- > project team will include myself, Fran Seager-Boss (MSB), Kirsten
- > Anderson (HDR) and possibly Brian Lindamood from the Alaska Railroad
- > Corporation.
- >
- > Please let me know at your earliest convenience your availability.
- > Again, thank you for taking the time to come to the Open House earlier
- > this month, as always it was a pleasure to see you.
- >
- > Regards,
- >
- > Rosetta
- >

*(This page intentionally left blank.)*

**Alcantra, Rosetta M.**

---

**From:** Debra Call [calld73@yahoo.com]  
**Sent:** Thursday, November 01, 2007 7:09 AM  
**To:** Alcantra, Rosetta M.; Fran Seager-Boss  
**Subject:** Fw: Knik Tribal Council Cultural & Historical Preservation Committee Meeting

Rosetta / Fran - It looks like we are on for Monday at 11AM for a meeting. I will probably put a vegetable soup in a crockpot with some bread and lemonade. Rosetta - Can HDR donate the dessert such as cookies or fruit?

I expect that we will have Rosie and Theo, not sure about Patsy. Theo and Patsy will hear the presentation for a second time from Brian.

Fran - what do you need us to do for your presentation? -- Thanks- Deb

*Debra L. Call*  
907-562-9670  
Cell - 907-230-7012

----- Forwarded Message -----

**From:** Rosie I. Choquette <RChoquette@kniktribalcouncil.org>  
**To:** Debra Call <calld73@yahoo.com>  
**Sent:** Tuesday, October 30, 2007 9:00:03 AM  
**Subject:** RE: Knik Tribal Council Cultural & Historical Preservation Committee Meeting

Debra, I have contacted both Theo and Patsy; they will both be there. Have a nice day. { ; )

Rosie I. Choquette  
Administrative Assistant  
Knik Tribal Council  
P.O. Box 871565  
Wasilla AK 99687-1565

Phone: (907) 373-7991  
Fax: (907) 373-2161  
[rchoquette@kniktribalcouncil.org](mailto:rchoquette@kniktribalcouncil.org)

---

**From:** Debra Call [mailto:calld73@yahoo.com]  
**Sent:** Mon 10/29/2007 8:11 PM  
**To:** Rosie I. Choquette; Theodore Garcia; Steve Garcia; Patsy Garcia  
**Cc:** Jack Alcorn; Fran Seager-Boss; Rosetta M. Alcantra; Jennifer L. Raschke  
**Subject:** Knik Tribal Council Cultural & Historical Preservation Committee Meeting

There will be a Knik Tribal Council Historical and Cultural Preservation Committee meeting at the Alaska Native Heritage Center at 11AM on Monday, November 5. Lunch will be served.\

We will be hearing from Rosetta Alcantra and Fran Seager Boss working on the extension of the Alaska Railroad to Pt. MacKenzie. They would like to discuss with us any issues that we might have regarding cultural sites that we want to protect. If there are things or places that we want to protect, now is the time to hear from them about their plans.

After they meet with us, the plan will go to a variety of potential contracts for bid to do an Environmental Impact Study on where the rail road might go.

#### AGENDA

1. Call to Order
2. Roll Call
3. Reports - Tribal Cultural preservation conference in Oklahoma
4. Status of the Pat MacClenahan report on Mat-Su area (Fran Seager Boss)
5. Status of the Alaska Railroad project planned for the Mat-Su / Wasilla area
6. Adjourn

Please let me know by Thursday, November 1 if this day will work for you.

Jennifer / Rosie - can you please call people and let me know what the attendance will look like for Monday, November 5? I have no phone numbers or addresses. I'm not even sure if this email will get to some of the people.

Thanks - Deb

Debra L. Call  
907-562-9670  
Cell - 907-230-7012

---

Do You Yahoo!?  
Tired of spam? Yahoo! Mail has the best spam protection around  
<http://mail.yahoo.com>

---

Do You Yahoo!?  
Tired of spam? Yahoo! Mail has the best spam protection around  
<http://mail.yahoo.com>



Port MacKenzie Rail Extension Project  
 Knik Tribal Council Historic Preservation Committee  
 Monday, November 5, 2007  
 11:00 AM  
 Alaska Native Heritage Center

First Name	Last Name	Address	City	Zip	Email
Rosie	Choquette	P.O. Box 871565 Wendler 99608	Wendler	99607	choquette@kniktribal.com
Patsy	Garcia	P.O. Box 142784 Anchorage, AK 99504	Anch	99504	garcia@mtaonline.net
Denise	Call	1090 Schumaker Dr Anch	Anch	99507	Call1473@yahoo.com
Raymond	Theodore	P.O. Box 872130 WASILLA, AK 99654	WASILLA	99654	r-theodore@cci.net
Pat	McClenahan	PO BOX 771203 2525 C ST	Eagle River	99577	plmcclenahan@cci.net
Kristen	Anderson		Anch		kristen.anderson@bdmne.com
Rosetta	Arceuthre	2525 C ST	Anch	99503	rosetta.arceuthre@bdmne.com
Brian	Lindemann	AKLEC	Anch		
Fran	Seagrass	MSB	Palmer	99645	

*(This page intentionally left blank.)*



## Minutes of Meeting

Date: November 5, 2007

Time: 11:00 AM (AKDT)

Location: Alaska Heritage Center, Anchorage, Alaska

Project: Port MacKenzie Rail Line Extension

Purpose: Historic Preservation and Cultural Committee

In Attendance: See attached sign in sheet

Meeting began at 11:00 am with introductions. Present were the following.

- Debra Call, Committee Chair, Knik Tribal Council Historic Cultural Committee
- Patsy Garcia, Member, Knik Tribal Council Historic Cultural Committee
- Rose Choquette, Member, Knik Tribal Council Historic Cultural Committee
- Brian Lindamood, ARRC, Port MacKenzie Rail Extension Project Manager
- Rosetta Alcantra, HDR Alaska, Public Involvement Specialist
- Kirsten Anderson, HDR Alaska, Cultural Resources
- Fran Seager-Boss, MSB, Cultural Resources
- Patricia McClenahan, Consultant
- Raymond Theodore, President, Knikatnu, Inc.

Debra Call provided an agenda for the meeting. Brian Lindamood, ARRC was the first on the agenda and he began with an overview of the Port MacKenzie Rail Extension project. Highlights from his presentation are:

- 2003 Study commissioned by the MSB Port MacKenzie and the Alaska Department of Transportation and Public Facilities was a high level overview study.
- In 2007 the Alaska State Legislature appropriated \$10 million to conduct a more detailed study. The motivation for this project is to look at marketing opportunities, such as the coal gasification project proposed by Agrium in Nikiski.
- The ARRC is a partner in this project with the Matanuska Susitna Borough. Jointly they are pursuing a license to construct and operate a rail extension.
- The Surface Transportation Board will complete an environmental study under the National Environmental Protection Act (NEPA) independent of the railroad. The study will likely be conducted by a 3<sup>rd</sup> party contractor.



- Recap of the public involvement activities to date and working with the regulatory agencies.

Ms. Call asked about the historic and cultural site probability in the project area. She indicated the only way to validate the sites is to go visit and the tribe does not have the resources to do this. She asked if the railroad would have the resources to support field visits and survey of the area?

Mr. Lindamood responded that it was going to be at the discretion of the STB, what the project team is doing now is compiling the information, documenting concerns and then will be giving the STB the information. He continued with informing the committee that the project team is in the process of developing a matrix, which the Section 106 information will be included in. The project team does understand the surface information, what is not known is the information regarding historic and cultural sites.

Ms. Call inquired about how and when to provide comments on the project. Mr. Lindamood indicated the project team is taking comments throughout the project and that the STB will also be conducting a formal public involvement process, possibly as soon as March 2008.

Ms. Call briefly discussed what is known of the area, there are family photos and family discussion, but nothing is documented. There was a general consensus that it takes time to gather the information. The discussion also moved into previous experience where the Knik Tribal Council was requesting recognition of the Tak'at site by the Port of Anchorage. There was frustration of who is advocating for the tribes.

Mr. Lindamood asked that the committee go through the map provided and circle the areas of concern for the Knik Tribal Council, from a project management perspective any information they have will help in developing the corridors and understanding existing constraints. Mr. Lindamood also reminded the group that the project is locked into a schedule and would be shipping the information to the STB.

Fran Seager-Boss then went over the probability map for the proposed rail extension corridors; the orange color indicates areas that needed to be surveyed, purple indicates areas where potential prehistoric sites are located, and the dots represent known sites or reported sites. There was concern for the Red Shirt area on the Willow route. This is a Den'aina area, the confluences are identified by red slashes (rivers and lakes). The confluences are typically good hunting and fishing areas.

Before closing, Mr. Lindamood requested a letter from the Knik Tribal Council. This letter should include the concerns, corridor preference, and concerns with other corridors. This information will go to the STB and be an avenue for the KTC to get recognition and have their concerns documented as the NEPA process begins in earnest. Mr. Lindamood also reminded the KTC about the Government to Government process. It is important for the project team to identify priorities and then provide possible mitigation measures.

Ms. Call thanked Mr. Lindamood for his presentation and asked a final question about who would be conducting the Section 106 and if it would be Steve Braund and Associates. Mr.

Lindamood indicated he did not know at this time. There was also a brief exchange about the Iditarod Trail Committee and KTC's desire to be at the same table when they are discussing the importance of the trail as a historic trail.

The meeting broke for lunch at 12:00 pm and Brian concluded the presentation.

*(This page intentionally left blank.)*

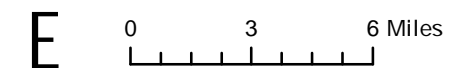


**LEGEND**

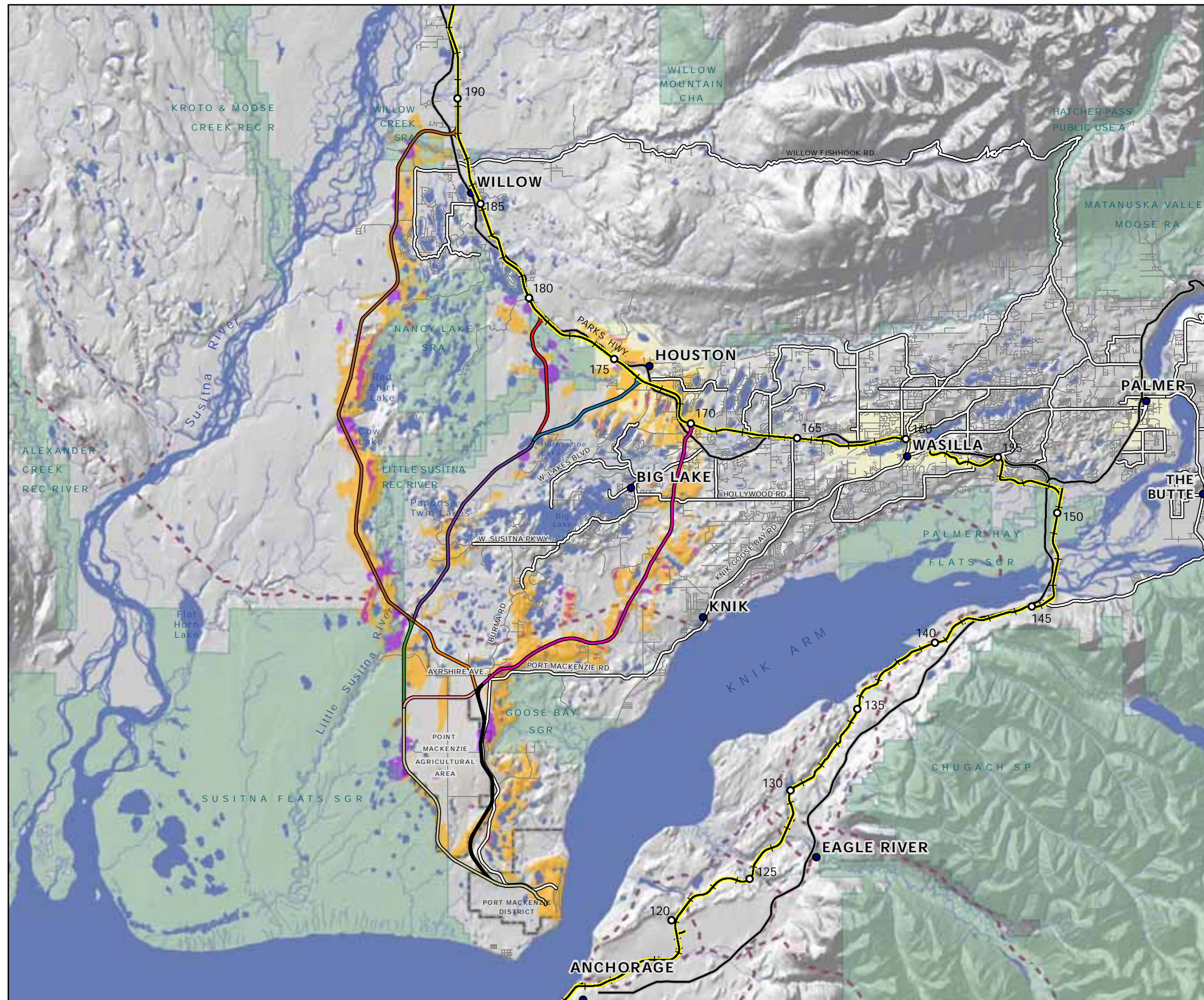
- Areas of High Potential for Prehistoric Sites
  - Ridges and Areas for Archeological Surveys
- Preliminary Alternatives\*
- ARRC Track
  - Mac East
  - Mac West
  - Conn 1
  - Conn 2
  - Conn 3
  - Houston
  - Houston North
  - Houston South
  - Willow
  - Big Lake
  - ARRC Milepost
  - Highway
  - Medium Rd.
  - Minor Rd.
  - Iditarod Trail
  - City Boundary
  - Park or Refuge

\*These lines generally represent corridors which are subject to further refinement.

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: November 13, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH- Hanson, USGS.



*(This page intentionally left blank.)*

**Alcantra, Rosetta M.**

---

**From:** Brian Lindamood [LindamoodB@akrr.com]  
**Sent:** Wednesday, October 24, 2007 8:44 AM  
**To:** Brad.Sworts@matsugov.us  
**Cc:** Wendy Lindscoog; Stephenie Wheeler; Robertson, Donna; Alcantra, Rosetta M.; patty.sullivan@matsugov.us; engrperk@msn.com  
**Subject:** Planning Commission Packet  
**Attachments:** Matrix\_10-19-07.pdf; Port MacKenzie Open House Presentation.pdf; trails\_usgs\_base\_10-22-07.pdf; Public Meeting Stations.v.final.pdf; Public Meeting Statistics.pdf

Brad-

I understand we need to get the packets to the Planning Commission today. Based upon our discussions, I am sending you the attached to provide to them. If you could turn it in to whomever needs it, I would appreciate it.

Attached please find the following:

- Open House Presentation
- Open House Station Boards
- Matrix
- Trails Map
- Open House Statistics

Brad, if you feel any of these things should not go, feel free to pull them out.

Thanks,

Brian

*(This page intentionally left blank.)*



*Welcome!*  
Port MacKenzie Rail Extension Project  
Public Open House



[www.portmacrail.com](http://www.portmacrail.com)





# Project Participants

Alaska Railroad Corporation \_\_\_\_\_ Applicant

Matanuska-Susitna Borough \_\_\_\_\_ Project sponsor

Surface Transportation Board \_\_\_\_\_ Lead federal agency

HDR Alaska, Inc. \_\_\_\_\_ Consultant

TNH-Hanson, LLC \_\_\_\_\_ Consultant

STB Section of Environmental Analysis \_\_\_\_\_ NEPA Compliance

To be determined \_\_\_\_\_ 3<sup>rd</sup> Party NEPA Contractor



[www.portmacrail.com](http://www.portmacrail.com)



# STB Process

## Railroad

- Files notice with STB
- Files petition for the construction and operation of railroad

## STB

- Considers application based upon Railroad's ability to operate/be stable and market needs
- Incorporates conditions from the SEA upon favorable finding

## SEA

- Conducts NEPA process independently of railroad\*
- Most likely utilizes 3rd party contractor

\*Railroad has very limited role/input in process beyond providing information



# What's Happening Now and Why



- 2003 Study was a reconnaissance study
- Re-evaluating and refining most promising corridors
- Incorporating latest environmental and planning information



[www.portmacrail.com](http://www.portmacrail.com)

# Constraints Mapping



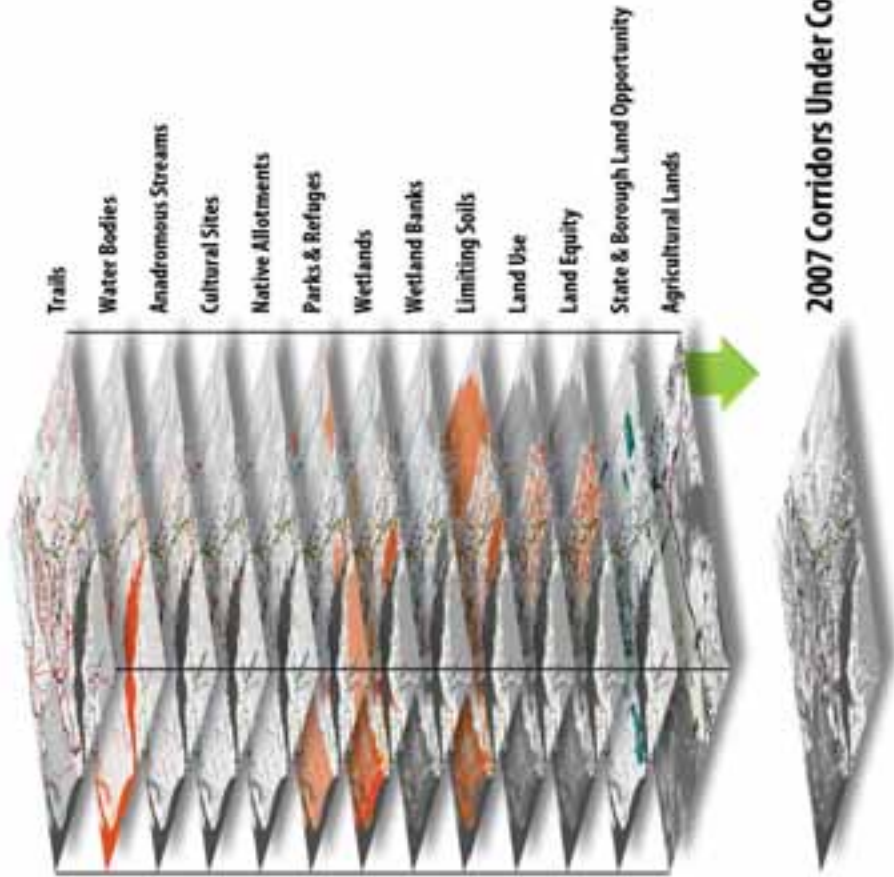
## Topics considered:

- Water bodies
- Anadromous streams
- Habitat
- Cultural sites
- Native allotments
- Parks and refuges
- Wetlands and wetland banks
- Limiting soils
- Land use
- Land value
- Land availability
- Agricultural lands
- Trails



[www.portmacrail.com](http://www.portmacrail.com)

# Corridor development process



[www.portmacrail.com](http://www.portmacrail.com)

# Working with the Public

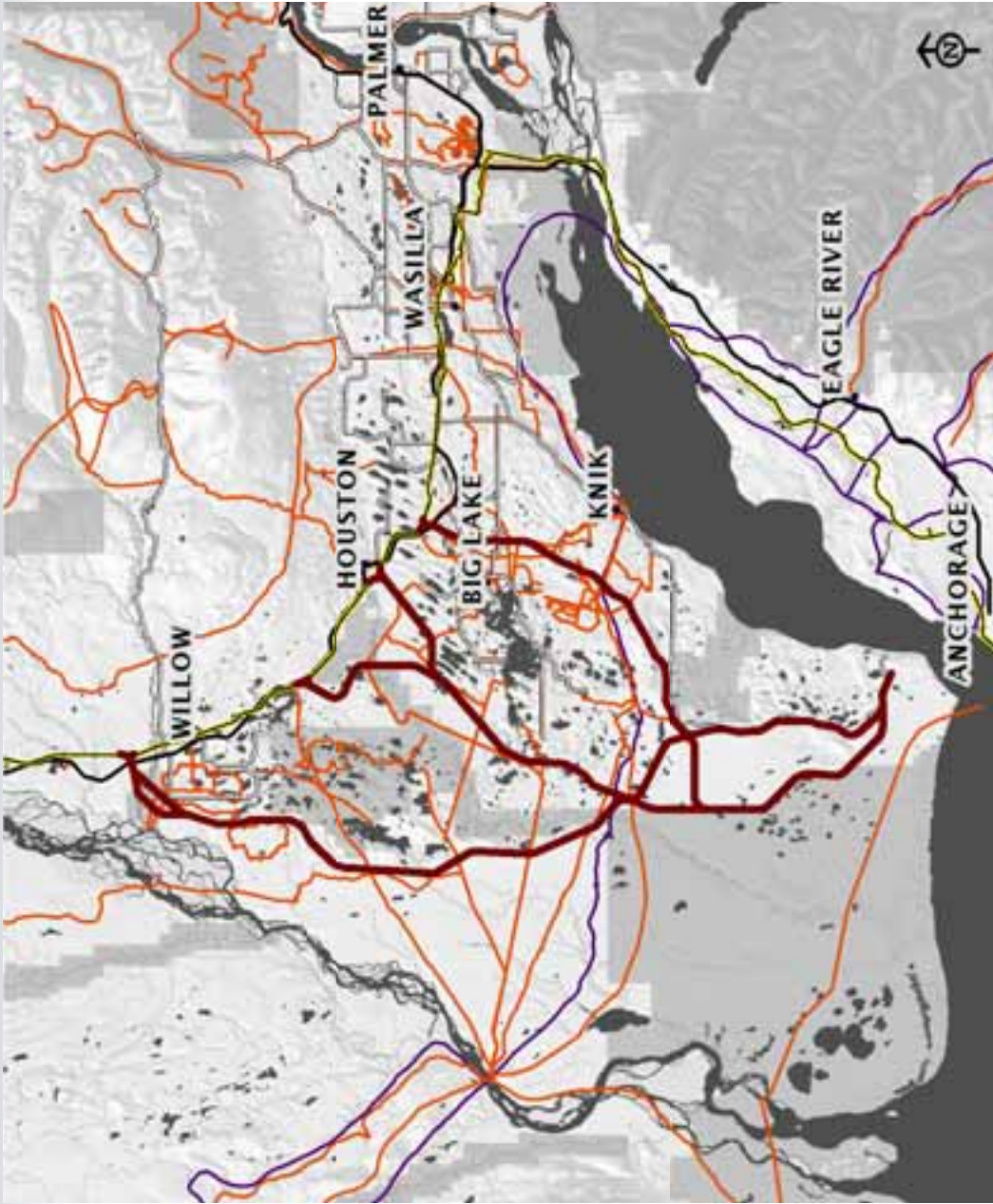


- Hosting series of public open house meetings
- Web site – informative and interactive
- Newsletter (forthcoming)
- Requesting public comment and using the comments to help determine corridors
- Meeting with small stakeholder groups
- Identification of mitigation ideas early in the process – Do you have solutions – We want to hear them
- Developing a comprehensive list of criteria to be considered and evaluated in the corridor development
- Conducting a more detailed study of the proposed corridors

**Your comments will help us find the best corridor.**



# Trails

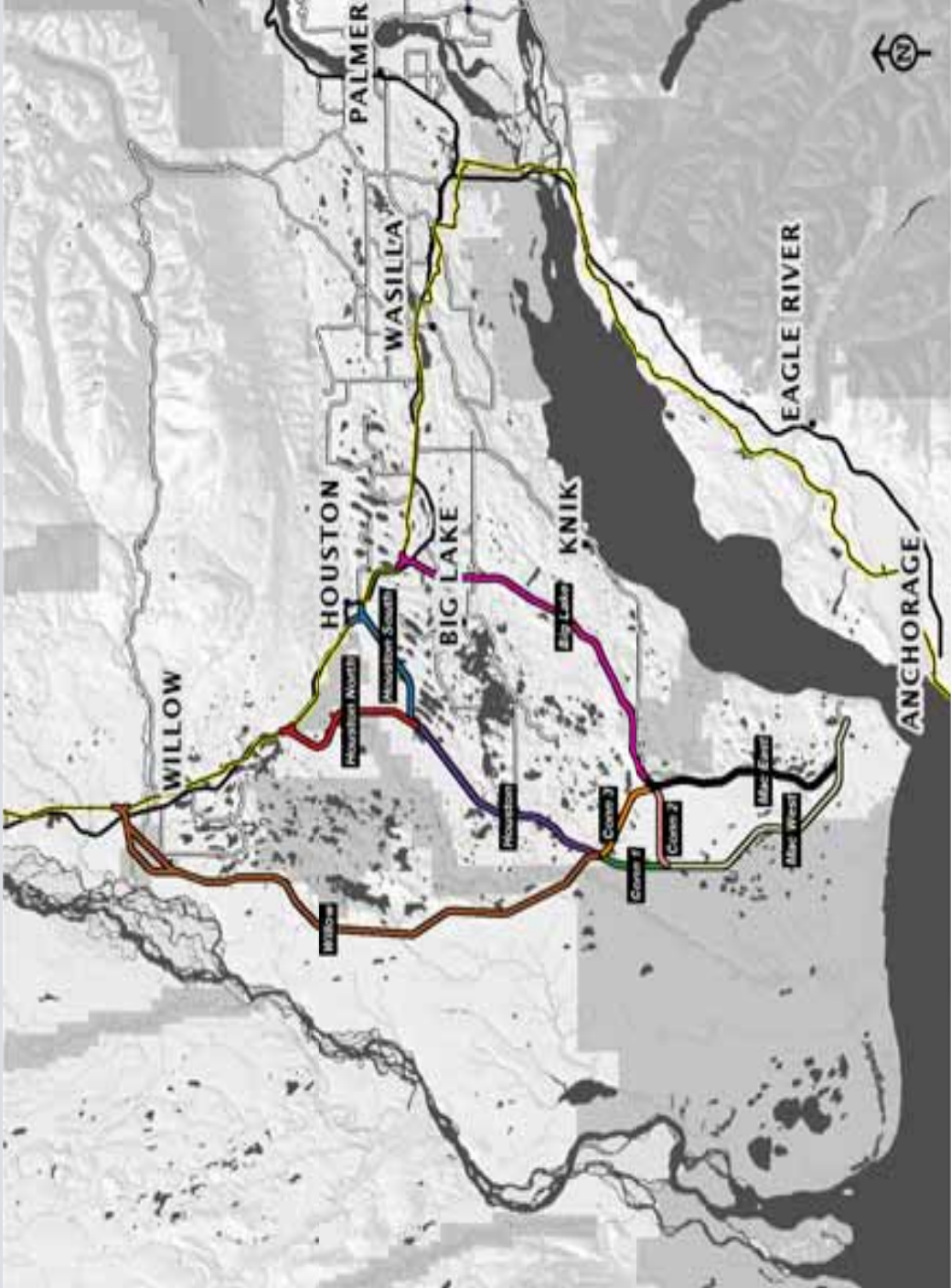


*Trails are an important consideration, especially as they relate to the quality of life concerns and recreational opportunities in the area.*



[www.portmacrail.com](http://www.portmacrail.com)

# Corridors Under Consideration



**Note: These lines represent possible corridors and are subject to change.**



[www.portmacrail.com](http://www.portmacrail.com)





# Anticipated Project Schedule

## October 2007

- Solicit input from public and regulatory agencies
- Consider possible impacts and mitigation associated with route alternatives
- File notice with STB end of October

## November 2007

- Finalize route considerations
- Refine possible mitigation based upon potential impacts

## December 2007

- Complete Route Alternatives document and submit to STB
- Develop possible MOA's/MOU's as appropriate
- Anticipate the STB to select 3<sup>rd</sup> Party NEPA contractor by end of 2007

## 2008 - 2009

- File petition in early spring 2008
- Anticipate the STB 3<sup>rd</sup> Party Contractor to complete environmental document by mid-2009





*Thank You for coming!*

Please visit the stations in the room and provide input and ask questions.



[www.portmacrail.com](http://www.portmacrail.com)



# *Welcome! Please Sign In*

## Port MacKenzie Rail Extension Project

### Public Open House Meeting



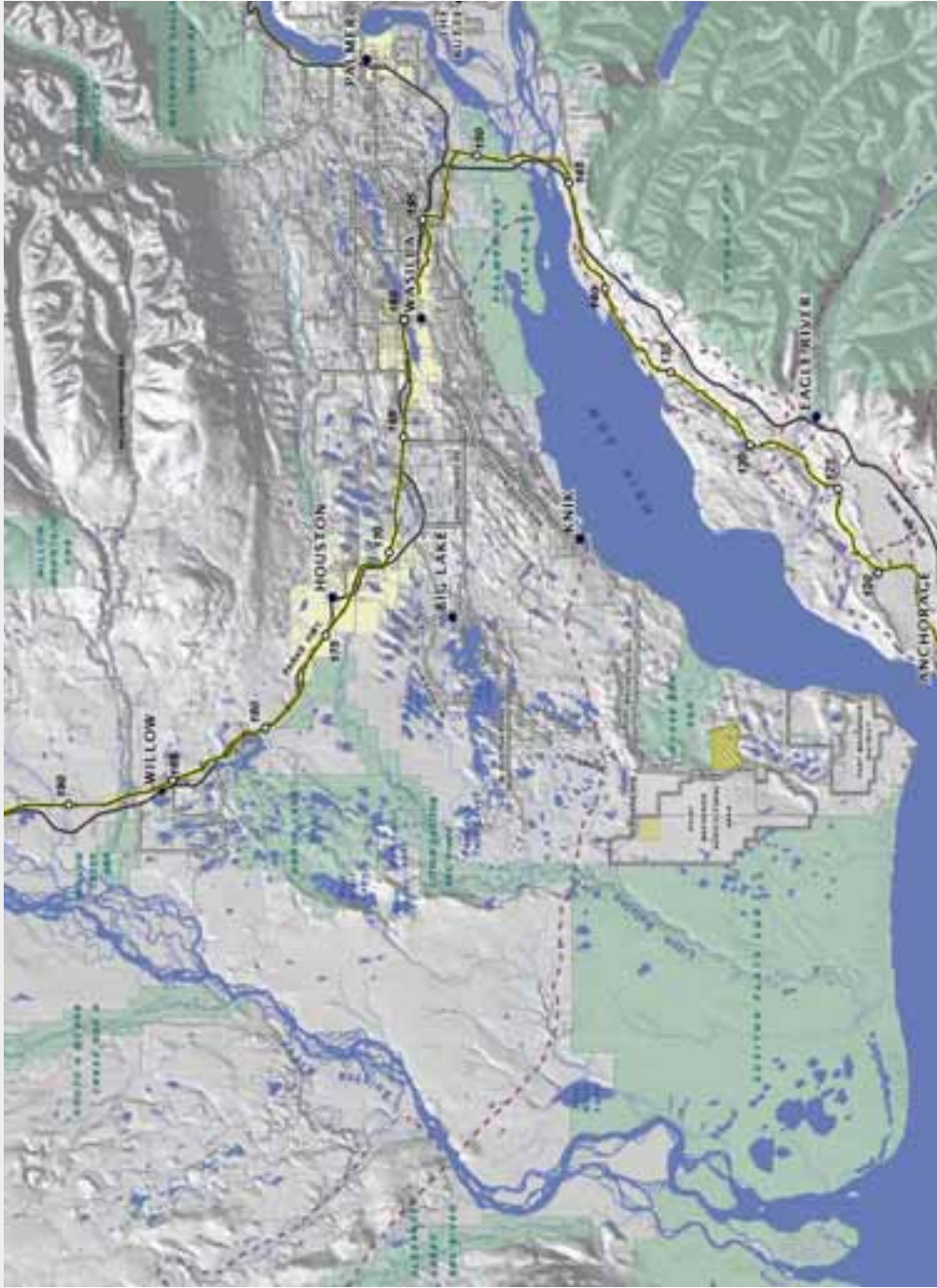
[www.portmacrail.com](http://www.portmacrail.com)

# Project History & Area



## HISTORY

- Port planning began in the early 1970s
- Port facilities developed in 1990s
- 2003 Railroad Corridor Study is the most recent analysis
- 2007 Legislature provided \$10 million to Mat-Su Borough
- June 2007 MSB and ARRC develop MOA to jointly pursue STB approval



[www.portmacrail.com](http://www.portmacrail.com)



# Project Purpose

## Elements of the project:

- *30 to 45 miles of new railroad*
- *200 foot right of way*
- *Railroad only (with maintenance road)*
- *Final design and construction estimated to cost \$300 million (2007 dollars)*
- *Could be operational by 2012*

Establish a rail link between Port MacKenzie and the Alaska Railroad, providing Port MacKenzie customers/ shippers efficient rail transportation between the port and interior Alaska.





# Project Participants

Alaska Railroad Corporation.....Applicant

Matanuska-Susitna Borough.....Project Sponsor

Surface Transportation Board...Lead Federal Agency

HDR Alaska, Inc. ....Environmental Consultant

TNH-Hanson, LLC .....Engineering Consultant





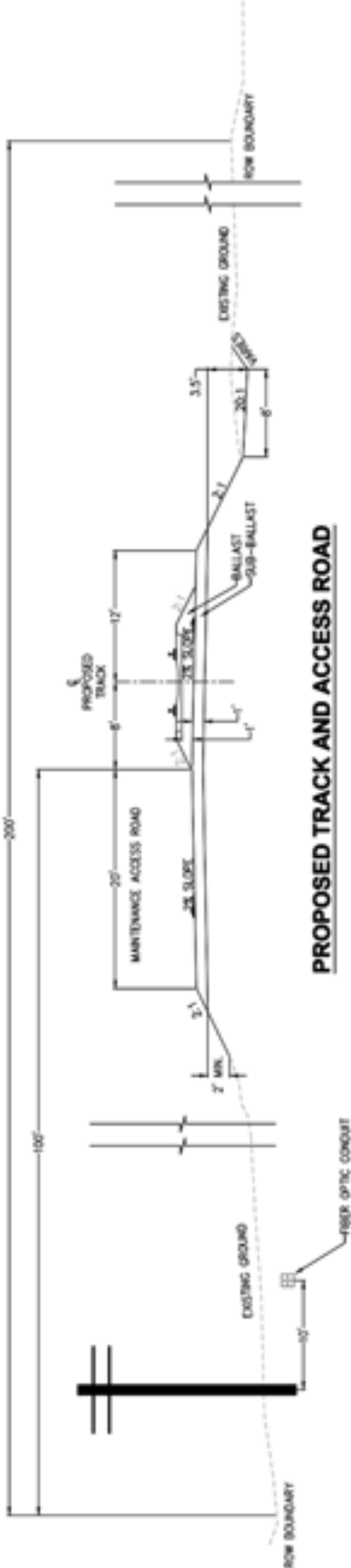
# Project Schedule

- October 2007 – Public Open Houses
  - o October 1 – Lake Lucille Inn, Wasilla
  - o October 2 – Big Lake Elementary, Big Lake
  - o October 3 – Willow Area Community Organization, Willow
  - o October 4 – Knik Elementary, Knik
  - o October 5 – Houston Middle School, Houston
- October 2007 – MSB Transportation Advisory Board
- November 5, 2007 – MSB Planning Commission Public Hearing
- November 20, 2007 – Presentation to MSB Assembly
- Mid-December 2007 – Submit Alignment Alternatives Document to STB

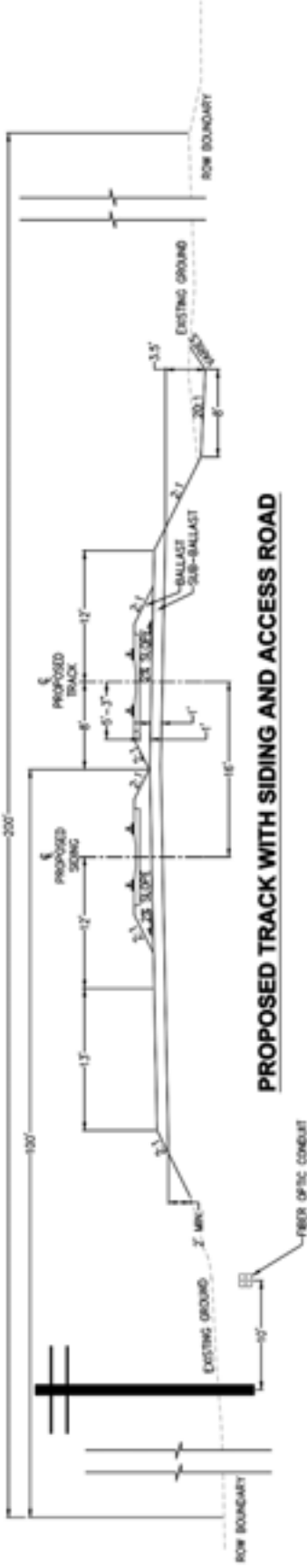


[www.portmacrail.com](http://www.portmacrail.com)

# Proposed Track and Maintenance Road



**PROPOSED TRACK AND ACCESS ROAD**



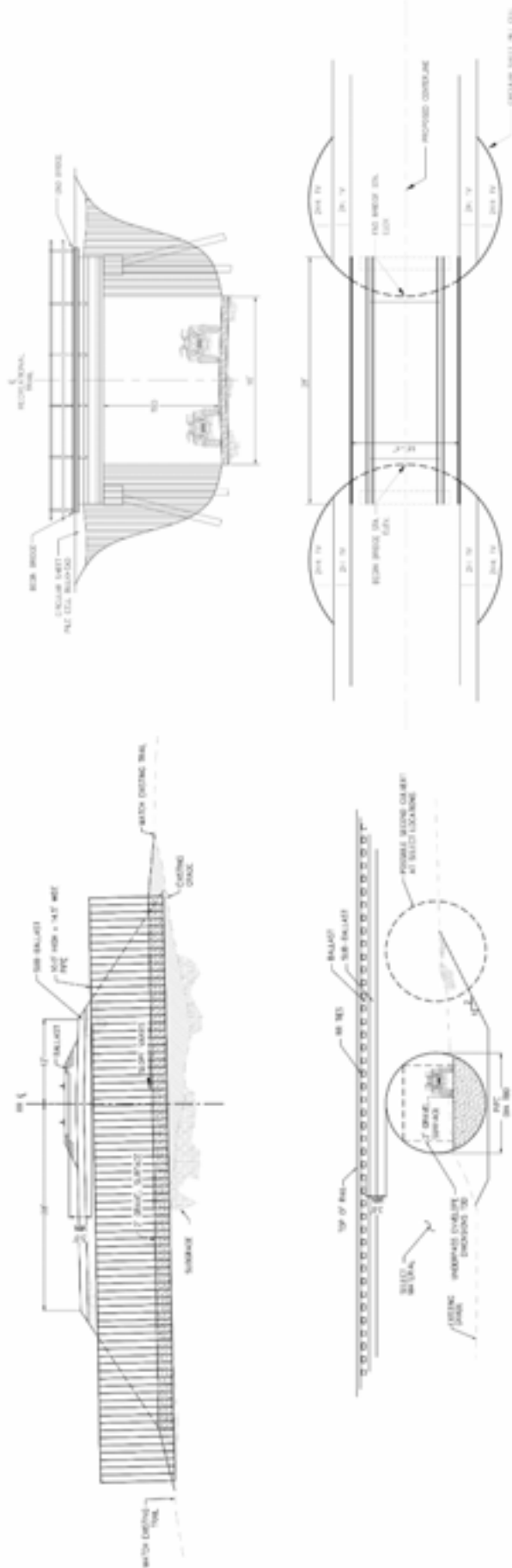
**PROPOSED TRACK WITH SIDING AND ACCESS ROAD**



[www.portmacrail.com](http://www.portmacrail.com)

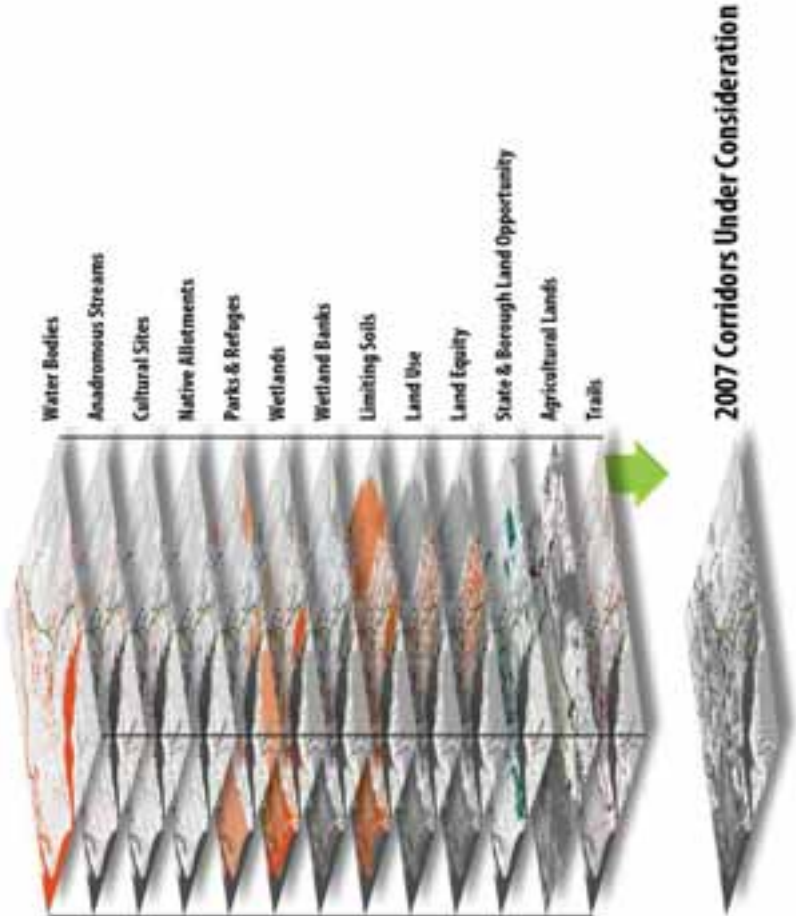


# Proposed Trail Crossing Alternatives



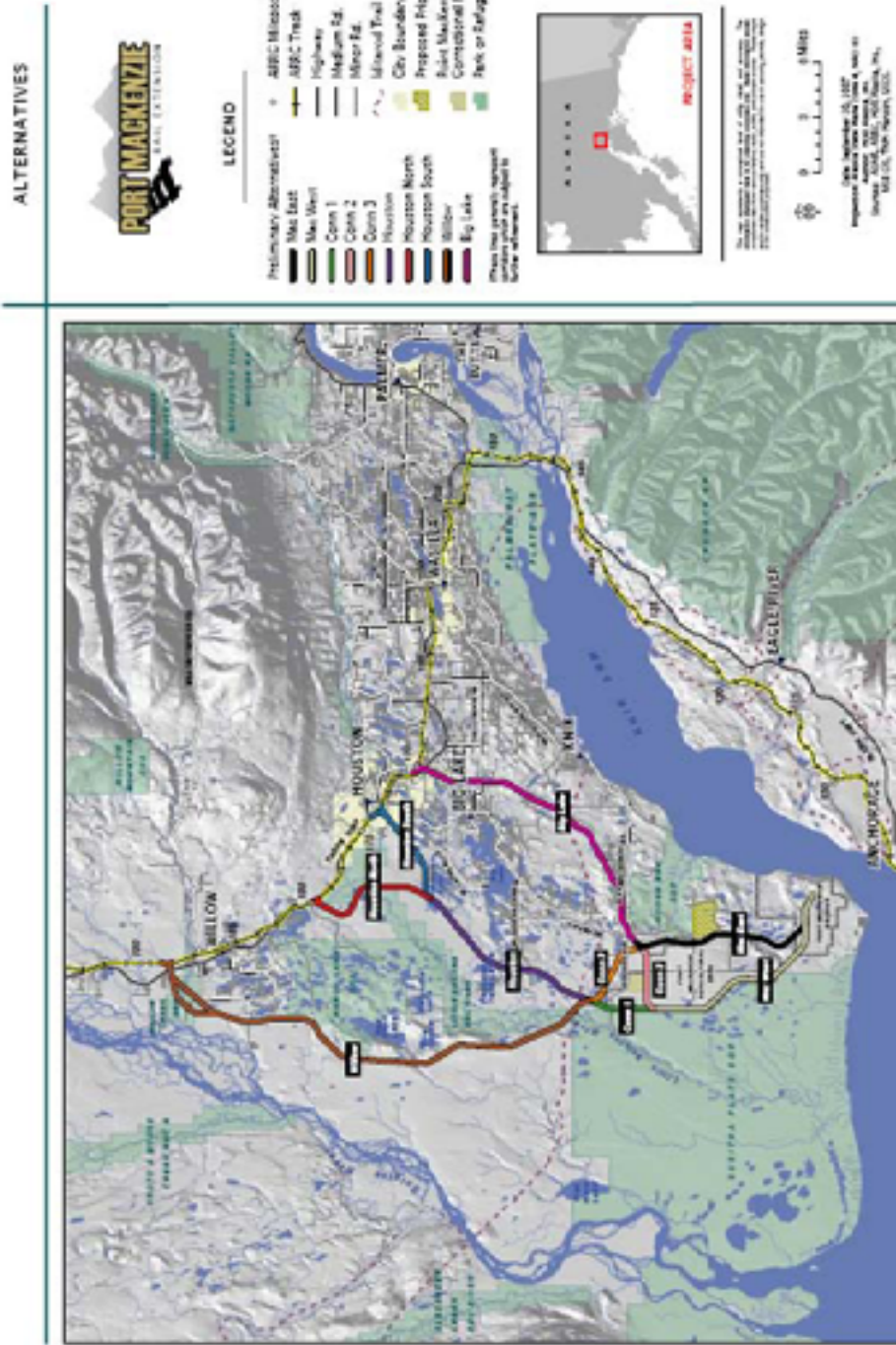
[www.portmacrail.com](http://www.portmacrail.com)

# Developing Alternatives



[www.portmacrail.com](http://www.portmacrail.com)

# Corridors Under Consideration



\*\*\* These lines represent possible corridors and are subject to change.



[www.portmacrail.com](http://www.portmacrail.com)



# STB Process

Because a new line with common carrier status triggers STB jurisdiction, the project requires a review under the National Environmental Policy Act.

## Alaska Railroad Corporation—Project Applicant

- Files requests with the STB for the construction and operation of a new rail line
- NEPA role limited to information provider

## Surface Transportation Board—Lead Federal Agency

- Final decision-making authority
- Considers application based upon market needs and the ARRC's ability to operate the new line

## Section of Environmental Analysis—a department of the STB

- Conducts NEPA process independently of ARRC
- Most likely utilizes 3rd party contractor to prepare NEPA document
- Recommends conditions for construction and operation to the STB upon favorable finding



[www.portmacrail.com](http://www.portmacrail.com)



# What will the STB Consider?

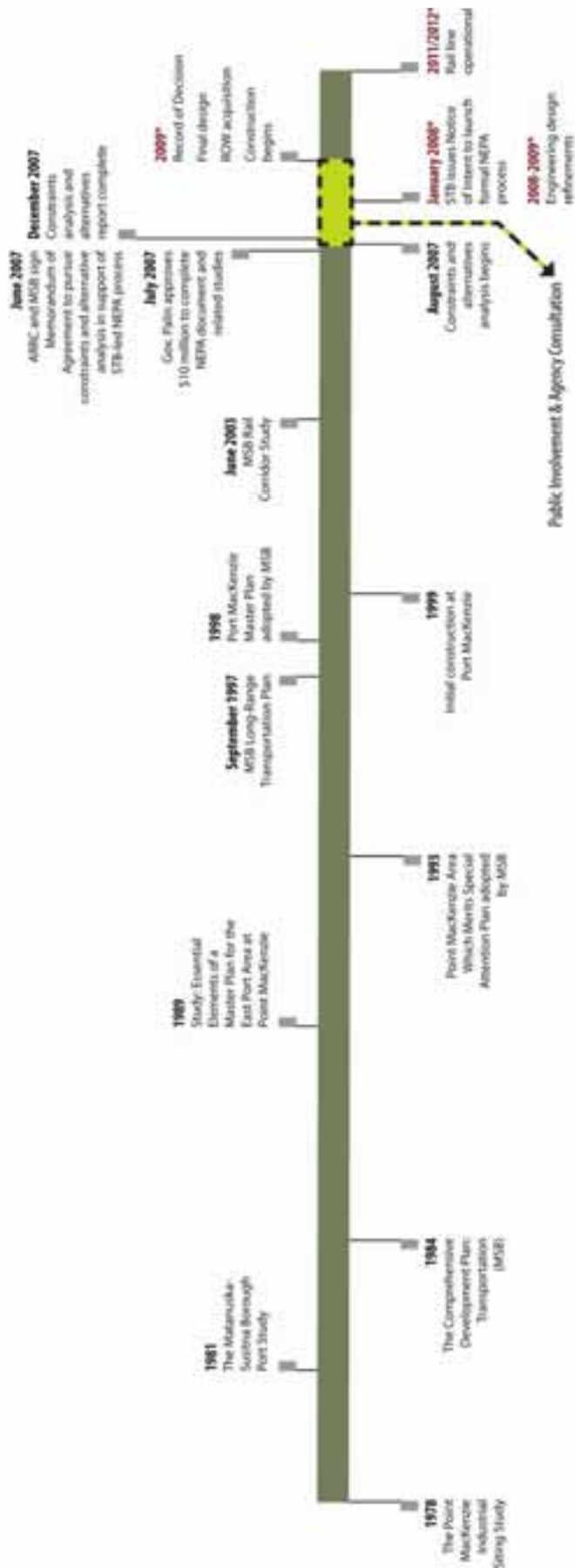
The Surface Transportation Board, at a minimum, will consider the following topics during the NEPA process and development of the environmental document:

- 
- ✓ Proposed action and alternatives
  - ✓ Cultural & Historic Resources
  - ✓ Safety
  - ✓ Regional and local transportation systems and patterns
  - ✓ Biological Resources (wildlife sanctuaries, refuges, National or State parks and forests)
  - ✓ Land use
  - ✓ Air
  - ✓ Energy
  - ✓ Water (wetlands and floodplains)
  - ✓ Noise
  - ✓ Proposed Mitigation



[www.portmacrail.com](http://www.portmacrail.com)

# Historic Milestones & Anticipated STB Schedule

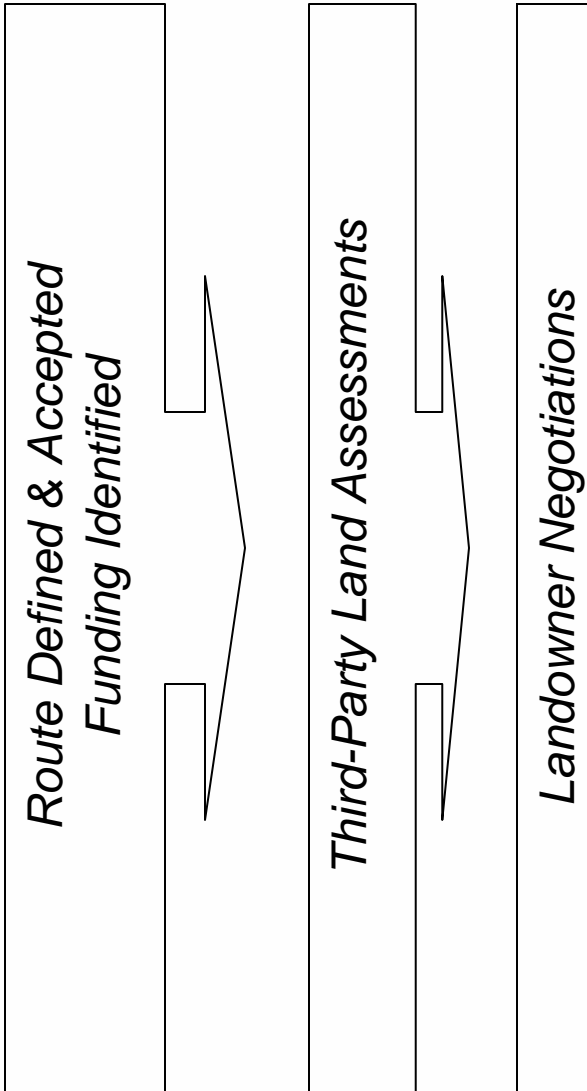


\*Anticipated dates. The schedule for the environmental document process is at the discretion of the Surface Transportation Board.



www.portmacrail.com

# Right of Way Acquisition Process



[www.portmacrail.com](http://www.portmacrail.com)

# Why Comments are important NOW!



- Provide early input as part of the alignment evaluation
- Help to identify areas of potential conflict early in the process



[www.portmacrail.com](http://www.portmacrail.com)





# For more information

## Please contact:

*Patty Sullivan, Public Affairs Director  
Matanuska-Susitna Borough  
Office of Public Affairs  
350 East Dahlia Avenue  
Palmer, Alaska 99645  
Phone: (907)745-9577  
E-mail: [patty.sullivan@matsugov.us](mailto:patty.sullivan@matsugov.us)*

*Stephanie Wheeler, Public Involvement Officer  
Alaska Railroad Corporation  
327 West Ship Creek Avenue  
Anchorage, Alaska 99501  
Phone: (907) 265-2671  
E-mail: [wheelers@akrr.com](mailto:wheelers@akrr.com)*

**Visit: [www.portmacrail.com](http://www.portmacrail.com)**

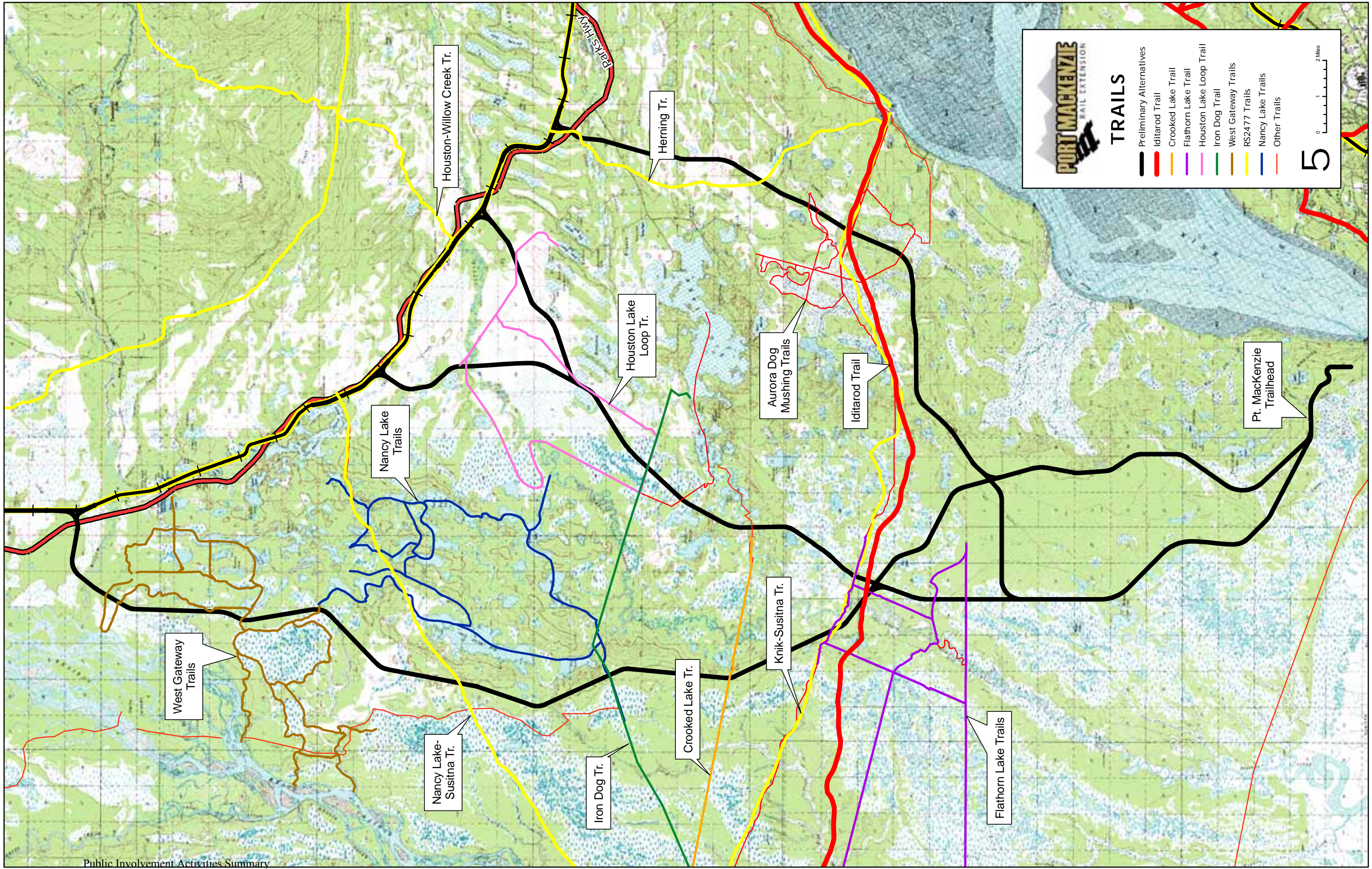


[www.portmacrail.com](http://www.portmacrail.com)

**Port MacKenzie Rail Extension  
Criteria Matrix**  
10-19-07

Criterion		1		2		3					4	5	6				7	8	9			Construction Costs
		Soils/ Geology	Faults	# of existing roads	# of new road crossings	Agricultural	Parks & Refuges	Government/ Institutional	Native Corporation	Private	Parcels with Structures	Train Energy	Lakes, ponds, and marshes	Emergent wetlands	Forested and scrub/shrub wetlands	Disturbed wetlands	Habitat	Archeological/ Historical Resources	# of agency-recognized trail crossings	# of other trail crossings	Recreation Fragmentation	
Impact Measurement		Poor or highly compressible soils (acres)				Acres					# of parcels with structures directly impacted		Acres				# of anadromous stream crossings	High probability (acres)	# of agency-recognized trail crossings	# of other trail crossings	Recreation Fragmentation	
Southern Routes	Mac West																					
	Mac East																					
Connectors	Conn 1																					
	Conn 2																					
	Conn 3																					
Northern Routes	Willow																					
	Houston																					
	Houston North																					
	Houston South																					
	Big Lake																					

*(This page intentionally left blank.)*



*(This page intentionally left blank.)*

<b>Meetings and comment format</b>	<b>Public Meetings Attendees</b>	<b>Written Comments</b>	<b>Recorder Comments</b>	<b>Map Comments</b>
October 1 – Lake Lucille Inn, Wasilla	68	4	6	18
October 2 – Big Lake Elementary, Big Lake	124	15	5	28
October 3 – Willow Area Community Organization, Willow	86	30	11	28
October 4 – Knik Elementary, Knik	45	4	4	28
October 5 – Houston Middle School, Houston	33	3	11	35
Mailed comment forms received		2		
Electronic comments received		32		
Letters received		5		
<b>Total</b>	<b>356</b>	<b>95</b>	<b>37</b>	<b>137</b>

Public Involvement on Port MacKenzie Rail Extension project  
Sullivan remarks to Assembly/Planning Commission Oct. 30

- Still gathering comment, deadline Nov. 2, for this stage, more opportunities to comment
- first week of October, one open house every night, Wasilla, Big Lake, Willow, Knik, & Houston more than 360 people attended, 95 written comments turned in, 37 people gave comments to a court reporter, 137 comments were written on large aerial maps
- a Web site portmacrail.com is dedicated to the project, so far 38 comments have come in
- sent out some 8500 postcards to landowners alerting them to the web site and the meetings
- we sent out more than 20 letters directly to certain groups, among them: cities, non-profits, community councils, trails groups. The letter offered a smaller meeting if they wanted one.
- The Willow Dog Musers Association took us up on our offer. Last week a question and answer type meeting was held in Willow. We brought along a trails map and again asked them their concerns about maintaining the connection of trails. Houston City also invited us to answer questions on the project.
- We met with large landowners individually, when proposed alignments crossed their lands, among them: Knikatu, CIRI, AK Mental Health Trust Land Office, and the University
- we met with the state and federal agencies
- we met with state Legislators and with our Port Commission
- we advertised in three papers three times, and hung fliers throughout the Borough
- for Anchorage residents who own land here, some have made it to the meetings. Anchorage residents can call for an appointment at the Alaska Railroad Headquarters in Anchorage, to come in and see maps and provide comments.
- public comment has been refining the proposed corridors
- comments in general support the rail extension as a good idea but often oppose it being in their backyard, a summary of comments will be posted on the web. We will also next month have compiled an appendix of public comments for the public to look at.
- areas of concern are noise, trails and recreation, effects on residential property, safety, effects on wildlife...finding this across all proposed alignments
- we update Planning Commission again on Nov. 5 and Assembly on Nov. 20

# Port MacKenzie Rail Extension Project

Matanuska-Susitna Borough & Alaska Railroad Corporation

November 20, 2007







# Project Participants

Alaska Railroad Corporation \_\_\_\_\_ Applicant

Matanuska-Susitna Borough \_\_\_\_\_ Project Sponsor

Surface Transportation Board \_\_\_\_\_ Lead Federal Agency

HDR Alaska, Inc. \_\_\_\_\_ Environmental Consultant

TNH-Hanson, LLC \_\_\_\_\_ Engineering Consultant

STB Section of Environmental Analysis \_\_\_\_\_ NEPA Compliance

To be determined \_\_\_\_\_ 3<sup>rd</sup> Party NEPA Contractor



# Project Milestones

June 29	Initial MOA between MSB and ARRC
August 7	TNH-Hanson and HDR retained by ARRC to assist in the STB process
Sept. 18	Met with DNR large projects and permitting agencies, and have had numerous individual meetings since
Sept. 24-28	Met with <ul style="list-style-type: none"><li>• Mental Health Trust</li><li>• University Land Office</li><li>• Knikatnu Corporation</li><li>• CIRI Corporation</li></ul>
Oct. 1-5	Project Open Houses



# Project Milestones (cont.)



- Oct. 22 Retained legal counsel in Washington DC to represent MSB and ARRC before the STB
- Oct. 24 Met with Iditarod Race Commissioner
- Oct. 24 Met with Willow Dog Musher's Association & Snow Machiners
- Oct. 24 Briefed MSB Transportation Advisory Board
- Oct. 25 Met with Houston City Council
- Oct. 30 Briefed joint MSB Planning Commission and Assembly
- Oct. 31 Introduced project before SEA (STB)
- Nov. 5 Provided information to the Planning Commission
- Nov. 5 Provided project update to Port Commission
- Nov. 6 Issued RFP for 3<sup>rd</sup> party NEPA contractor



# How have we gathered the information?

- Reviewed existing plans and studies
- Conducted public involvement activities
- Met with small stakeholder groups
- Met with large landowners
- Conducted meetings with resource agencies
- Open house meetings



# Open Houses

- Wasilla
- Big Lake
- Willow (followed by 3 separate meetings)
- Houston (followed by 2 separate meetings)
- Knik



# Results of Public Involvement Activities

Public Comment Statistics  
(as of November 2, 2007)



Meetings and Comment Format	Public Meeting Attendees	Written Comments	Recorder Comments	Map Comments
Oct. 1 – Wasilla	68	4	6	18
Oct. 2 – Big Lake	124	15	5	28
Oct. 3 – Willow	86	30	11	28
Oct. 4 – Knik	45	4	4	28
Oct. 5 – Houston	33	3	11	35
Mailed Comments		2		
Electronic Comments		32		
Letters		5		
<b>Total</b>	<b>356</b>	<b>95</b>	<b>37</b>	<b>137</b>



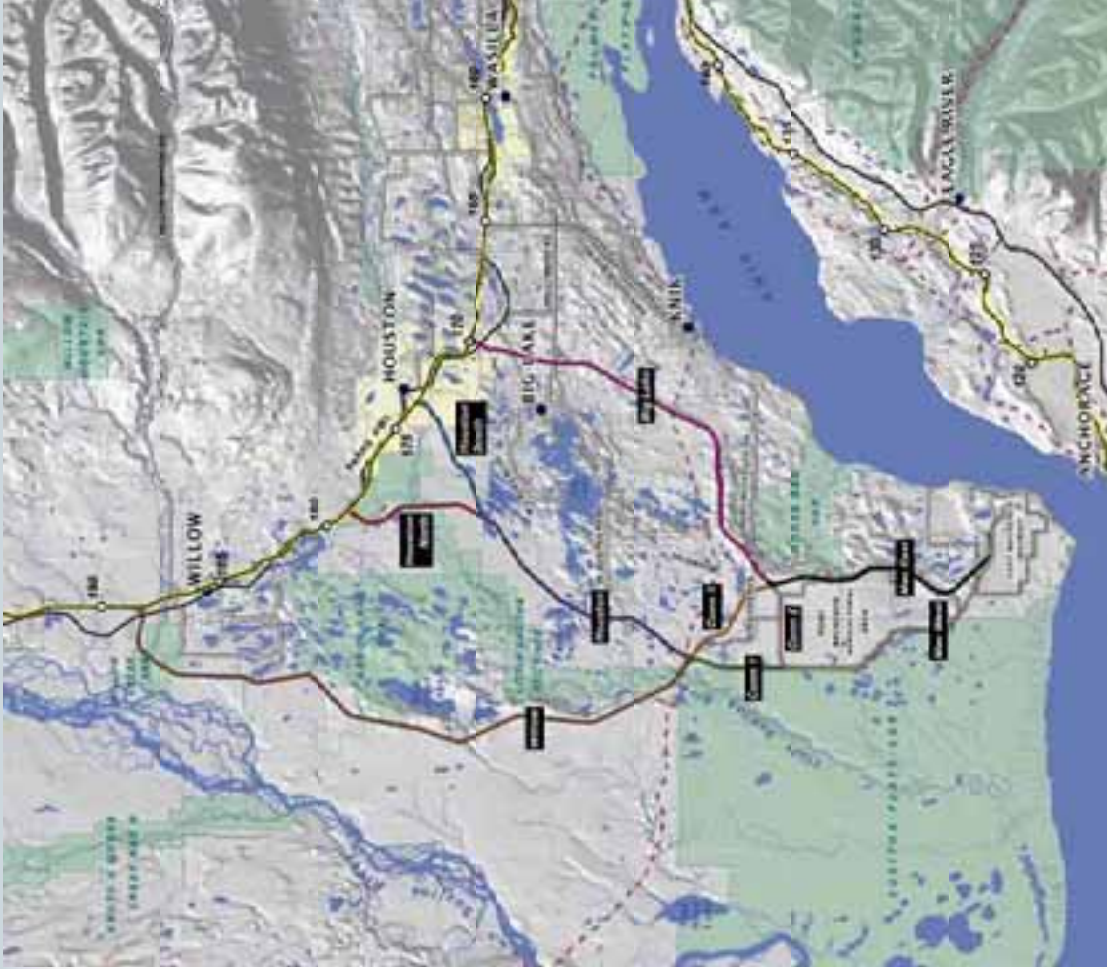
## Sample of what we heard...

- The rail extension would provide future business and employment opportunities.
- Choose a route that is logistical, economic, and best for communities who live near the rail belt.
- Concerns for wetlands, wildlife habitat, and agricultural lands.
- Recreation and trails are important to the area and the communities.





# Corridors Under Consideration



**Note: These lines represent possible corridors and are subject to change. A 3rd party contractor may arrive at additional routes as part of EIS process.**





# Evaluation Matrix

REVISED 12/6/07

A-264

PROPOSED ROUTES	CRITERION 1, 2									Preliminary Cost Estimate (millions) \$	
	1	2	3	4	5	6	7	8	9		
Mac West - Willow	0										\$320
Mac West - Houston											\$250
Mac West - So.									+	0	\$220
Mac West - Big Lake							0			0	\$290
Mac East - Willow		+		+			+				\$330
Mac East - Houston North	0	0	0	+	+	+	0	0	+		\$260
Mac East - Houston South	0	0	+	+	+	0	+	0	+	+	\$230
Mac East - Big Lake	+	-	0	-	+	-	+	-	-	+	\$285

**UPDATE!**  
 The Criteria Matrix has been updated since the Nov. 20, 2007, presentation to the Matanuska-Susitna Borough Assembly.  
 Go to: [http://www.portmacrail.com/documents/matrix\\_12-18-07.pdf](http://www.portmacrail.com/documents/matrix_12-18-07.pdf)

(+) Positive; (0) Neutral; (-) Negative  
 Criteria not weighted / Routes are unranked  
 Large parcels of undeveloped land owned by State of Alaska MSB, University of Alaska, Mental Health Trust, and Alaska Native corporations  
 Lands that are designated for parks, refuges, residential, or agricultural uses  
 Costs do not include approximately \$10 million for loop track construction within the port (all alternatives)

- 4.
- 5.

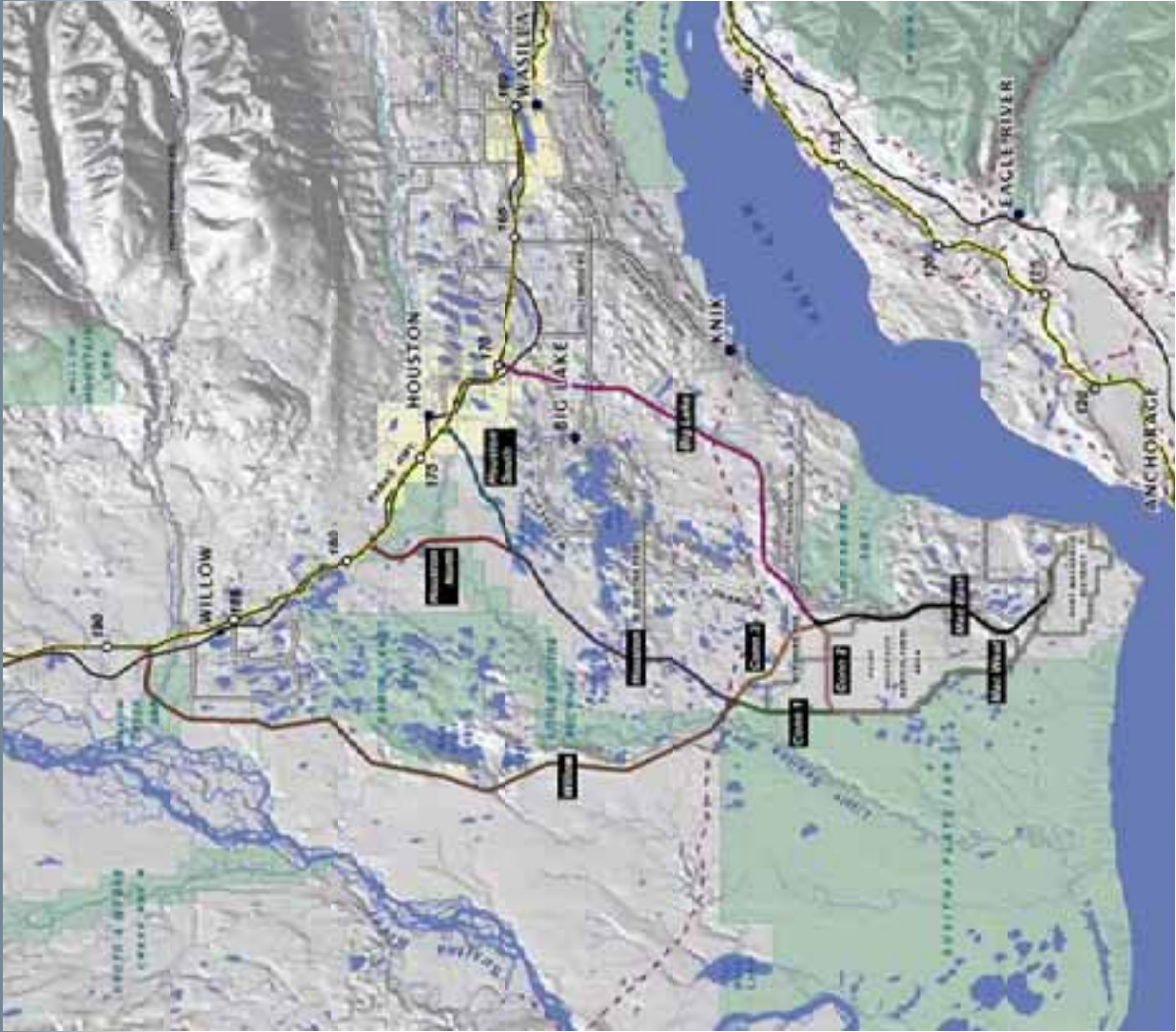


# Now What?



- Railroad files application with STB around end of year
- SEA (STB) selects 3<sup>rd</sup> party contractor beginning 2008
- SEA prepares environmental document through 2008 and beginning of 2009
- STB issues decision (est. mid 2009)





[www.portmacrail.com](http://www.portmacrail.com)

**PORT MACKENZIE**  
RAIL EXTENSION



**MATANUSKA-SUSITNA  
BOROUGH**


**Borough Manager**

350 East Dahlia Avenue □ Palmer, AK 99645

Phone (907) 745-9688 □ Fax (907) 745-9669

[www.matsugov.us](http://www.matsugov.us)

**MEMORANDUM**

**Date:** December 7, 2007  
**To:** Matanuska-Susitna Borough Assembly  
**From:** John Duffy, Borough Manager   
**Subject:** Port MacKenzie Rail Extension

---

Attached for your information is a detailed description of the matrix, which was presented to you at the Assembly meeting on Nov. 20, 2007.

This matrix provides the technical engineering and environmental measurable data for each alternative.

The attached information includes a detailed discussion of the criteria evaluation, the strengths & weaknesses of each alternative, and the final matrix evaluation spreadsheet. This spreadsheet provides numerical justification and is the backup document for the (+), (0), (-) matrix.

At the special meeting on Dec. 11, 2007, the project team will be available to answer questions about the project.

Thank you.

JD:ds

*(This page intentionally left blank.)*



# Port MacKenzie Rail Extension Project Alternatives Comparison

The Matanuska-Susitna Borough (MSB) and the Alaska Railroad Corporation (ARRC) have developed a range of alternatives for the Port MacKenzie Rail Extension project based on the purpose and need for the project, engineering requirements, and environmental considerations. A constraints analysis was performed to develop preliminary corridors, the alignments that resulted from the constraints analysis were evaluated based on preliminary environmental information, and quantitative data available for the project area were used to compare the alternatives relative to one another. This preliminary report summarizes the comparison of the alternatives based upon objective factors, and subsequently presented in matrix form.

The purpose of the matrix is to provide an expeditious representation using quantitative measures to compare the strengths and weaknesses of the Port MacKenzie Rail Extension Project alternatives. For planning and policy analyses, a *Goeller scorecard*, called a matrix in this report, is useful as it utilizes a disaggregate method to display and compare the strengths and weaknesses of the alternatives (Patton and Sawicki, 1986). This method describes the impacts for each alternative in “natural” units, such as physical units, monetary terms, or other quantifiable terms, and then allows decision-makers to assign a score by which to compare the proposed alternatives. Each column of the matrix represents one impact and each row represents a proposed alternative. The result is a final analysis summary matrix used to present the various impacts.

## 1.0 Criteria Identification

The matrix evaluation criteria used in this analysis were derived from STB criteria (49 CFR 1105.7) and environmental and engineering criteria specific to the project area. Criteria were selected based on (1) availability of a quantifiable measure and (2) differences in measurements for alternatives that allowed for comparison. Quantifiable criteria that resulted in similar results for all alternatives were removed from the analysis as they would not be useful in differentiating the alternatives. The criteria used in the matrix to compare alternatives are described in Table 1. A weighting of the various criteria was avoided to ensure that each criterion was considered equally.

The ARRC is seeking to acquire a 200-foot ROW along the entire length of the proposed alignment. The ARRC would build the rail and all other associated infrastructure within this ROW. Based on this specification, a 200-foot wide buffer along the proposed alignments was used to calculate the areal impacts to the various evaluation criterions using the GIS.

Each criterion was evaluated separately. Alternatives were assigned a plus (+), neutral (0), or minus (-) for each criterion. This was determined by assigning the least impacting alternative(s) a plus and the most impacting alternative(s) were assigned a minus. Alternatives falling in the middle were then assigned a neutral value. The numbers of pluses, neutrals and minuses were totaled for each alternative, and the number of minuses was subtracted from the number of pluses to compare the overall strengths and weaknesses of the alternatives. The number of neutral criteria was not calculated into the final comparison number. Matrix measurements and evaluations are presented in Table 2.



The environmental and engineering opportunities and constraints were compiled by alternative into the final matrix (Table 2). The cells of each criterion column contain two results; the actual unit value of each category as well as the overall scoring result. The final score of each alternative is represented in the ‘total’ column. Construction costs of each alternative were not a part of the final score, although they are included in the matrix as a separate column.

**Table 1: Description of criteria used in the matrix**

Matrix Criterion	Description
Poorly or highly compressible soils (cubic yards)	Poor soils are often located in wetlands, may include hydrology considerations, and can complicate constructability. Geotechnical fieldwork, including subsurface probing that evaluated all soil types, including frozen material and peat which is highly compressible, was conducted for the proposed routes. Based on a 200-foot-wide ROW, the amounts (cubic yards) of poor or highly compressible soils were calculated by alternative and compared.
Number of new road crossings	Crossing roads, with either a grade separated bridge crossing or an at-grade crossing add cost to the project. At-grade crossings also may impact traffic patterns on existing roads, increase noise levels at development near road intersections, and affect local vehicular transit times. Additionally, grade crossings require driver attentiveness and ongoing maintenance. As a measure of each of these potential impacts, the number of new road crossings was tallied for each alternative and compared.
Land availability (acres/mile)	The project area is comprised of privately owned parcels; local and state owned designated parcels (parks, refuges, and other public uses); and large tracts of undeveloped or undesignated parcels owned by government (State of Alaska and MSB), Native Corporations, Trust Land Office, and University of Alaska. This category includes only these large tracts of undeveloped and undesignated land as they represent this category and are land types that are the most compatible for locating a rail line and minimize impacts to more developed areas and people. Because the lengths of each alternative varies, the acreages of undeveloped lands within a 200-foot-wide ROW were calculated for each alternative and divided by the length of that alternative. More acres per mile in this category were considered positive.
Number of developed parcels	Among the privately owned parcels in the project area, many are currently undeveloped. To evaluate the greater impact to human development, the MSB GIS parcel database was queried for parcels containing an appraised building value. The number of parcels crossed by each alternative that contained structures was counted. A higher number of developed parcels along a route represent impacts to more densely populated areas, individual property owners, and community cohesion.

**(Table 1 Continued)**

<b>Matrix Criterion</b>	<b>Description</b>
Designated land use (acres)	Designated lands include state and MSB lands that are designated as refuges, recreation areas, and parks; state or private lands established for agricultural uses, and private lands designated as residential areas. MSB and state GIS land use databases were examined and the acres of designated lands within each 200-foot-wide ROW were estimated and compared.
Train energy (horsepower-hours)	The movement of trains is resisted by forces related to distance, grades, and curvature. The amount of energy needed for a standardized train to overcome these forces was calculated for each alternative and compared.
Wetlands (acres)	The NWI dataset was examined. Wetland types within the project area are highly diverse and have varying levels of functionality and overall value. For this analysis, all wetland types were combined. Wetland impacts were avoided and minimized to the greatest extent possible. The total wetland impacts were calculated for each alternative and compared.
Number of mapped anadromous fish stream crossings	Crossing anadromous streams could impact salmon populations, habitat, and/or sport-fishing. State regulations require that culverts and bridges with in-water support structures be designed to allow for fish passage. The ADF&G catalog of anadromous fish streams was used to tally and compare the number of stream crossings by each alternative
High potential for archeological sites (acres)	Areas of cultural and historic significance were avoided where practicable. An archeological probability map provided by MSB cultural resource specialists was used to calculate the acres of potential impact to areas with highest probability of having cultural resources along each alternative.
Fragmentation of designated refuge and recreation areas (Yes/No)	State lands designated as recreation areas and refuges represent areas of high quality wildlife habitat and areas of significant recreational opportunity for fishing, boating, hunting and other outdoor activities. Crossing or encroaching on the boundary of these areas is seen as fragmentation. An alternative that encroached on the boundary of one or more recreation area or refuge was given a 'yes' for fragmentation, while alternatives that had no impact on these areas were given a 'no'. In the scoring, alternatives with no encroachment were assigned a plus (+), alternatives that bisected one or more of these areas were assigned a minus (-), and alternatives that encroached a border but did not bisect the property were assigned as neutral (0).
Construction costs	Preliminary total construction cost estimates, including ROW acquisition, were compiled and used to compare each of the alternatives. Construction costs were not included as part of the overall comparison of strengths and weaknesses.



*(This page intentionally left blank.)*



Table 2: Final matrix evaluation

Proposed Routes	Criterion										+ / 0 / -	Total	Cost Estimate (millions)	
	1	2	3	4	5	6	7	8	9	10				
	Poor or highly compressible soils (cubic yards in 10 <sup>6</sup> yds <sup>3</sup> )	New road crossings (#)	Land availability (acres/mile)	Developed parcels (#)	Designated land use (acres)	Train energy (horsepower-hours)	Wetlands (acres)	Mapped anadromous fish streams (#)	High potential for archeological sites (acres)	Fragmentation of designated refuges and recreation areas (yes/no)				
Mac West - Willow	1.15	5	15.8	15	440	8,100	200	5	585	YES	1/6/3	-2	\$320	-
	0	+	0	0	-	0	0	0	-	-				
Mac West - Houston North	1.73	4	14.6	13	440	7,300	350	7	225	YES	4/1/5	-1	\$250	0
	-	+	0	+	-	+	-	-	+	-				
Mac West - Houston South	1.19	5	16.3	13	415	8,100	280	6	280	NO	3/5/2	+1	\$220	+
	0	+	0	+	-	0	-	0	+	0				
Mac West - Big Lake	0.66	11	11.3	35	320	9,800	220	7	530	NO	1/2/7	-6	\$290	0
	+	-	-	-	-	-	0	-	-	0				
Mac East - Willow	1.05	7	18.9	7	150	8,700	90	4	580	YES	4/4/2	+2	\$330	-
	0	0	0	+	+	0	+	+	-	-				
Mac East - Houston North	1.62	6	18.7	5	145	7,900	240	6	225	YES	4/4/2	+2	\$260	0
	-	0	0	+	+	+	0	0	+	-				
Mac East - Houston South	1.08	7	20.4	5	130	8,700	175	5	280	NO	6/4/0	+6	\$230	+
	0	0	+	+	+	0	+	0	+	+				
Mac East - Big Lake	0.56	11	15.4	26	120	9,100	160	7	540	NO	4/1/5	-1	\$285	0
	+	-	0	-	+	-	+	-	-	+				

*(This page intentionally left blank.)*



## 2.0 Topics Not Covered in Matrix

Several topics were not directly addressed in the evaluation matrix. As previously mentioned, some topics were not applicable to the project, some were difficult to quantify, and others were so similar once quantified that they did not contribute to the comparison. The criteria not included in the matrix are described in Table 3.

**Table 3: Criteria not included in the evaluation matrix**

Criterion removed	Description
Air Quality	There are no non-attainment areas in the MSB and therefore in the project area (USEPA 2007). Air quality is generally reported to be good. Each of the alternatives is a similar length and would result in similar air emissions from train traffic. None of the alternatives are likely to increase air emissions to a level of non-attainment.
Noise	Because most train noise is generated at crossings, alternatives that require more new at-grade crossings may have a greater noise impact. Noise was not included as a stand-alone criterion in the matrix as it seems to duplicate the "number of new road crossings" criterion.
Safety	Safety is an issue for all alternatives, and is generally incorporated in the "number of new road crossings" criterion. While some alternatives would have more at-grade road crossings than others, all alternatives cross recreational trails that may pose a safety concern. All proposed alternatives would have equal potential impacts from the frequency or probability of a release to the areas from hazardous materials. Hazardous spills contingency plans would be modified by ARRC to include the new alignment. Because safety concerns are essentially equal among each of the alternatives, this issue was not a discriminating factor for comparing the alternatives.
Endangered Species	There are no threatened or endangered species present within the project area.
Coastal Zone	All proposed alternatives would include construction in Alaska's coastal zone and would be subject to consistency review under the ACMP and the MSB CMP including the Point MacKenzie Area Which Merits Special Attention (AMSA) Plan (as amended in 2006). Therefore, coastal zone impacts were not a discriminating factor for comparing the alternatives.
Operations and Maintenance	Operations and maintenance requirements would be similar for all alternatives and are not a discriminating factor for comparing the alternatives.



(Table 3 Continued)

Criterion removed	Description
Public Comment	Public comment was not included in the matrix evaluation because it is difficult to measure public comment in quantifiable terms. However, public comment was important for developing the alternatives and for understanding issues specific to the project area. Public comments were considered and helped to refine the proposed corridors and influenced preliminary design elements.
Socio-economics	The potential socio-economic impacts of the project are an important consideration. This criterion, however, is complex and is difficult to quantify and compare the impacts between the alternatives in a matrix format. U.S. Census Bureau data from 2000 were analyzed to evaluate project area demographics, housing, and the economy.
Habitat Fragmentation	Most of the project area provides quality habitat for a wide range of wildlife species because of the undeveloped to moderately developed nature of the project area. For this reason it was difficult to quantify wildlife habitat fragmentation and impact. This criterion was included in the fragmentation of a park or refuge category.
Trail Crossings	The project area has a multitude of trails, both officially recognized through easements and unofficial 'social' trails. Extra effort was taken to identify trail locations and to maintain trail connectivity. The number of trail crossings for each alternative was originally included in the analysis, but was dropped because the numbers of officially recognized trails were similar for all alternatives and the numbers of social trails are not easily quantifiable.



### 3.0 Strengths and Weaknesses

Based on the results of the matrix, the various alternatives were described by their strengths and weaknesses (Table 4). This was done to provide an easy to comprehend list, without numbers, of the pros and cons of each alternative.

**Table 4: Alternatives strengths and weaknesses**

Route	Strengths	Weaknesses
Mac West/ Willow	<ul style="list-style-type: none"> <li>• Requires fewer number of new road crossings</li> </ul>	<ul style="list-style-type: none"> <li>• Crosses more incompatible land uses</li> <li>• Has higher probability of impacting archeological and/or historical sites</li> <li>• Has greatest impacts to designated state refuges and recreation areas</li> </ul>
Mac West/ Houston North	<ul style="list-style-type: none"> <li>• Requires fewest number of new road crossings</li> <li>• Impacts fewer developed parcels</li> <li>• Requires smaller expenditure of train energy</li> <li>• Has less probability of impacting archeological and/or historical sites</li> </ul>	<ul style="list-style-type: none"> <li>• Crosses more incompatible land uses</li> <li>• Has less suitable soil conditions</li> <li>• Impacts more wetlands</li> <li>• Impacts a designated refuge and fragments a state recreation area</li> <li>• Crosses greater number of mapped anadromous streams</li> </ul>
Mac West/ Houston South	<ul style="list-style-type: none"> <li>• Requires fewer number of new road crossings</li> <li>• Impacts fewer developed parcels</li> <li>• Has less probability of impacting archeological and/or historical sites</li> </ul>	<ul style="list-style-type: none"> <li>• Crosses more incompatible land uses</li> <li>• Impacts more wetlands</li> </ul>
Mac West/ Big Lake	<ul style="list-style-type: none"> <li>• Has more suitable soil conditions</li> </ul>	<ul style="list-style-type: none"> <li>• Involves greater number of new road crossings</li> <li>• Impacts more developed parcels</li> <li>• Crosses more incompatible land uses</li> <li>• Requires greater expenditure of train energy</li> <li>• Crosses greater number of mapped anadromous streams</li> <li>• Has higher probability of impacting archeological and/or historical sites</li> </ul>
Mac East/ Willow	<ul style="list-style-type: none"> <li>• Impacts fewer developed parcels</li> <li>• Crosses more compatible land uses</li> <li>• Impact less wetlands</li> <li>• Crosses fewest mapped anadromous streams</li> </ul>	<ul style="list-style-type: none"> <li>• Has higher probability of impacting archeological and/or historical sites</li> <li>• Fragments designated state recreation areas</li> </ul>



**Table 4 (Continued)**

<b>Route</b>	<b>Strengths</b>	<b>Weaknesses</b>
Mac East/ Houston North	<ul style="list-style-type: none"> <li>• Crosses more compatible land uses</li> <li>• Impacts fewer developed parcels</li> <li>• Requires smaller expenditure of train energy</li> <li>• Less probability of impacting archeological and/or historical sites</li> </ul>	<ul style="list-style-type: none"> <li>• Has less suitable soil conditions</li> <li>• Fragments a designated state recreation area</li> </ul>
Mac East/ Houston South	<ul style="list-style-type: none"> <li>• Impacts fewer developed parcels</li> <li>• Crosses more compatible land uses</li> <li>• Impacts less wetlands</li> <li>• Has less probability of impacting archeological and/or historical sites</li> <li>• Avoids designated state refuges and recreation areas</li> </ul>	<ul style="list-style-type: none"> <li>• Requires moderate expenditure of train energy, but less than the two Big Lake alternatives</li> <li>• Involves moderate number of new road crossings, but less than the two Big Lake alternatives</li> <li>• Has less suitable soil conditions than the Big Lake alternatives, but better than the Houston North alternatives</li> </ul>
Mac East/Big Lake	<ul style="list-style-type: none"> <li>• Avoids designated state refuges and state recreation areas</li> <li>• Crosses more compatible land uses</li> <li>• Has more suitable soils</li> <li>• Impacts less wetlands</li> </ul>	<ul style="list-style-type: none"> <li>• Impacts more developed parcels</li> <li>• Involves greater number of new road crossings</li> <li>• Requires greater expenditure of train energy</li> <li>• Crosses greater number of mapped anadromous streams</li> <li>• Has higher probability of impacting archeological and/or historical sites.</li> </ul>

## Surface Transportation Board (STB), Section of Environmental Analysis (SEA) Environmental Impact Statement (EIS) Considerations

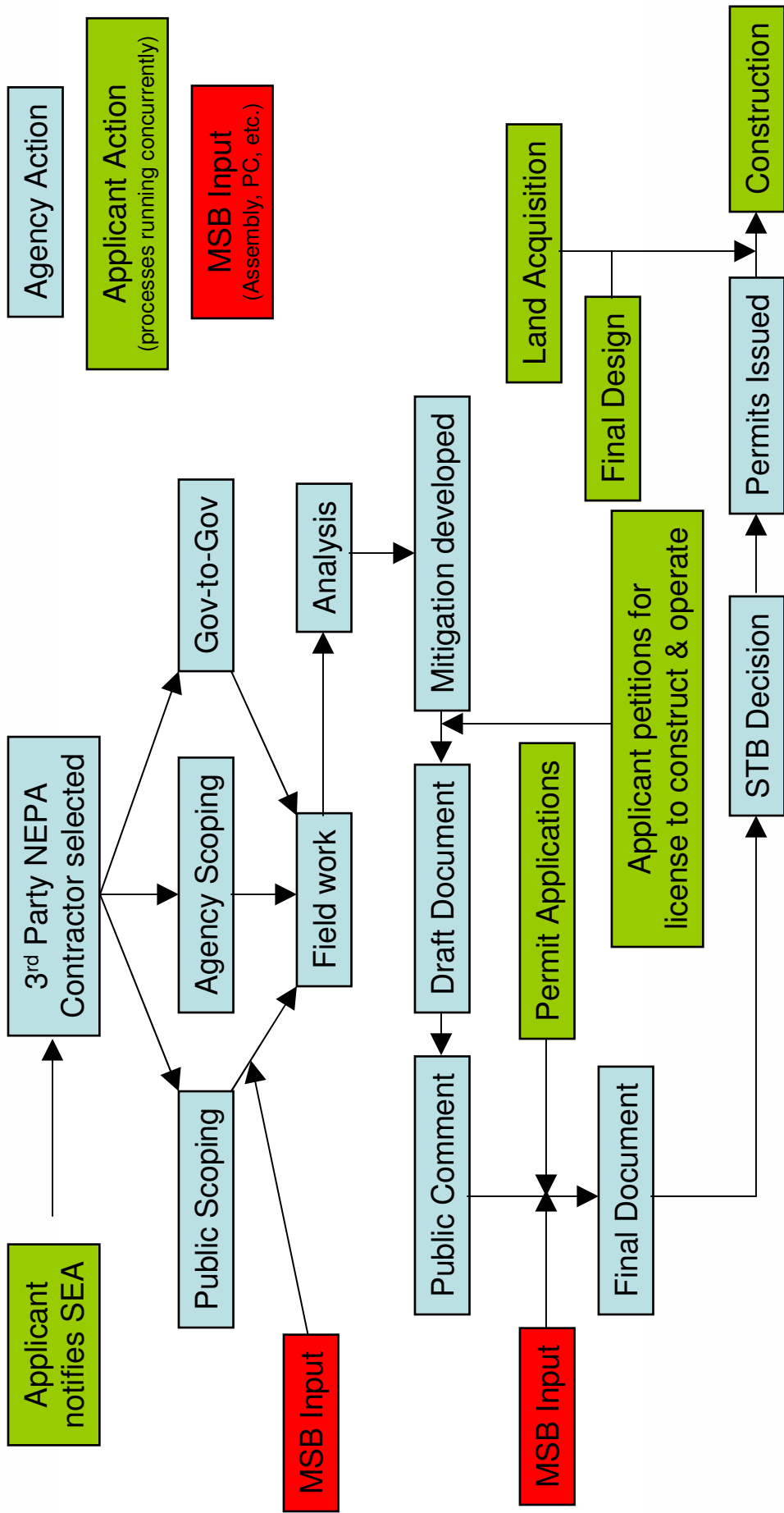
According to NEPA, the STB must examine the environmental impacts of any action subject to their jurisdiction. 49 CFR 1105.7 *Environmental Reports* requires that an applicant to the STB for an action must submit an Environmental Report on the proposed action prior to or concurrent with their application. The Environmental Report must contain the following:

- (1) **Proposed action and alternatives.** Describe the proposed action and any reasonable alternatives to the proposed action. Include a readable detailed map and drawings clearly delineating the project.
- (2) **Transportation system.** Describe the effects of the proposed action on regional or local transportation systems and patterns. Estimate the amount of traffic that will be diverted.
- (3) **Land use.** Describe the proposed action's consistency or inconsistency with local and regional plans including agricultural land and coastal zones.
- (4) **Energy.** Describe the effect of the proposed action on energy resources. Consider recyclable commodities, overall energy efficiencies, and change in energy consumption.
- (5) **Air.** Determine if the proposed action will result in an increase in rail traffic, rail yard activity, or truck traffic, and quantify the anticipated effect on air emissions, and whether these increases are within the parameters of the State Implementation Plan. Describe any ozone depleting materials that may be transported, safety practices, the applicant's safety record, and contingency plans.
- (6) **Noise.** If any thresholds for air quality are surpassed, state whether the proposed action will cause an increase in noise levels. If the increase is beyond noise thresholds, identify noise receptors in the project area and quantify their noise increases.
- (7) **Safety.** Describe effects on public health and safety. If hazardous material may be transported, identify them, safety practices, the applicant's safety record, and contingency plans. Identify the location of any known hazardous waste sites or spill sites and their contents.
- (8) **Biological Resources.** State effects of the proposed action on endangered and threatened species or critical habitat. List any effects on wildlife sanctuaries and refuges, National or State parks and forests.
- (9) **Water.** Determine whether action is consistent with Federal, State, and local water quality standards, whether section 402 or 404 Clean Water Act permits are required, and whether action will affect any designated wetlands or 100-year flood plains.



- (10) **Proposed Mitigation:** Describe actions that are proposed to mitigate adverse environmental impacts.
- (11) **Additional required information specific to rail constructions includes:**
  - a. Describe proposed routes in plan view by State, country, and subdivision, showing relationship to existing transportation network.
  - b. Describe alternative routes considered and why they were not selected.
  - c. Describe construction plans and their effect on the human environment, labor force requirements, location of borrow pits, and earthwork estimates.
  - d. Describe the rail operations to be conducted on the line including freight estimates, daily number of train movements, number of cars per train, types of cars, motive power requirements, proposed speeds, labor force, and proposed maintenance-of-way.
  - e. If energy, noise, or air impact thresholds are met, describe the effects of new and diverted traffic.
  - f. Describe effects of essential public services, public roads, and adjoining properties. Discuss societal impacts including employment changes during and after construction.
  - g. Additional information specifically requested by the STB.

# STB Process



www.portmacrail.com

# MEDIA COVERAGE

*(This page intentionally left blank.)*



## **NEWS RELEASE**

**CONTACT: PATTY SULLIVAN, (907) 745-9577**

**SEPTEMBER 14, 2007**

### **Public invited to 5 Open House events to review and comment on Port MacKenzie Rail Extension Project**

MAT-SU BOROUGH, Alaska – The Matanuska-Susitna Borough (MSB) and the Alaska Railroad Corporation (ARRC) will host five Port MacKenzie Rail Extension Project public open house events during the first week of October in Wasilla, Big Lake, Houston, Willow and Knik. The purpose is to inform citizens about the joint effort to plan, engineer, design and construct a 30- to 45-mile rail line from Port Mackenzie, connecting to the existing rail system at a point between Meadow Lakes and north of Willow. Project team members will be on-hand to discuss route proposals and to answer questions from interested citizens.

All events are 6:00 to 8:00 p.m., and open-house style to facilitate one-on-one discussion with project team members. Each event will also include a project overview presentation beginning at 7:00 p.m. The schedule of project open houses is:

- **WASILLA, OCTOBER 1** Lake Lucille Inn, 1300 W. Lake Lucille Drive
- **BIG LAKE, OCTOBER 2** Big Lake Elementary School, 3808 S. Big Lake Road
- **WILLOW, OCTOBER 3** Willow Area Community Center, Mile 69.5 Parks Hwy
- **KNIK, OCTOBER 4** Knik Elementary School, 6350 W. Hollywood Drive
- **HOUSTON, OCTOBER 5** Houston Middle School, 12801 W. Hawk Lane

In June 2007, MSB and ARRC signed a Memorandum of Agreement to jointly pursue the Port MacKenzie Rail Extension Project. Meanwhile, the State of Alaska appropriated \$10 million to fund an environmental study. Construction of a new rail line requires federal approval. The Surface Transportation Board (STB) is the federal agency responsible for completing the environmental document and approving a new route.

**-more-**

After nearly three decades of study, the Port MacKenzie Rail Extension project is now poised to move into the environmental documentation phase that precedes final design and construction. The most recent rail route study was commissioned by the Mat-Su Borough in 2003, and it recommended connecting to the Alaska Railroad near Willow.

The Borough has been the state's fastest growing community for the past 16 years. This growth, along with new technical data, warrant a fresh look at the strongest alignment options. As a progression of the preliminary engineering effort, the project team is considering the 2003 recommended route, as well as two to three additional routes.

Between now and December 2007, the project team will gather public comment, and conduct fact-finding and preliminary engineering activities prior to submitting a project application to the STB in January 2008. While conducting the environmental study process, the STB may modify and/or eliminate routes and could possibly select a route not yet considered. The STB will host public scoping meetings and accept public comment throughout the process, providing additional opportunity for public input. Ultimately, the final route will be selected at the STB's discretion at the completion of the environmental study and document process.

The final design and construction of a rail extension to Port MacKenzie is estimated to cost \$300 million (2007 dollars). The Matanuska-Susitna Borough and the Alaska Railroad will jointly conduct a financing study to assess options for funding the project.

For more information, the public can visit the project web site at [www.portmacrail.com](http://www.portmacrail.com); or contact Borough Public Affairs Director Patty Sullivan at 745-9577 and [Patty.Sullivan@matsugov.us](mailto:Patty.Sullivan@matsugov.us); or contact Railroad Public Involvement Officer Stephenie Wheeler at 265-2671 and [wheelers@akrr.com](mailto:wheelers@akrr.com).

###



## ***RADIO RELEASE***

FOR IMMEDIATE RELEASE  
SEPTEMBER 14, 2007

CONTACT: PATTY SULLIVAN, (907) 745-9577

### **Public invited to 5 Open House events to review and comment on Port MacKenzie Rail Extension Project**

MAT-SU BOROUGH, Alaska – The Matanuska-Susitna Borough and the Alaska Railroad invite the public to an open house featuring the Port MacKenzie Rail Extension Project. Open house events will be held the first week of October in Wasilla, Big Lake, Houston, Willow and Knik. Citizens can learn about the borough and railroad joint effort to construct a rail line connecting Port Mackenzie to the existing rail system at a point between Meadow Lakes and Willow. Project team members will be on-hand to discuss route proposals and to answer questions from interested citizens. All open houses are 6:00 to 8:00 p.m., with a project presentation at 7:00 p.m. Project Open House events are scheduled in:

- **WASILLA, OCTOBER 1** at Lake Lucille Inn
- **BIG LAKE, OCTOBER 2** at Big Lake Elementary School
- **WILLOW, OCTOBER 3** at the Willow Area Community Center
- **KNIK, OCTOBER 4** at Knik Elementary School
- **HOUSTON, OCTOBER 5** at Houston Middle School

For more information, visit [www.portmacrail.com](http://www.portmacrail.com), or call Borough Public Affairs Director Patty Sullivan at 745-9577. That's [www.portmacrail.com](http://www.portmacrail.com) or 745-9577.

###

*(This page intentionally left blank.)*





## ***NEWS RELEASE***

**CONTACT: PATTY SULLIVAN, (907) 746-9577**

**Oct. 2, 2007**

### **Wasilla open house starts off series of meetings**

WASILLA – Some 70 residents and landowners attended the first Open House for the Port MacKenzie Rail Extension Project Monday night in Wasilla at the Lake Lucille/Best Western Inn. Participants looked over exhibit posters, asked questions, and wrote concerns on large aerial maps. They came from beyond Wasilla, from Point MacKenzie, from Horseshoe Lake, from Willow, as well as landowners from Cow Lake, among others.

The public comments at the Open Houses will help in finding the best corridor for the new rail link.

The approximately \$300 million rail extension project is a joint effort between the Matanuska-Susitna Borough and the Alaska Railroad Corporation. Three proposed corridors are on the table. The three begin in the Point MacKenzie area and run between 28 miles and 45 miles to different locations, where they connect with the Railroad's mainline. From East to West, the proposed corridors names are Big Lake, Houston, and Willow. The corridors drawn on the map at this stage are highly conceptual and could swing up to a half mile from the depicted line. There is no preferred corridor at this stage.

Connecting Port MacKenzie by rail will help diversify the State and Borough economy, Borough Manager John Duffy told the crowd. "Our country is a net importer of cement," Duffy said. "If this rail link is built we could end up being the supplier for five percent of our nation's cement," he said, referring to the new minerals markets that would develop because of the cheaper transportation costs on the new rail line to the Port.

Project Manager Brian Lindamood, with the Alaska Railroad, reminded the audience that the corridors are highly conceptual. The project will try to avoid or minimize direct impacts to property.

Lindamood said he expects to submit a final alternatives analysis to the federal Surface Transportation Board at the end of December. The STB ultimately determines where the

route will go. The STB will hold its own public process as part of the environmental evaluation under the National Environmental Policy Act. He said the Surface Transportation Board could make its decision in mid 2009.

Among the questions Monday night:

Q: How will my property values be affected if a rail line goes in nearby?

A: It's not clear and will be specific to individual properties. Rail has the potential to increase access and economic development. Construction would require gravel pit development and other impacts. Rail operations would generate some noise, but would also boost employment and general economic developments.

Q: Will public comment be a consideration?

A: Yes. Public comments will help in the evaluation process and will be carefully considered. Public comments that give specific knowledge of the region and/or concerns on proposed routes will help make a better route.

Meetings are every night this week. Tuesday night is at Big Lake Elementary from 6 p.m. to 8 p.m. Wednesday night: Willow; Thursday night: Knik Elementary; Friday night: Houston. More information is on the project Web site [www.portmacrail.com](http://www.portmacrail.com)

Or call Patty Sullivan, Mat-Su Public Affairs Director at (907) 745-9577 or Stephenie Wheeler, Public Involvement Officer with the Alaska Railroad (907) 265-2671.

-end-



## **NEWS RELEASE**

**CONTACT: PATTY SULLIVAN, (907) 745-9577**

**Oct. 5, 2007**

**MAT-SU BOROUGH**—Hundreds of landowners in the Matanuska-Susitna Borough have attended meetings this week on the proposed Port MacKenzie Rail Extension project. At Wasilla Monday night some 70 people attended. At Big Lake Tuesday night 120 attended. In Willow Wednesday night 83 signed in. At Knik Elementary last night 50 attended.

Tonight (Friday) is the last open house on the rail project. The open house begins at Houston Middle School off Hawk Lane at 6 p.m. until 8 p.m. with a presentation at 7 p.m.

The approximately \$300 million rail extension project is a joint effort between the Matanuska-Susitna Borough and the Alaska Railroad Corporation. Three proposed corridors are on the table.

Participants at meetings have provided written comments on aerial maps, marking their concerns. Among the concerns are: maintaining trail connectivity, as well as protecting private property, parks, water crossings, wildlife habitat, and a tranquil quality of life for lakeside cabin owners.

The public comments at the Open Houses will help in finding the best corridor for the new rail link. The Borough and the Alaska Railroad Corporation are holding the five open houses to gain more local knowledge of the proposed corridors. Many comments will be incorporated and used to help refine the proposed corridors.

"The public comments from these meetings really help in refining our engineering work on all the proposed corridors," said Brian Lindamood, the Railroad's project manager. Hundreds of comments are being gathered at the meetings, on the Web site, through mail, email, and fax, as well as through a court reporter at each meeting.

Last night Joe Perkins, the project executive for the Borough, told the audience at Knik Elementary about the next stage of the project. A federal board, the Surface Transportation Board, ultimately determines the final rail alignment. The STB will hold its own public process as part of the environmental evaluation under the National

Environmental Policy Act. The Borough and Railroad will hire a contractor from a list of approved contractors, which may or may not be from Alaska.

"These folks are going to put the final blessing on the preferred route," Perkins said. "The information we're gathering now reflects the uniqueness of Alaska and our lifestyle. The work we're doing now is to provide them more information so we can have a better application."

The three corridors begin in the Point MacKenzie area and run between 28 miles and 45 miles to different locations, where they connect with the Railroad's mainline. From East to West, the proposed corridors names are Big Lake, Houston, and Willow. The corridors drawn on the map at this stage are highly conceptual and could swing up to a half mile from the depicted line. There is no preferred corridor at this stage.

Among the questions this week were:

Q: If in the end, the chosen alignment affects my private property what are the steps taken?

A: First, these meetings are helping avoid and mitigate impacts to landowners. When possible the proposed alignment will be moved away from private property. If that's not possible, negotiations would take place starting with the fair market value. The Railroad would mutually agree with the landowner on an independent third party appraiser.

Q: What happens to all the trails criss-crossing the area?

A: We will maintain the connectivity of all legally dedicated trails as negotiated with the trail administrators. All trails will be analyzed on a case by case basis. Trail plans are being incorporated into the project map. An additional meeting with trail supporters in Willow will be Oct. 24 at 7 p.m. at the Willow Community Center.

More information is on the project Web site [www.portmacrail.com](http://www.portmacrail.com)

Or call Patty Sullivan, Mat-Su Public Affairs Director at (907) 745-9577 or Stephenie Wheeler, Public Involvement Officer with the Alaska Railroad (907) 265-2671.

-end-



## **NEWS RELEASE**

CONTACT: PATTY SULLIVAN, (907) 746-9577

Oct. 10, 2007

### **Houston to explore rail opportunity**

HOUSTON – Some members of the Houston City Council see opportunity in a proposed rail corridor connecting Houston to the Alaska Railroad and to Port MacKenzie. The proposed Houston alignment is one of three under consideration.

More than 50 residents and landowners attended the final open house Friday night (Oct. 5) in Houston at Houston Middle School. Participants looked over exhibit posters, asked questions, and wrote concerns on large aerial maps.

Public comments from the open houses will help in finding the best corridor for the new rail link.

Newly-elected Houston Councilmember Roger Purcell said the city has been approached by constituents and a manufacturing company interested in a loading dock. Purcell said the City is interested in exploring the opportunity of a rail connection for Houston if a loading facility and depot were included.

Joe Perkins, a project executive for the Matanuska-Susitna Borough, reminded open house participants of the transformation in economic development that came with the last significant infrastructure project nearly 40 years ago, namely, the George Parks Highway. This rail connection to the Port will dramatically contribute to the economy of the Borough and to the state of Alaska, he said. When the proposed natural gas pipeline for the North Slope gets underway, it will be cheaper to transport the construction materials through Port MacKenzie than through other ports, Perkins said.

The rail extension, approximately a \$300 million project, is a partnership between the Matanuska-Susitna Borough and the Alaska Railroad Corporation. Project managers and others will meet with the Houston City Council on Oct. 25 at 7 p.m. in Houston to answer questions.

-more-

Another meeting open to the public is with the Willow Dog Musers Association on Oct. 24, Wed. at 7 p.m. at the Willow Community Center. Snowmobile clubs are welcome and have been invited.

Anchorage residents who own property in the Mat-Su and could not attend any of the five meetings last week can submit comment through the Web site [www.portmacrail.com](http://www.portmacrail.com) Large aerial maps of proposed corridors are available at:

- HDR Alaska, project consultant, 2525 C Street 3<sup>rd</sup> floor, contact Donna Robertson or Leslie Robbins at 644-2000 to make an appointment.
- Railroad Headquarters building, 327 W. Ship Creek Ave., contact Stephenie Wheeler at (907) 265-2671 to make an appointment.

Three proposed corridors are under final consideration for an application before a federal agency. The three begin in the Point MacKenzie area and range between 28 miles and 45 miles in length, connecting with the Railroad's mainline at different locations. From east to west, the proposed corridor names are Big Lake, Houston, and Willow. The corridors drawn on the map at this stage are highly conceptual and are expected to be refined as the process continues.

A table of the strengths and weaknesses of the three proposed corridors will be presented to the Borough Planning Commission and Borough Assembly in November for comment and inquiries. Ultimately a federal agency called the Surface Transportation Board approves the final alignment.

Project Manager Brian Lindamood, of the Alaska Railroad, said he expects to submit a final alternatives analysis to the federal Surface Transportation Board by the end of December. The STB will hold its own public review process as part of the environmental evaluation conducted to comply with the National Environmental Policy Act. Construction of the project is anticipated in 2012.

More information is on the project Web site [www.portmacrail.com](http://www.portmacrail.com)

Or call Patty Sullivan, Mat-Su Borough Public Affairs Director at (907) 745-9577 or Stephenie Wheeler, Public Involvement Officer with the Alaska Railroad (907) 265-2671.

-end-



## **NEWS RELEASE**

**CONTACT: PATTY SULLIVAN, (907) 745-9577 or 355-0103**

**Nov. 20, 2007**

### **Assembly calls for work session on Port Mac Rail project**

PALMER--Location alternatives for a rail route that will spur significant economic development for the Borough and the State were presented as part of a status report to the Matanuska-Susitna Borough Assembly tonight (Tues. Nov. 20).

Assembly Members listened to an overview of the project and studied a matrix of the strengths and weaknesses of the proposed railroad routes. In all, there are eight proposed routes, including the different proposed connectors.

Assembly Member Cindy Bettine asked that an Assembly resolution be added to the rail project application that is being submitted to a federal board. The resolution would express concerns related to growth trends and how the railroad extension would affect future development, among others.

"We all support this high-speed freight train to our Port as a statewide legacy project," Bettine said. "I think this Assembly needs to have input and time to evaluate the matrix." Bettine asked for a work session. A date was set for Dec. 11.

The information presented was developed over the last five months by the engineering and environmental consultants under the guidance of the Borough/Railroad project team. Technical data, extensive research, and information provided by public comments are reflected in the work.

Many public comments helped refine the proposed corridors, said Project Manager for the Railroad, Brian Lindamood. "The public pointed out a lot of things that weren't really evident. They provided depth in certain areas, such as soils issues near Horse Shoe Lake and land use such as the snow machine trails and access to recreational areas."

The project team held numerous public meetings, including five open houses, as well as meetings with specific groups such as The Willow Dog Musers Association, the Iditarod Trail Committee Executive Director, numerous state and federal agencies, and large landowners such as CRI Corporation, among others. So far, 269 comments have

been received and will be forwarded with the application to build and operate the project. Public hearings will be held during an upcoming environmental study.

After the meeting, Brad Sworts, project manager for the Borough, said an Environmental Impact Statement normally includes the socio-economic effect of a project on a community. "We concentrated on the environmental and engineering aspects of the project because we knew the EIS would look at the socio-economic impacts."

Throughout the fact-finding stage, the project team has tried to avoid or minimize direct impacts to property when considering proposed alignments. The corridors are still highly conceptual.

The rail extension project is a joint effort between the Mat-Su Borough and the Alaska Railroad Corporation. This year, the state of Alaska directed \$10 million to the project's environmental document and related studies.

"The reason we are doing this is to provide information to the people who will be preparing the EIS. The more information, the better off we are," said Joe Perkins, project consultant for the Borough as well as a former State Dept. of Transportation Commissioner. "We provided Alaska-specific information that we thought they needed, to save them time," Perkins said.

The environmental study could begin in the spring and continue for 18 months.

The matrix or table compares the eight proposed rail routes by considering ten criteria: poor soils, new road crossings, land availability, developed parcels, designated land use, train energy, wetlands, mapped anadromous fish streams, potential for archaeological sites, and fragmentation of designated recreation areas or refuges.

The preliminary cost estimates were also included. The figures do not include the approximately \$10 million for a loop track at Port MacKenzie as this improvement will be common to all routes.

Connecting Port MacKenzie by rail will help diversify the State and Borough economy, Borough Manager John Duffy has said at open houses. "This is a project that has statewide benefits. Our state has to become less dependent on the dwindling oil reserves. The rail project will develop new natural resources near Fairbanks," Duffy said. "When the natural gas pipeline gets going, shipping the massive construction materials through Port MacKenzie will be the most cost effective for the state."

The federal Surface Transportation Board ultimately approves where the route will go. The STB will hold its own public process as part of the environmental evaluation under the National Environmental Policy Act. The Surface Transportation Board could reach a decision in mid 2009.

Please continue checking the project Web site [www.portmacrail.com](http://www.portmacrail.com)



A summary of public comments will be posted on the project Web site. A newsletter will update those on the mailing list in early December. The Web site will remain active during the future environmental study process, announcing meetings among other information.

A binder of all public comments and agency comments is at Patty Sullivan's desk, please call ahead. For more information call Patty Sullivan, Mat-Su Borough Public Affairs Director at (907) 745-9577 or 355-0103. Or Project Manager for the Borough, Brad Sworts, at 746-7430. Or Tim Thompson, Railroad Manager of External Affairs (907) 265-2695.

-end-

*(This page intentionally left blank.)*



## **NEWS RELEASE**

**CONTACT: PATTY SULLIVAN, (907) 745-9577 or 355-0103**

**Dec. 11, 2007**

### **Assembly passes resolution on Port MacKenzie Rail project**

PALMER—Matanuska-Susitna Borough Assemblymembers passed a resolution tonight (Tues.), on the Port MacKenzie Rail project. The resolution accompanies a preliminary report that will be sent to the Surface Transportation Board, the federal entity that approves new rail lines in the nation.

The Assembly did not adopt a preference for a particular route.

Sponsored by Assemblymember Cindy Bettine, the resolution seeks to focus the attention of the federal board on important social, economic, and environmental issues during an upcoming environmental study.

"All three routes go across my district, that's why I'm so involved in it," Bettine said.

Bettine made multiple amendments to the resolution, including a request for the environmental study to fully explore the potential effect of rail line use along any of the proposed corridors, and on future community development with special attention to residential housing and commercial development and other economic development opportunities.

An STB-approved contractor will be conducting the environmental study, likely an environmental impact statement. That contract has not been awarded yet, but is expected to begin in the spring and continue for 18 months.

Assemblymember Michelle Church was also among her peers in adding some amendments to the resolution. "This resolution is simply saying we're ready to go to the next step. We want to look at lots more that the STB comes up with."

Before the resolution, members from the project team addressed how the matrix of strengths and weaknesses on the proposed corridors is a starting point.

Joe Perkins is a consultant for the Borough on the project. He is also a former Commissioner of the Alaska Dept. of Transportation. "The reason we've done this matrix is to give the STB ten measurable areas, where hopefully they can validate the information we have and can speed up the EIS, (Environmental Impact Statement.)"

The Assembly will have more chances to comment. The Surface Transportation Board could reach a decision as early as mid 2009.

Perkins highlighted that some of the public comments helped refine the corridors, and, he said, the STB will pay attention to public comments.

All public comments and a sizeable bibliography of local, state, and federal plans, reports, and studies, pertinent to the project area, will be forwarded to the STB as part of the preliminary information.

Project manager for the Alaska Railroad Corporation, Brian Lindamood, discussed how the Castle Mountain earthquake fault was considered.

"Railroad tracks tend to survive very well," Lindamood said. "During the 9.2 earthquake we had three trains operating from Wasilla to Seward, none of them derailed." Lindamood further said that the Railroad designs the tracks to codes that anticipate big magnitude quakes, and the Railroad has emergency response plans in place.

Borough Manager John Duffy emphasized that the preliminary nature of these recent studies. "What we are submitting is not binding on the STB. They will likely look at other variants and other corridors ... And there's a whole other section yet to weigh in here— socio-economics, that is going to have a significant bearing on the proposed corridor." Duffy said.

Tuesday night, 17 members of the public commented on the project during the nearly three-hour work session/special meeting. Those comments and comments from a Nov. 20 meeting will be added to the 317 comments already gathered for the report.

Communities have come out with diverse opinions. Among them: the Willow Chamber of Commerce and the Big Lake Chamber of Commerce passed resolutions in support of the Willow route; the Willow Area Community Organization as well as trails groups passed resolutions opposing the Willow route. Houston Chamber of Commerce supports the Houston South proposed route, among many other opinions.

The resolution will be posted on the Web site as soon as it is finalized. The documents the Assembly reviewed today are posted with this press release and will also be posted on the project Web site at [www.portmacrail.com](http://www.portmacrail.com)

The information presented was developed over the last five months by the engineering and environmental consultants under the guidance of the Borough/Railroad project team. Technical data, extensive research, and information provided by public comments are reflected in the work.

The project team held several public meetings, including five open houses, as well as meetings with specific groups such as The Willow Dog Musers Association, the Iditarod Trail Committee Executive Director, numerous state and federal agencies, and large landowners such as CIRI Corporation, among others. So far, more than 317 comments have been received and will be forwarded with the application to build and operate the project. Public hearings will be held during an upcoming environmental study.

The rail extension project is a joint effort between the Mat-Su Borough and the Alaska Railroad Corporation. This year, the state of Alaska directed \$10 million to the project's environmental document and related studies.

Please continue checking the project Web site [www.portmacrail.com](http://www.portmacrail.com).

A summary of public comments will be posted on the project Web site. A newsletter is in the mail and will update those on the mailing list. The Web site will remain active during the future environmental study process, announcing meetings among other information.

A binder of all public comments and agency comments is at Patty Sullivan's desk, please call ahead. For more information call Patty Sullivan, Mat-Su Borough Public Affairs Director at (907) 745-9577 or 355-0103. Or Project Manager for the Borough, Brad Sworts, at 746-7430. Or Tim Thompson, Railroad Manager of External Affairs (907) 265-2695.

-end-

*(This page intentionally left blank.)*

**NEWS**

**FEATURES**

**USEFUL**

**SPECIAL SECTIONS**

**JOBS**

Web posted Friday, May 4, 2007

## Breaking News Legislature nears approval of financing for Agrimium coal project

### By the Journal of Commerce

JUNEAU ? Legislation that would allow the Alaska Railroad Corp. to finance, tax free, \$2.9 billion for Agrimium's proposed coal gasification project in Nikiski was reported out of the Senate Resources Committee yesterday and is now in the Senate Rules Committee ? one stop short of final passage by the Legislature.

The bill is House Bill 229, sponsored by Rep. Mike Chenault, R-Kenai.

The bill includes authorization for \$300 million for an extension of rail track from Willow to the port at Point MacKenzie in the Matanuska-Susitna Borough, although Agrimium proposes to ship coal through the Port of Anchorage.

[Email this story](#)

[Print this story](#)

**Auto Insurance Quotes**  
Save Hundreds In Minutes w/ Fast, No Hassle Auto Ins From The Hartford AARP. TheHartford.com

**Permanent Fund (\$M USD)**

**38,637**  
+ 139

◆ **Wednesday's close**  
(Most Recent Available)

**AK North Slope Crude (W.Coast delivery)**

**93.97**  
- .68

◆ **Wednesday's close**  
(Most Recent Available)



**Yellow Pages Search**

Search:

City: anchorage

State: AK

Go!

By [acesyellowpages.com](http://acesyellowpages.com)

*(This page intentionally left blank.)*



## Port MacKenzie angles for rail extension for coal

By Tim Bradner

*Alaska Journal of Commerce*

Publication Date: 05/20/07

JUNEAU — The state Legislature has approved authorization for the state-owned Alaska Railroad Corp. to sell bonds for a 38-mile rail spur from near Willow to the Matanuska-Susitna Borough's Port MacKenzie, a bulk commodities port on Knik Arm, across from Anchorage.

Track-laying crews aren't about to start work, however. The railroad must first find a way to pay for the estimated \$300 million cost of the spur line.

Still, the Mat-Su Borough is pushing ahead with the rail link, which it sees as a vital new transportation link between Southcentral and Interior Alaska. State lawmakers approved \$12 million for preliminary environmental and engineering studies for the rail link in the state capital budget May 16. The work, which will be managed by the borough, includes a required federal environmental impact statement.

In presentations to legislators, borough officials have laid out an ambitious schedule for the project, with completion timed to coincide with a plan by Agrium U.S. Inc. to convert its fertilizer plant in Nikiski from natural gas to coal.

The borough plans design and environmental work to begin this year and be finished in 2009, and construction from 2009 to 2011. Agrium hopes to have its plant conversion done the same year.

The company plans to purchase coal from a coal mine in Healy operated by Usibelli Mine Inc. Agrium will need about 3 million tons of coal yearly for a coal gasification unit built at the fertilizer plant.

The company and the Alaska Railroad are now planning to ship the coal through the Port of Anchorage. Mat-Su officials want to see the coal shipped along the new rail spur to Port MacKenzie and loaded on barges there for transport across Cook Inlet to Agrium's plant near Kenai.

Mat-Su Borough Manager John Duffy said the rail extension to Port MacKenzie would cut 26.4 miles off the distance coal would be moved by train compared with shipping through Anchorage, resulting in a \$5.21 million annual savings for Agrium.

The savings total \$257 million over 30 years, almost enough to pay for the rail extension, Duffy said.

Additional benefits include less expensive shipping for Usibelli for coal it now exports to South Korea through Seward, involving a much longer rail shipment, and lowering the cost of shipping new resources that could be developed along the rail corridor to Interior Alaska.

Port MacKenzie has more than 9,000 acres of borough-owned upland and tideland acreage available for industrial use and storage compared with constricted space at the Port of Anchorage, Duffy said. The port can also load Cape- and Panamax-sized cargo vessels today with no dredging, he said.

The problem is financing the expansion itself, Alaska Railroad officials said. Revenue bonds sold by the railroad would have to be paid by additional fees tacked on to freight charges for moving coal or other cargo over the spur, railroad president Pat Gamble said.

Agrium has, so far, been reluctant to pay the additional costs. The track to the Port of Anchorage is paid for, although the distance is greater.

Gamble agreed that a rail extension to Port MacKenzie would be the most efficient long-term option for supplying coal to Agrium. The company and the railroad are proceeding with their current plan of shipping through Anchorage because of uncertainties on whether the rail extension can be completed in time, and on how the capital costs would be paid for.

Duffy said the rail extension is a matter of reducing congestion and public safety hazards within the borough as well as economic development.

“Moving 100-car-plus coal trains on the established tracks through the already-congested Wasilla-to-Anchorage corridor will cause serious auto and rail conflicts along numerous at-grade rail crossings in the borough,” he told the Senate Resources Committee in mid-April. The cost of alleviating safety problems at road-rail crossings total \$140 million, which can be avoided with the alternative of a rail link to Port MacKenzie, Duffy said.

The congestion would further erode opportunities for commuter rail service between the Mat-Su and Anchorage, Duffy said.

Tim Bradner can be reached at [tim.bradner@alaskajournal.com](mailto:tim.bradner@alaskajournal.com).

Click here to return to story:

[http://alaskajournal.com/stories/052007/tra\\_20070520023.shtml](http://alaskajournal.com/stories/052007/tra_20070520023.shtml)

© The Alaska Journal of Commerce Online

# ALL ABOARD

JULY / AUGUST 2007

THE WYE OF IT: OUR CUSTOMERS AND COMMUNITIES

## CHUGACH WHISTLE STOP SERVICE BEGINS

In mid-August the Alaska Railroad joined the U.S. Forest Service (USFS) to introduce a new service and experience in Chugach National Forest. The Chugach Forest Whistle Stop project has been in the works since 2003, when the ARRC and USFS formed a partnership to offer rail service to remote and pristine areas accessible only by train.

The August 17 ribbon-cutting marked the opening of the project's first whistle stop site at Spencer Glacier. Participants in the ribbon-cutting included (pictured at right, L-R:) Forrest Hannan (in old-style USFS uniform); Gloria Manning, Associate Deputy Chief, National Forest System; Senator Lisa Murkowski; Joe Meade, Chugach Forest Supervisor; Senator Ted Stevens; ARRC Chair John Binkley and Conductor Erik Forland.

With environmental work and design complete in 2006, construction got underway this spring at Spencer Glacier. The result: a passenger loading and unloading platform, a pavilion shelter, restrooms and a trail.

(PHOTO BY AL GRILLO)



Beginning August 23, passengers can board the *Glacier Discovery* train in Anchorage, Girdwood or Portage and travel to the Spencer Glacier Whistle Stop. Here, they step off the train with a U.S. Forest Service interpreter who will lead a narrated walk to a viewing platform, about 1.5 miles from the unloading platform. Passengers can also participate in unguided activities.

(See "Whistle Stop" on page 18)

THE WYE OF IT: OUR CUSTOMERS AND COMMUNITIES

## MAT-SU, ARRC PARTNER ON PORT PROJECT

Earlier this summer, the Alaska Railroad and Matanuska-Susitna Borough (MSB) became partners in pursuing a Port MacKenzie Rail Extension project. The Alaska Railroad, as owner and operator of the railroad and rail extension, is the project "applicant." The borough, as the owner and operator of Port MacKenzie, is the project sponsor.

The proposed project would result in a 30- to 45-mile rail line between Port MacKenzie and a connection to the ARRC main line at a point between Wasilla and Willow. The project is rail-only, including a rail maintenance road, but not a public road. Possible rail alignments and crossing locations will be evaluated based on potential environmental impacts, engineering

feasibility, and right-of-way concerns.

Similar to the Northern Rail Extension project (which calls for a new rail between Eielson AFB and the Delta Junction area) this project involves construction of new line, and thus requires federal approval. The Surface Transportation Board is the federal agency responsible for completing an Environmental Impact Statement (EIS) and for ultimately approving the new route.

Over the past 30 years, several entities have considered construction of a railroad between Port MacKenzie and the Alaska Railroad system to support the development and operation of the port. The most

See "Port MacKenzie" on page 3)

## INSIDE...

CROSSINGS:  
PASSENGER PHOTO  
CONTEST WINNERS

SIGNALS; PPT  
REMINDER; SAFETY  
POSTER CONTEST

CROSSINGS:  
SALVAGING THE  
UNSALVAGABLE

CROSSINGS: SHI  
CREEK ITC PHASE  
BREAKS GROUND

I HEARD IT  
THROUGH THE RAIL  
LINE — ASK PAT

SIDINGS: GREEN  
TIPS FOR  
RAILROADER

CROSSINGS: OUR  
TENANTS IN  
FAIRBANKS



MILEPOSTS

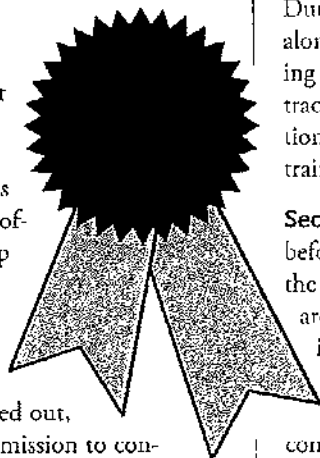
# SUMMER MONTH SPOTLIGHT AWARDS

The following individuals and groups were nominated for a Spotlight Award in June, July or August 2007.

**Print Shop Equipment Operator Mia Cain and Corporation Mail Clerk Katie Johnson** took to heart every employee's responsibility for being an extension of ARRC's security department when it comes to staying alert and reporting out-of-the-ordinary events. When a group of contractors congregated in the computer room, Cain and Johnson stopped the contractor's activity and consulted an I.S.

Department supervisor. As it turned out, these contractors did not have permission to conduct training or be in the computer room at that time. The foresight of Cain and Johnson may well have prevented an electrical mishap during the busiest time of year.

**HR Retirement Specialist Ana Perez-Wood** put a great deal of time and effort to revamp the Railroad's Corporate Pension Plan benefit statement. With a personalized approach, the new statement is a tremendous improvement. It is visually pleasing, easy to read and will help employees to understand and better appreciate the benefits they are offered by the Railroad.



**Locomotive Electrician Russell Martin** took quick action to avert a potentially dangerous situation. During a recent drive on the Seward Highway, ride-along guest Jane Burri noticed flames and smoke coming from a wheel on a passenger coach rolling by on the tracks. When Ms. Burri brought this to Martin's attention, he immediately notified the right people and the train was stopped safely and the problem rectified.

**Security Officer William Moreland** caught two felons before they could pose future danger to employees or the public. Moreland was inspecting the Powder Spur area near MP 132 when he observe two men trespassing on the right-of-way. When one of the trespassers reached behind his back to withdraw a semi-automatic pistol from his waistband, Moreland saw it coming and was able to secure the weapon and the individual, before requesting local law enforcement help in locating the other man who had run from the scene.

**Carman Martin Hupe** spied a hard-to-see defect on on the Railroad's 651 bi-level dome car, allowing corrective action to occur and preventing a possible future mishap. In particular, Hupe discovered a six-inch crack in the car's holster spring pocket during a recent mechanical inspection. Thanks to Hupe's experienced eye, close attention to detail and technical expertise, this defect did not go unchecked. ○○○○

## PORT MACKENZIE...

*(continued from Page 1)*

of these studies was completed in 2003, which culminated in the recommendation of a route that connects to the Alaska Railroad near Willow.

The Mat-Su Borough has been the fastest growing area of the state for the past 16 years. Conditions have changed since the project was last studied. That development along with new technical data make it critical to take a fresh look.

In May 2007, the State of Alaska approved \$10 million to pursue the project. In June, ARRC and MSB signed a Memorandum of Agreement to accomplish alternative analysis, preliminary engineering, environmental documentation and a financial feasibility study in support of a STB-led EIS.

This means an application should be submitted to the STB by the end of the year. In order to compile an accurate and comprehensive application, focused public involvement, agency consultation and alternative analysis will take place in the next few months.

Public meetings are set for the first week in October in five MSB communities. (see box at right for dates and locations). All meetings will be conducted

6:00-8:00 p.m., with a presentation at 7:00 p.m.

If all goes well, the STB would launch a formal EIS process that could conclude by the end of 2008. The hope is that final design, and right-of-way acquisition and construction can begin in 2009, with completion in 2011, and rail operation by 2012.

Final design and construction of a rail spur to Port MacKenzie is estimated to cost \$300 million. Funding is not yet identified.

Railroaders and the public can keep apprised of project progress by visiting the project web site at [www.portmacrail.com](http://www.portmacrail.com) ○○○○

3



### PUBLIC MEETINGS

**WASILLA, OCTOBER 1**  
Lake Lucille Inn  
1300 West Lake Lucille Drive

**BIG LAKE, OCTOBER 2**  
Big Lake Elementary School  
3808 S. Big Lake Road

**WILLOW, OCTOBER 3**  
Willow Area Community Center  
Mile 69.5 Parks Hwy

**KNIK, OCTOBER 4**  
Knik Elementary School  
6350 West Hollywood (Wasilla)

**HOUSTON, OCTOBER 5**  
Houston Middle School  
12801 W. Hawk Lane

# Meetings set to begin on Point MacKenzie rail expansion plan

BY JOHN R. MOSES

Frontier Star

MAT-SU — They've been 30 years in the making. A series of openhouses will stretch from Wasilla to Willow as the Mat-Su Borough invites the public to review and discuss routes for a 30- to 45-mile extension of the Alaska Railroad to Port MacKenzie.

This part of the project is the environmental assessment phase, the last part before undertaking final design and construction on the project.

The rail line "will drastically change the nature of the port," said Borough spokeswoman Patty Sullivan.

While the rail project could open new mineral markets for export, including possibly Alaska-produced cement, Sullivan

See MEET, Page A12

## WHEN AND WHERE

Meetings on the planned rail expansion to Port MacKenzie will be held from 6-8 p.m. on the following dates:

Oct. 1, Lake Lucille Inn, 1300 W. Lake Lucille Dr., Wasilla

Oct. 2, Big Lake Elementary School, 3808 S. Big Lake Road, Big Lake

Oct. 3, Willow Area Community Center, Mile 69.5 Parks Highway, Willow

Oct. 4, Knik Elementary School, 6350 W. Hollywood Dr., Knik

Oct. 5, Houston Middle School, 12801 W. Hawk Lane, Houston

## MEET

Continued from Page A1

said there has been controversy about the proposed routes for the expansion. One plan affects the Willow area. Another puts tracks near Houston. All are available online at [www.port-mackrail.com](http://www.port-mackrail.com).

Sullivan said all routes are on the table for discussion and designers could eventually choose a route not yet thought of based on public suggestions. The most recent study, conducted in 2003, selected a route near Willow, she said. That plan drew some local opposition, especially from dog mushers and owners of cabins.

The final design and construction phases could cost as much as \$500 million. The Borough and Alaska Railroad Corp. are jointly studying the best way to finance the extension. Public meetings are all from 6-8 p.m. with a project overview at 7 p.m. They are

• Wasilla, Oct. 1, Lake Lucille Inn, 1300 W. Lake Lucille Dr.  
• Big Lake, Oct. 2, Big Lake Elementary School, 3808 S. Big Lake Road

• Willow, Oct. 3, Willow Area Community Center, Mile 69.5 Parks Highway

• Knik, Oct. 4, Knik Elementary School, 6350 W. Hollywood Dr.

• Houston, Oct. 5, Houston Middle School, 12801 W. Hawk Lane

Sullivan expects a strong turnout for what she said is the beginning of the end of a long process. The project has been considered for 30 years. It won't be a surprise to many.

Contact John R. Moses at 352-2270 or [john.r.moses@frontierstar.com](mailto:john.r.moses@frontierstar.com)

## Agrium plant closure in Kenai won't slow Port Mac rail extension

BY JOHN R. MOSES

*Frontiersman*

**MAT-SU** — A Kenai fertilizer plant that could be a main user of a planned \$300 million rail extension to Port MacKenzie is closing its doors soon and cashing 100 workers due to a shortage of natural gas in the Cook Inlet. A Mat-Su Borough official said the closure has no impact on the Borough's plans for the rail line that might feed the plant's energy needs in the future if it converts to coal.

While coal heading to Agrium's Kenai facility would be one use, many other uses justify the rail line's construction, Borough Economic Development Director Dave Hanson said.

"The rail extension is not a one-horse project," he said. The 30- to 45-mile rail spur would bring trains to Port MacKenzie and open up a new corridor for moving minerals out of interior Alaska and goods and equipment in. A series of public open houses is happening next week to gather ideas about what route should be used.

Agrium, a major North American producer of specialty fertilizers, is studying whether to convert to coal and re-open the plant with that new power source. That decision could come later this year or next year.

Gov. Sarah Palin on Wednesday pledged state support for workers displaced by the Nikiski plant's closure and deployed Department of Labor rapid response teams to work with them.

"It's unfortunate to see the closure of a facility that has provided so many jobs that support families on the Peninsula," Palin said. "I am heartened to hear that Agrium is willing to keep its options open if sufficient long-term supplies of gas can be found. We know there is more gas to be

found and developed in Cook Inlet, so I remain hopeful that those jobs can be preserved."

Among those options is converting the plant from natural gas to coal.

A 2012 date for conversion to coal by that plant was a major driver of the Matanuska-Susitna Borough's and the Alaska Railroad Corp.'s 2012 deadline to get a new rail spur to Port MacKenzie finished.

Hanson said Agrium officials would consider using Anchorage to import coal a less attractive option than Port MacKenzie. He was in conversation with Agrium officials Wednesday morning after news of the closure broke. Agrium wanted assurances that the Borough and railroad were still working toward building the rail project.

Hanson said gas supply to run the plant, not economics, caused the shut-down. Agrium couldn't round up even a three-month supply.

The closure "has nothing to do with coal gasification, except to show the urgency of it," he said. "They are very interested in seeing that we are staying on our timeline."

Agrium managers said they were struggling to get adequate natural gas contracts, but could not buy enough gas in volume to make the nitrogen fertilizer operations run.

"It is a sad day for us to have to close this facility, which has added much value to the Alaskan economy for the past 40 years," said Mike Wilson, president and CEO of Agrium, in a prepared statement. "It has been a major supplier to international markets in the Pacific region and was Alaska's third largest exporter in 2006, despite running at 50 percent of capacity."

*The Associated Press contributed to this report.*



Oct. 1, 2007

## Alaska Railroad to host open houses in five communities

The Matanuska-Susitna Borough and the Alaska Railroad invite the public to an open house on the Port MacKenzie Rail Extension Project. Five open houses on the Port MacKenzie Rail Extension Project will be held the first week of October by the Matanuska-Susitna Borough and the Alaska Railroad.

The open houses will be in Wasilla, Big Lake, Houston, Willow and Knik. All are from 6 to 8 p.m., with a presentation at 7. Scheduled dates are:

**Wasilla, Oct. 1**, Lake Lucille Inn; **Big Lake, Oct. 2**, Big Lake Elementary School; **Willow, Oct. 3**, Willow Area Community Center; **Knik, Oct. 4**, Knik Elementary School; **Houston, Oct. 5**, Houston Middle School.

The project is a joint effort to build a \$300 million-dollar rail line that would connect Port Mackenzie to the existing rail. Where it connects has not been decided — it will be at a point somewhere between Meadow Lakes and north of Willow.

New technical data is being gathered, and more than one alignment is being studied. Project staff will be on hand for questions about exhibits and to take comments.

In June 2007, MSB and ARRC signed a memorandum of agreement to jointly pursue the Port MacKenzie

Rail Extension Project. Meanwhile, the State of Alaska appropriated \$10 million to fund an environmental study.

Construction of a new rail line requires federal approval. The Surface Transportation Board (STB) is the federal agency responsible for completing the environmental document and approving a

new route.

After nearly three decades of study, the Port MacKenzie Rail Extension project is now poised to move into the environmental documentation phase that precedes final design and construction. The most recent rail route study was commissioned by the Mat-Su Borough in 2003, and it recommended connecting to the Alaska Railroad near Willow.

The Borough has been the state's fastest growing community for the past 16 years. This growth, along with new technical data, warrant a fresh look at the strongest alignment options. As a progression of the preliminary engineering effort, the project team is considering the 2003 recommended route, as well as two to three additional routes.

Between now and December, the project team will gather public comment and conduct fact-finding and preliminary engineering activities prior to submitting a project application to the STB in January 2008.

While conducting the environmental study process, the STB may modify and/or eliminate routes and could possibly select a route not yet considered.

The STB will host public scoping meetings and accept public comment throughout the process, providing additional opportunity for public input. Ultimately, the final route will be selected at the STB's discretion at the completion of the environmental study and document process.

The final design and construction of a rail extension to Port MacKenzie is estimated to cost \$300 million in 2007 dollars. The Matanuska-Susitna Borough and the Alaska Railroad will jointly conduct a financing study to assess options for funding the project.



ADVERTISEMENT



**Online Exclusive**  
Capture every moment with a  
**FREE Camera Phone**  
from Verizon Wireless  
WITH NEW 2 YR ACTIVATION

Free Shipping

MEMBER CENTER: Create Account | Log In



**2 NEWS**  
KTUU.COM  
Alaska's News Source

HOME NEWS WEATHER SPORTS FEATURES REAL ESTATE  
WHAT'S ON 2


**ENTER THE PROMO CODE "KTUU"**  
FOR A 10% DISCOUNT WHEN YOU PURCHASE THE 2008 IDITAROD INSIDER  
~~\$19.95~~ Now Only \$17.95\*  
BECOME A MEMBER OF THE IDITAROD Insider

LIVE VIDEO DAILY UPDATES

ADVERTISEMENT

## Evolution of the

### The GenoType



**START THE DIET** Designed for YOU THE GENO

Email Print

Text Size

### Point MacKenzie railroad debated



by Jason Moore  
Tuesday, Oct. 2, 2007

PALMER, Alaska -- The Matanuska-Susitna Borough launched a series of public hearings on the vision of a railroad extension to the borough's port at Point MacKenzie.

The Matanuska-Susitna Borough launched a series of public hearings on the vision of a railroad extension to the borough's port at Point MacKenzie. (Jason Kohler/KTUU-TV)

The project's cost is estimated at approximately \$300 million, although no one seems to have any idea where the funding would come from.



Patty Sullivan, Mat-Su borough public affairs manager, said the plan makes sense for the state. (Jason Kohler/KTUU-TV)

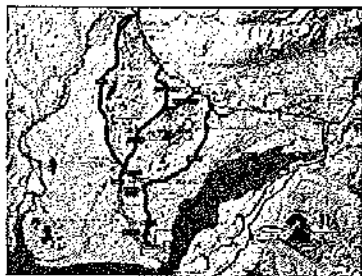
Most of the raw material freight hauled out of Alaska's interior by rail goes to ports in Seward and Whittier. When the Mat-Su borough welcomed the first ship to Point MacKenzie in 2005, they hoped if a rail was built, businesses would follow.

Patty Sullivan, Mat-Su borough public affairs manager, said the plan makes sense for the state.

"It will affect the state's economy as well as the Mat-Su borough's, because the transportation costs for bulk resources will be significantly lower using Port MacKenzie," Sullivan said.

The borough began holding public hearings this week to gather input on three proposed

- News Minute: Here is the latest Alaska news from The Associated Press
- Agrium wins bid for federal unemployment assistance
- Labor Department predicts job growth, but less of it
- Clinton plans to open campaign office in Alaska
- Traveler passes
- Prose theft buy
- Steve listin natur
- Idita again Wasi

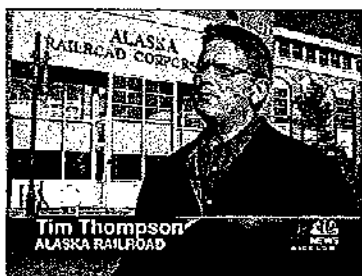


The borough began holding public hearings this week to gather input on three proposed routes for the Spur Line. (Courtesy photo)

routes for the Spur Line. The furthest north would take off near Willow. Another proposal would start from the Houston area, while a third possibility would be further south, around Big Lake.

Each route faces challenges. The Willow Chamber of Commerce voted to support the Spur Line there, but Jim Huston with Chamber admitted the community is divided.

"A lot of it is pro-development versus non-development," Huston said.



Alaska Railroad Spokesman Tim Thompson said it's still early in the process, but the plan has support. (Jason Kohler/KTUU-TV)

Huston added that many who oppose the project moved to the Valley to get away from Anchorage's growth, but fail to recognize their own immigration has triggered the necessity for more development in the Mat-Su.

"The people who moved up here to get away from development and they don't look beyond their own moving into the subdivisions as development, they just want to end it at that," Huston said.



Along with the financing, a detailed environmental study still needs to be conducted. (Jason Kohler/KTUU-TV)

The state gave the borough \$10 million to study the project. But a source for the \$300 million to build it has not been determined.

Alaska Railroad Spokesman Tim Thompson said it's still early in the process, but the plan has support.

"This project provides us with another option for a port. We have ports in Seward, ports in Whittier; we also connect with the

Anchorage port. This would allow us another option that we could have move any type of goods that are out there," Thompson said.

Along with the financing, a detailed environmental study still needs to be conducted. The railroad said the project would be completed around the year 2011 or 2012. As far as the borough is concerned, the train has left the station.

The open house events where people can offer comments started last night in Wasilla. They continue tonight in Big Lake, tomorrow in Willow, Thursday at Knik Elementary School and Friday at Houston Middle School. All the events start at 7:00 p.m. Ultimately the route will be chosen after the environmental impact statement is concluded, the open houses are just early efforts to gather public input.

Contact Jason Moore at [jmoore@ktuu.com](mailto:jmoore@ktuu.com)



10/21/07

## Point MacKenzie railroad spur debate begins

by Jason Moore

KTUU-TV

updated 11:23 a.m. HT, Wed., Dec. 12, 2007

Anchorage, Alaska - The Matanuska-Susitna Borough launched a series of public hearings on the vision of a railroad extension to the borough's port at Point MacKenzie. The project's cost is estimated at approximately \$300 million, although no one seems to have any idea where the funding would come from.

Most of the raw material freight hauled out of Alaska's interior by rail goes to ports in Seward and Whittier. When the Mat-Su borough welcomed the first ship to Point MacKenzie in 2005, they hoped if a rail was built, businesses would follow.

Patty Sullivan with the Mat-Su borough said the plan makes sense for the state.

"It will affect the state's economy as well as the Mat-Su borough's, because the transportation costs for bulk resources will be significantly lower using Port MacKenzie," Sullivan said.

The borough began holding public hearings this week to gather input on three proposed routes for the spur line.

The furthest north would take off near Willow. Another proposal would start from the Houston area, while a third possibility would be further south, around Big Lake.

Each route faces challenges. The Willow Chamber of Commerce voted to support the spur line there, but Jim Huston with Chamber admitted the community is divided.

"A lot of it is pro-development versus non-development," Huston said.

Huston added that many who oppose the project moved to the Valley to get away from Anchorage's growth, but fail to recognize their own immigration has triggered the necessity for more development in the Mat-Su.

"The people who moved up here to get away from development, they don't look beyond their own moving into the subdivisions as development. They just want to end it at that," Huston said.

The state gave the borough \$10 million to study the project. But a source for the \$300 million to build it has not been determined.

Alaska Railroad Spokesman Tim Thompson said it's still early in the process.

"This project provides us with another option for a port. We have ports in Seward, ports in Whittier; we also connect with the Anchorage port. This would allow us another option that we could have move any type of goods that are out there," Thompson said.

Along with the financing, a detailed environmental study still needs to be conducted. The railroad said the project would be completed around the year 2011 or 2012. As far as the borough is concerned, the train has left the station.

Open house events allowing public comment began last night in Wasilla. They continue tomorrow in Willow, Thursday at Knik Elementary School and Friday at Houston Middle School. All the events start at 7:00 p.m.

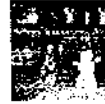
Ultimately the route will be chosen after the environmental Impact statement is concluded, the open houses are just early efforts to gather public input.

URL: <http://www.msnbc.msn.com/id/21118637/from/ET/>

MSN Privacy . Legal



# Anchorage Daily News



**Holiday lights map**  
Post a photo of your lights to our map and plot out the best tour.



**23°** 25° |  
forecast (statewide)  
Daylight : 10:15 a.m. - 3:4

Subscribe | manage subscription | E-Edition

Search In: **All** for

[News](#) [Sports](#) [Outdoors](#) [Features](#) [Money](#) [Entertainment](#) [Opinion](#) [Blog](#)

[Home](#) | [Alaska Newsreader](#) | [Obituaries](#) | [Archives](#) [Movies](#) | [Music](#) | [Restaurants](#) | [Submit an Event](#) [Deals](#)

## Rail link to Port MacKenzie explored

### MACKENZIE: Line would tie Willow area, shipping facility together.

By RINDI WHITE  
rwhite@adn.com

Published: October 3, 2007  
Last Modified: October 5, 2007 at 02:39 AM

WASILLA -- The Matanuska-Susitna Borough wants a rail line to connect the Alaska Railroad main line north of Wasilla to Port MacKenzie, and hopes trains are running on it by 2012.

#### Story tools

- [Comment on this story](#)
- [E-mail a friend](#)
- [Print](#)
- [Digg this](#)
- [Seed Newsvine](#)
- [Send link via AIM](#)

Font size : A | A | A

There are several hurdles, however. Before a single railroad tie is laid, the borough and the Alaska Railroad need permission to build from the federal Surface Transportation Board, which oversees railroad rates, mergers and new routes.

To that end, the borough and railroad corporation since June have been working on a route analysis by packaging past route studies with new environmental information. This week, a crew hired for the project is wrapping up soil testing along possible rail corridors, and the borough kicked off a weeklong round of public meetings on the issue.

After the information is gathered, borough and railroad officials hope to pass along to the Surface Transportation Board in December environmental data, land ownership, public comments and other information.

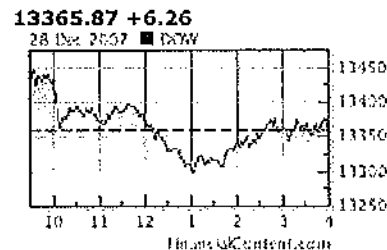
A route has not been selected. The Mat-Su Assembly this year endorsed a route that skirts Nancy Lakes State Recreation

#### More mat-su stories >>

- [Mat-Su datebook](#)
- [Winter activities](#)
- [In brief](#)
- [Scanner](#)
- [Couple loses farm, gains old Kentucky home](#)

#### Stocks

**DOW NSDQ NYSE S&P 500**  
Dow Jones Industrial Avg.



Symbol Lookup

[Market Summary](#) | [Market Movers](#)  
[My Watchlist](#) | [My Portfolio](#)

[Public Involvement Activities Summary](#)  
January 2008



#### Top Job

- [Social Services](#)
- [Children](#)
- [Bristol Bay](#)
- [Construct](#)
- [Commur](#)
- [Teck Cor](#)
- [Skilled/Tr](#)
- [Facilities](#)
- [Alaska US](#)
- [Human/S](#)
- [Outreach](#)
- [YWCA An](#)
- [Health Ca](#)
- [RN's](#)
- [Mat-Su R](#)

Area and connects with the Alaska Railroad north of Willow, one of three proposed routes.

But all options are on the table, said Borough Manager John Duffy.

"There is no preferred alternative," he said.

Also being studied are links in Houston and Big Lake. Two options on the southern end of the rail line have emerged as well, one running east of the Point MacKenzie Agricultural Area and one running west of it, between the farms and the Susitna Flats State Game Refuge.

With all of the routes, borough spokeswoman Patty Sullivan said, there is room to tweak the line to reduce impacts.

"We're trying to keep people whole," Sullivan said.

Crowded around big maps on tables at one end of a Best Western Lake Lucille Inn banquet room Monday, property owners in the triangle from Point MacKenzie to Big Lake and Willow made notes about what the 200-foot right-of-way route for a rail line should avoid.

Recreational property owner Jon McCracken sketched in a ridgeline that runs north and south on the west side of Red Shirt Lake. If the rail line stayed west of the ridgeline, he said, it would reduce the impact on recreational users.

Kit Roberts, a South Knik River resident who has land in the Point MacKenzie area, said he likes the plan to build a rail line to Port MacKenzie. The port needs it to be economically viable, he said.

Roberts, a member of the local snowmachining group Mat-Su Motor Musers, said he is worried about how the rail line could affect his hobby. Borough and railroad officials at the meeting said they plan to build crossings for trails at the rail line, but only if the trails are legally recognized. Roberts said that means property owners who are used to firing up their machines and riding out of their yard to get to trails might have to go a mile or two out of their way to get to a crossing.

"Personally, I don't think going a couple miles out of the way is going to be a problem," Roberts said. "I just want to make sure what they do is done reasonably."

The borough hopes to build the new rail line by 2012 to serve a coal-to-gas plant that fertilizer manufacturer Agrilum hopes to have running then. Depending on the route, the rail line is between 28 and 45 miles long and is estimated to cost \$300 million.

Brian Lindamood, special projects manager with the Alaska Railroad, said project leaders hope for a decision from the Surface Transportation Board by mid 2009. If approved, borough and railroad officials will work on a financing package and move ahead on the project.

Borough leaders say the new route would make limestone and other mineral mining north of the borough economically feasible and would transform the borough-owned Port MacKenzie into the natural resource-shipping facility it was designed to be.

"If this project moves forward, it would be the first major transportation project in the state since the Parks Highway," Duffy said. "Just think for a moment about that, the economic activity related to the Parks Highway. That's what this project would do."

Railroad spokesman Tim Thompson said a route to Port MacKenzie would free up space on the busy rail line between Anchorage and Fairbanks.

"This area does get congested. We have a lot of trains competing for space right now," Thompson

Homes |

Eastrid  
Near ho  
3 BR, 2.  
garage.

Sear

Top Ren

The Clu  
South An  
away fro  
Providen  
Alaska N

Top Aut



Searc

Best De

Pets & F  
Find pup  
and serv

Other Tr  
Find grea  
ATV'S, we

Merchan  
Antiques,  
Find deal  
More...

said.

Daily News reporter Rindi White can be reached at [www.adn.com/contact/rwhite](http://www.adn.com/contact/rwhite) or call 352-6709.

FOR MORE: Find additional information about the project at

[portmacrail.com](http://portmacrail.com)

### Get involved

**IF YOU GO:** Public meetings on the proposed railroad routes are scheduled tonight at Willow Area Community Center, Mile 69.5 Parks Highway; Thursday at Knik Elementary School, 6350 W. Hollywood Drive; and Friday at Houston Middle School, 12801 W. Hawk Lane. All open houses are 6-8 p.m., with a presentation at 7 p.m.

### Comments

Please **log in** to post comments.

More gre  
Advar

[Anchorage](#)  
[Browse Li](#)  
[Neighbor](#)  
[www.Trulia](#)

[Alaska Fi](#)  
[Superb L](#)  
[Halibut-Tr](#)  
[www.alask](#)

[Homes W](#)  
[View thou](#)  
[free!](#)  
[homegain.](#)

[Search A](#)  
[Search fo](#)  
[search fe](#)  
[www.AKH](#)

© Copyright 2007, The Anchorage Daily News, a subsidiary of The McClatchy Company  
[Contact Us](#) | [Newsroom Contacts](#) | [Communication Forms](#) | [Subscriptions](#) | [Advertising](#) | [Ter](#)  
[Daily News Jobs](#) | [RSS Feeds](#) | [ADN Store](#) | [Newspapers In Education](#) | [Privacy Agree](#)

A member of the  
Real Cities Network

# RAIL LINE: Route would cross trails, streams

Continued from J-1

come through here," said Dee Pralle, who, with her husband, Rich, oversees the Willow Area Seniors Inc. housing complex. "Why not embrace change?"

A route hasn't been chosen for the rail line. Borough and railroad officials at the open house gathered comments on the five proposed routes. Two connections on the southern half of the line would send rail cars either east or west of the existing Point MacKenzie Agricultural Area. Three routes on the northern half would branch off the main Alaska Railroad line in Willow, Houston or Big Lake.

The routes range from 28 to 45 miles long. The new line is expected to cost \$300 million, and officials hope to have it built by 2012. Construction financing has not been determined. The borough is cur-

## If you go

The Willow Dog Musher Association is hosting a public question-and-answer session with Matanuska-Susitna Borough and Alaska Railroad officials about the proposed rail line at 7 p.m. Oct. 24 at Willow Community Center, Mile 69 Parts Highway, Willow.

Erin McLarnon, a musher and president of the Willow Dog Musher's Association, said her group hasn't taken a stand on the issue. Musher's are waiting until after an Oct. 24 meeting at which borough

and railroad officials have agreed to answer questions about the line, she said. But mushers have a number of concerns, she said.

McLarnon pointed out 10 places where a Willow rail route would cross existing trails and three salmon streams, including Willow Creek, a popular fishing spot.

"The vision for Willow is as kind of a recreational area," said Dean Davidson, a Willow musher and retiree. "We see that as incompatible, running the route through one of the biggest recreational areas in Southcentral. Choose another route."

Find Daily News reporter Rhinda White online at [www.adn.com/contact/rwhite](http://www.adn.com/contact/rwhite) or call 352-6709.

# Willow split in opinions on rail plan

## ECOLOGY VS. ECONOMY: One of five proposed routes to Port MacKenzie would come through town.

By RINDI WHITE  
rwhite@adn.com

ADN 10/15/07

WILLOW — A rail line splitting Willow would sacrifice the area's recreational identity and endanger some of the area's most valuable assets: fishing, wildlife and trail access, some community leaders say.

"It'll destroy this community, that's what it will do," said Linda Oxley, Willow Area Community Organization chairwoman. "It'll make it an industrial community."

Oxley said WACO opposes a Willow route for a rail line the Matanuska-Susitna Borough and Alaska Railroad plan to build to Port MacKenzie in order to transport coal and other natural resources for shipment.

Not everyone in Willow shares Oxley's view. Some who attended an open house Wednesday to gather information about the proposed rail routes said trail users are a vocal minority. Willow could use the economic boost a rail line could bring, they said.

"Progress is necessary," said Larry Jacobson, a Willow salsa maker who's lived in the area for more than a decade.

"I just want to see the railroad

See Page J-7, RAIL LINE

# Proposed rail routes enough to rile residents

10/05/07 Frontiersman

Final public meeting today in Houston

BY JOHN R. MOSES  
*Frontiersman*

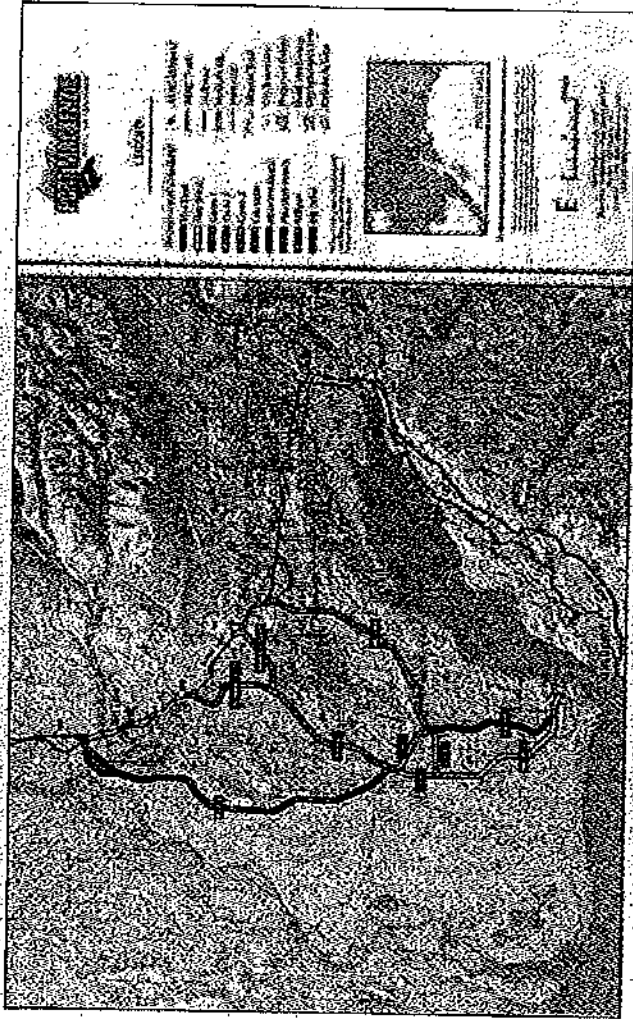
MAT-SU -- Valley residents have one more opportunity to help determine the route of the Alaska Railroad's new spur to Port MacKenzie.

Gatherings in Big Lake, Wasilla, Willow and Knik this week were well-attended, said Patty Sullivan, project spokeswoman for the Mat-Su Borough.

Tonight the venue is 6 to 8 at

Houston Middle School, 12801 Hawk Lane, where consultants will post aerial maps, talk to residents one-on-one or in groups and watch a Power Point presentation detailing the project. There were 120 in the audience Tuesday in Big Lake, and Willow's Wednesday session drew about 90 people, many with concerns about how a new spur would affect recreational trails.

Proposed rail routes are between 28 and 45 miles long and -- as proposed in concept -- and could start in Houston, Big Lake or Willow.



COURTESY ILLUSTRATION  
Possible routes for a new Alaska Railroad Spur to Port MacKenzie are being debated in a series of public meetings around the Mat-Su Valley.

Sullivan said many at Wednesday's Willow meeting expressed concern about area trails and maintaining those connections. Borough Manager John Duffy attended Monday's Wasilla meeting to remind the crowd of about 70 people that the \$300-million project will boost exports of Alaska minerals and help the state and

Borough economies. "Our country is a net importer of cement," Duffy said. "If this rail link is built we

See RAIL, Page A14



## RAIL

*Continued from Page A1*

could end up being the supplier for 5 percent of our nation's cement."

Duffy was also impressed by the Big Lake turnout and the level of interest he observed in Willow.

"We received applause when our presentation was over," he said of the Willow crowd. "There wasn't any consensus, but that wasn't the purpose of the meeting."

The purpose was to gather information, he said.

The meetings are part of a necessary environmental assessment phase of a joint project between the railroad and the Borough. The Borough and railroad agreed in June to work together on creating a new line and the state kicked in \$10 million for the study. The environmental report and the route plans require federal approval from the Surface Transportation Board.

Railroad project manager Brian Lindamood expects to present a route proposal by the end of December. A separate federal process will go forward and the plans could receive approval as soon as 2009. The railroad and Borough hope trains can roll along the new spur — whatever its route — by 2012.

The project will not be affected by the Agrium decision to close its Kenal fertilizer facility, railroad and Borough officials have said. That plant, if converted from natural gas, would be a major consumer of coal expected to roll down the tracks from Interior producers and shipped through Port MacKenzie.

MSB and ARKC signed an agreement in June to work together on the Port MacKenzie Rail Extension Project. Construction of a new rail line requires federal approval.

Lindamood tells each audience the rail corridors are concepts, and the project will try to avoid or minimize direct impacts to property.

Sullivan said the meetings have been helpful for consultants, one of whom told her, "Every night I hear something I wasn't aware of."

In Willow, one activist offered to take a large map to seniors and others who could not make Wednesday's meeting. People can make notes on maps the Borough brings to these sessions to denote the locations of trails, runways and other land uses.

More information is available on the project's Web site, [www.pointmacrail.com](http://www.pointmacrail.com).

Contact John R. Moses at 352-2270 or [john.moses@frontiersman.com](mailto:john.moses@frontiersman.com).

*(This page intentionally left blank.)*

## Houston wants Borough to make tracks

*By John R. Moses  
Frontiersman*

HOUSTON — Newly elected City Councillman Roger Purcell told Matanuska-Susitna Borough and Alaska Railroad Corp. officials there is interest among members of the council in attracting a rail spur through Houston that could lead to development of a manufacturing business.

Purcell said the city was approached by constituents and a company that wants a loading dock. Purcell said Houston will look into getting a rail connection if such a facility and a depot were included.

Purcell's comments came at the last of a series of meetings held by planners of the \$300 million Port MacKenzie line rail extension, and was good news to those Willow residents who don't want the line but support Houston's efforts to get the line for themselves.

Outgoing Houston Mayor Dale Adams, who is being replaced by Purcell, said Houston has been working to get a rail link for "10 to 15 years."

Adams said he has talked with some firms. He said a depot is less important than the proximity of a rail line to make Houston more attractive to some type of industry.

There are three proposed routes, one starting in Big Lake, one in Houston and one in Willow. Project officials rejected the results of a 2003 study that chose a route starting in Willow as the best choice.

Planning consultants who ran the open houses gathered information so they could gauge community concerns and learn more about the existing land uses on proposed rail routes and each route's potential impacts on communities and recreational trails.

The rail line will be between 28 and 45 miles in length and is scheduled for completion by 2012. It is anticipated by Borough project executive Joe Perkins that the rail line will spur state economic growth similar to the impact of George Parks Highway, completed 40 years ago. He cited the need to bring in equipment for construction projects, including the natural gas pipeline, should that go through.

Project spokesperson Patty Sullivan said as many as 400 people may have gone through the five open houses, and 373 people signed in on paper. She said a comment summary document is being prepared and could be ready in time for the meeting with the Willow Dog Musers Association on Oct. 24 at 7 p.m. in the Willow Community Center. Snowmachine clubs have been invited and the general public is also welcome, Sullivan said.

Out-of-area residents who own property in the Borough and could not attend any of last week's open houses may submit comments through a Web site, [www.portmacrail.com](http://www.portmacrail.com).

Once a recommendation is made to the federal government, its Surface Transportation Board will study the proposed route and begin a separate review process that will include public comment. That board will approve the route and provide approved contractors who may be hired to build the line.

Contact John R. Moses at [john.moses@frontiersman.com](mailto:john.moses@frontiersman.com) or call 352-2270.

## Houston invites comment on rail spur

This is regarding the story headlined "Houston wants to make tracks" in the Oct. 12 *Frontiersman* from a Houston City Council member.

I would like to extend an invitation to an informal question-and-answer presentation by the Mat-Su Borough for the Port MacKenzie Rail Extension. Nothing has been discussed by this council and we are looking forward to public comment from residents and neighbors of Big Lake. This meeting will be held at 7 p.m. Oct. 25 at Houston City Hall. As a city we are always looking for ways to support our needs — roads, police and fire — and to strengthen the economy by bringing in jobs.

This rail spur may or may not fit in with what we desire, but the only way to find out is to be fully informed. Please consider joining us and voice your opinions now.

This is our opportunity as a city to respond to the needs of a growing state and Borough with respect to our own desires for what happens in our backyards. As council members serving you, please let us hear your questions and concerns in person Thursday, or contact me by e-mail at [getinvolvedmatsu@hotmail.com](mailto:getinvolvedmatsu@hotmail.com). Comments can also be submitted through a Borough Web site, [www.portmacrail.com](http://www.portmacrail.com).

Sandy McDonald

Houston City Council

## Rail link proposal stirs interest from Mat-Su residents

By Margaret Bauman  
*Alaska Journal of Commerce*  
Publication Date: 10/21/07

A proposed \$300 million extension of the Alaska Railroad to Port MacKenzie in the Matanuska-Susitna Borough is still in the early planning stages, but the project has aroused a lot of curiosity.

More than 300 people turned out for community information meetings at Big Lake, Willow, Wasilla, Knik and Houston in late September and early October to ask questions and voice concerns about the proposed project.

Additional meetings are scheduled Oct. 24 with the Willow Dog Musers Association and Oct. 25 with the Houston City Council.

A project team representing the Alaska Railroad Corp. and Mat-Su Borough is putting together a summary of those comments to present to the borough's planning commission on Nov. 5 and the borough assembly on Nov. 20, borough spokeswoman Patty Sullivan said Oct. 11.

The ultimate decision on whether to proceed will lie with the federal Surface Transportation Board, which would be required under the National Environmental Policy Act to do an environmental impact statement on the proposed area, and schedule a series of public hearings on the EIS.

Tim Thompson, spokesman for the Alaska Railroad, said he anticipates having the application in to the Surface Transportation Board by mid-December, and that the board should make its decision on whether to approve the rail extension by late 2008 or early 2009.

Thompson said the railroad would float bonds for the project only if studies show a way to pay for the bonds.

"We are just doing conduit financing," Thompson said. "We don't make any money on the deal. The Alaska Railroad can actually bond, but we need a dedicated revenue stream; we need to know the money is out there to pay back the bond."

The public information sessions and related efforts by the borough are being paid for through a \$10 million appropriation of state funds allocated during the 2007 Alaska Legislature.

The borough hired HDR Alaska Inc. to analyze environmental issues associated with the range of alternatives, and conduct public meetings. TNH-Hanson LLC was hired to develop the rail alternatives and provide conceptual engineering services to the railroad.

The project, which would involve construction of 30 to 45 miles of new rail line, would cost roughly \$4 million to \$8 million per mile, Thompson said. The

three proposed corridors begin in the Point MacKenzie area and range between 28 miles and 45 miles in length, connecting with the railroad's main line at different locations.

Maps showing the routes under consideration are posted on the project Web site at [www.portmacrail.com](http://www.portmacrail.com).

Borough officials would like to see the new rail link in operation by the end of 2011 or the start of 2012, Thompson said.

While some borough residents are reserving judgment until they get more information on the project, businessmen like Roger Purcell of Houston and Iditarod Trail dog mushing veterans Martin Buser and Lynda Plettner, both of Big Lake, are already voicing enthusiasm.

Purcell, president of the Houston Chamber of Commerce, said he sees the rail link as an opportunity for Houston to keep its lifestyle and get the economic development to promote that lifestyle.

"We have some of the most beautiful country in Alaska for living in, but we need industry to come in and reduce the burden of taxes on home owners," Purcell said.

Purcell said a manufacturing firm that is interested in having a loading dock to move out its products has approached the city.

While residents favor economic development, they don't want coal or strip mining, he added.

Buser sees the rail extension as a "long reaching, tremendous benefit" to the state.

"It can be labeled the first major capital transportation project in the state since the opening of the Parks Highway, with as much impact as the Parks Highway," he said. "I will be impacted as a multi-trail user, but I believe the state will be positively impacted even more than a few individuals like me. We are seeing Alaska mature with projects like that. We will be able to dictate more of our future. It will make the state more independent."

Margaret Bauman can be reached at [margie.bauman@alaskajournal.com](mailto:margie.bauman@alaskajournal.com).

Click here to return to story:

[http://alaskajournal.com/stories/102107/tra\\_20071021033.shtml](http://alaskajournal.com/stories/102107/tra_20071021033.shtml)

© The Alaska Journal of Commerce Online

**adn.com**

Anchorage Daily News

Print Page

Close Window

## Rail link proposal stirs interest from Mat-Su residents

By MARGARET BAUMAN, Alaska Journal of Commerce

(Published: October 27, 2007)

ANCHORAGE, Alaska (AP) - A proposed \$300 million extension of the Alaska Railroad to Port MacKenzie in the Matanuska-Susitna Borough is still in the early planning stages, but the project has aroused a lot of curiosity.

More than 300 people turned out for community information meetings at Big Lake, Willow, Wasilla, Knik and Houston in late September and early October to ask questions and voice concerns about the proposed project.

A project team representing the Alaska Railroad Corp. and Mat-Su Borough is putting together a summary of those comments to present to the borough's planning commission on Nov. 5 and the borough assembly on Nov. 20, borough spokeswoman Patty Sullivan said.

The ultimate decision on whether to proceed will lie with the federal Surface Transportation Board, which would be required under the National Environmental Policy Act to do an environmental impact statement on the proposed area, and schedule a series of public hearings on the EIS.

Tim Thompson, spokesman for the Alaska Railroad, said he anticipates having the application in to the Surface Transportation Board by mid-December, and that the board should make its decision on whether to approve the rail extension by late 2008 or early 2009.

Thompson said the railroad would float bonds for the project only if studies show a way to pay for the bonds.

"We are just doing conduit financing," Thompson said. "We don't make any money on the deal. The Alaska Railroad can actually bond, but we need a dedicated revenue stream; we need to know the money is out there to pay back the bond."

The public information sessions and related efforts by the borough are being paid for through a \$10 million appropriation of state funds allocated during the 2007 Alaska Legislature.

The borough hired HDR Alaska Inc. to analyze environmental issues associated with the range of alternatives, and conduct public meetings. TNH-Hanson LLC was hired to develop the rail alternatives and provide conceptual engineering services to the railroad.

The project, which would involve construction of 30 to 45 miles of new rail line, would cost roughly \$4 million to \$8 million per mile, Thompson said. The three proposed corridors begin in the Point MacKenzie area and range between 28 miles and 45 miles in length, connecting with the railroad's main line at different locations.

Maps showing the routes under consideration are posted on the project Web site at <http://www.portmacrail.com>.

Borough officials would like to see the new rail link in operation by the end of 2011 or the start of 2012, Thompson said.

While some borough residents are reserving judgment until they get more information on the project, businessmen like Roger Purcell of Houston and Iditarod Trail dog mushing veterans Martin Buser and Lynda Plettner, both of Big Lake, are already voicing enthusiasm.

Purcell, president of the Houston Chamber of Commerce, said he sees the rail link as an opportunity for Houston to keep its lifestyle and get the economic development to promote that lifestyle.

"We have some of the most beautiful country in Alaska for living in, but we need industry to come in and reduce the burden of taxes on home owners," Purcell said.

Purcell said a manufacturing firm that is interested in having a loading dock to move out its products has approached the city.

While residents favor economic development, they don't want coal or strip mining, he added.

Buser sees the rail extension as a "long reaching, tremendous benefit" to the state.

"It can be labeled the first major capital transportation project in the state since the opening of the Parks Highway, with as much impact as the Parks Highway," he said. "I will be impacted as a multi-trail user, but I believe the state will be positively impacted even more than a few individuals like me. We are seeing Alaska mature with projects like that. We will be able to dictate more of our future. It will make the state more independent."

Print Page

Close Window

Copyright © 2007 The Anchorage Daily News (www.adn.com)



# Houston wants rail guarantees

10/29/07  
ADN

**SPUR:** Line to Port MacKenzie could bring industry with it.

By RINDI WHITE  
rwhite@adn.com

**HOUSTON** - Houston residents would eagerly accept a branch of the Alaska Railroad through their city, but only if it brings jobs and industry to the struggling local economy, some said at a recent meeting.

Houston, with more than 1,500 residents, is growing in size. But city government struggles to pay for road maintenance and other services with the property tax and 2 percent sales tax it collects. The town needs industry, residents say.

Now, the Matanuska-Susitna Borough hopes by 2012 to extend a branch of the Alaska Railroad from the main line, which already divides Houston, to Port MacKenzie.

A branch line could attract industry to Houston, but only if trains stop long enough for goods or passengers to be loaded and unloaded. Some Houston residents want that guarantee before they give the project any support.

However, community support is but one factor in the decision. The federal Surface Transportation Board, and not the railroad nor the borough nor the city, would decide where a line should be built, if at all, said Alaska Railroad special projects manager Brian Lindamood. He said the railroad this week will present the concept for the first time to the Surface Transportation Board. The railroad expects a decision by mid-2009, Lindamood said.

Houston has its roots in rail. In 1917, it was a railroad siding named after congressman William C. Houston of Tennessee. Coal mines in the area fueled Anchorage stoves. But trains no longer stop in Houston and most people who live there commute to jobs elsewhere. Commerce in Houston amounts to some tourism, retail and services but little manufacturing.

"Houston grew up as a railroad town. I don't think we should give up our history just to let a railroad go through. But we need some guarantees to make it beneficial to all of us," said newly elected City

## onLine

Comments about the plan to build a rail line to Port MacKenzie will be accepted until Nov. 2. Learn more about project at

[www.portmacrail.com](http://www.portmacrail.com)

Councilman Roger Purcell.

The City Council has taken no stance on the project. But Purcell said he wants promises of a railroad siding and loading dock in place before Houston agrees to the line.

Purcell isn't alone. Rick Dille, owner of Alaska Cozy Coal and Furnace, said he'd like to get coal shipments by rail instead of trucking the coal from Healy himself.

Dille is a Usibelli coal distributor for the Valley and Anchorage.

A former North Slope worker, Dille three years ago started his coal-furnace and coal-supply business because he got sick of paying high fuel oil bills.

He sold 12 tons of coal the first year. This year, he sold 72 tons.

"I don't want to see the railroad come through Houston and just cycle right through. I want to see industrial development in Houston. I want Houston to be the next star. It's our turn," Dille said.

Vaughn Nadeau, manager of Spenard Builders Supply's Big Lake truss manufacturing plant, said SBS might benefit from a local railroad siding as well.

The Big Lake plant, which is within Houston city limits, shipped about \$1.5 million worth of trusses to Fairbanks this year, Nadeau said, and more were shipped out of the Anchorage port to Bush communities.

Lindamood said it's far too early in the planning process to make promises to Houston, or to any other community.

"We're trying to figure out if the route goes to Willow, to Houston or to Big Lake," Lindamood said.

The Alaska Railroad and the borough are wrapping up an analysis of several routes from the main line to the borough-owned Port MacKenzie. A rail line is key to making the port profitable.

Find Daily News reporter Rindi White online at [www.adn.com/contact/white](http://www.adn.com/contact/white) or call 1-907-352-6709.

10/30/07

# Houston on board early with rail plan

## Borough, railroad in early stages of proposed spur line

BY JOHN R. MOSES  
*Frontiersman*

HOUSTON — A new rail line to Port MacKenzie is welcome in Houston, as long as it stops there and helps build industry. That's the message the Mat-Su Borough and railroad are receiving from some on Houston City Council. Others say trains should roll through even without a depot.

Borough and railroad officials said it's natural for a town or city to want new services. The problem with the Port MacKenzie spur line is it's so early in the planning process that no meaningful negotiations can happen. No one yet knows where the federally approved rail route will begin. The tracks will run a course between 25 and 40 miles long, depending on where the line starts, which could be in Big Lake, Willow or Houston.

"First, they've got to decide the route," Borough Economic Development Director Dave Hanson said.

It isn't unreasonable for a city like Houston to want transportation options like a railroad depot. Wherever the tracks

# RAIL

*Continued from Page A1*

wind up going, project planners will work with the affected communities, Hanson said. There was no strong public opposition to the rail line at a recent Houston City Council meeting, but some want more than just new tracks and a fast-moving train.

Councilwoman Sandy McDonald said the council will likely revisit the issue when it meets again Nov. 15.

No council vote has been taken, but members have their own opinions. McDonald said she agrees with newly elected Councilman Roger Purcell that there should be a siding and/or a depot in Houston.

"If it comes through here we would like it to benefit the city," she said.

One obstacle to any deal-making is uncertainty facing even railroad planners. The final route plans will be decided in December and then submitted to federal authorities.

"At this point the railroad's not ready to bargain," McDon-

ald said.

Once a route is decided locally it still must be approved by the federal Surface Transportation Board, which is likely to OK the chosen route, she said. That federal board also investigates the proposed route through an environmental impact process that is open to the public and — if the route is approved — gives the railroad and Borough a list of contractors to choose from for construction.

Houston Councilwoman Carla Hendrix said she's in favor of having the new line go through the city, even if there is no guarantee of a depot or siding. "My initial reaction is, hey, bring it on."

Without a route, Hendrix understands why there can't be guarantees, and she thinks if Houston is chosen the city can work with the railroad.

And if the railroad refuses a depot or siding as part of the deal?

"There's always the future," Hendrix said.

The \$300 million rail project is scheduled for completion in 2012.

*See RAIL, Page A14*

# OPINION

MANAGING EDITOR: GREG JOHNSON PHONE: 352-2268 FAX: 352-2276 E-MAIL: [gjohnson@portmacenzie.com](mailto:gjohnson@portmacenzie.com) 10/30/07

## FRONTIERSMAN EDITORIAL

# *Houston needs follow-through with rail desires*

Plans to build a railroad spur to Point MacKenzie are chugging along, and it's time for interested communities to get aboard.

The rail route will be anywhere from 25 to 40 miles long, depending on whether it starts in Houston, Willow or Big Lake. While Willow representatives have spurned the spur with a lukewarm reception, some Houston city leaders are waving flags in the hope the train will stop in their community, complete with a depot.

While it is too early to put details to the new rail line plans, it isn't too early for an interested community like Houston to conceptualize how the railroad could benefit its community. City council members are already lobbying for the Borough and Alaska Railroad to seat the spur there and are eager for the advantages a rail line originating in Houston could bring.

Some businesses could benefit greatly from having rail access. More importantly, city officials believe rail could be just the ticket Houston needs to stoke its boilers as a viable place to locate new business and industry. They may be on the right track. We are certain the wheels are spinning among some leaders in Houston as they consider the potential.

What Houston needs now is to be more proactive in luring the rail spur. Don't stop at simply saying, "Hey, over here!" Develop a rail plan. If economic development is the key to Houston's success, invest in an economic development coordinator to help draft a plan that specifically spells out how Houston and the Mat-Su Borough could use rail access and a depot as a vehicle to chug more industry and wealth into Houston and the Valley.

Houston may want to be Alaska's version of "The Little Engine That Could," but will need more than an "I think I can" approach to make believers out of the Borough and railroad.

The council expects to examine the railroad issue again at its Nov. 15 meeting. Perhaps by that time, it will be ready to assign to committee or other body some of the preliminary work of pushing Houston's agenda in regard to the railroad. While final plans aren't expected until December, Houston can influence the process by putting initiative to its interest.

The same is true for residents of Big Lake or Willow (although meetings in Willow indicate that community would prefer the rail locate elsewhere). Perhaps by starting in Houston and running a route favorable to Big Lake, the railroad spur could aid both communities and still reach its destination in Point MacKenzie.

<<Back



### Valley train could bring economic growth



Linda Oxley (Kris Riley/KTUU-TV)

by Lori Tipton  
Wednesday, Nov. 21, 2007

PALMER, Alaska -- Officials with the Matanuska-Susitna Borough envision a new rail system for the Valley.

The high-speed freight train would begin in Point MacKenzie, although the extension's route is still in the works.

New information about the proposed rail extension was presented at last night's borough Assembly meeting.



New information about the proposed rail extension was presented at last night's borough Assembly meeting. (Kris Riley/KTUU-TV)

Joe Perkins, the borough's executive project manager, gave an overview of the project and the strengths and weaknesses of the eight proposed routes.

Perkins says community feedback shows strong support for the project although exactly where the railroad should go is a topic of debate.

Many residents in Willow and Big Lake say they see the need a railroad extension but don't want it running through their communities.

The Houston community, though, is fully backing the project. Residents there say they want an extension, which they believe would promote economic growth.



Cindy Bettine (Kris Riley/KTUU-TV)

"We got all the great things in Houston to do for the residents, but we also have to have some sort of economic development to get people to want to live here, and to stay here and raise their families," Houston City Council member Roger Purcell said. "This would be a great start."

Still opposition to the project exists.

"There certainly are some folks that have been in support of the rail spur coming through there, but the majority are opposed to it," Linda Oxley of the Willow Action Community Organization. "That's the position we have taken."

The information provided at last night's Assembly meeting will be submitted to the Surface Transportation Board, or STB, which ultimately approves where the route will go.

Assembly member Cindy Bettine questioned whether the project included information about potential growth in the outlined areas.

Perkins says they only focused on environmental impacts and the engineering.

Bettine asked that an Assembly resolution expressing the Assembly's concerns be added to the application before being submitted to the STB.

After the application is submitted, transportation officials will select a third-party contractor. An

environmental study will be conducted before announcing which route will be chosen.

The project will cost about \$300 million, depending on which routes are chosen. Borough officials did not discuss how the extension costs would be covered.

Perkins says the route could be chosen in the next several months or longer.

Construction of the rail extension is not scheduled to begin until mid 2009.

Contact Lori Tipton at [lтиpton@ktuu.com](mailto:lтиpton@ktuu.com)



All content © Copyright 2000 - 2008 WorldNow and KTUU. All Rights Reserved.  
For more information on this site, please read our [Privacy Policy](#) and [Terms of Service](#).

*(This page intentionally left blank.)*

# Houston's fire chief resigns, deputy clerk quits

BY JOHN R. MOSES

Frontiersman

**HOUSTON** — Anyone hoping last month's election for new council members would calm the climate at Houston City Hall can keep on hoping. The deputy city clerk quit Friday following a meeting with the new city's new mayor. The fire chief's resignation becomes effective at the end of this year.

Fire Chief John Hodge credited a heavy-handed "belittle and berate" management style by new Mayor Steve Frost with spurring his departure.

Former Deputy Clerk Cat Bullington said Monday she resigned in part because she felt she was being used as a political pawn in a power game between the mayor and a council member. Adding to the complexity is the 20-year personal relationship

between Hodge and Bullington.

Frost said the departure of Bullington was a surprise and both resignations can be attributed to an especially rough time in Houston politics.

"I'd like to figure out what's going on too," Frost said of the upheaval that has marked Houston's politics of late.

Bullington filed a two-page complaint against city council member Roger Purcell last month covering many issues, including fears for her safety because the councilman has a concealed weapon carry permit. On Monday, Bullington circulated by hand and e-mail a letter to Houston residents detailing her reasons for leaving, including the mayor's opposition to her taking time off while most of the council was at the Alaska Municipal League conference in Fairbanks last week. Purcell and a political ally were the only council members in town.

Houston City Attorney Michael Gatti of Anchorage would not discuss the contents of Bullington's complaint nor other issues of a personal or personnel nature. Gatti chalked up many of Houston's recent council troubles to "growing pains" faced by being a city in the middle of a changing area.

Houston has faced its share of challenges, especially in its attempts to develop its local economy. Houston is a city of about 1,600 that last year lost its chance to be home to the new Mat-Su prison.

Council is presently considering officially pushing Houston as the start of a rail spur the Borough and Alaska Railroad Corp. will build to Port MacKenzie. Frost said he hopes to bring the rail issue before the council as soon as Dec. 13. Some Houston officials hope for a siding to attract a man-

ufacturing plant and a depot for future tourism if the railroad and Borough will go along. Some, like Frost, say the spur should go through Houston — even without a siding or depot — for the good of the state's economy.

Faces were changing on the seven-member council, which elects a mayor from among its members, even prior to the October election. Two council members resigned last year and one left after it was discovered he had been living out of town.

A spirited political fight for council seats culminated this October with the newly elected Purcell beating out a former mayor. Purcell then tried unsuccessfully to become mayor. Purcell and Frost are political opponents with differing visions of how the city should operate.

Like the city attorney, Purcell said growing pains are a big part of Hous-

ton's problems. He prescribes a solid dose of municipal action to get the city on the right track.

"We're kind of tired of the Borough pushing us around," Purcell said.

Purcell said the city has to move faster to solve problems like safe bus routes for kids and making sure the Department of Transportation doesn't create a Parks Highway widening route that will bypass Houston businesses.

While Purcell and Bullington have differences, Purcell said he understands why Bullington's meeting with the mayor might not have gone well. "I've been on the receiving end of Mr. Frost's outbursts," he said.

Council member Rosemary Burnett, a local businesswoman who has served six years on the council, said she is also part of a city investigation into Bulling-

See HOUSTON, Page A11

*(This page intentionally left blank.)*



## Feds weigh in on impacts of planned rail spur

*By John R. Moses  
Frontiersman*

PALMER — As the Mat-Su Borough continues to debate a new rail spur line to Port MacKenzie, federal agencies are weighing in on the potential environmental impacts of the project.

The Borough assembly has examined several proposed routes to Port MacKenzie, including ones through Willow, Big Lake and Houston. Borough and railroad officials have said the new \$300 million line would provide direct rail access for companies that export resources from the Interior by 2010 under current plans.

“We all support this high-speed freight train to our port as a statewide legacy project,” assembly member Cindy Bettine said. “I think this assembly needs to have input and time to evaluate the matrix.”

The matrix to which Bettini referred is a table comparing proposed rail routes and connectors by considering 10 factors, which include:

- soil quality,
- need for new road crossings,
- whether land is available for acquisition,
- kinds of development in the area,
- designated land uses,
- impacts on wetlands and mapped anadromous fish streams,
- potential for archaeological sites in the area,
- fragmentation of designated recreation areas or refuges.

The last issue weighs heavily on the minds of state and federal agencies charged with protecting such land. The assembly has set a workshop date for Dec. 11. An agenda for that meeting has not yet been published.

Noting the amount of flexibility the railroad has in picking a route, “the expectation is that many of the project impacts will be avoidable,” says Aquatic Resources Unit manager Michael Szerlog of the U.S. Environmental Protection Agency in a written assessment. Given the potential economic returns from the new rail line the EPA further states that anything feasible from an engineering standpoint to avoid impacts to the environment should be done.

Szerlog advises the EPA feels stream crossings should not impede fish or the natural functioning of bodies of water. He also warns of impacts to wetlands and suggests bridges and elevated portions of the rail line might best avoid impacting sensitive areas.

The U.S. Department of the Interior’s Fish and Wildlife Service has also weighed in with concerns about potential impacts on bald eagle populations and the potential for fragmentation of high-value, previously undisturbed habitat that could result from rail development.

Fish and Game Field Supervisor Ann Rappaport has told the Borough and railroad to begin planning

mitigations for the extra development that can be expected around a railroad line, including expanded port facilities, as new development affects migratory birds, other wildlife and the habitat in general. Each alternative route should have in its development a study that quantifies and defines existing wildlife populations and makes plans to "avoid, mitigate and compensate" for those impacts, Rappaport says in her report to the Borough.

A 2003 rail corridor study estimates the project could destroy up to 294 acres of wetlands due to a wider right-of-way than currently proposed. Even a lesser fraction of that total would be a "substantial" loss, EPA manager Szerlog says.

Szerlog's recommendation is that wetlands be avoided and floodplains and streams be bridged to protect existing drainage and water quality.

"Economic development can occur in a responsible manner that is protective of the rich natural heritage enjoyed by residents of the Matanuska-Susitna Borough," he says in his recommendation.

The Borough also has strong input from the state's Department of Natural Resources, which reports it would advocate for archaeological studies of any final route due to the lack of surveys done in that region and the likelihood that there may be sites of cultural significance.

Also, State Historic Preservation Officer Judith E. Bittner has noted that all of the routes intersect with the Iditarod Trail, designated by Congress in 1978 as a significant transportation route.

Project planners said public workshops in Willow, Wasilla, Big Lake and Houston provided invaluable information. Planners also met with the Willow Dog Musers Association, the Iditarod Trail Committee's executive director, state and federal agencies, and large landowners, including CIRI Corp. Planners gathered 269 individual public comments.

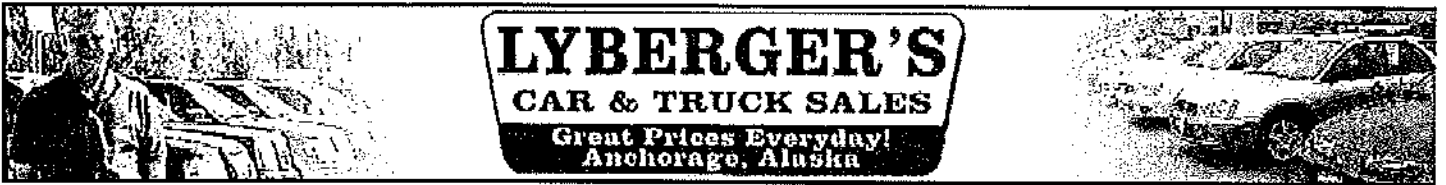
"The public pointed out a lot of things that weren't really evident," said project planner Brian Lindamood of the Alaska Railroad Corp., the Borough's business partner in the rail spur. "They provided depth in certain areas, such as soils issues near Horse Shoe Lake and land use such as the snowmachine trails and access to recreational areas."

"The reason we are doing this is to provide information to the people who will be preparing the [environmental impact study]. The more information, the better off we are," says Joe Perkins, project consultant for the Borough as well as a former state Department of Transportation Commissioner, in a report to the Borough. "We provided Alaska-specific information that we thought they needed to save them time."

The federal Surface Transportation Board ultimately must OK the final route. The STB will hold its own public process as part of the environmental evaluation under the National Environmental Policy Act. The Surface Transportation Board could reach a decision in mid-2009, Borough spokesperson Patty Sullivan said.

The Borough remains fully behind the project and has so far spent \$10 million in state money for impact studies.

The project is detailed at [www.portmacrail.com](http://www.portmacrail.com).



# Anchorage Daily News



**Holiday lights map**  
Post a photo of your lights to our map and plot out the best tour.

Subscribe | manage subscription | E-Edition

Search in **All** for find >>  
**News Sports Outdoors Features Money Entertainment Opinion Blog**

**Home** | Alaska Newsreader | Obituaries | Archives Movies | Music | Restaurants | Submit an Event **Deals**

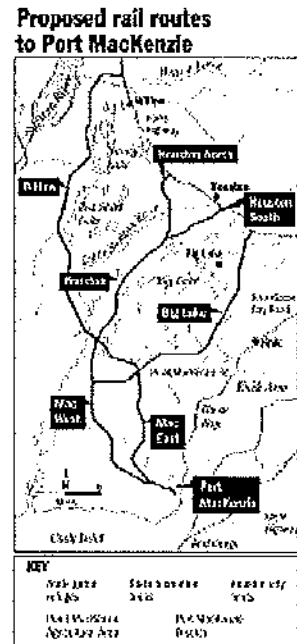
## South route for rail line to port rated highest

### HOUSTON TO PORT MACKENZIE: Federal board will make final pick.

By RINDI WHITE  
rwhite@adn.com

Published: November 28, 2007  
Last Modified: November 30, 2007 at 02:13 AM

**PALMER** -- A rail route branching off the Alaska Railroad line from Houston and running south to Port MacKenzie got the highest marks among eight possible routes studied in a recently completed review. But members of a team evaluating the potential rail lines are avoiding calling it the preferred route.



Click to enlarge

The so-called Houston South route crossed fewer wetlands, had less developed land nearby, crossed fewer salmon-spawning streams and split fewer designated refuges or recreation areas than other routes studied. But the evaluation team refrained from calling it a preferred route because that decision is up to the Surface Transportation Board, a federal board tasked with overseeing railroad activity. The board is expected to choose a route in the next 18 months.

A team of borough and railroad officials worked with contractors HDR Alaska Inc. and TNH-Hanson LLC to review the proposed routes for the project, which the Matanuska-Susitna Borough and Alaska Railroad want to build by 2012. The team also compiled environmental studies, engineering data and cost estimates for the line.

A \$10 million state appropriation paid for the preliminary studies done this year and environmental work scheduled for next year.

Borough officials believe the route will spur development at the borough-owned Port MacKenzie by making it easier to ship natural resources like coal and limestone from Interior Alaska.

The group looked at four northern routes connecting to the existing

#### More mat-su stories >>

- Mat-Su datebook
- Winter activities
- In brief
- Scanner
- Couple loses farm, gains old Kentucky home



Adv:

Sear

Top Ren

**Foxwor**  
South An  
garage p  
fireplace  
dining ne

Comment on this story  
E-mail a friend  
Print  
Digg this  
Seed Newsvine  
Send link via AIM

Font size : A | A | A

Alaska Railroad line between Big Lake and Willow. Two southern route options, one east and one west of the Point MacKenzie Agricultural Area, were also studied. The southern and northern route options together produce eight possible paths for the proposed line.

The group also compiled cost estimates, which ranged from \$220 million for a route that runs west of Point MacKenzie farmland and connects with the Alaska Rail line about a mile and a half north of Houston, to \$330 million for a route that runs east of the farmland and connects with an existing rail line north of Willow Creek.

The highest-ranked Houston South route had the second-lowest cost at an estimated \$230 million. Borough and railroad officials are discussing how to finance the 28- to 45-mile rail line.

The team studied soils and wetlands along each of the routes, how many new road crossings each would require, how much developed land or land designated for parks, refuges, residential or agricultural uses would be crossed, as well as which sites crossed salmon-spawning streams and were likely to cross archaeological sites. To see the routes and evaluation matrix, go online to [www.portmacrail.com](http://www.portmacrail.com).

### Comments

Please **log in** to post comments.

Top Aut



Search

Best De

Pets & F  
Find pup  
and serv

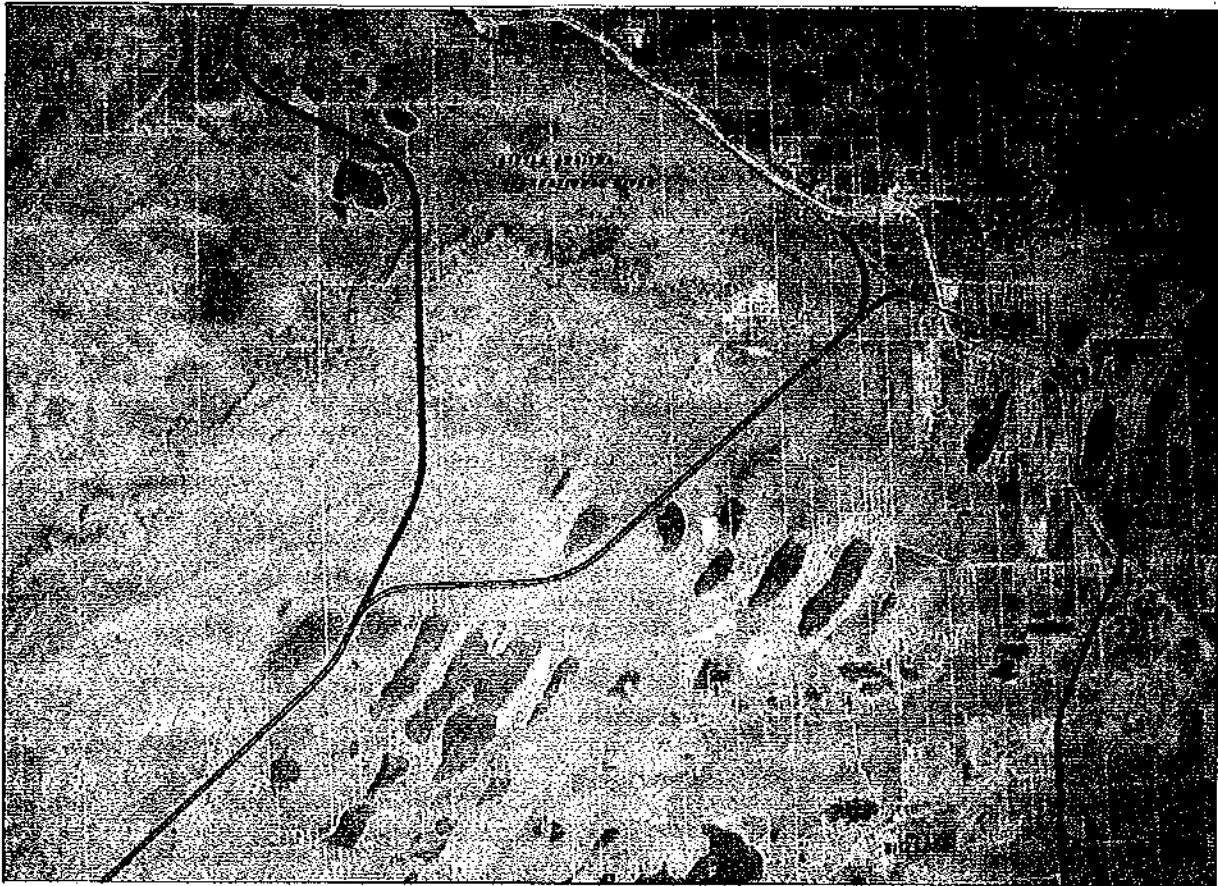
Other Tr  
Find grea  
ATV's, wa

Merchan  
Antiques,  
Find deal  
More...

More gre  
Advar

© Copyright 2007, The Anchorage Daily News, a subsidiary of The McClatchy Company  
Contact Us | Newsroom Contacts | Communication Forms | Subscriptions | Advertising | Terms  
Daily News Jobs | RSS Feeds | ADN Store | Newspapers in Education | Privacy Agreement

A member of the  
Real Cities Network



Two of three potential routes for a new rail spur line to Port MacKenzie fall within the project's projected budget of \$300 million. The longest route to Willow, far left, is estimated at well over budget. The other possibilities are near Houston, marked in the center, and Big Lake, right.

## Houston may be ahead for rail route

BY JOHN R. MOSES  
*Frontiersman*

HOUSTON — Some city council members here are pleased a rail extension route to Port MacKenzie highlighted by a project planning document runs right through the Houston/Big Lake area.

Mayor Steve Frost said an upcoming city council work session 5 p.m. Monday, Dec. 3 may result in a draft resolution supporting that route — one he said he hopes council will approve when it meets Dec. 13.

Monday's meeting will be to discuss capital improvement projects and the proposed route, which runs tracks near Horseshoe Lake and other lakes and travels through a largely undeveloped area, Frost said.

The Borough Assembly will hold a workshop examining the planning matrix and proposed routes at 3 p.m. Dec. 11 in the Borough Assembly Chambers in Palmer.

Federal agencies recently weighed in with concerns about the rail routes, especially a longer route from Willow, which would heavily impact trails and wetlands. Environmental and wildlife experts warned the railroad and Borough to avoid damaging wetlands and spend the extra cash to prop-

Frost said the Houston line is a lower-cost route than others, so as far as environmental and other mitigation he hopes "a little extra money to do that probably won't be an issue."

The \$300 million extension project's findings are due for submission soon to the federal Surface Transportation Board. That panel will conduct another round of public studies through 2008 before choosing a final route and a list of qualified contractors for a bid process. That decision isn't expected until mid-2009.

Rail spur lines starting in Houston, Willow and Big Lake are all on the table, but a planning matrix presented to the Borough Assembly recently studies eight possible configurations and ranked them by cost and potential environmental and other impacts.

The lowest-cost alternative is the Mac West Houston South route, which is estimated to cost \$220 million to build. The planning chart indicates soils along the route are good, there is little impact to road crossings, land is available and there would be a neutral impact on fish streams. While the route is not seen as one that would fragment designated refuges or recreation areas,

sites being impacted.

The other Houston approaches have \$230 million and \$290 million price tags. Starting the rail in Willow, where the community council and many residents actively oppose the route, is estimated to cost \$320 million, \$20 million more than the current construction estimate in 2007 dollars. Those estimates also don't include an extra \$10 million for track at Port MacKenzie.

Borough spokeswoman Patty Sullivan, the point person for the rail extension project, said no route has been chosen and the ultimate decision is up to the government.

The matrix was designed after many meetings among experts and consultants hired by the Borough and Alaska Railroad Corp. The process has consumed about \$1 million of a \$10 million state grant allocated for environmental studies of the new spur line.

The Houston City Council meets at 7 p.m. Dec. 13. The agenda for that meeting is not set. The Dec. 3 council work session on capital improvements and the proposed rail route begins at 5 p.m. Both meetings are in Houston City Hall's Council Chambers, 13965 Armstrong Road.

# Rail study off to feds

## Borough now waits for decision on route of new spur line

BY CHRIS GILLOW  
*Frontier*

**MAT-SU** — With the Port MacKenzie Rail Extension Project nearing federal review, the Matanuska-Susitna Borough is concentrating on the details.

The Borough Assembly has passed a resolution outlining several issues it hopes the Surface Transportation Board (STB) — a federal board that will approve a new spur line to the port — will consider in its review of the \$220 million to \$330 million project. The route will be decided by STB, likely sometime in 2009, with completion projected in 2012.

Borough officials are asking the STB to examine the social, economic, and environmental impacts of three proposed routes for the new rail line. The Borough and Alaska Railroad Company have been studying routes that would run the new line to Willow, Houston or Big Lake. Reaction has been mixed from groups in Willow. While Houston City Council has lobbied hard for the railroad, the Borough has not selected a preferred route.

With research showing projected growth along the Parks Highway in the Willow area, Borough Assembly Member Cindy Betine said it is important to consider how residential and commercial development would be affected by the rail. In addition to impact on future development, the resolution outlines several environmental issues the Borough is recommending for further review, including

- how a rail line would affect drainage, particularly in the Big Lake Watershed and the Little Susitna River, and
- how the rail would affect recreation, agriculture and fish and wildlife along the proposed routes.

Along with the ordinance, the STB will receive the Port MacKenzie Rail Extension Project Alternatives Comparison —

See FEEDS, Page A8

*Frontier*

PAGE A8 SUNDAY, DECEMBER 16, 2007

## FEEDS

*Continued from Page A1*

a report compiled by the Borough, railroad and two private consultants, said Patry Sullivan, public affairs director for the Mat-Su Borough. More than 18 project team members worked on the report for about five months analyzing the proposed routes to identify the strengths and weaknesses of each.

"What this study did was to give [the STB] a starting place," Sullivan said. Now that the project is up for federal review, Sullivan said the report will help ensure the STB has information from the level that might not be available otherwise. For example, the report makes note of recreation areas that could be affected by the rail line.

Although the report addresses many local issues, both Sullivan and Betine believe the rail spur will have an overall positive impact

statewide, whichever route is finally chosen. With a large portion of the state's valuable mineral resources located in northern Alaska, the rail line is expected to be an instrumental part in the transportation of resources from the Interior to local shipping lanes.

"My position is that this is a legacy project," Betine said. "There hasn't been anything like this since the construction of the Parks Highway."

The Willow and Big Lake chambers of commerce passed resolutions favoring the Willow route, while the Willow Area Community Organization and trails groups opposed to it.

Although the Borough and ARRC have outlined three possible routes and the positives and negatives of each, the STB will ultimately decide the future of the rail spur, Betine said.

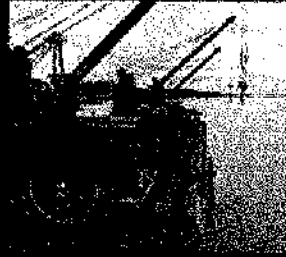
The federal board will hire a consultant to conduct an environmental impact statement and base its approval on that and other supporting material. A decision by the STB is not expected until mid-2009 or later.

# PROJECT WEBSITE

*(This page intentionally left blank.)*



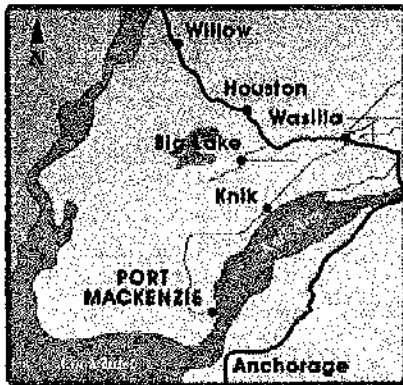
# PORT MACKENZIE RAIL EXTENSION



[Project Overview](#) | [FAQs](#) | [Get Involved](#) | [Project Library](#) | [Useful Links](#)

## Welcome

[Alternatives Map](#)



Matanuska-Susitna Borough Assembly Special Meeting materials have been added to the Project Library

### Welcome to the Port MacKenzie Rail Extension Project.

The Matanuska-Susitna Borough and the Alaska Railroad Corporation propose to build and operate a rail line connecting the Borough's Port MacKenzie to the existing ARRC rail system. The proposed rail line would provide freight and potentially passenger services between the Port and Interior Alaska.

Here you'll find up-to-date information on the project, the [alternatives map](#) which shows where the proposed line could extend, how you can [comment](#), answers to [frequently asked questions](#), and more information about the proposed extension.



### What's Happening Now?

The project team is evaluating whether the top alignments from a [previous rail corridor study](#) conducted in 2003 are still the best alternatives for consideration. This review and information-gathering phase, which includes preliminary engineering studies, preliminary environmental studies, and opportunities for public involvement, will be conducted through December 2007. The information collected during this phase will support the development of an environmental document, which will also include public involvement opportunities. Read about the background of the proposed rail extension on the [Project History](#) page.

Comments received through November 2, 2007, were considered in analyzing the alignments and preparing the project application to the Surface Transportation Board. Comments received after this deadline will be forwarded to the STB.

[Print This Page](#)

# PORT MACKENZIE RAIL EXTENSION

[CONTACT US](#)

[Home](#) | [Project Overview](#) | [FAQs](#) | [Get Involved](#) | [Project Library](#) | [Useful Links](#)

## Get Involved

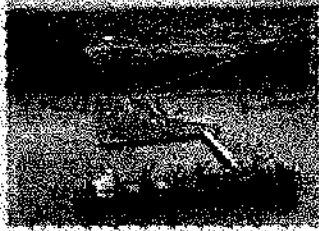
[Print Page](#) > [Home](#) > [Get Involved](#) > [Contact Us](#)

[Attend a Meeting](#)

[Submit a Comment](#)

[Join the Mailing List](#)

[Contact Us](#)



### Contact Us

Project questions or comments? Email us at: [info@portmacrail.com](mailto:info@portmacrail.com)

**Patty Sullivan, Public Affairs Director**

Matanuska-Susitna Borough

Office of Public Affairs

350 East Dahlia Avenue

Palmer, Alaska 99645

Phone: (907)745-9577

E-mail: [patty.sullivan@matsugov.us](mailto:patty.sullivan@matsugov.us)



**Stephenie Wheeler, Public Involvement Officer**

Alaska Railroad Corporation

327 West Ship Creek Avenue

Anchorage, Alaska 99501

Phone: (907) 265-2671

E-mail: [wheelers@akrr.com](mailto:wheelers@akrr.com)

[Web Master & Design - Homestead Graphics](#)

[HOME](#) | [PROJECT OVERVIEW](#) | [FAQS](#) | [GET INVOLVED](#) | [PROJECT LIBRARY](#) | [USEFUL LINKS](#) | [CONTACT US](#)

# PORT MACKENZIE RAIL EXTENSION

[CONTACT US](#)

[Home](#) | [Project Overview](#) | [FAQs](#) | [Get Involved](#) | [Project Library](#) | [Useful Links](#)

## Project Overview

[Print Page](#) > [Home](#) > [Project Overview](#)

[Project History](#)

[Project Benefits](#)

[Project Area](#)

[Project Participants](#)

[Issues](#)

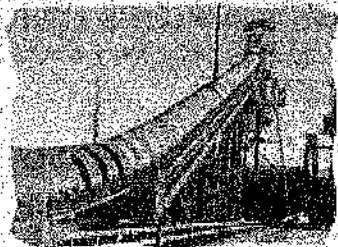
[Cost and Funding](#)

[Terms and Definitions](#)

[NEPA Process](#)

Use the links at left to learn about the Port MacKenzie Rail Extension Project.

You will find information about the [history](#) of the project, the [alternatives map](#) which shows where the proposed line could extend, [potential project benefits](#) and [costs](#). Please feel free to [contact us](#) with any questions or comments you may have.



[HOME](#) | [PROJECT OVERVIEW](#) | [FAQS](#) | [GET INVOLVED](#) | [PROJECT LIBRARY](#) | [USEFUL LINKS](#) | [CONTACT US](#)

# PORT MACKENZIE RAIL EXTENSION

[CONTACT US](#)



[Home](#) | [Project Overview](#) | [FAQs](#) | [Get Involved](#) | [Project Library](#) | [Useful Links](#)

## Project Overview

[Print Page](#) > [Home](#) > [Project Overview](#) > [Project History](#)

[Project History](#)

[Project Benefits](#)

[Project Area](#)

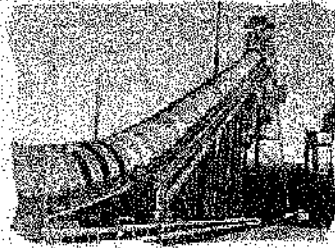
[Project Participants](#)

[Issues](#)

[Cost and Funding](#)

[Terms and Definitions](#)

[NEPA Process](#)



### Project History

A rail line serving the Port MacKenzie area has been considered since the early 1970s. In 2003, consultants TNH conducted a rail corridor study, which developed various alignment alternatives, at the request of the Matanuska-Susitna Borough.

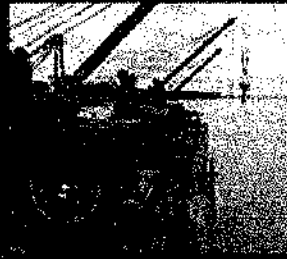
While the 2003 report recommended a specific alternative for further consideration, the current project will reconsider some of the previous feasible alignments and possibly develop new alternatives for consideration.

Information gathered during the current phase will provide an up-to-date report on conditions and public opinion, which will help focus the development of an environmental document by the [Surface Transportation Board \(STB\)](#).



[HOME](#) [PROJECT OVERVIEW](#) [FAQS](#) [GET INVOLVED](#) [PROJECT LIBRARY](#) [USEFUL LINKS](#) [CONTACT US](#)

# PORT MACKENZIE RAIL EXTENSION



[Home](#) | [Project Overview](#) | [FAQs](#) | [Get Involved](#) | [Project Library](#) | [Useful Links](#)

## Project Overview

[Print Page](#) > [Home](#) > [Project Overview](#) > [Project Benefits](#)

[Project History](#)

[Project Benefits](#)

[Project Area](#)

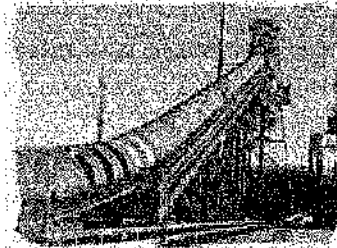
[Project Participants](#)

[Issues](#)

[Cost and Funding](#)

[Terms and Definitions](#)

[NEPA Process](#)



### Project Benefits

The purpose of the Port MacKenzie rail extension project is to establish a rail link between Port MacKenzie and the Alaska Railroad, providing Port MacKenzie customers/shippers efficient rail transportation between the Port and Interior Alaska. The rail line would travel north from the port facility and connect to the existing rail system at a point between Meadow Lakes and north of Willow.

The proposed rail link is consistent with ARRC's enabling statute, to foster and promote long-term economic growth and development of the state's land and natural resources. It draws from more than over 30 years of planning documents and studies relating to port development and access. The project would expand and complement the regional transportation network, and represents a milestone in the development of the port.

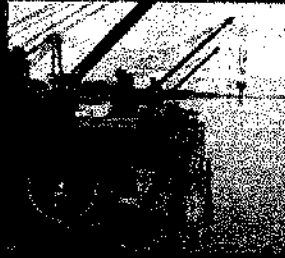
Specifically, the project would:

- Support Port MacKenzie's development as a bulk material resources export and import facility, and as a multi-modal facility.
- Provide transportation via rail for the development of minerals and other natural resources.
- Provide an alternative for transporting materials and equipment for large construction projects.

The project size and significance to transportation in the area are expected to have additional positive effects both locally and regionally. Some of these secondary project benefits could include:

- Positive impact on the economics associated with development of strategic mineral resources, leading to industrial expansion and diversification of the state's economy.
- Increase in employment and support for the tax base for communities benefiting from newly stimulated natural resource development and rail freight activity.
- An opportunity for future development of rail passenger service to the western Matanuska-Susitna Borough.

# PORT MACKENZIE RAIL EXTENSION



[Home](#) | [Project Overview](#) | [FAQs](#) | [Get Involved](#) | [Project Library](#) | [Useful Links](#)

## Project Overview

[Print Page](#) > [Home](#) > [Project Overview](#) > [Project Participants](#)

[Project History](#)

[Project Benefits](#)

[Project Area](#)

[Project Participants](#)

[Issues](#)

[Cost and Funding](#)

[Terms and Definitions](#)

[NEPA Process](#)

### Project Area

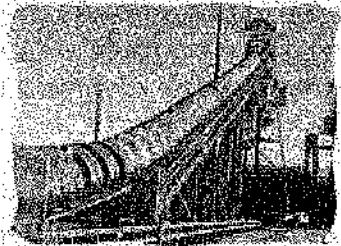
The proposed rail-only project would result in a 30- to 45-mile rail line in the Susitna River valley. The rail line would travel north from the port facility and connect to the existing rail system at a point between Meadow Lakes and north of Willow.

[View the Alternatives Map](#)

The new rail line would be an extension of the ARRC system, which currently connects ports in Seward, Whittier, and Anchorage with Interior Alaska, including Denali National Park, Fairbanks, and North Pole.

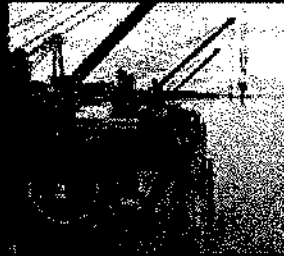
The track will lie roughly in the center of a 200-foot right-of-way. The project is rail-only, with corresponding facilities including a maintenance road, and does not include a public road.

The possible rail alignments and crossing locations will be evaluated on the basis of numerous factors including potential environmental impacts, engineering feasibility, and right-of-way concerns.



[HOME](#) [PROJECT OVERVIEW](#) [FAQS](#) [GET INVOLVED](#) [PROJECT LIBRARY](#) [USEFUL LINKS](#) [CONTACT US](#)

# PORT MACKENZIE RAIL EXTENSION



[Home](#) | [Project Overview](#) | [FAQs](#) | [Get Involved](#) | [Project Library](#) | [Useful Links](#)

## Project Overview

[Print Page](#) > [Home](#) > [Project Overview](#) > [Project Participants](#)

[Project History](#)

[Project Benefits](#)

[Project Area](#)

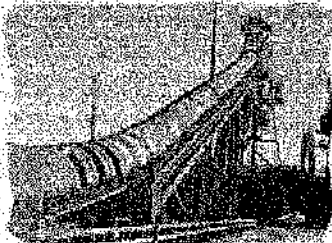
[Project Participants](#)

[Issues](#)

[Cost and Funding](#)

[Terms and Definitions](#)

[NEPA Process](#)



### Project Participants

**ARRC** (Alaska Railroad Corporation) is the project applicant and co-manager.

**Borough** (Matanuska-Susitna Borough) is the operator of Port MacKenzie, the project sponsor, and co-manager.

**HDR** (HDR Alaska, Inc.) is the consultant hired by the co-managers to analyze the environmental issues associated with the range of alternatives, conduct public involvement activities, and support the ARRC.

**TNH-Hanson** (TNH-Hanson, LLC) is the consultant hired by the co-managers to develop the rail alternatives and provide conceptual engineering services to the ARRC.

**STB** (Surface Transportation Board) is the lead federal agency responsible for approving the rail extension. The STB is affiliated with the U.S. Department of Transportation. See also [Surface Transportation Board](#) on the [Terms & Definitions](#) page.

**SEA** (Section of Environmental Analysis) is the department within the STB responsible for ensuring NEPA compliance. See also [Section of Environmental Analysis](#) on the [Terms & Definitions](#) page.

**Third-Party Contractor** is a consultant team hired by the STB to evaluate project impacts and prepare the environmental document independent of the project applicant.

[HOME](#) | [PROJECT OVERVIEW](#) | [FAQS](#) | [GET INVOLVED](#) | [PROJECT LIBRARY](#) | [USEFUL LINKS](#) | [CONTACT US](#)

# PORT MACKENZIE RAIL EXTENSION

[CONTACT US](#)

[Home](#) | [Project Overview](#) | [FAQs](#) | [Get Involved](#) | [Project Library](#) | [Useful Links](#)

## Project Overview

[Print Page](#) > [Home](#) > [Project Overview](#) > [Issues](#)

[Project History](#)

[Project Benefits](#)

[Project Area](#)

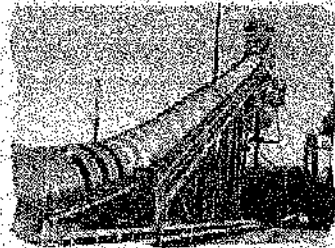
[Project Participants](#)

[Issues](#)

[Cost and Funding](#)

[Terms and Definitions](#)

[NEPA Process](#)



### Issues

Constructing 30-45 miles of new rail line has the potential to have significant impacts to the environment and will require preparation of an environmental document, either an Environmental Assessment (EA) or an Environmental Impact Statement (EIS), in compliance with the [National Environmental Policy Act \(NEPA\)](#).

The current phase is to complete sufficient engineering and environmental analysis to support the ARRC's application for to the [Surface Transportation Board \(STB\)](#) for license to construct and operate the rail line. Click on the link to read more about the [NEPA Process](#).

The Surface Transportation Board, the lead federal agency responsible for conducting an environmental review of the Port MacKenzie Rail Extension Project, will study a number of issues and concerns including:

- Natural environmental impacts
- Noise
- Crossing locations
- Wetlands
- Potential impacts to private property
- Socio-economic impacts
- Historic and cultural resource impacts
- Construction and operational costs
- Engineering and construction feasibility
- Local and regional trails

What other concerns or issues do you have about the project? [Submit a Comment](#) or [Contact Us](#).



# PORT MACKENZIE RAIL EXTENSION



[Home](#) | [Project Overview](#) | [FAQs](#) | [Get Involved](#) | [Project Library](#) | [Useful Links](#)

## Project Overview

[Print Page](#) > [Home](#) > [Project Overview](#) > [Cost and Funding](#)

[Project History](#)

[Project Benefits](#)

[Project Area](#)

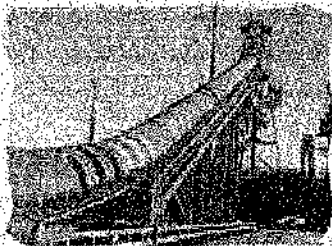
[Project Participants](#)

[Issues](#)

[Cost and Funding](#)

[Terms and Definitions](#)

[NEPA Process](#)



### Cost and Funding

#### Analysis

The State of Alaska appropriated \$10 million during the 2007 legislative session to support the [Surface Transportation Board \(STB\)](#)'s process for achieving a license to construct and operate the rail line. This includes alternative analysis, preliminary engineering, [National Environmental Policy Act \(NEPA\)](#) environmental documentation, and a financial feasibility study.

#### Design & Construction

The final design and construction of a rail extension to Port MacKenzie is estimated to cost \$300 million (2007 dollars). The Matanuska-Susitna Borough and the Alaska Railroad will jointly conduct a financing study to assess options for funding the project.



[HOME](#) | [PROJECT OVERVIEW](#) | [FAQS](#) | [GET INVOLVED](#) | [PROJECT LIBRARY](#) | [USEFUL LINKS](#) | [CONTACT US](#)

# PORT MACKENZIE RAIL EXTENSION

CONTACT US

[Home](#) | [Project Overview](#) | [FAQs](#) | [Get Involved](#) | [Project Library](#) | [Useful Links](#)

## Project Overview

[Print Page](#) > [Home](#) > [Project Overview](#) > [Terms and Definitions](#)

[Project History](#)

[Project Benefits](#)

[Project Area](#)

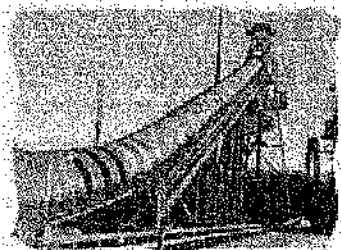
[Project Participants](#)

[Issues](#)

[Cost and Funding](#)

[Terms and Definitions](#)

[NEPA Process](#)



## Terms and Definitions

Familiarize yourself with these terms and definitions common to the Port MacKenzie Rail Extension Project:

**Environmental Impact Statement (EIS):** An EIS is the documentation of a study for federal projects that are expected to have significant social, economic, and/or environmental impacts. The purpose is to determine what impacts a transportation project might generate in regard to the natural and physical environment as well as the relationship of people with that environment. Issues such as impacts to wetlands, streams, rivers and wildlife as well as homes, businesses and communities, will be studied in relation to the proposed project. An EIS describes the impacts; the purpose of, and need for, the action; alternatives (including "no action"); and the potential environmental consequences.

**Federal Railroad Administration (FRA):** The Federal Railroad Administration publicizes and enforces rail safety regulations; administers railroad assistance programs; conducts research and development in support of improved railroad safety and national rail transportation policy; and consolidates government support of rail transportation activities. The FRA is one of ten agencies within the U.S. Department of Transportation concerned with intermodal transportation.

**Federal Transit Administration (FTA):** FTA is one of eleven modal administrations within the U.S. Department of Transportation. The Federal government, through the FTA, provides financial assistance to develop new transit systems and improve, maintain, and operate existing systems.

**Mass Transit:** Another name for public transportation. Transportation by bus, rail, or other conveyance, either publicly or privately owned, which provides to the public general or special service on a regular and continuing basis.

**National Environmental Policy Act (NEPA):** NEPA is the National Environmental Policy Act of 1969. It declared a national policy to "encourage productive and enjoyable harmony between man and his environment." NEPA requires federal agencies to consider potential environmental impacts before approving major projects that are subject to federal control and responsibility. To meet this requirement, federal agencies may prepare a detailed statement known as an Environmental Impact Statement (EIS). The Environmental Protection Agency (EPA) reviews and comments on EISs prepared by other federal agencies and maintains a national filing system for all EISs.

**Section of Environmental Analysis (SEA):** The SEA is the office within the STB responsible for directing the environmental review process, conducting independent analysis of all environmental data, and making environmental recommendations to the STB. The SEA uses third-party contractors to aid in preparing environmental documentation necessary to comply with the requirements of NEPA and related environmental laws in STB proceedings.

**Surface Transportation Board (STB):** The STB is the lead federal agency for the Port MacKenzie Rail Extension Project. It is an economic regulatory agency that Congress charged with the fundamental missions of resolving railroad rate and service disputes and reviewing proposed railroad mergers. The STB is affiliated with the U.S. Department of Transportation.



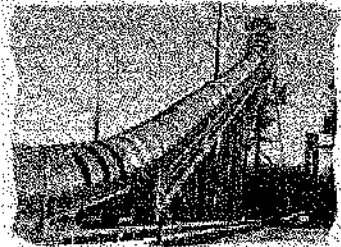
[HOME](#) [PROJECT OVERVIEW](#) [FAQS](#) [GET INVOLVED](#) [PROJECT LIBRARY](#) [USEFUL LINKS](#) [CONTACT US](#)

# PORT MACKENZIE

## RAIL EXTENSION

[CONTACT US](#)
[Home](#) | [Project Overview](#) | [FAQs](#) | [Get Involved](#) | [Project Library](#) | [Useful Links](#)

## Project Overview

[Print Page](#) > [Home](#) > [Project Overview](#) > [NEPA Process](#)
[Project History](#)
[Project Benefits](#)
[Project Area](#)
[Project Participants](#)
[Issues](#)
[Cost and Funding](#)
[Terms and Definitions](#)
[NEPA Process](#)


### NEPA Process

#### *What is NEPA?*

The National Environmental Policy Act (NEPA) was signed into law in 1969. In passing NEPA, Congress declared that it is the policy of the Federal Government, in cooperation with State and local governments, "to foster and promote the general welfare, to create and maintain conditions under which man and nature can exist in productive harmony, and fulfill the social, economic, and other requirements of present and future generations of Americans."

NEPA requires the preparation of detailed Environmental Impact Statements (EIS) when projects requiring federal action are determined to have potential for significant impacts to historic, cultural, or natural aspects of our national heritage.

#### *The NEPA Process*

All Federal agencies have developed processes for conducting environmental impact analysis. NEPA sets forth a framework for how government agencies and project proponents conduct environmental reviews, although how those procedures are carried out varies from agency to agency. Generally, the NEPA process occurs in a number of well-defined stages:

- Define purpose and need
- Scoping
- Analyze Reasonable Alternatives
- Prepare a Draft EIS
- Public Comment on Draft EIS
- Prepare a Final EIS
- Record of Decision

The process can take as little as a year and a half, but some projects can take five years or longer. For the Port MacKenzie Rail Extension Project, the Surface Transportation Board's (STB) Section for Environmental Analysis (SEA) is the lead federal agency charged with ensuring compliance with NEPA and related environmental statutes. The Alaska Railroad Corporation (ARRC) anticipates the process may take approximately two years.

#### *Define Purpose and Need*

The purpose and need for a project provides the basis for developing reasonable alternatives, establishing project objectives, and helping to define criteria for assessing alternatives, including the option of not doing the project at all (often called the no build alternative). Purpose and Need helps agencies to prepare a Notice of Intent (NOI), which formally announces that the agency intends to prepare an EIS. Read more about the Port MacKenzie Rail Extension Project's purpose and need under [Project Benefits](#).

### *Third Party Process*

While responsibility for the NEPA process belongs to the federal agency, some federal agencies employ independent, third-party contractors to prepare the environmental documentation necessary to comply with NEPA requirements. In a voluntary third-party contracting arrangement, the applicant pays for an approved agency-selected contractor to assist the agency.

### *Scoping*

Scoping meetings help to determine what issues will need to be assessed in the EIS. STB conducts scoping independently of the applicant, reviews the scoping comments and defines the final Scope of Study, which will serve as the work plan for preparing a Draft EIS.

### *Draft EIS*

The purpose of a Draft EIS is to present the purpose and need for the project, describe the project area and the social and natural features, and evaluate potential direct, indirect, and cumulative impacts that may result from the project. As the SEA evaluates potential impacts, they may request that ARRC submit additional engineering and design elements, as well as operational procedures intended to avoid, minimize, or mitigate potential impacts. Upon completion, the Draft EIS will be available for review and comment by the public, government agencies, and other interested parties.

### *Final EIS*

The Final EIS will be based on SEA's independent analysis, consultation with agencies, and review of comments on the Draft EIS. It will then recommend a preferred alternative to STB, as well as the basis for choosing that alternative, mitigation measures, and other environmental compliance findings. The Final EIS will be filed with the Environmental Protection Agency, which will then publish a notice of its availability in the Federal Register.

### *Final Decision*

The STB will consider the recommendations of the entire environmental record, including the Draft and Final EISs, as part of its decision-making process. If the STB were to approve the proposed project, construction could begin when the decision became effective.

# PORT MACKENZIE

## RAIL EXTENSION



[Home](#) | [Project Overview](#) | [FAQs](#) | [Get Involved](#) | [Project Library](#) | [Useful Links](#)

[Print Page](#) > [Home](#) > [Frequently Asked Questions](#)



### Frequently Asked Questions

#### Q. Why do we need an extension to Port MacKenzie?

A. The extension will provide bulk transport of goods and commodities between Port MacKenzie and Interior Alaska. This will achieve the Borough's vision for the Port as a multi-modal economic engine for both the Matanuska-Susitna Borough and the State of Alaska and support the Alaska Railroad Corporation's mission of fostering the development of Alaska's economy by integrating railroad development with rail belt community development plans.

#### Q. Who is responsible for the project?

A. The Port MacKenzie Rail Extension Project will be owned and operated by the Alaska Railroad Corporation (ARRC). The Borough, as the owner and operator of Port MacKenzie, is the project sponsor. A Memorandum of Agreement has been signed between the two entities, establishing "co-management" in the study of this project. Because it involves construction of a new rail line, the project will require federal approval. The Surface Transportation Board (STB) is the federal agency responsible for completing an environmental document and for ultimately approving the new route.

#### Q. This project has been considered since the 1970s. What has changed?

A. Over the past 30 years, several entities have considered construction of a rail connection to a port in the Mat-Su Borough. Port MacKenzie is now a port with ocean-going commerce and a road, soon-to-be-paved, but still requires rail to expand its development. Marine ports don't reach their full potential unless they are multi-modal. The most recent and detailed of these studies was completed in 2003 ([Matanuska-Susitna Borough Rail Corridor Study](#)) which culminated in the recommendation of a route that would ultimately connect to the Alaska Railroad near Willow. The Mat-Su Borough has been the fastest growing area of the State for the past 16 years. Conditions have changed since the project was first considered. This growth along with new technical data make it critical to take a fresh look at the various alignments. The Borough and the Alaska Railroad are partners in this project. The Borough has asked the Alaska Railroad to prepare and submit an application to construct and operate a railroad between their rail system and Port MacKenzie. This is beyond a planning action and represents a specific request to construct the rail line. Attached to it will likely be explicit conditions under which such a railroad would be constructed. An application of this type requires an environmental document (either an Environmental Assessment or Environmental Impact Statement) as required by the National Environmental Policy Act or NEPA. The

environmental document will be started sometime after January of 2008 and will be another way for you to comment. The environmental document is carried out by the federal Surface Transportation Board. Right now, we're gathering as much information as possible in preparation for the application before the STB and the environmental document, which will follow. Because of the finality associated with this action, the Borough and the ARRC felt it prudent to take a final review of all the studies and information previously assembled to ensure that the route proposed to the STB be the best possible.

**Q. Where and when will there be public involvement opportunities?**

A. There will be many opportunities to participate in the Port MacKenzie Rail Extension Project. The Borough and the ARRC are currently gathering public comment, which will be considered prior to the formal application to the STB. The STB will also hold formal public scoping meetings to gather additional information on community concerns and issues regarding this project. The public's involvement will be critical in identifying possible project impacts. Additional opportunities include a Web site and submission of comments to the project team.

**Q. What are the benefits to the Borough?**

A. The rail extension could potentially open new markets for minerals and stimulate the creation of new jobs. The rail could also provide long-term benefits by lowering property taxes (through the infusion of statewide and local economic opportunities).

**Q. Was the rail extension project included in the Borough's Long Range Transportation Plan?**

A. The rail extension from Port MacKenzie to the mainline was discussed at many of the public meetings held as part of the Borough's Long Range Transportation Plan (LRTP) project. As a consequence it was added to the Official Streets and Highways Plan Map in the LRTP showing it as a future project. Communities undergoing local comprehensive planning processes may consider including discussion of the rail extension project as part of the planning effort.

**Q. What happens to the existing rail line?**

A. There will be no changes to the existing line between Anchorage and Fairbanks. The proposed extension will provide access to Port MacKenzie.

**Q. What are some of the issues that will be considered?**

A. The Surface Transportation Board, the lead federal agency responsible for conducting an environmental review of the Port MacKenzie Rail Extension Project, will study a number of issues and concerns including:

- Natural environmental impacts
- Noise
- Crossing locations
-

Wetlands

Potential impacts to private property

Socio-economic impacts

Historic and cultural resource impacts

Construction and operational costs

Engineering and construction feasibility

Local and regional trails

What other concerns or issues do YOU have about the project? Go to [www.portmacrail.com](http://www.portmacrail.com) to provide comments.

**Q. How will the project be funded?**

A. The State of Alaska appropriated \$10 million during the 2007 legislative session to studies that will give a more thorough picture to the Surface Transportation Board. The studies will accompany an application for a license for constructing and operating the rail line. This includes alternative analysis, preliminary engineering, National Environmental Policy Act (NEPA) environmental documentation, and a financial feasibility study. The final design and construction of a rail extension to Port MacKenzie is estimated to cost \$300 million (2007 dollars). The Matanuska-Susitna Borough and the Alaska Railroad will jointly conduct a financing study to assess options for funding the project. User fees could play a role.

**Q. When will the project be completed?**

A. The project sponsor (MSB) and applicant (ARRC) plan for rail extension operation by the year 2012. This will require a fast-paced timeline that assumes submission of the application to the STB by the beginning of 2008, STB completion of an environmental document by the end of 2008, engineering and design in 2009, and construction between 2010 and 2012. This is only an estimate, however, with the actual design and completion relying on the pace of the STB's environmental process, as well as funding availability, and the land acquisition process and actual construction conditions.

**Q. How will the land be acquired?**

A. Once a route has been defined and accepted, and funding identified, the railroad will obtain third-party assessments of land that is needed for the new right-of-way. The railroad will then negotiate with landowners based primarily on fair market value. While the railroad, as a public transportation entity, can evoke eminent domain to obtain land from an unwilling landowner, it has never had to do so. Rather, the railroad has strived for fair and comprehensive negotiations, or in some cases modifying design when no resolution can be obtained with a landowner. If all avenues were exhausted and it was necessary to evoke eminent domain and acquire land without landowner consent, ARRC would comply with the Uniform Relocation Act (URA), and the landowner would be compensated under the URA for the property loss.



# PORT MACKENZIE RAIL EXTENSION



[Home](#) | [Project Overview](#) | [FAQs](#) | [Get Involved](#) | [Project Library](#) | [Useful Links](#)

## Get Involved

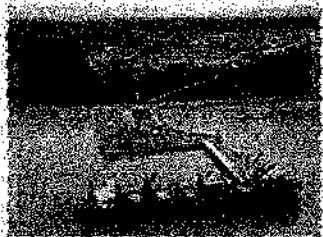
[Print Page](#) > [Home](#) > [Get Involved](#)

[Attend a Meeting](#)

[Submit a Comment](#)

[Join the Mailing List](#)

[Contact Us](#)



### Thank You for Participating!

The proposed Port MacKenzie Rail Extension Project has numerous stakeholders including businesses, agencies, communities, Alaska Native corporations, tribal entities, resident and non-resident landowners, and local citizens. Every voice is important. Gathering input from a wide spectrum of voices and interests will ensure that the Matanuska-Susitna Borough and the Alaska Railroad have an opportunity to consider stakeholder viewpoints when making project decisions about alternatives.



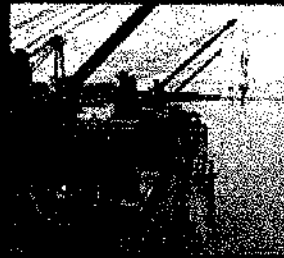
Point MacKenzie farmer Lyn Baskin (right) discusses the proposed rail extension alignments with Brian Lindamood, project manager from the Alaska Railroad Corporation, and Donna Robertson, environmental project manager from HDR Alaska, at a public open house meeting in early October 2007.

More opportunities for public involvement will occur during the preparation of the environmental document. Stay informed by checking this Web site often and [joining the mailing list](#).

[HOME](#) [PROJECT OVERVIEW](#) [FAQS](#) [GET INVOLVED](#) [PROJECT LIBRARY](#) [USEFUL LINKS](#) [CONTACT US](#)

# PORT MACKENZIE RAIL EXTENSION

[CONTACT US](#)



[Home](#) | [Project Overview](#) | [FAQs](#) | [Get Involved](#) | [Project Library](#) | [Useful Links](#)

## Get Involved

[Print Page](#) > [Home](#) > [Get Involved](#) > [Attend a Meeting](#)

[Attend a Meeting](#)

[Submit a Comment](#)

[Join the Mailing List](#)

[Contact Us](#)



### Attend a Meeting

Public meetings to gather information were held in early October 2007. Meeting handouts and a presentation are posted on the [Project Library](#) page. Thank you to everyone who participated!

#### PAST Meetings

Date	What	Where
Oct. 5, Fri.	Public Meeting Open House <a href="#">News Release 9/14/07</a> <a href="#">News Release 10/5/07</a> <a href="#">Meeting Material</a>	Houston Middle School
Oct. 4, Thu.	Public Meeting Open House <a href="#">News Release 9/14/07</a> <a href="#">Meeting Material</a>	Knik Elementary School
Oct. 3, Wed.	Public Meeting Open House <a href="#">News Release 9/14/07</a> <a href="#">Meeting Material</a>	Willow Area Community Center
Oct. 2, Tues.	Public Meeting Open House <a href="#">News Release 9/14/07</a> <a href="#">News Release 10/2/07</a> <a href="#">Meeting Material</a>	Big Lake Elementary School
Oct. 1, Mon.	Public Meeting Open House <a href="#">News Release 9/14/07</a> <a href="#">Meeting Material</a>	Lake Lucille Inn, Wasilla

[HOME](#) [PROJECT OVERVIEW](#) [FAQS](#) [GET INVOLVED](#) [PROJECT LIBRARY](#) [USEFUL LINKS](#) [CONTACT US](#)

# PORT MACKENZIE RAIL EXTENSION



[Home](#) | [Project Overview](#) | [FAQs](#) | [Get Involved](#) | [Project Library](#) | [Useful Links](#)

## Get Involved

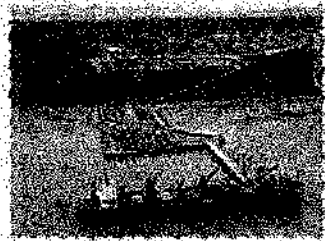
[Home](#) > [Get Involved](#) < [Submit a Comment](#)

[Attend a Meeting](#)

[Submit a Comment](#)

[Join the Mailing List](#)

[Contact Us](#)



### Submit a Comment

Comments received through Nov. 2, 2007, were considered in analyzing the alignments and preparing the project application to the Surface Transportation Board. Comments received after this deadline will be forwarded to the STB.

Do you have concerns or ideas about the proposed rail line extension to Port MacKenzie? Click "Submit" at the bottom of the form when finished, or send a separate letter, e-mail, or fax using the information on the [Contact Us](#) page. (If you submit a comment, your contact information will be added to the project mailing list.)



\*Required Fields

Name  \*

Street Address  \*

City  \*

State  \*

Zip  \*

Phone

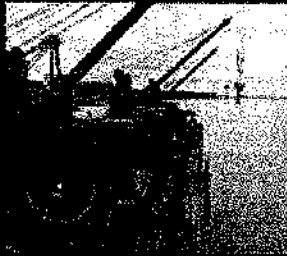
E-mail  \*

Confirm E-mail  \*

Comments

Your personal information will be kept in strict confidentiality. However, if you post a comment, it becomes a part of our public participation process.

# PORT MACKENZIE RAIL EXTENSION



[Home](#) | [Project Overview](#) | [FAQs](#) | [Get Involved](#) | [Project Library](#) | [Useful Links](#)

## Get Involved

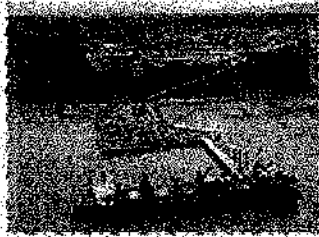
[Home](#) > [Get Involved](#) < [Join the Mailing List](#)

[Attend a Meeting](#)

[Submit a Comment](#)

[Join the Mailing List](#)

[Contact Us](#)



### Join the Mailing List

Do you want to stay informed about the proposed rail line extension to Port MacKenzie? Click "Submit" at the bottom of the form when finished, or send a separate letter, e-mail, or fax using the information on the [Contact Us](#) page. If you [Submit a Comment](#), it is not necessary to also submit this mailing list form as your contact information will be added to the project mailing list.

\*Required Fields

Name  \*

Street Address  \*

City  \*

State  \*

Zip  \*

Phone

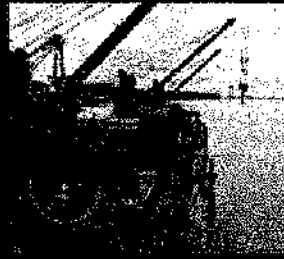
E-mail  \*

Confirm E-mail  \*

Your personal information will be kept in strict confidentiality.

[HOME](#) | [PROJECT OVERVIEW](#) | [FAQS](#) | [GET INVOLVED](#) | [PROJECT LIBRARY](#) | [USEFUL LINKS](#) | [CONTACT US](#)

# PORT MACKENZIE RAIL EXTENSION



[Home](#) | [Project Overview](#) | [FAQs](#) | [Get Involved](#) | [Project Library](#) | [Useful Links](#)

## Project Library

[Print Page](#) > [Home](#) > [Project Library](#)



Some PDF files are large and may take a few moments to load. Documents marked with an asterisk (\*) are too large to post here. You may be able to locate the documents at your local library or by contacting the author.



NOTE: Most of these documents are Adobe PDF Files. If you do not have Acrobat on your computer, download the Adobe Acrobat Reader to view these files.

### News Releases

- [Dec 2007 MSB Assembly Passes Resolution 12/11/07](#)
- [Nov 2007 MSB Assembly News Release 11/20/07](#)
- [Oct 2007 Open House News Release 10/10/07](#)
- [Oct 2007 Open House News Release 10/5/07](#)
- [Oct 2007 Open House News Release 10/2/07](#)
- [Oct 2007 Open House Radio Release](#)
- [Oct 2007 Open House Media Release 9/14/07](#)

### Meeting Advertisements, Handouts and Presentations

- [Oct 2007 Open House Postcard](#)
- [Oct 2007 Open House Community Poster](#)
- [Oct 2007 Open House Presentation](#)
- [Oct 2007 Open House Boards \(Revised 11/7/07\)](#)
- [Nov 2007 Presentation to MSB Assembly \(Revised 12/6/07\)](#)
- [Dec 2007 Criteria Matrix](#)
- Dec 2007 MSB Assembly Special Meeting:
  - o [Cover Memo](#)
  - o [STB Criteria](#)

- o [STB Process](#)
- o [Alternatives Analysis](#)
- o [Link to MSB Assembly Resolutions](#)

[FAQs - PFD](#)

---

**Newsletter**


[Dec 2007 Port Mackenzie Rail Extension Project](#)

---

**Project Reports and Maps**

- [Alternatives Map](#)
  - [Trails Map with Alternatives](#)
  - [Agricultural Area Map with Alternatives](#)
- 

**Other Media**

 [Listen to KSKA Radio Community Forum 10/10/07](#)  
(click on grey play button on left, middle section of page)

---

**Related Port & Transportation Planning Reports:**

- [Cook Inlet Ferry NEPA Supplemental Environmental Assessment \[09/2006\]](#)
  - [Matanuska-Susitna Borough Long-Range Transportation Plan \[under review\] \\*](#)
  - [Matanuska-Susitna Borough Rail Corridor Study \[06/2003\]](#)
  - [Port MacKenzie Master Plan \[1998, amended 1999\] - 2.6 MB PDF](#)
  - [Point MacKenzie Area Which Merits Special Attention Plan \[2006\] - 418 KB PDF](#)
  - [Economic Evaluation and Planning of a Cook Inlet Marine Transportation System \[7/1990\]](#)
  - [Economic Evaluation of the Port of Alaska \[03/1990\] - 502 KB PDF](#)
  - [Essential Elements of a Master Plan for the East Port Area at Point MacKenzie \[1989\]](#)
  - [Matanuska-Susitna Borough: Comprehensive Development Plan: Transportation \[1984\]](#)
  - [Matanuska-Susitna Borough Port Study \[1981\] \\*](#)
  - [The Point MacKenzie Industrial Siting Study \[1978\] \\*](#)
- 

**Related Community Planning and Other Information:**

- [Fish Creek Management Plan, Draft Alternatives \[05/2007\] - 890 KB PDF](#)
- [Matanuska-Susitna Borough Comprehensive Plan \[updated 2005\]](#)

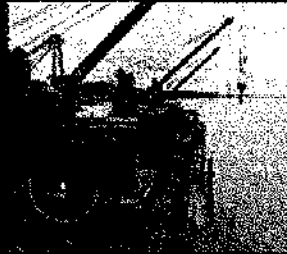
- [Meadow Lakes Comprehensive Plan \[10/2005\] - 4 MB PDF](#)
  - [City of Houston Comprehensive Plan \[04/1999, amended 07/2003\] - 274 KB PDF](#)
  - [Palmer Hay Flats State Game Refuge Revised Management Plan \[12/2002\] 2 MB PDF](#)
  - [Matanuska-Susitna Borough Trail Plan \[03/2000\]](#)
  - [Knik-Fairview Comprehensive Plan \[05/1997\] - 9 MB PDF](#)
  - [Big Lake Comprehensive Plan \[02/1996\] \(Currently being revised\) - 8 MB PDF](#)
  - [Susitna Basin Recreation Rivers Management Plan \[08/1991\] \\*](#)
  - [Susitna Flats State Game Refuge Management Plan \[03/1988\]](#)
  - [Fish Creek Management Plan \[08/1984, amended 03/1987\] \\*](#)
  - [Willow Sub-Basin Area Plan: A Land Use Plan for Public Lands \[10/1982\] \\*](#)
  - [Nancy Lake State Recreation Area Established \[AS 41.21.455\]](#)
  - [Little Susitna River Public Use Facility Brochure - 675 KB PDF](#)
  - [Alaska Public Lands Campgrounds Map - 1 MB PDF](#)
  - [Iditarod National Historic Trail Map & Land Status - 4.5 MB PDF](#)
- 



[HOME](#) [PROJECT OVERVIEW](#) [FAQS](#) [GET INVOLVED](#) [PROJECT LIBRARY](#) [USEFUL LINKS](#) [CONTACT US](#)

© 2008 Alaska Department of Transportation

# PORT MACKENZIE RAIL EXTENSION



[CONTACT US](#)

[Home](#) | [Project Overview](#) | [FAQs](#) | [Get Involved](#) | [Project Library](#) | [Useful Links](#)

## Useful Links

[Print Page](#) > [Home](#) > [Useful Links](#)

For more information about the Port MacKenzie Rail Extension Project and those involved in the project, explore the following Web sites:

[Matanuska-Susitna Borough](#)

[Port MacKenzie](#)

[Alaska Railroad Corporation](#)

[Surface Transportation Board](#)



[HOME](#) | [PROJECT OVERVIEW](#) | [FAQS](#) | [GET INVOLVED](#) | [PROJECT LIBRARY](#) | [USEFUL LINKS](#) | [CONTACT US](#)



# PROJECT NEWSLETTER

*(This page intentionally left blank.)*

CONTINUED FROM FRONT PAGE

that weren't really evident," said Lindamood. "They provided depth in certain areas, such as soils issues near Horseshoe Lake and land use such as the snow machine trails and access to recreational areas."

Discussions about trails — not only where they are but how they are used — will likely affect the types of trail crossings, Lindamood said. "Individual alignments were modified after the public meetings to avoid direct impacts in some areas."

The project team also received public feedback about potentially poor soil conditions, which has been used to help guide the geotechnical investigations.

At all meetings, many people's comments supported the rail extension as a good idea, but often opposed locations close to residential areas. Comments included concerns for trail connectivity, noise, safety, wildlife impacts, and disturbances to recreational cabins, among many others. A comments summary is posted on the project Web site ([www.portmacrail.com](http://www.portmacrail.com)). Agency and public comments are also available for public review at the Borough public affairs office, 350 E. Dahlia Ave., in Palmer. Please call Patty Sullivan, public affairs director for the Borough, at (907) 745-9577 to make arrangements to review the comments.

Project team members also provided project briefings at a



WENDY LONGTIN/HDR ALASKA

**MORE THAN 317 COMMENTS** have been collected, to date, from the public and agencies during the preliminary engineering and environmental analysis for the Port MacKenzie Rail Extension Project. Above, members of the public discuss the project with staff from the Alaska Railroad at a public meeting held in Wasilla in early October.

work session with the Borough Planning Commission on Nov. 5 and reviewed the matrix of proposed alignments at the Borough Assembly meeting on Nov. 20. A work session is planned for 3 p.m., Tuesday, Dec. 11, to allow members of the Assembly time to review and evaluate the matrix.



HDR ALASKA INC.  
ATTN: ROSETTA ALCANTRA  
832 S. COLONY WAY  
PALMER, AK 99645

### Questions or Comments?

Please contact:

**Patty Sullivan**  
Public Affairs Director,  
Matanuska-Susitna Borough

350 E. Dahlia Ave.  
Palmer, AK 99645

907-745-9577 tel  
psullivan@matsugov.us

**Stephenie Wheeler**  
Public Involvement Officer,  
Alaska Railroad Corporation

327 W. Ship Creek Ave.  
Anchorage, AK 99501

907-265-2671 tel  
wheelers@akrr.com

PRE-SORTED STD.  
U.S. POSTAGE PAID  
PERMIT NO. 500  
ANCHORAGE, AK



# Port MacKenzie Rail Extension Project

A cooperative effort of the Matanuska-Susitna Borough and the Alaska Railroad Corporation



## Public input helps refine corridors

### ■ MANY SUPPORT RAIL EXTENSION BUT VOICE CONCERNS ABOUT PROPERTY, TRAILS

The Matanuska-Susitna Borough and the Alaska Railroad Corporation hosted a series of public open house meetings Oct. 1-5, 2007, for the Port MacKenzie Rail Extension Project. More than 350 people showed their interest by participating in the meetings at Wasilla, Houston, Willow, Knik, and Big Lake. Project consultants HDR Alaska and TNH-Hanson also attended. Landowners and residents asked questions about engineering and environmental issues, project timeline, right-of-way, and how specific properties might be affected by the project.

"These meetings were critical to prepare an effective application to the

### Comments by the numbers

At the project open house meetings held in early October, **37** people gave their remarks to a court reporter, **95** wrote and submitted their comments, and **137** comments were written on large aerial maps that were provided at each meeting. To date, the project team has received more than **317** comments.

Surface Transportation Board (STB)," said Brian Lindamood, project manager from the Alaska Railroad. "People who live, recreate, and do business in this area helped inform the project team of issues not otherwise readily apparent."

The STB, a federal agency based in Washington, D.C., is responsible for approving the new rail extension.

"The reason for this preliminary analysis is to provide information to the people who will be preparing the environmental document. The more information we give to the STB the better off we are," said Joe Perkins, project consultant for the Borough, as well as former commissioner of the Alaska Department of Transportation.

"We're providing Alaska-specific information that we think they need to save them time," Perkins said.

The purpose of the public outreach was to gain as much information as possible about potential impacts and environmental concerns. Some of this information has already helped refine the proposed corridors.

"The public pointed out a lot of things

CONTINUED ON BACK PAGE

MEETING PHOTOS: PATTY SULLIVAN/MSB AND WENDY LONGTIN/HDR ALASKA ■ SHIP PHOTO: JORDAN MAY ■ RAILROAD PHOTOS: COURTESY OF THE ALASKA RAILROAD CORP.

**ABOVE LEFT: POINT MACKENZIE FARMER** Lyn Baskin (right) discusses the proposed rail extension alignments with Brian Lindamood, project manager from the Alaska Railroad Corporation, and Donna Robertson, environmental project manager from HDR Alaska, at a public open house meeting in early October.

**ABOVE RIGHT: ANCHORAGE RESIDENT** Jim Seeley (left), a member of the Red Shirt Lake Landowner's Association, a group with about 50 cabins, discusses the proposed rail extension alignments with Brad Sworts (right), project manager from the Matanuska-Susitna Borough.

*(This page intentionally left blank.)*

# What happens next?

## COLLECTION OF PRELIMINARY DATA FORWARDED TO STB FOR CONSIDERATION

Now that the Borough and the Railroad have completed their fact-finding efforts, the preliminary engineering and environmental data will be sent to the STB, and an application to construct and operate the new rail will follow. The application will describe the project, identify areas of concern, and discuss proposed alignments. Upon receipt, the STB is expected to publish a Notice of Intent in the Federal Register, kicking off the formal National Environmental Policy Act (NEPA) environmental documentation process. At this time, it is not known if the environmental document will be an Environmental Assessment (EA) or an Environmental Impact Statement (EIS).

Once the formal process begins, the STB will select a third-party contractor to prepare the environmental document. At this point, the role of the Borough and the Railroad becomes secondary – providing information to the STB as requested.

The current consultants, HDR Alaska and TNH-Hanson, will remain on contract to provide supplemental information to the Railroad and the Borough as needed. The public will continue to be involved throughout the process. To learn more about the STB, go to: [www.stb.dot.gov/](http://www.stb.dot.gov/).

While NEPA establishes a framework for conducting environmental reviews, procedures vary from agency to agency. The process can take as little as a year and a half, but some projects can take five years or longer.

The Borough and Railroad estimate that the environmental documentation process for the Port MacKenzie Rail Extension Project could begin as early as the end of the year with the hiring of a third-party contractor. The STB will likely hold public scoping meetings to solicit comments.

Although the Borough and the Railroad cannot be directly involved with the project during the environmental documentation stage, our goal is to continue to offer updates on the project Web site ([www.portmacrail.com](http://www.portmacrail.com)).

## Thank you!

The Borough and the Railroad thank the following groups that met with the project team and everyone who expressed interest and participated in this phase of the project:

- Alaska Department of Natural Resources
- Alaska Department of Fish and Game
- State Historic Preservation Office
- NOAA, National Marine Fisheries Service
- Bureau of Land Management
- U.S. Environmental Protection Agency
- U.S. Army Corps of Engineers
- U.S. Fish and Wildlife Service
- Area Legislators
- Willow Dog Musers Association
- Houston City Council
- Iditarod Trails Committee, Inc.
- Knik Tribal Council Historic Preservation Committee
- University of Alaska Lands Office
- Mental Health Trust, The Trust Land Office
- CIRI
- Knikatnu, Inc.
- Willow Area Community Organization
- MSB Assembly
- MSB Planning Commission
- MSB Transportation Advisory Board
- MSB Port Commission
- MSB Historic Preservation Commission

Comments received by Nov. 2 were considered in analyzing the alignments and preparing the project application. We will continue to accept comments and relay them to the STB.

# How were the alternatives evaluated?

The project team developed eight alternative alignments (see map on insert) based on previous studies, constraints analysis, other factors such as engineering and environmental considerations, and discussions with agencies and the public.

The project team then developed the strengths and weaknesses of these proposed routes using 10 measureable categories. The preliminary engineering and environmental data

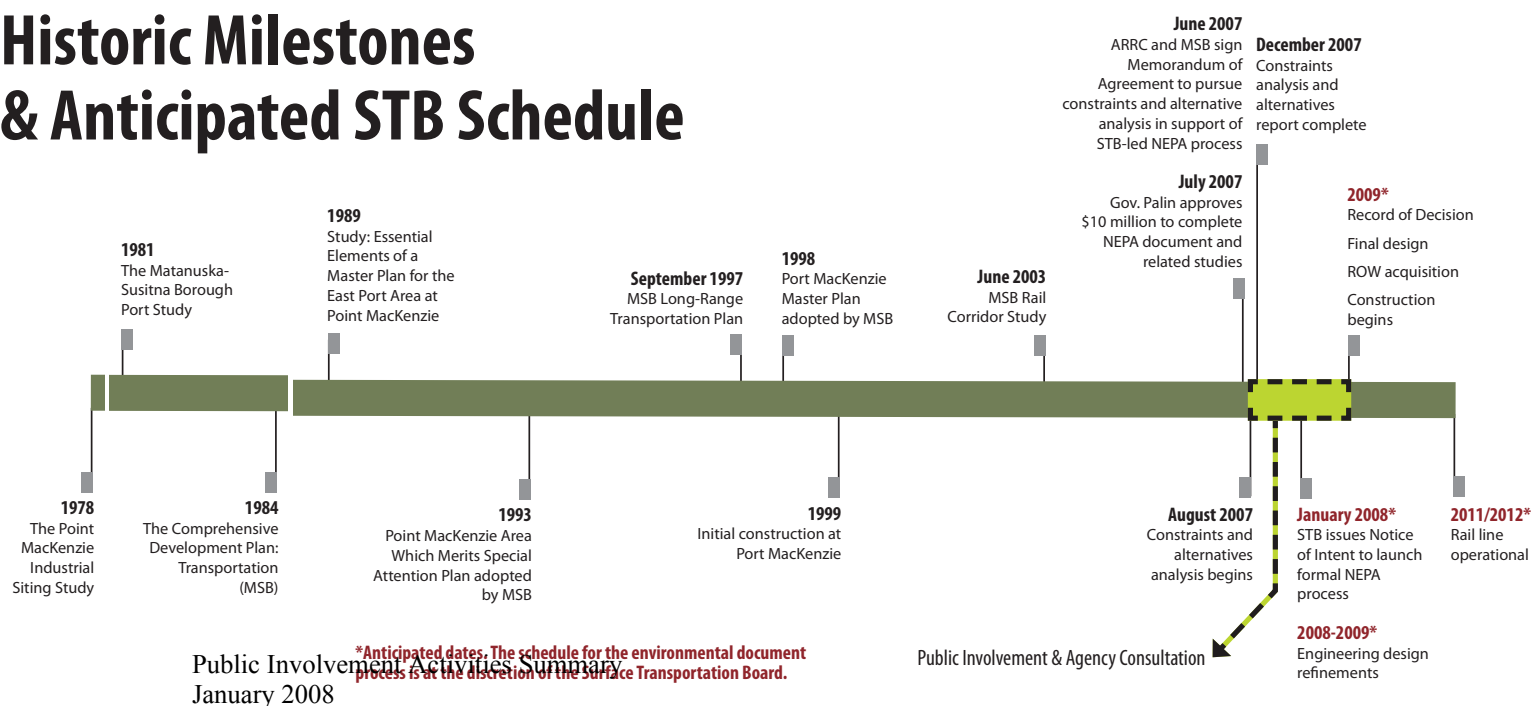
is presented in a matrix (see matrix on insert), which will be considered by the STB. Below is a summary of the strengths and weaknesses of each proposed route based on the matrix results.

Matrix categories include factors related to constructibility, the natural environment, and impacts to local communities. The matrix, as part of a larger project background report, will lay the foundation for the STB's environmental document.

## Proposed Routes Strengths & Weaknesses

PROPOSED ROUTES	STRENGTHS	WEAKNESSES
<b>Mac West - Willow</b>	<ul style="list-style-type: none"> <li>Requires fewer number of new road crossings</li> </ul>	<ul style="list-style-type: none"> <li>Crosses more incompatible land uses</li> <li>Has higher probability of impacting archeological and/or historical sites</li> <li>Has greatest impacts to designated state refuges and recreation areas</li> </ul>
<b>Mac West - Houston North</b>	<ul style="list-style-type: none"> <li>Requires fewest number of new road crossings</li> <li>Impacts fewer developed parcels</li> <li>Requires smaller expenditure of train energy</li> <li>Has less probability of impacting archeological and/or historical sites</li> </ul>	<ul style="list-style-type: none"> <li>Crosses more incompatible land uses</li> <li>Has less suitable soil conditions</li> <li>Impacts more wetlands</li> <li>Impacts a designated refuge and fragments a state recreation area</li> <li>Crosses greater number of anadromous streams</li> </ul>
<b>Mac West - Houston South</b>	<ul style="list-style-type: none"> <li>Requires fewer number of new road crossings</li> <li>Impacts fewer developed parcels</li> <li>Has less probability of impacting archeological and/or historical sites</li> </ul>	<ul style="list-style-type: none"> <li>Crosses more incompatible land uses</li> <li>Impacts more wetlands</li> </ul>
<b>Mac West - Big Lake</b>	<ul style="list-style-type: none"> <li>Has more suitable soil conditions</li> </ul>	<ul style="list-style-type: none"> <li>Involves greater number of new road crossings</li> <li>Impacts more developed parcels</li> <li>Crosses more incompatible land uses</li> <li>Requires greater expenditure of train energy</li> <li>Crosses greater number of mapped anadromous streams</li> <li>Has higher probability of impacting archeological and/or historical sites</li> </ul>
<b>Mac East - Willow</b>	<ul style="list-style-type: none"> <li>Impacts fewer developed parcels</li> <li>Crosses more compatible land uses</li> <li>Impacts less wetlands</li> <li>Crosses fewest number of mapped anadromous streams</li> </ul>	<ul style="list-style-type: none"> <li>Has higher probability of impacting archeological and/or historical sites</li> <li>Fragments designated state recreation areas</li> </ul>
<b>Mac East - Houston North</b>	<ul style="list-style-type: none"> <li>Crosses more compatible land uses</li> <li>Impacts fewer developed parcels</li> <li>Requires smaller expenditure of train energy</li> <li>Has less probability of impacting archeological and/or historical sites</li> </ul>	<ul style="list-style-type: none"> <li>Has less suitable soil conditions</li> <li>Fragments a designated state recreation area</li> </ul>
<b>Mac East - Houston South</b>	<ul style="list-style-type: none"> <li>Impacts fewer developed parcels</li> <li>Crosses more compatible land uses</li> <li>Impacts less wetlands</li> <li>Has less probability of impacting archeological and/or historical sites</li> <li>Avoids designated state refuges and recreation areas</li> </ul>	<ul style="list-style-type: none"> <li>Requires moderate expenditure of train energy, but less than the Big Lake alternatives</li> <li>Involves moderate number of new road crossings, but less than the Big Lake alternatives</li> <li>Has less suitable soil conditions than the Big Lake alternatives, but better than the Houston North alternatives</li> </ul>
<b>Mac East - Big Lake</b>	<ul style="list-style-type: none"> <li>Avoids designated state refuges and state recreation areas</li> <li>Crosses more compatible land uses</li> <li>Has more suitable soil conditions</li> <li>Impacts less wetlands</li> </ul>	<ul style="list-style-type: none"> <li>Impacts more developed parcels</li> <li>Involves greater number of new road crossings</li> <li>Requires greater expenditure of train energy</li> <li>Crosses greater number of mapped anadromous streams</li> <li>Has higher probability of impacting archeological and/or historical sites</li> </ul>

# Historic Milestones & Anticipated STB Schedule



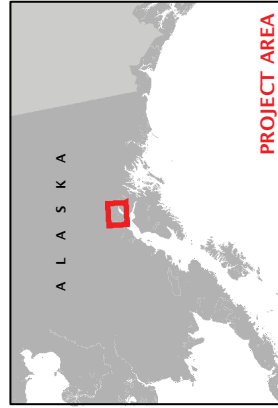
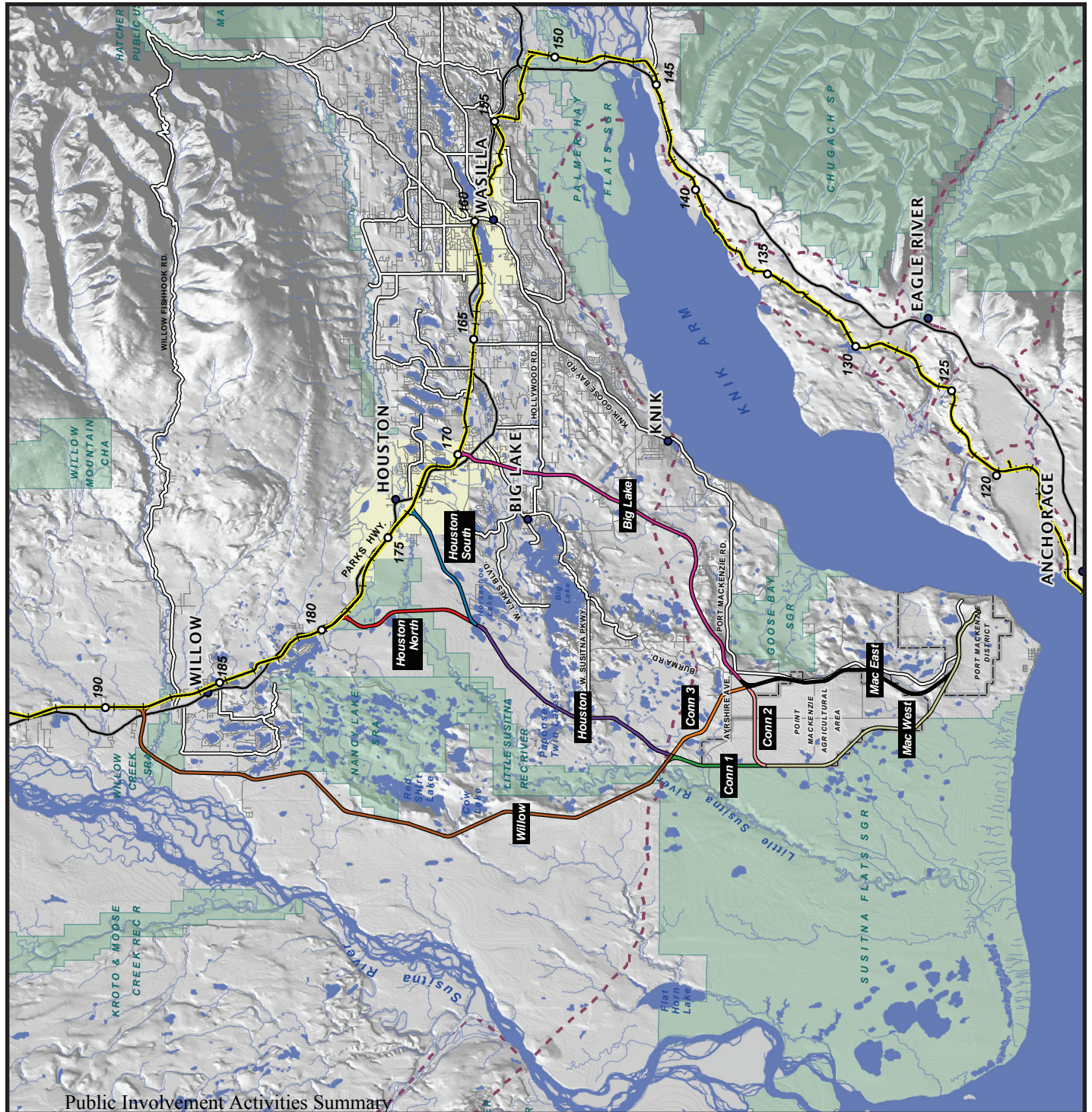
*(This page intentionally left blank.)*

ALTERNATIVES



LEGEND

- Preliminary Alternatives\*
- Mac East
  - Mac West
  - Conn 1
  - Conn 2
  - Conn 3
  - Houston
  - Houston North
  - Houston South
  - Willow
  - Big Lake
- ARRC Track
  - ARRC Milepost
  - Highway
  - Medium Rd.
  - Minor Rd.
  - Iditarod Trail
  - City Boundary
  - Park or Refuge



\*These lines generally represent corridors which are subject to further refinement.

This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown is for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: November 5, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, TNH-Hanson, USGS.

*(This page intentionally left blank.)*



# Port MacKenzie Rail Extension Criteria Matrix

Public Involvement Activities Summary  
January 2008

PROPOSED ROUTES	CRITERION <sup>a</sup>										+/-	Total	Cost Estimate <sup>e</sup> (millions)	
	1	2	3	4	5	6	7	8	9	10				
	Poor or Highly Compressible Soils (million cubic yards)	New Road Crossings (#)	Land Availability <sup>b</sup> (acres/mile)	Developed Parcels (#)	Designated Land Use <sup>c</sup> (acres)	Train Energy Required (horsepower-hours)	Wetlands (acres)	Mapped anadromous fish streams (#)	High Potential for Archaeological Sites <sup>d</sup> (acres)	Fragmentation of designated refuges and recreation areas (yes/no)				
Mac West - Willow	0	+	0	0	-	0	0	0	-	-	1/6/3	-2	\$320	-
Mac West - Houston North	-	+	0	+	-	+	-	-	+	-	4/1/5	-1	\$250	0
Mac West - Houston South	0	+	0	+	-	0	-	0	+	0	3/5/2	+1	\$220	+
Mac West - Big Lake	+	-	-	-	-	-	0	-	-	0	1/2/7	-6	\$290	0
Mac East - Willow	0	0	0	+	+	0	+	+	-	-	4/4/2	+2	\$330	-
Mac East - Houston North	-	0	0	+	+	+	0	0	+	-	4/4/2	+2	\$260	0
Mac East - Houston South	0	0	+	+	+	0	+	0	+	+	6/4/0	+6	\$230	+
Mac East - Big Lake	+	-	0	-	+	-	+	-	-	+	4/1/5	-1	\$285	0

a (+) Positive; (0) Neutral; (-) Negative. Criteria not weighted and routes are unranked.  
 b Large parcels of undeveloped land owned by the State of Alaska (land not specifically designated for parks or refuges),  
 c Lands that are designated for parks, refuges, or agricultural uses.  
 d Routes impacting greater than 500 acres were given a minus and  
 Matanuska-Susitna Borough, University of Alaska, Mental Health Trust, and Alaska Native corporations.  
 e Costs do not include approximately \$10 million for loop track construction within the port (all alternatives)  
 routes impacting less than 300 acres were given a plus.

# PROJECT MAILING LIST

*(This page intentionally left blank.)*

Name	Business	Address	City	State	Zip
Abbott Rod & Kathy		P.O. Box 1338	Palmer	AK	99645
Adams Dale	City of Houston	P.O. Box 940027	Houston	AK	99694
ADAMS HYDE PATRICIA		7021 MEADOW ST	ANCHORAGE	AK	99507
Adams Jim	National Wildlife Federation - Alaska	750 W. 2nd Avenue, Ste 200	Anchorage	AK	99501
Adams Juem		P.O. Box 940041	Houston	AK	99694
Akiesstad Gordon	Quality Auto Supply of AK	217 E. Arctic Street	Palmer	AK	99645
Alban Ron	Alban & Company CPAs	800 E. Dimond Blvd., Ste 3-625	Anchorage	AK	99515
Albertsen Ken	Law Offices of Kenneth D	P.O. Box 4368	Palmer	AK	99645
Acantra Rosetta	KNIK TRIBAL COUNCIL	1155 W. JOSSELIN LANE	PALMER	AK	99645
Alcorn Jack		P.O. BOX 871565	WASILLA	AK	99687
Alderman Leroy		P.O. Box 670045	Chugiak	AK	99567
Alexander Alice	Federal Railroad Administration	1120 Vermont Avenue NW, Room 6061 Mail Stop	Washington	DC	200005
ALICIA DEBORAH & VIK		110 W 38TH	ANCHORAGE	AK	99503
ALee Guy	Palmer Planning & Zoning Commission	463 N. Bonanza Street	Palmer	AK	99645
Allen Bill	City of Palmer	231 W. Evergreen Ave.	Palmer	AK	99645
AMBROSE CARROLL EDWARD		PO BOX 770987	EAGLE RIVER	AK	99577
Anderson Chris	Alaska Industrial Development & Export Authority	813 West Northern Lights Blvd.	Anchorage	AK	99503
Anderson Chris		19220 McCrary Road	Eagle River	AK	99577
ANDERSON JOHN & JEANINE		13741 ERVIN RD	ANCHORAGE	AK	99516
ANDERSON JOHN D & PATRICIA		8120 RESURRECTION DR	ANCHORAGE	AK	99504
ANDERSON JOHN H		PO BOX 111770	ANCHORAGE	AK	99511
ANDERSON JOHN L		PO BOX 1072	WILLOW	AK	99688
ANDERSON JON W & AMBER		PO BOX 498	WILLOW	AK	99688
ANDERSON JULIE B		432 W 10TH AVE	ANCHORAGE	AK	99501
Anderson Knute	IBEW	3333 Denali Street, Suite 200	Anchorage	AK	99503
ANDERSON MARK DAVE		PO BOX 175	WILLOW	AK	99688
Anderson Mary		PO Box 870854	Wasilla	AK	99687
Anderson Mary		PO Box 870854	Wasilla	AK	99687
ANDERSON MICHAEL K		PO BOX 221452	ANCHORAGE	AK	99522
ANDERSON NANCY J		11946 WILDERNESS DR	ANCHORAGE	AK	99516
ANDERSON RAYMOND L		PO BOX 100873	ANCHORAGE	AK	99510
ANDERSON SCOTT D		20325 LEPREHAUN DR	CHUGIAK	AK	99567
ANDERSON-SMITH MERVYN JR		PO BOX 200868	ANCHORAGE	AK	99520
ANDERSSON ENAR & PATRICIA		3761 PERENOSA BAY CIR	ANCHORAGE	AK	99515
ANDRESEN CARL IRVING		PO BOX 100001	ANCHORAGE	AK	99510
ANDREWS ALLEN A & CHRISTA		PO BOX 86	WILLOW	AK	99688
Andrews-Mee Barbara	U.S. Dept. of Defense	4724 Kershner Ave.	Anchorage	AK	99517
ANDRUS ROBT & JUDY L		9411 STRATHMORE DR	ANCHORAGE	AK	99502
ANGELL JACQUELINE R		PO BOX 140006	ANCHORAGE	AK	99514
ANGVIK JANE R		PO BOX 201348	ANCHORAGE	AK	99520
Anna Levinson		PO Box 689	WILLOW	AK	99688
ANSELM MICHAEL R & CHERI D		PO BOX 520007	BIG LAKE	AK	99652

ANSELM RAYMOND L & L F	PO BOX 7	WILLOW	AK	99688
ANSELM ROBT J & DONNA G	PO BOX 1054	WILLOW	AK	99688
ANTHONY ROBT C	PO BOX 90818	ANCHORAGE	AK	99509
ANTONACCI ANGELIKA C	9499 BRAYTON DRIVE	ANCHORAGE	AK	99507
ANTONIO JAS L & ALISSA L	1049 S FELTON ST	PALMER	AK	99645
ANTONIO JOVEN & PIE	1650 ELCADOR DR	ANCHORAGE	AK	99507
APPELO TRACY L	14639 W MARGINAL ACCESS	HOUSTON	AK	99694
AQUINO OSWALD R & ELVIRA	4230 VISCOUNT CIR	ANCHORAGE	AK	99502
ARAS LESLIE C	PO BOX 771895	EAGLE RIVER	AK	99577
ARCAND GEO L	4530 OMALLEY RD	ANCHORAGE	AK	99507
ARD PAUL & KELLEY D	200 W 34TH AVE	ANCHORAGE	AK	99503
ARDIZZONE CHAS D & DENISE	19623 HIGHLAND RIDGE DR	EAGLE RIVER	AK	99577
ARENAS RIZA N	1881 E DELLWOOD ST	WASILLA	AK	99654
AREND GREG	8110 KRONOS DR	ANCHORAGE	AK	99502
ARIMA WALLACE S & KATHLEEN	PO BOX 520740	BIG LAKE	AK	99652
ARLT ANTHONY R	PO BOX 110984	ANCHORAGE	AK	99511
ARMISTEAD CLYDE G	19432 FIRST ST	EAGLE RIVER	AK	99577
ARMSTRONG CHAD & CARLA	PO BOX 671463	CHUGIAK	AK	99567
ARMSTRONG JAS D	PO BOX 445	WILLOW	AK	99688
ARMSTRONG JEFF	102 W JONES ST	YACOLT	WA	98675
ARMSTRONG KEVIN & DIANE S	620 HIGHVIEW DR	ANCHORAGE	AK	99515
ARMSTRONG LUJELLA CHARMLEY	7431 PECK AVE	ANCHORAGE	AK	99504
ARMSTRONG MARGUERITE	8617 LAKE OTIS PKY	ANCHORAGE	AK	99507
ARMSTRONG RICHARD S & M M	3700 BONIFACE PKY	ANCHORAGE	AK	99504
ARNDT CHRISTOPHER N & S K	PO BOX 372	WILLOW	AK	99688
ARNDT CURT	PO BOX 520750	BIG LAKE	AK	99652
ARNETT REV TR	6740 MARQUEZ CIR	ANCHORAGE	AK	99516
ARNOLD J & KAYLA M WOODARD	10900 LAKE OTIS PKWY	ANCHORAGE	AK	99516
ARNOLD JEFFERY R	PO BOX 520780	BIG LAKE	AK	99652
AROSA JOHN C	6189 DEBARR RD	ANCHORAGE	AK	99509
ARTERBURN WM N & MARY JO	PO BOX 609	WILLOW	AK	99688
ARTHUR C MATHIAS	1231 W 7TH AVE	ANCHORAGE	AK	99501
ASCHE GEO P & P.J.	PO BOX 3230	PALMER	AK	99645
ASCHEBRENNER LAWRENCE&CS	601 VINE AVE	ANCHORAGE	AK	99501
ASHER KEVIN C	22724 MILLA RD	CHUGIAK	AK	99567
ASHLAND STAN G&BRITTANY M	PO BOX 520606	BIG LAKE	AK	99652
ASHLEY DONALD E & ANGELA	PO BOX 575	WILLOW	AK	99688
ASKREN WILLARD R & CAROL A	6211 PROSPERITY DR	ANCHORAGE	AK	99504
ASLAKSON SCOTT DOUGLAS	PO BOX 940121	HOUSTON	AK	99694
ASPGREN JON R	17508 KAHILTNA DR	EAGLE RIVER	AK	99577
ATCHLEY STACEY G&HERSCHEL	PO BOX 243556	ANCHORAGE	AK	99524
ATHANAS LARRY & JANET	PO BOX 1444	BETHEL	AK	99559
ATIENZA ALMARIE S	PO BOX 202583	ANCHORAGE	AK	99520

ATKINS SYBYL	PO BOX 253	WILLOW	AK	99688
ATKINSON KENNETH R	1336 W 15TH AVE	ANCHORAGE	AK	99501
ATKINSON TIMOTHY&YONG CHA	PO BOX 770596	EAGLE RIVER	AK	99577
Attaliades Gordon	18297 Pt. MacKenzie Road	Wasilla	AK	99654
ATTUNGANA HILDA	PO BOX 520644	BIG LAKE	AK	99652
ATWATER VALERIE GAIL	PO BOX 672129	CHUGIAK	AK	99567
Aube William	P.O. Box 1107	Palmer	AK	99645
AUBIN GEO	PO BOX 521205	BIG LAKE	AK	99652
AUBREY EDITH CATHERINE	1905 ROOSEVELT DR	ANCHORAGE	AK	99517
AUBRY DAVID D & BRENDA J	PO BOX 940103	HOUSTON	AK	99694
AUDETTE CHAS	5071 VANCE DR	ANCHORAGE	AK	99508
AUDINO MARY JANE SHEETS	16330 NIKITA DR	EAGLE RIVER	AK	99577
AUER ROBT & ELISABETH	6310 WOODDED CIR	ANCHORAGE	AK	99502
AUF DER HEIDE BRIAN T	9143 CRANBERRY WAY	ANCHORAGE	AK	99502
AUGENSTEIN LUTHER R & Y I	3223 W 32ND AVE	ANCHORAGE	AK	99517
AUGUSTIN ROY & GELIA	2806 IRIS DR	ANCHORAGE	AK	99517
AUMICK TODD C & KRISTA M	PO BOX 520825	BIG LAKE	AK	99652
AURE JOHN E & TARA M	PO BOX 773653	EAGLE RIVER	AK	99577
AUSDAHL DAVID M	PO BOX 940214	HOUSTON	AK	99694
AUSMAN EARLE V	1503 W 33RD AVE # 310	ANCHORAGE	AK	99503
AUSTERMAN JOEY & JULIE	937 DAVID PL	ANCHORAGE	AK	99501
AUSTERMUHL DONALD& NOREEN	PO BOX 521281	BIG LAKE	AK	99652
AUSTIN B JENSEN	10870 KAMISHAK BAY CIR	ANCHORAGE	AK	99515
AUSTIN JEFFREY A& RUTHANN	2220 KENTLY CT	ANCHORAGE	AK	99502
Austin Mark	P.O. Box 1354	Palmer	AK	99645
AVALON SLADE & ZOLARI B	PO BOX 325	WILLOW	AK	99688
AVELLANEDA JAS & MARIA T	3402 LOIS DR	ANCHORAGE	AK	99517
EVERY JON P	PO BOX 140181	ANCHORAGE	AK	99514
AVEZAC FRANCIS L	4240 RABBIT CRK RD	ANCHORAGE	AK	99516
AVILA JASON G	PO BOX 48	WILLOW	AK	99688
AWE CLAYTON M & KAY M	PO BOX 112014	ANCHORAGE	AK	99511
AX DALE A & SHELLEY L	PO BOX 520512	BIG LAKE	AK	99652
AXTELL CHRISTOPHER	16505 MARCUS ST	EAGLE RIVER	AK	99577
Axtell Ron	2501 Commercial Dr	Anchorage	AK	99501
Axtell Ron	2501 Commercial Dr	Anchorage	AK	99501
AYERS JEAN	7800 TALISMAN RD	ANCHORAGE	AK	99516
Ayers Wanetta	3300 Arctic Blvd., Ste. 203	Anchorage	AK	99503
AYOT PATRICIA P	PO BOX 520927	BIG LAKE	AK	99652
AZAR JUNG H	4530 MONTROSE CIR	ANCHORAGE	AK	99515
AZZAM ALI	6928 TIMOTHY ST	ANCHORAGE	AK	99502
AZZAM HUSEIN A	16101 ST JAMES ST	ANCHORAGE	AK	99516
BABB RONALD A & LINDA K	25460 EAGLE RIVER RD	EAGLE RIVER	AK	99577
BABBITT KEVIN	HC 89 BOX 240B	WILLOW	AK	99688
	Pt. MacKenzie Community Council			
	Daybreak Inc.			
	Laborers Local 341			
	Laborers Local 341			
	Southwest Alaska Municipal Conference			

Babcock Tuckerman	Matanuska Electric Association				
BACHAND A L & RIVERO JOS M		P. O. Box 2929		Palmer	99645
BACHE ROBT E		6710 E 10TH AVE		ANCHORAGE	99504
BACHELDER JOEL E & JENNY L		6950 BAXTER TER		ANCHORAGE	99504
BACKLIN CHAS		1011 W 76TH AVE		ANCHORAGE	99518
BACKUS DAVID A		PO BOX 520376		BIG LAKE	99652
BACON A ROBT TR		2629 E 88TH AVE		ANCHORAGE	99507
BAGCAL ADRIAN QUINTUA II		PO BOX 200743		ANCHORAGE	99520
BAGGETT ROGER LEE		820 JAY CIR		ANCHORAGE	99504
BAGNALL ROBT		PO BOX 940328		HOUSTON	99694
BAGNESCHI JERRY W & DREMA		2400 BARROW		ANCHORAGE	99503
BAHNG KEE		2822 KRISTEN CIR		ANCHORAGE	99507
BAHR ERNEST J		16126 BRIDGEVIEW DR		ANCHORAGE	99516
BAIK INSOOK		PO BOX 13450		TRAPPER CREEK AK	99683
BAILEY BERT R & BONNIE J		2231 BELMONT DR		ANCHORAGE	99517
BAILEY CARA E		PO BOX 520432		BIG LAKE	99652
BAILEY CLAUDE C & RUTH P		18747 MONASTERY DR		EAGLE RIVER	99577
BAILEY FAM TR		350 W INTL AIRPORT STE100		ANCHORAGE	99518
BAILEY FRANK T & JANEEN M		9200 PROSPECT DR		ANCHORAGE	99507
BAILEY GLEN S & DAWN M		3630 LOC SAULT		ANCHORAGE	99516
BAILEY MICHAEL J		1821 WESTCHESTER CIR		ANCHORAGE	99507
BAILEY ROBT W & BECKY L		5400 ARCTIC BLVD		ANCHORAGE	99518
BAILEY RONALD		5432 W 73RD AVE		ANCHORAGE	99502
BAILEY RONALD C & TERESA E		1150 S COLONY WAY STE 3		Palmer	99645
BAILEY S RANDOLPH & LORA A		910 KATHY PL		ANCHORAGE	99504
BAIN RODERICK & DONELLE		PO BOX 210995		ANCHORAGE	99521
BAINTER TILLIE MAY		2406 E 27TH AVE		ANCHORAGE	99508
BAIOCCHI ANTHONY M & A M		216 N FLOWER ST		ANCHORAGE	99508
BAIRD CHAS F III & HOPE E		1555 D ST		ANCHORAGE	99501
BAIRD DOUGLAS H		22833 EAGLE RIVER RD		EAGLE RIVER	99577
BAIRD ROBT W & WILMA H		8221 KIP CT		ANCHORAGE	99507
Baker Carol		1201 DENALI STREET #213		ANCHORAGE	99501
BAKER DEBRA R		PO Box 90084		Anchorage	99501
BAKER DONALD ROBT & MARY		4300 HARRISON ST		ANCHORAGE	99503
BAKER GARY		6274 TRAPPERS TRAIL RD		ANCHORAGE	99516
Baker Gary		P O BOX 964		TALKEETNA	99676
BAKER GORDON A & CAROL K		PO BOX 13318		TRAPPER CREEK AK	99683
BAKER JOS M		PO BOX 90084		ANCHORAGE	99509
BAKER JOS M & ROBIN		PO BOX 520736		BIG LAKE	99652
BAKER KENNETH E		4668 S HYDROP LANE CIR		BIG LAKE	99652
BAKER PHILLIP		PO BOX 521181		BIG LAKE	99652
BAKER ROBERTA A GUINN		2305 SUSITNA DR		ANCHORAGE	99517
BAKER SANDRA K		PO BOX 111905		ANCHORAGE	99511
		18848 W JEFFS LOOP		BIG LAKE	99652

BAKICH CATHERINE C	PO BOX 940252	HOUSTON	AK	99694
BAKKEN MARK G & JENNIFER A	3540 DORA AVE	ANCHORAGE	AK	99516
BALDWIN GREGG	8215 STORMY PL	ANCHORAGE	AK	99518
BALDWIN KEITH A & CYNTHIA	3340 SEAWIND DR	ANCHORAGE	AK	99516
BALDWIN WALTER L & LEONA	3127 COTTONWOOD ST	ANCHORAGE	AK	99508
BALES STEVEN R & JILL E	300 OCEANVIEW DR	ANCHORAGE	AK	99515
BALHAR PETER & CHRISTIE	PO BOX 520486	BIG LAKE	AK	99652
BALICH CAROL A	600 W 34TH	ANCHORAGE	AK	99503
BALLARD DAVID R & DEBORAH L	PO BOX 190962	ANCHORAGE	AK	99519
BALLARD DENNIS & MIKI M	6221 PETERSBURG ST	ANCHORAGE	AK	99507
BALLARD JUDITH A	PO BOX 13034	TRAPPER CREEK	AK	99683
BALLARD THERESA	PO BOX 520036	BIG LAKE	AK	99652
BALLARD WALTER W & SHARON	PO BOX 520564	BIG LAKE	AK	99652
BANCROFT EDW & FRANCINE	8670 KUSHTAKA CIR	ANCHORAGE	AK	99504
BANDEL DANE L & JUDY A	16435 BETTY	ANCHORAGE	AK	99516
BANDULET ROMAN W	PO BOX 210704	ANCHORAGE	AK	99521
BANKS DAVID	11706 LOVELAND CIR	EAGLE RIVER	AK	99577
Banks David	715 L Street, Suite 100	Anchorage	AK	99501
BANKS HARRY L	PO BOX 534	WILLOW	AK	99688
BANOVICH THOS R & LINDA C	1431 EARLYVIEW DR	ANCHORAGE	AK	99504
BAPTISTE NORBERT JOHN	7071 BAXTER TERRACE	ANCHORAGE	AK	99504
BARBEE MARK R J	800 E DIMOND STE 3-151	ANCHORAGE	AK	99515
BARBER JEFF	4841 E 115TH ST	ANCHORAGE	AK	99516
BARBER TAMMY & SHANE	PO BOX 323	WILLOW	AK	99688
BARCE DAVID & BETTY	2231 MINERVA WAY	ANCHORAGE	AK	99515
BARCOME LYNDA M	PO BOX 202	WILLOW	AK	99688
Barelka Paul	PO Box 898692	Wasilla	AK	99687
BARENZ RALPH & MARGARET	PO BOX 770967	EAGLE RIVER	AK	99577
BARGABOS KURT	PO BOX 521836	BIG LAKE	AK	99652
Bargabos Mike	PO Box 521546	Big Lake	AK	99652
BARGABOS STEVEN M	17201 W LIGHTHOUSE DR	BIG LAKE	AK	99652
BARKER DOUGLAS E	2419 N TAHITI LOOP	ANCHORAGE	AK	99507
Barker Jeanette	723 S. Valley Way	Palmer	AK	99645
BARKER MARIE T	1305 S CENTER POINT DR	Wasilla	AK	99654
BARKER MARILYN H	13431 WINDRUSH CIR	ANCHORAGE	AK	99516
BARKLEY KENNETH W & LOTES	PO BOX 670984	CHUGIAK	AK	99567
BARKSDALE DAVID A	PO BOX 244894	ANCHORAGE	AK	99524
BARLEEN DENNY D & JEANNETT	3848 IMAGE DR	ANCHORAGE	AK	99504
Barlow Richard	P.O. Box 1887	Palmer	AK	99645
BARNELL CORRINE	16613 CARLA ST	EAGLE RIVER	AK	99577
BARNER DEBORAH K	1141 DELWOOD DR	Wasilla	AK	99654
BARNER MICHAEL H	4261 LAKERIDGE CIR	ANCHORAGE	AK	99502
BARNES GENE P & ANNA M	2416 CLEMENTS DR	ANCHORAGE	AK	99515

The Nature Conservancy of Alaska

Palmer Chamber of Commerce  
DOMABE LTD PRTRNSHP

Valley Alliance of Labor Employees



Barnes John	Marathon Oil Company	PO Box 196168	Anchorage	AK	99519-61
BARNES MONICA A		4001 IONA CIR	ANCHORAGE	AK	99507
BARNES PAMELA JO		5416 W DIMOND BLVD	ANCHORAGE	AK	99502
BARNETT JESSE L & CHI-NAM		700 W FIREWEED LN	ANCHORAGE	AK	99503
BARNETT LAURIE A		1561 ELCADORE	ANCHORAGE	AK	99507
BARNEY DAVID R & KATHLEEN		PO BOX 520894	BIG LAKE	AK	99652
BARNEY STANLEY L		340 E 95TH CT	ANCHORAGE	AK	99515
BARNHILL BENJAMIN E & J M		1623 BRINK DR	ANCHORAGE	AK	99504
Barr Chas		11609 Hebron Dr	Eagle River	AK	99577
BARR ELIZABETH		3301 SHERRIE ST	ANCHORAGE	AK	99504
BARRETT JACK A & DAWN E		PO BOX 770468	EAGLE RIVER	AK	99577
BARRETT LANNY F		PO BOX 521264	BIG LAKE	AK	99652
BARRETT PAULA		2136 LOUSSAC DR	ANCHORAGE	AK	99517
Barrett Roscoe	City of Palmer	231 W. Evergreen Ave.	Palmer	AK	99645
BARRETT WM F		PO BOX 520754	BIG LAKE	AK	99652
Barrier Rick	Commonwealth North	810 N Street, Suite 202	Anchorage	AK	99501
BARRINGTON ROBIN		5420 TONGA ST	ANCHORAGE	AK	99507
BARRON JEFFEREY K & M I		PO BOX 940443	HOUSTON	AK	99694
BARRON ROY R JR EST		7400 UPPER DEARMOUN RD	ANCHORAGE	AK	99516
BARROWS THOS B		PO BOX 521434	BIG LAKE	AK	99652
Barry Straton	Alaska Dept. of Fish & Game	333 Raspberry Road	Anchorage	AK	99518
BARRY WM S		1901 JARVIS AVE	ANCHORAGE	AK	99515
BARTEE TERENCE		PO BOX 112947	ANCHORAGE	AK	99511
BARTEL MARTIN WADE		HC 89 BOX 417	WILLOW	AK	99688
BARTENSTEIN JEFFREY C		411 HIGHVIEW DR	ANCHORAGE	AK	99515
BARTH JOS A & K M TRES		3931 JAMES DR	ANCHORAGE	AK	99504
BARTLETT MARCI		PO BOX 867	WILLOW	AK	99688
BARTLETT PETER		13700 JARVI DR	ANCHORAGE	AK	99515
BARTON DAVE & MICHELLE		6108 STAEDEM DR	ANCHORAGE	AK	99504
BARVE LANCE A & PAMELA A		HC 89 BOX 248	WILLOW	AK	99688
BARVINCHAK JOS M		4794 MILLS DR	ANCHORAGE	AK	99508
BASCONCILLO EPIFANIO D		2600 PUFFIN POINT	ANCHORAGE	AK	99507
Baskin Lance	B&B Farms	PO Box 877298	Wasilla	AK	99687
BASLER JOHN T		PO BOX 685	WILLOW	AK	99688
BASS CHAS ROBT		PO BOX 520293	BIG LAKE	AK	99652
BASSNEY PETER J&ELIZABETH		1101 CHESAPEAKE AVE	Wasilla	AK	99654
BATES PAUL S & LEIGH M		17421 KAHILTNA DR	EAGLE RIVER	AK	99577
BATES THOS W & PATRICIA J		1672 IRA DR	ANCHORAGE	AK	99515
BATHE DAVID J & SUSAN M		4351 DEARMOUN RD	ANCHORAGE	AK	99516
BAUGHMAN LANCE E & TAMARA		4700 MILLS DR	ANCHORAGE	AK	99508
BAUM JAS L		4389 MARS DR	ANCHORAGE	AK	99507
BAUMLER GARY G & DONNA G		9627 EAGLE RIVER LN	EAGLE RIVER	AK	99577
BAXTER CHRISTOPHER VON		1437 I ST	ANCHORAGE	AK	99501

BAXTER JESSE D	PO BOX 375	WILLOW	AK	99688
BAXTER LETITIA A	2301 EAGLE ST	ANCHORAGE	AK	99503
BAXTER TIM & SUSAN L	PO BOX 249	WILLOW	AK	99688
BAYER ROBT & SANDRA	12940 SUMMER CIR	ANCHORAGE	AK	99516
BAYLE TRACY M & MARINA S	206 E FIREWEED LN	ANCHORAGE	AK	99503
BAYS MARY A	111 FIRST AVE SW	AITKIN	MN	56431
Bays Phyllis	P.O. Box 527	Talkeetna	AK	99676-05
BEACH PAMELA K	16210 E HIDDEN HILLS RD	WILLOW	AK	99688
BEACHAM SHERMAN	1320 NICHOLS ST	ANCHORAGE	AK	99508
BEAGLEY KEVIN & SHERRIE	1011 W 20TH AVE	ANCHORAGE	AK	99503
BEALL GARY W	11213 S LEWIS LOOP	WASILLA	AK	99654
BEALL JAMES L	2225 CULVER PL	ANCHORAGE	AK	99503
BEALS DONALD O	11235 VIA BALBOA	ANCHORAGE	AK	99515
BEASLEY WM D SR	PO BOX 520302	BIG LAKE	AK	99652
BEATON BARBARA J	10346 HALF HITCH DR	ANCHORAGE	AK	99515
BEATON TABB R & DEVONA R	4150 GALACTICA DR	ANCHORAGE	AK	99517
BEAUJEAN DON C	PO BOX 240771	ANCHORAGE	AK	99524
BEAUSHAW MARK	PO BOX 940333	HOUSTON	AK	99694
BEBEE DAVID C & VIRGINIA R	1150 E PULLMAN DR	WASILLA	AK	99654
BECK EMILY A	4051 E 84TH AVE	ANCHORAGE	AK	99507
BECK JESSICA J	PO BOX 220566	ANCHORAGE	AK	99522
BECK LEWIS M & CYNTHIA M	1230 N 5900 E	EDEN	UT	84310
BECK MICHAEL F & CANDICE L	2160 INNES CIR	ANCHORAGE	AK	99515
BECK RICHARD W & ALICE M	22541 MCMANUS	CHUGIAK	AK	99567
BECKER JAS A & VIRGINIA E	3116 MOUNTAIN VIEW DR	ANCHORAGE	AK	99501
BECKER STEVEN M	HC 89 BOX 455	WILLOW	AK	99688
BEDARD RANDALL L & LAURA J	PO BOX 564	WILLOW	AK	99688
BEECH ANTHONY L	PO BOX 520865	BIG LAKE	AK	99652
BEECH BERNARD E & DOREEN J	PO BOX 520193	BIG LAKE	AK	99652
BEECH JOSEPH E & AIMEE M	PO BOX 520823	BIG LAKE	AK	99652
BEECH LAVINA L	PO BOX 520388	BIG LAKE	AK	99652
BEEK DAN'L BURT	PO BOX 111714	ANCHORAGE	AK	99511
BEEMAN DAVID	2224 DAYBREAK CT	ANCHORAGE	AK	99501
BEESENG THOS P & WINONA M	PO BOX 672226	CHUGIAK	AK	99567
BEESON JAS M & SHARON	PO BOX 1117	WILLOW	AK	99688
Begich Mark	632 W 6th Ave Ste 840	Anchorage	AK	99501
BEGNAUD LLOYD L/E	PO BOX 520841	BIG LAKE	AK	99652
BEHM ARTHUR M	370 OCEANVIEW DR	ANCHORAGE	AK	99515
BEHM SIMON J & SANDRA A	5940 WINDING WAY	ANCHORAGE	AK	99504
BEHNKE FREDRICK D	PO BOX 521522	BIG LAKE	AK	99652
BEHNKE WM C & SANDRA L	3323 LAKESIDE DR	ANCHORAGE	AK	99515
BEHRENS LISA A	PO BOX 520443	BIG LAKE	AK	99652
BEILMAN TED J	7361 HUNTSMEN CIR	ANCHORAGE	AK	99518

Bays Bed & Breakfast

Municipality of Anchorage

Beischer Greg	Bristol Environmental Engineering	111 W. 16th Avenue, Ste 301	Anchorage	AK	99501
BEISEMEYER DAVID P		11500 HAWKINS LN	ANCHORAGE	AK	99516
BEISWENGER ALLAN & SUSAN		4111 URSA CIR	ANCHORAGE	AK	99517
BEITINGER JAVAN M		2137 CRATAEGUS AVE	ANCHORAGE	AK	99508
BEITTER HORST & URSULA		1646 BARTLETT DR	ANCHORAGE	AK	99507
BELAIR PAUL R & BETHANY		PO BOX 521520	BIG LAKE	AK	99652
BELANGER MICHAEL L		PO BOX 520346	BIG LAKE	AK	99652
BELARDE C S & PATRICIA A		1217 E 66TH AVE	ANCHORAGE	AK	99518
Belaugh Dave		225 Cordova Street	Anchorage	AK	99501
BELCHER JAS C		PO BOX 520994	BIG LAKE	AK	99652
BELCHER ROBT D		PO BOX 770604	EAGLE RIVER	AK	99577
BELK FRANCO R & KRISTIE L		16907 HANSON DR	EAGLE RIVER	AK	99577
BELK HAROLD & NANETTE		PO BOX 771193	EAGLE RIVER	AK	99577
BELL CHARLOTTE		PO BOX 147	WILLOW	AK	99688
BELL GEO ROBT & JOANNE		1124 JAY ST	COLUSA	CA	95932
BELL GREG		2048 ESQUIRE DR	ANCHORAGE	AK	99517
BELL JEFFREY O & TAMERA J		13145 HILLSIDE DR	ANCHORAGE	AK	99516
BELL JESSE C		1400 W 12TH	ANCHORAGE	AK	99501
BELL JIMMIE		1736 KATRINA CIR	ANCHORAGE	AK	99504
BELL JORDON L		PO BOX 436	WILLOW	AK	99688
Bell Michael	Alaska Trucking Association	3443 Minnesata Dr.	Anchorage	AK	99503
BELL RICHARD		PO BOX 940343	HOUSTON	AK	99694
BELL THOS H		PO BOX 131	WILLOW	AK	99688
BELLEN EDNA D		180 GRAND LARRY	ANCHORAGE	AK	99504
Beller Linda		1020 S. Bailey Street	Palmer	AK	99645
Bellinger Margaret	Mat-Valley Federal Credit Union	P.O. Box 520931	Big Lake	AK	99652
BELMAN ROBT E & BARBARA	Big Lake Community Council	PO BOX 520453	BIG LAKE	AK	99652
BELOHLAV JASON B		640 E 74TH AVE	ANCHORAGE	AK	99518
BELTZ GREGORY A		437 BLISS ST	ANCHORAGE	AK	99508
BELTZ THOS L JR		1270 N WAMPAM CIR	WASILLA	AK	99654
BEMMA LUE FLOYD		1016 E 17TH AVE	ANCHORAGE	AK	99501
BENDER CLAYTON E & L S		PO BOX 774403	ANCHORAGE	AK	99577
Bender Mike	Whittier Ports and Harbor	5754 Waterfall Dr	Eagle River	AK	99577
BENDLE PAUL R JR & ERIN M		PO BOX 671536	CHUGIAK	AK	99567
BENGTSON DAREN		PO BOX 520155	BIG LAKE	AK	99652
Benjamin Aaron	Talkeetna River Adventures LLC	P.O. Box 473	Talkeetna	AK	99676-
BENKERS INEKE C.		2530 CHANDALAR DR	ANCHORAGE	AK	99504
BENNETT FRANK E		PO BOX 924	WILLOW	AK	99688
BENNETT KEVIN J & COLLETTE		8826 SPRUCE BROOK ST	ANCHORAGE	AK	99507
BENNETT LARRY D		PO BOX 447	WILLOW	AK	99688
BENNETT ROSS D & ERIKA E		4301 MANYTELL AVE	ANCHORAGE	AK	99516
BENNETT STEVE & TAMMY		PO BOX 940377	HOUSTON	AK	99694
BENNETT WM J		2329 E 20TH AVE	ANCHORAGE	AK	99508

BENSON BILLY J & SARA C	Northern Stars	PO BOX 520565	BIG LAKE	AK	99652
Benson Diane		P.O. Box 770369	Eagle River	AK	99577
BENSON MICHAEL J		5916 WINDING WAY	ANCHORAGE	AK	99504
BENSON PAUL L & CHERYL A		12145 RAINBOW AVE	ANCHORAGE	AK	99516
BENSON RANDY & CHERYL		1810 WESTVIEW CIR	ANCHORAGE	AK	99504
BENSON RONALD & DONNA		PO BOX 520701	BIG LAKE	AK	99652
BENTON DAN'L		PO BOX 4724	PALMER	AK	99645
BENTON DAN'L		PO BOX 4724	PALMER	AK	99645
BENTON JAS F & JUDITH L		13320 COVE CIR	ANCHORAGE	AK	99515
BENTZ JEFF		PO BOX 104383	ANCHORAGE	AK	99510
Bentz Jeff	North Star Terminal & Stevedore Company LLC	P.O. Box 102019	Anchorage	AK	99510
BENTZ JOHN J		PO BOX 112663	ANCHORAGE	AK	99511
Berberich Greg	Matanuska Telephone Association	1740 S. Chugach	Palmer	AK	99645
BEREAN SANDRA D		PO BOX 520526	BIG LAKE	AK	99652
Berg Catherine	U.S. Fish & Wildlife Service	605 W. 4th Avenue, Room G-62	Anchorage	AK	99501
BERG DEAN S REV TR TRE		7913 ARLENE ST	ANCHORAGE	AK	99502
BERG KEVIN C & SHARON L		PO BOX 521165	BIG LAKE	AK	99652
BERGERON HENRI & SUE A		PO BOX 100692	ANCHORAGE	AK	99510
Bergerson Jason	North Slope Borough	3000 C St Ste 200	Anchorage	AK	99503
BERGGREN LISA ANN SYREN		9671 MAINTREE DR	ANCHORAGE	AK	99516
BERGLUND ERIC		4100 WILLOW DR	ANCHORAGE	AK	99517
BERGMAN ELISABETH M		PO BOX 112167	ANCHORAGE	AK	99511
BERGSTEDT MACK W & SHARON		11500 OUR RD	ANCHORAGE	AK	99516
Berkley Tilton	Knik-Fairview Community Council	P.O. Box 870948	Wasilla	AK	99687
BERNARD & PENELOPE SAWYER		10301 OUR RD	ANCHORAGE	AK	99516
Bernier Kristy	Greater Palmer Chamber of Commerce	723 South Valley Way	Palmer	AK	99645
Berry Clint	Denali Borough	P.O. Box 3008	Anderson	AK	99744
BERRY ROBT THOS JR & D M		PO BOX 231034	ANCHORAGE	AK	99523
Berry Ted		PO BOX 49	WILLOW	AK	99688
BERRY TED L & JEAN A		PO BOX 492	WILLOW	AK	99688
Berta Gardner	Alaska State Legislature	716 W. 4th Avenue, Ste 340	Anchorage	AK	99501
BERTINO JOHN O & NANCY J		2621 E 20TH AVE	ANCHORAGE	AK	99508
BEST HELEN M		PO BOX 671282	CHUGIAK	AK	99567
BEST LANCE		PO BOX 745	WILLOW	AK	99688
Best Richard	City of Palmer	1150 W. Josselin Lane	Palmer	AK	99645
BESTRAM TIMOTHY E		PO BOX 940403	HOUSTON	AK	99694
BESTRY DOUG		PO BOX 772113	EAGLE RIVER	AK	99577
Bethe Mike	Alaska Dept. of Natural Resources	1800 Glenn Hwy, Suite 12	Palmer	AK	99645-67
BETHEL PAUL & YOLANDA		2421 LEE ST	ANCHORAGE	AK	99504
BETHKA RICHARD & ALICE M		3506 COTTONWOOD ST	ANCHORAGE	AK	99508
BETHKE ARTHUR W & MILDRED		2424 JUNEAU ST	ANCHORAGE	AK	99508
Bettine Cindy	ABC Travel Time	234 West Evergreen Ave	Palmer	AK	99645
BETTISWORTH KENNETH&ROBIN		1518 E ST	ANCHORAGE	AK	99501

Bettye Davis	Alaska State Legislature	716 W. 4th Avenue, Ste 450	Anchorage	AK	99501
BETZ TIMOTHY & BETZ		7125 ALATNA AVE	ANCHORAGE	AK	99516
BEYER LOIS A		1730 W DIMOND BLVD	ANCHORAGE	AK	99515
BICHSEL TOM		2705 EAGLE ST	ANCHORAGE	AK	99503
BICKFORD JAY & COLLEEN		8300 DIMOND HOOK DR	ANCHORAGE	AK	99507
BIDASOLO RICHARD& REBECCA		PO BOX 520929	BIG LAKE	AK	99652
BIDDLE JAS & CHERIE		3307 BONIFACE PKY	ANCHORAGE	AK	99504
BIEDERMAN PAUL A & J E		121 W FIREWEED LN	ANCHORAGE	AK	99503
Bielawski Patty	Jade North LLC	1336 W 12th Ave	Anchorage	AK	99501
BIEVER GENE A & DEBRA M		3019 DONNINGTON DR	ANCHORAGE	AK	99504
BIGELOW ROBERT & KAREN		400 PEPPERTREE LOOP	ANCHORAGE	AK	99504
BIGGER STEVEN L		PO BOX 968	WILLOW	AK	99688
BIGGERSTAFF MEGAN		PO BOX 520393	BIG LAKE	AK	99652
BIGGS MELBA R		3299 MONTPELIER CT	ANCHORAGE	AK	99503
BIGGS THOS A & LOURDES N		8740 BELL PL	ANCHORAGE	AK	99507
BIGLER STUART & KATHERINE		PO BOX 336	WILLOW	AK	99688
BIGNELL SHANE J & DEBRA S		PO BOX 520207	BIG LAKE	AK	99652
BIGWOOD NICHOLAS A		3424 SHERRIE ST	ANCHORAGE	AK	99504
BILBAO PEDRO		4660 CARAVELLE DR	ANCHORAGE	AK	99502
Bill & Donna Marks		PO BOX 329	WILLOW	AK	99688
Bill Fikes		HC 34 Box 2157	Wasilla	AK	99654
BILLINGER GERARD S & M W		PO BOX 520593	BIG LAKE	AK	99652
Billingier Margaret	Big Lake Chamber of Commerce	P.O. Box 520067	Big Lake	AK	99652
Billingier Margaret	Prudential Jack White/ Vista Real Estate	P.O. Box 520593	Big Lake	AK	99652
BILLINGS ARTHUR BARRY		PO BOX 521427	BIG LAKE	AK	99652
BILLS MARK		PO BOX 495	WILLOW	AK	99688
BILLS PATRICIA SUE		PO BOX 44	WILLOW	AK	99688
BILLSBOROUGH TROY C & H		PO BOX 112594	ANCHORAGE	AK	99511
BILLY UPCHURCH		108 W COOK AVE	ANCHORAGE	AK	99501
Bingham Allen		3601 C St Ste 600	Anchorage	AK	99503
BINGHAM ALLEN & BEVERLY	Mikunda Cottrell & Co., CPA's	7241 STAMPS CIR	ANCHORAGE	AK	99507
BINKOWSKI MABLE K		PO BOX 744	WILLOW	AK	99688
BIRCH STEPHANIE		4740 E 115TH AVE	ANCHORAGE	AK	99516
BIRD CARL C		PO BOX 671961	CHUGIAK	AK	99567
BIRD KARL & KIRA		PO BOX 672575	CHUGIAK	AK	99567
Birdsall Sarah	KTNA-FM	P.O. Box 300	Talkeetna	AK	99676
BISH MARVIN F & NANCY K		2011 TUDOR HILLS DR	ANCHORAGE	AK	99507
BISHOP ERIK R & CRISTY D		PO BOX 671433	CHUGIAK	AK	99567
BISHOP KURT J & CORA M		1234 S REBECCA DR	PALMER	AK	99645
BISHOP WM H		8009 COUNTRY WOODS DR	ANCHORAGE	AK	99503
Bisson Henri	Bureau of Land Management	222 W 7th Ave Unit 13	Anchorage	AK	99513
Bittner Judith	Alaska Dept. of Natural Resources	550 West 7th Ave, Suite 1310	Anchorage	AK	99501-35
BIWER DARWIN		426 G ST	ANCHORAGE	AK	99501

BJORN-ROLI PER	PO BOX 110903	ANCHORAGE	AK	99511
BJORNTON THOS P & PEGGY L	PO BOX 520597	BIG LAKE	AK	99652
BLACK CLIFFORD A	4100 E 66TH	ANCHORAGE	AK	99507
BLACK CONSTANCE A	PO BOX 110962	ANCHORAGE	AK	99511
BLACK DENNIS J & BARBARA	16336 ELIZABETH ST	ANCHORAGE	AK	99516
BLACK WARREN A SR	PO BOX 940105	HOUSTON	AK	99694
BLACKADAR ARNOLD B & M A	2630 PORTER PL	ANCHORAGE	AK	99508
BLACKARD DAN	1749 SHIP AVE	ANCHORAGE	AK	99501
BLACKMORE DON & KAREN	PO BOX 804	WILLOW	AK	99688
BLAIR EDWIN R & MERRY K	2120 TUDOR HILLS CR	ANCHORAGE	AK	99507
BLAIR SCOTT L & KRISTINA E	18557 WALRUS CIR	EAGLE RIVER	AK	99577
Blair Valen	P O BOX 2274	PALMER	AK	99645
Blake Johnson	Local 341; Kenai Peninsula	Anchorage	AK	99501
BLAKE THOS D & MAXINE A	2501 Commercial Drive	ANCHORAGE	AK	99518
Blake THOS D & MAXINE A HILLIS	8214 MENTRA ST	ANCHORAGE	AK	99501
BLAKELEY CUFF K	2020 Viking Drive	ANCHORAGE	AK	99518
BLAKESLEE MICHAEL W & B T	8244 RAINY PL	ANCHORAGE	AK	99516
Blanchard Ruth	7501 E 140TH AVE	ANCHORAGE	AK	99694
BLANCHARD RUTH G	P.O. Box 940027	Houston	AK	99694
BLAND GREG	PO BOX 940092	HOUSTON	AK	99694
BLASINGAME JAMES B	PO BOX 190282	ANCHORAGE	AK	99519
BLAVKA THOS F&GENEVIEVE C	PO BOX 101338	ANCHORAGE	AK	99510
Blincoe CHAD E & JULIE A	19741 OLD GLENN HWY	CHUGIAK	AK	99567
BLISS BRIDGET	1013 W 16TH AVE	ANCHORAGE	AK	99501
BLISS JAS B & NANETTE L	PO BOX 520482	BIG LAKE	AK	99652
BLISS MICHAELYN A	13300 COVE CIR	ANCHORAGE	AK	99515
BLOCK PHILIP A & BARBARA A	1305 THORNDYKE RD	PORT LUDLOW	WA	98365
BLOCKER EDW & SHELLY	3840 O'MALLEY RD	ANCHORAGE	AK	99507
Blodgen Ray & Debra	PO BOX 520506	BIG LAKE	AK	99652
BLODGETT RAYMOND R & DEBRA	P O. Box 940276	Houston	AK	99694-02
BLODGETT ROBERT R	PO BOX 940276	HOUSTON	AK	99694
BLOKER DALE D	PO BOX 243992	ANCHORAGE	AK	99524
BLOMKER KEITH & KAREN	2500 HASTINGS LN	ANCHORAGE	AK	99504
BLOOD DEAN C & JULIE A	1160 W WOODSTOCK DR	PALMER	AK	99645
BLOOM CAROLYN E	PO BOX 670884	CHUGIAK	AK	99567
BLOOMFIELD ROBT C & S	PO BOX 233271	ANCHORAGE	AK	99523
BLOUCH ROGER EVAN	7761 CANAL ST	ANCHORAGE	AK	99502
BLOUIN ROBT B & SYLVIA P	7221 TICONDEROGA PL	ANCHORAGE	AK	99502
BLUE CHARLES RAY	9226 W PARKVIEW TER	EAGLE RIVER	AK	99577
Blumner Zack	4424 E 7TH AVE	ANCHORAGE	AK	99508
BLUMSTEIN PHILIP	P.O. Box 337	Talkeetna	AK	99676
BLYTHE CLAUDIA RAE	2420 CHILLIGAN DR	ANCHORAGE	AK	99517
Bob Chlupach	13310 GLEN ALPS RD	ANCHORAGE	AK	99516
	PO Box 931	WILLOW	AK	99688

Bob Flint	Alaska Dept. of Environmental Conservation	555 Cordova Street	Anchorage	AK	99501
Bob French		685 Birch St	ANCHORAGE	AK	99501
Bob Jones		PO Box 521016	BIG LAKE	AK	99652
Bob Lynn	Alaska State Legislature	716 W. 4th Avenue, Ste 650	Anchorage	AK	99501
Bob Roses	Alaska State Legislature	8200 E. 2nd Avenue	Anchorage	AK	99504
Bobbie Lewis		PO Box 661	WILLOW	AK	99688
BOCAST WM F & NANCY LEE		PO BOX 520626	BIG LAKE	AK	99652
BOCK GARY R		PO BOX 772771	EAGLE RIVER	AK	99577
Bock Suzi	Best Western Lake Lucille Inn	1300 W. Lake Lucille Dr	Wasilla	AK	99654
BODEMAN DALE W & G		PO BOX 672006	CHUGIAK	AK	99567
Bodenstadt Melissa	Machinists & Aerospace Workers Local 601-Air Tr	8141 Schoon St., Suite 201	Anchorage	AK	99501
BODNAR BELA A		1558 H ST	ANCHORAGE	AK	99501
BOEDECKER GEO W & HELEN M		10900 TRAILS END RD	ANCHORAGE	AK	99516
BOEDEKER LARRY & HELEN		PO BOX 914	ANCHORAGE	AK	99688
BOEHM JOSEF F		2192 VIKING DR	ANCHORAGE	AK	99501
BOEVE GORDON P & TAMARA J		HC 89 BOX 124	WILLOW	AK	99688
BOGDAN STANLEY ROBT		PO BOX 90820	ANCHORAGE	AK	99509
BOGGS DOUGLAS		6930 TRAFFORD DR	ANCHORAGE	AK	99504
BOGIEL PETER A		5301 CHENA AVE	ANCHORAGE	AK	99508
Bogle-Munson Gail	Carr-Gottstein Properties	4000 W. Dimond Blvd, Suite 240	Anchorage	AK	99502
BOHAC DOUGLAS K & HELEN I		12320 LIBERTY RD	ANCHORAGE	AK	99516
BOHANNON ORVILLE E & P E		PO BOX 110388	ANCHORAGE	AK	99511
BOIRE ROGER E		3719 W 80TH AVE	ANCHORAGE	AK	99502
BOITNOTT GLENN C& VERNA J		4003 MINNESOTA DR	ANCHORAGE	AK	99503
BOLEA ALBERT N& CELESTE R		18520 KING EIDER LN	ANCHORAGE	AK	99516
BOLEY STAN L		PO BOX 653	WILLOW	AK	99688
BOLIN THOS		26130 IMPERIAL DR	EAGLE RIVER	AK	99577
BOLING THOS E		3661 SPINNAKER DR	ANCHORAGE	AK	99516
Boling Tim		PO BOX 934	WILLOW	AK	99688
BOLINGER MORRIS S & G M		117 F ST	ANCHORAGE	AK	99501
BOLLERUD DAN'L M		6000 ALPINE WOODS DR	ANCHORAGE	AK	99516
BOLYAN ELIZABETH S		1300 SOUTHAMPTON DR	ANCHORAGE	AK	99503
Bomalaski Susan	Catholic Social Services	225 Cordova Street	Anchorage	AK	99501
BOMAN ANNA LENA		PO BOX 520025	BIG LAKE	AK	99652
BOND RAYMOND W & LELA D		PO BOX 1033	WILLOW	AK	99688
BONI JEANETTE A		12501 NE 31ST ST	VANCOUVER	WA	98682
BONIFACE SHIRLEY W		2727 COLLIE HILL WAY	ANCHORAGE	AK	99504
BONNER LESTER K		1340 BIRCHWOOD ST	ANCHORAGE	AK	99508
Bonnie Quill, Executive Director	Mat-Su Convention & Visitors Bureau	7744 E. Visitors View Court	Palmer	AK	99645
BONTEMS MARK		18126 TEDROW DR	EAGLE RIVER	AK	99577
BOOKMAN BRUCE A & MARY R		3751 CHINIAK BAY DR	ANCHORAGE	AK	99515
BOOMERSHINE SKIPPY W& C A		631 CLIPPER SHIP CT	ANCHORAGE	AK	99515
Boothby Taunnie		HC01 Box 6415	Palmer	AK	99645

BOOTS LYNN & MICHAEL	10130 NANTUCKET LOOP	ANCHORAGE	AK	99507
BOOTS MICHAEL C & LYNN M	2120 BELMONT DR	ANCHORAGE	AK	99517
BOOZER TOD JEFFERY	PO BOX 672241	CHUGIAK	AK	99567
BOJECOSA CHRISOPHER R	PO BOX 672035	CHUGIAK	AK	99567
BORCHARDT DOUGLAS G & D A	PO BOX 418	WILLOW	AK	99688
BORCHERDING STEVEN T SR	7447 BERN ST	ANCHORAGE	AK	99507
BORDEN WANETA JEAN	PO BOX 303	WILLOW	AK	99688
BOREIS LEO G	PO BOX 110103	ANCHORAGE	AK	99511
Borell Steven	3305 Arctic Blvd., #105	Anchorage	AK	99503
Borer Jason	560 E. 34th Avenue	Anchorage	AK	99503
BORGEN LESTER G	PO BOX 92452	ANCHORAGE	AK	99509
BORING MERRILEIGH	8241 SPRUCE ST	ANCHORAGE	AK	99507
BORNSCHEIN DAVID C & I D	7130 MIRANDA DR	ANCHORAGE	AK	99507
BORSETTI RICHARD C	PO BOX 141951	ANCHORAGE	AK	99514
BOSCH GERALD EST	1724 CARA LP	ANCHORAGE	AK	99515
BOSHELL DAVID K	PO BOX 190751	ANCHORAGE	AK	99501
BOSLOUGH BEATRICE M REVTR	1733 W 13TH AVE	ANCHORAGE	AK	99501
BOSTEDT ERIC S & DENISE A	3730 CHAFFEE CIR	ANCHORAGE	AK	99517
BOSTON CLARK D & TERESA L	PO BOX 164	WILLOW	AK	99688
BOSTROM EDWIN A & AGNES M	PO BOX 520332	BIG LAKE	AK	99652
BOSVELD RICHARD JAS	10777 SPADA CIR	ANCHORAGE	AK	99516
BOTE NAOMING I	PO BOX 227	TALKEETNA	AK	99676
BOUNDS LARKIN J & SUZANNE	PO BOX 1024	WILLOW	AK	99688
BOURASSA DAN'L A & AIMEE R	PO BOX 520082	BIG LAKE	AK	99652
BOURDON THOS J & DEBORAH M	2815 DILIGENCE CIR	ANCHORAGE	AK	99515
Bouterse Bill	PO BOX 41	TALKEETNA	AK	99676
BOWDEN WAYNE R & ROSEMARY	PO BOX 210367	ANCHORAGE	AK	99521
Bowdoin Stuart	P.O. Box 1929	Palmer	AK	99645
BOWELL RICHARD II	8851 PLUNGE CREEK CIR	EAGLE RIVER	AK	99577
BOWEN ELDON J	PO BOX 520121	BIG LAKE	AK	99652
Bower Janette	231 W. Evergreen Ave.	Palmer	AK	99645
BOWLES SANFORD D	PO BOX 520001	BIG LAKE	AK	99652
BOYCE MICHAEL J & NADINE	2901 E 88TH AVE	ANCHORAGE	AK	99507
BOYCE VALERIE M	9315 ENDICOTT ST	ANCHORAGE	AK	99515
BOYD DAVID P	3050 NUGGET LN	ANCHORAGE	AK	99516
BOYD GALE F	1055 E SENECA AVE	WASILLA	AK	99654
BOYD GARY C & DIXIE L	804 D RESURRECTION DR	ANCHORAGE	AK	99504
BOYD GREGORY W	2103 W 44TH CT	ANCHORAGE	AK	99517
BOYER ROBT H & MARIANNE E	PO BOX 91376	ANCHORAGE	AK	99509
BOYETTE DAN'L E	4315 E 102ND AVE	ANCHORAGE	AK	99507
BOYKO BREEZE & FLANSBURG	745 W 4TH AVE	ANCHORAGE	AK	99501
BOYLAN THOS P & SHARON G	PO BOX 520112	ANCHORAGE	AK	99652
BOYLE TERESA	PO BOX 432	BIG LAKE	AK	99652
		CHUGIAK	AK	99567

Alaska Miners Association  
Chugach Heritage Foundation

City of Palmer



BRADEN ALAN LEE	PO BOX 521384	BIG LAKE	AK	99652
BRADEN BRUCE	PO BOX 13318	TRAPPER CREEK	AK	99683
BRADFORD ETHAN	4010 E 65TH AVE	ANCHORAGE	AK	99507
BRADLEY CHARLOTTE	1807 E DIMOND BLVD	ANCHORAGE	AK	99507
BRADLEY JOHN	PO BOX 752	WILLOW	AK	99688
BRADLEY MARTHA	HC 35 BOX 5475A	BIG LAKE	AK	99652
BRADLEY SEAN	6112 COLLINS WAY	ANCHORAGE	AK	99502
BRADLEY SEAN M	11532 DISCOVERY HTS CIR	ANCHORAGE	AK	99515
BRADSHAW BRENDA J	2500 BENZ CIR	ANCHORAGE	AK	99502
BRADY CARL F JR	2100 ATWOOD DR	ANCHORAGE	AK	99517
BRADY JAS K & LYNDEL M	12410 CARAGANA CIR	ANCHORAGE	AK	99515
Brady Judy	121 W. Fireweed Lane, #207	Anchorage	AK	99507
Brady Laura	PO Box 521274	Big Lake	AK	99652
BRADY RAYMOND E & PAULA J	16541 DAVIS ST	EAGLE RIVER	AK	99577
BRAKORA PETER A & DENISE	4311 RENDEZVOUS CIR	ANCHORAGE	AK	99504
Bramsedt Susan	4750 W. International Airport Rd.	Anchorage	AK	99502
BRANDFORD BRIAN B&MELITA K	11251 AVION ST	ANCHORAGE	AK	99516
BRANDON ROBERT D & LYNN M	PO BOX 110655	ANCHORAGE	AK	99511
BRANDON RUFUS J& BEATRICE	HC 89 BOX 546	WILLOW	AK	99688
BRANDT-ERICHCEN NANCY	PO BOX 101766	ANCHORAGE	AK	99510
BRANHOLM DAVID & LINDSAY	PO BOX 521160	BIG LAKE	AK	99652
BRANSTITER WILLARD & C A	7231 E 22ND AVE	ANCHORAGE	AK	99504
BRANTLEY MILBURN D & V L	PO BOX 110792	ANCHORAGE	AK	99511
BRASHEAR KAREN	PO BOX 520117	BIG LAKE	AK	99652
BRASSFIELD TOM J& CAROL A	7400 HILLSIDE WAY	ANCHORAGE	AK	99516
Bratton Paul	P.O. Box 320	Talkeetna	AK	99676
BRAULT NORMAN D & MARY A	4701 E 115TH AVE	ANCHORAGE	AK	99516
BRAULT PAULA S	11420 VIA APPIA	ANCHORAGE	AK	99515
BRAUN DONALD J& PAULINE M	2454 TULANE ST	ANCHORAGE	AK	99504
Brautig J.	HC-30 Box S480-B	Wasilla	AK	99687
BRAWN GERALD P & PAMELA J	362 FERN LAKE	ANCHORAGE	AK	99504
Brazeau Melissa	P.O. Box 1048	Talkeetna	AK	99676
BREESE DONALD A JR	HC 89 BOX 522	WILLOW	AK	99688
BREMNER HORACE E & LOUISE	2308 CULVER PL	ANCHORAGE	AK	99503
BREMONT TELON R & MONA M	3838 W 63RD AVE	ANCHORAGE	AK	99502
BREN AUDREY	5522 COPE ST	ANCHORAGE	AK	99518
Brenda Jager	21128 Bowery & Shavon Drive	WILLOW	AK	99688
BRENNAN KEVIN H	11721 SUNCREST CIR	ANCHORAGE	AK	99515
Bret T. Hamerick Sr.	HC89 BOX 1578	WILLOW	AK	99688
BRETT BRIAN E & JILL A	PO BOX 795	WILLOW	AK	99688
BREWER ROBT CHRIS	7310 AUGUSTINE	ANCHORAGE	AK	99504
BREWER SHIRLEY SHANNON	16143 MAMMOTH CT	EAGLE RIVER	AK	99577
BREWER THOS B& FLORESTENE	24327 THUNDERBIRD DR	CHUGIAK	AK	99567

BREY THOMAS	PO BOX 521041	BIG LAKE	AK	99652
Brian Morgan	HC 35 5420-L	Wasilla	AK	99654
Brian Morgan	HC 35 5420-L	Wasilla	AK	99654
Brian Twohy	26990 W. Holstein	Wasilla	AK	99654
BRICKEY PAUL	2430 E 16TH AVE	ANCHORAGE	AK	99508
BRIDGES HARVEY L JR	PO BOX 546	WILLOW	AK	99688
BRIDGEWATER N SCOTT	PO BOX 521135	BIG LAKE	AK	99652
BRIGGS DARLENE	3900 LARON LN	ANCHORAGE	AK	99504
BRILEY CHAD W	3022 W 30TH AVE	ANCHORAGE	AK	99517
BRILEY ROGER E & THERESA M	5330 CAPE SEVILLE DR	ANCHORAGE	AK	99516
BRION TOM B & PATRICIA A	PO BOX 670052	CHUGIAK	AK	99567
BRITCH ROBT A & BARBARA E	2454 TELEQUANA	ANCHORAGE	AK	99517
Britten C. Eric	1717 Tidewater Rd.	Anchorage	AK	99501
BROCKET CARL LYNN	PO BOX 520521	BIG LAKE	AK	99652
BRODIE NANCYE C	5900 YUKON RD	ANCHORAGE	AK	99507
BRODY DANIAL L	3608 MINNESOTA DR	ANCHORAGE	AK	99503
BROILES SHAWN J& MICHELLE	9030 SPRUCE RUN CIR	ANCHORAGE	AK	99507
BROMLEY DENNIS NATHAN	1574 WINTERGREEN ST	ANCHORAGE	AK	99508
BRONCZYK STANLEY H & K E	9020 NOBLE CIR	ANCHORAGE	AK	99502
BROOKS ADAM T	PO BOX 243291	ANCHORAGE	AK	99524
BROOKS DONALD & CINDY	5360 E 41ST AVE	ANCHORAGE	AK	99508
BROOKS FLOYD A	PO BOX 156	WILLOW	AK	99688
Brooks Gary	3333 Denali Street #200	Anchorage	AK	99503
BROOKS KEVIN S & MYRNA R	PO BOX 521324	BIG LAKE	AK	99652
BROOKS RAYMOND J & VIVIEN	343 W BENSON #3	ANCHORAGE	AK	99503
BROOKS RONALD E	2400 COPPERWOOD DR	ANCHORAGE	AK	99516
BROOKS RYAN E	PO BOX 112042	ANCHORAGE	AK	99511
Brooks Tom	P.O. BOX 107500	ANCHORAGE	AK	99510
BROSTE WAYNE & M ANNETTE	7900 ALATNA AVE	ANCHORAGE	AK	99507
BROTEN ERNEST G	PO BOX 824	WILLOW	AK	99688
BROTHERTON TAHNI WARNER	3861 STELLER DR	ANCHORAGE	AK	99504
BROUGHTEN STEVEN O & S L	PO BOX 521092	BIG LAKE	AK	99652
BROWN ALLEN E & MARGIE S	222 E 7TH AVE	ANCHORAGE	AK	99501
Brown Alphe	7404 E 4th Ave.	Anchorage	AK	99504
BROWN BILLY L & KAYLENE M	4405 FAIRMONT RD	ANCHORAGE	AK	99516
BROWN BRENDAN	340 W 76TH ST	ANCHORAGE	AK	99518
Brown CHAS S JR&CAROLYN M	10505 MAIN TREE DR	ANCHORAGE	AK	99507
BROWN DAVID RICHARD	8371 ELEUSIS	ANCHORAGE	AK	99502
BROWN DOUGLAS W & SUSAN W	7733 HONEYSUCKLE DR	ANCHORAGE	AK	99502
BROWN E M THRONSON P	PO BOX 110845	ANCHORAGE	AK	99511
BROWN EDGAR E	1403 W 34TH AVE	ANCHORAGE	AK	99503
Brown Frank	PO Box 201269	Anchorage	AK	99520-12
BROWN FRANKLIN L & LILA A	617 N LANE	ANCHORAGE	AK	99508

Horizon Lines

IBEW Union Local 1547

ALASKA RAILROAD CORPORATION

BROWN GEO A & JEAN M	3910 CROSSON DR	ANCHORAGE	AK	99517
BROWN GREGORY REED	8831 SAHALEE DR	ANCHORAGE	AK	99507
BROWN JASE	3330 MORGANSEY AVE	ANCHORAGE	AK	99516
BROWN JAS L & SHANNON K	PO BOX 101017	ANCHORAGE	AK	99510
BROWN KRISTAL F	PO BOX 520832	BIG LAKE	AK	99652
BROWN LAWRENCE C	7741 MARYLAND AVE	ANCHORAGE	AK	99504
BROWN LAWRENCE L&PATRICIA	12440 SHELBURNE RD	ANCHORAGE	AK	99516
BROWN LEON	3001 MOUNTAIN VIEW DR	ANCHORAGE	AK	99501
BROWN LONNIE F & DENISE A	9939 MIDDLE ROCK RD	ANCHORAGE	AK	99507
BROWN LT BROWN LT JR	365 INDUSTRIAL WAY	ANCHORAGE	AK	99501
BROWN LUDIE A	1300 W MELANIE AVE	WASILLA	AK	99654
Brown Margaret L.	P.O. Box 98330	Anchorage	AK	99509-33
BROWN MICHAEL ANTHONY	1840 ORCHARD PL	ANCHORAGE	AK	99502
BROWN NORMAN D & DEBORAH K	4521 MCALISTER DR	ANCHORAGE	AK	99502
BROWN ROBBY LEE	PO BOX 520834	BIG LAKE	AK	99652
Brown Robert	HC02 Box 7860	Palmer	AK	99645
BROWN ROBT W & ALPHA M	7404 E 4TH AVE	ANCHORAGE	AK	99504
BROWN TAMARA C	18336 JAMIE DR	EAGLE RIVER	AK	99577
BROWN TOMMIE L	616 PRICE ST	ANCHORAGE	AK	99508
BROWNE JANEIL MARY	2611 LATOUCHE ST	ANCHORAGE	AK	99508
BROWNGOETZ GREGG & SARAH	1135 W WINTER AVE	WASILLA	AK	99654
BROWN-HELLER PATRICIA	2132 SORBUS WAY	ANCHORAGE	AK	99508
BROWNING CHARLES C	PO BOX 520275	BIG LAKE	AK	99652
BROWNING JOHN BOWMAN	PO BOX 208152	ANCHORAGE	AK	99520
BROWN-KINARD RIO RITHA	3441 AMBER LOOP RD	ANCHORAGE	AK	99515
BROWNLEE CHERIE D	221 E 7TH AVE	ANCHORAGE	AK	99501
Bruce & Karen Mattson	PO Box 233	WILLOW	AK	99688
Bruce Carr	PO BOX 107500	ANCHORAGE	AK	99683
BRUCE D WAUGH	11631 BIRCH TRAIL CIR # B	ANCHORAGE	AK	99515
BRUCE L MILLER	13140 MIDORI DR	ANCHORAGE	AK	99516
Bruce Merrell	P.O. Box 100299	Anchorage	AK	99510
Bruce Miller	13140 Midori Dr	Anchorage	AK	99516
Bruce Tammy	HC01Box 6166 J21	Palmer	AK	99645
BRUCE THOS R & PATRICIA A	PO BOX 243301	ANCHORAGE	AK	99524
BRUDIE P F	14320 SABINE ST	ANCHORAGE	AK	99516
BRUEGGEBOERS RICHARD & B	PO BOX 206	WILLOW	AK	99688
BRULE LENNARD P & LINDA G	PO BOX 722	WILLOW	AK	99688
BRULOTTE CLIFFORD A	4100 DEBARR RD	ANCHORAGE	AK	99508
BRUMMER JEFFREY L & S M	1031 E CREEKSIDE DR	WASILLA	AK	99654
Brune Jason	121 W. Fireweed Lane, Ste 250	Anchorage	AK	99503
BRUNER FRANK B	6241 COLLINS WAY	ANCHORAGE	AK	99502
BRUNS BRUCE DAVID	7709 ADOBE DR	ANCHORAGE	AK	99507
BRUNS DORIS J	5767 RAYMOND DR	ANCHORAGE	AK	99507

Cook Inlet Regional Corporation

Alaska Railroad Corporation

Alaska Historical Society

Mat-Su CVB

Resource Development Council for Alaska

BRUNS DWAYNE R	4243 APOLLO DR	ANCHORAGE	AK	99504
BRUNS JAS M & JANIS F	PO BOX 520588	BIG LAKE	AK	99652
BRUNS RICHARD J & JOYCE E	4700 KLONDIKE AVE	ANCHORAGE	AK	99508
BRUNSVOLD PENNY	820 BREAKWATER CIR	ANCHORAGE	AK	99515
BRUNTON JOHN & JACQUELINE	11711 CLERKE CIR	ANCHORAGE	AK	99515
BRYAN BOB D & DAWN A	PO BOX 930	WILLOW	AK	99688
BRYAN BOB D JR	3611 SPENARD RD	ANCHORAGE	AK	99503
BRYAN MICHAEL R&SHIRLEY J	PO BOX 244286	ANCHORAGE	AK	99524
BRYANT JUBAL	7120 E 4TH AVE	ANCHORAGE	AK	99504
BRYNER BRUNO & JOAN	PO BOX 247	WILLOW	AK	99688
BRY S MARTHA G	PO BOX 520185	BIG LAKE	AK	99652
Bucaria Garvan	PO Box 870298	Wasilla	AK	99687
Buch Robert	3160 W. 71st Avenue	Anchorage	AK	99502
BUCHANAN CHERYL	PO BOX 521351	BIG LAKE	AK	99652
BUCHANON JAS O & RUBY DEE	1908 MEANDER CIR	ANCHORAGE	AK	99516
BUCHTA ALDA M	PO BOX 211076	ANCHORAGE	AK	99521
BUCHTA CLIFFORD N	17308 TEKLANIKA DR	EAGLE RIVER	AK	99577
BUCINSKY IVAN & ANNA	1408 HYDER ST #B	ANCHORAGE	AK	99501
BUCK KENT A & MICHELLE D	2120 LAUREN ANN CIR	ANCHORAGE	AK	99515
BUCKWALTER FRANCIS M& M G	PO BOX 520274	BIG LAKE	AK	99652
BUD MCGEE	10261 BETULA DR	ANCHORAGE	AK	99507
BUDD DAVID	801 IRWIN ST	ANCHORAGE	AK	99508
BUDD GERALD R	7041 CLARKS RD	ANCHORAGE	AK	99516
Budd Kathy	9991 Puffin Drive	Palmer	AK	99645
BUDDE LEE WM & JENNIFER	PO BOX 521795	BIG LAKE	AK	99652
BUENAFE JULIA RAMANA	7910 ALPINE VIEW CIR	ANCHORAGE	AK	99507
BUGH JEFFREY C & BOBBI J	6811 VIBURNUM DR	ANCHORAGE	AK	99507
Buirge Dave	Attn: APVR-RDL-OSV 977 Davis Hwy	Ft. Richardson	AK	99505-70
Bulard Armeta	P. O. Box 196	Cantwell	AK	99729
BULKOW JOHN R & LISA R	8605 SALAHEE DR	ANCHORAGE	AK	99507
BULLA JEFFREY A	PO BOX 521400	BIG LAKE	AK	99652
BULLEGER HENRY U&VICTORIA	1046 PINE ST	ANCHORAGE	AK	99508
BULLER LAVERNE D & THELMA	423 KENNY PL	ANCHORAGE	AK	99504
BULLINGTON CATHERINE	PO BOX 521395	BIG LAKE	AK	99652
BULLOCH KENNETH S& SIGRID	2260 COUNTRY BROOK CIR	ANCHORAGE	AK	99502
BULLOCK JOHN W & RUTHANNE	PO BOX 796	WILLOW	AK	99688
BULTMAN ADAM	425 DAILEY AVE	ANCHORAGE	AK	99515
BUMGARNER WARREN B & C	PO BOX 520037	BIG LAKE	AK	99652
Bunde Con	State Capitol, Room 506	Juneau	AK	99801-11
Bunde Con	716 W. 4th Avenue, Ste 400	Anchorage	AK	99501
BUNDY ROBT & JERRIAN	PO BOX 231876	ANCHORAGE	AK	99523
BUNKER DEAN & MAZIE	HC 89 BOX 375	WILLOW	AK	99688
BUNKER GLENN & JANET	3581 SAILBOARD CIR	ANCHORAGE	AK	99516

BUNN MARIE SUTHERLAND A	PO BOX 9	HYDER	AK	99923
BURAK JOHN W & SAMANTHA E	PO BOX 521276	BIG LAKE	AK	99652
BURCHAM ROBT	PO BOX 220733	ANCHORAGE	AK	99522
BUREK THOS	PO BOX 770797	EAGLE RIVER	AK	99577
Burette Sheri	560 E. 34th Avenue	Anchorage	AK	99503
BURGENER RICHARD P	HC 89 BOX 553	WILLOW	AK	99688
BURGER DOC	PO BOX 520797	BIG LAKE	AK	99652
Burgin Bob	PO Box 343	Sutton	AK	99674
BURGOYNE CURT	3100 E 142ND AVE	ANCHORAGE	AK	99516
BURK DONALD E& VIVIENNE R	2805 DAWSON ST	ANCHORAGE	AK	99503
BURKE ANNA K	26251 BERRYHILL RD	EAGLE RIVER	AK	99577
BURKE GARY W	PO BOX 521483	BIG LAKE	AK	99652
BURKHART ROY J & E J	PO BOX 204	WILLOW	AK	99688
BURKHOLDER JOHN J	4848 CANTERBURY WAY	ANCHORAGE	AK	99503
BURLESON NEAL	PO BOX 521	WILLOW	AK	99688
BURNETT R BRUCE & C M	4640 SANDY BEACH DR	ANCHORAGE	AK	99502
Burnett Rosemary	P.O. Box 940047	Houston	AK	99694
BURNS AL & EJ & THEO R JR	PO BOX 140625	ANCHORAGE	AK	99514
BURNS DAVID J	16200 LUNA ST	ANCHORAGE	AK	99516
BURNS JASON LEE& JENNIFER	16730 ELEANOR ST	EAGLE RIVER	AK	99577
BURNS KATHLEEN C	3624 DEBARR RD	ANCHORAGE	AK	99508
BURNS KATHY L	3319 W 81ST AVE	ANCHORAGE	AK	99502
BURNS TAMMY D	6900 SHERWOOD AVE	ANCHORAGE	AK	99504
Burnu Watkins	PO BOX 13129	TRAPPER CREEK	AK	99683
BURR MONTI	PO BOX 521054	BIG LAKE	AK	99652
BURRELL ROMAINE	1751 GEORGE BELL CIR	ANCHORAGE	AK	99515
BURROW ODESSA DELAINA	PO BOX 240683	ANCHORAGE	AK	99524
BURT MARY ANN	PO BOX 210465	ANCHORAGE	AK	99521
BURT ROXANNE L	HC 89 BOX 8530	TALKEETNA	AK	99676
BURTIS PAUL B	4256 ROSS CT	ANCHORAGE	AK	99508
BUSBY MICHAEL L	PO BOX 874	WILLOW	AK	99688
BUSCHUR GLENA H	19838 1ST ST	EAGLE RIVER	AK	99577
BUSER MARTIN P	PO BOX 520997	BIG LAKE	AK	99652
Bush J. Bruce	PO BOX 1251	PALMER	AK	99645
BUSH PETER H & ROBIN S	17644 MONTE RD	EAGLE RIVER	AK	99577
BUSH WM E	PO BOX 520072	BIG LAKE	AK	99652
BUSH WM R	4228 CHESS DR	ANCHORAGE	AK	99508
BUSH WM R	PO BOX 90053	ANCHORAGE	AK	99509
BUSHNELL ERIC M & DEBRA D	19932 KALKA CIR	EAGLE RIVER	AK	99577
BUSSING JOHN W JR	PO BOX 520689	BIG LAKE	AK	99652
Buswell Jennifer	P O BOX 289	PALMER	AK	99645
BUTCH DOROTHY M	5759 SAPHIRE LOOP	ANCHORAGE	AK	99504
BUTCHER ARLA CHARLENE	940 W 11TH AVE	ANCHORAGE	AK	99501

Chugach Alaska Corporation

City of Houston

MSB Planning Commission

BUTCHER OWEN JOHN BETTINE	PO BOX 520405	AK	BIG LAKE	99652
BUTERA LOUIS A & SANDRA K	PO BOX 773294	AK	EAGLE RIVER	99577
BUTLER BYRON LEE	2371 YORKTOWN CIR	AK	ANCHORAGE	99507
BUTLER J BLAKE & CAROL J	2110 CASTNER CIR	AK	ANCHORAGE	99517
BUTLER KENNETH W JR	PO BOX 521493	AK	BIG LAKE	99652
BUTLER MICHAEL E SR	PO BOX 671091	AK	CHUGIAK	99567
BUTLER REX LAMONT	PO BOX 200025	AK	ANCHORAGE	99520
Butler William D	P.O. Box 2889	AK	Palmer	99645
Button Donna	535 W Evergreen Ste 12	AK	Palmer	99645
BUTTON JACK W	PO BOX 521743	AK	BIG LAKE	99652
BUTTON MARK	PO BOX 521115	AK	BIG LAKE	99652
BUZARD RALPH R & CLAUDIA L	PO BOX 940272	AK	HOUSTON	99694
BYARS SCOTT	8128 BLACKBERRY ST	AK	ANCHORAGE	99502
BYAS CONCHITA PAL	PO BOX 143436	AK	ANCHORAGE	99514
BYBEE RUSS & JILL VELOSKE	PO BOX 768	AK	WILLOW	99688
BYERS JOHN L & CAROLINE U	PO BOX 521301	AK	BIG LAKE	99652
BYKER DIANE LEE	145 W HARVARD AVE	AK	ANCHORAGE	99501
BYRD CHAD M	PO BOX 940081	AK	HOUSTON	99694
BYRON G R HURLBUT A M R	4051 WINCHESTER LOOP	AK	ANCHORAGE	99507
CABANA KEVIN EUGENE	PO BOX 520770	AK	BIG LAKE	99652
CABLE HAROLD F & VICKIE N	4501 TRAPLINE CIR	AK	ANCHORAGE	99516
CACY JOHN & ROBIN LEE	PO BOX 91082	AK	ANCHORAGE	99509
CACY MARY LOU	3246 WILEY POST LOOP	AK	ANCHORAGE	99503
Cacy Robin	3801 Center Point Drive, Ste 500	AK	Anchorage	99503
CACY ROBT C	PO BOX 230716	AK	ANCHORAGE	99523
CADA GEORGE	2926 BRITTANY DR	AK	ANCHORAGE	99504
CADIZ EFREN B	11107 BLUFF CREEK DR	AK	ANCHORAGE	99515
CADMAN GERALD V	PO BOX 940407	AK	HOUSTON	99694
CADRA DAN	PO BOX 940442	AK	HOUSTON	99694
CAGLE DEAN & KIMBERLY	7003 STANLEY DR	AK	ANCHORAGE	99518
CAGLE LANCE	PO BOX 670441	AK	CHUGIAK	99567
CAILLOUET DEBRA J WEAVER	8521 GREENHILL WAY	AK	ANCHORAGE	99502
CAIN BRIAN M & KATRINA J	540 KARLUK ST	AK	ANCHORAGE	99501
CALBICK LEE H & DARLENE M G	13930 MALASPINA ST	AK	EAGLE RIVER	99577
CALDWELL CHRISTI J	PO BOX 520314	AK	BIG LAKE	99652
CALKINS ERIC LYNN	20049 TENADA ST	AK	CHUGIAK	99567
CALL BOBBIE B	19739 DOTIE DR	AK	CHUGIAK	99567
CALL TROY L & VALERIE K	PO BOX 672251	AK	CHUGIAK	99567
CALLADINE BONNIE J	4750 W 88TH	AK	ANCHORAGE	99502
CALVIN D BAILEY JR	PO BOX 101422	AK	ANCHORAGE	99510
CAMERO MABELITA S	2603 SHEPHERDIA RD	AK	ANCHORAGE	99508
CAMERON CAROLYN R	PO BOX 202785	AK	ANCHORAGE	99520
CAMERON SCOTT & DEBBIE A	2805 DAWSON ST STE 201	AK	ANCHORAGE	99503

Mat-Su College - UAA  
Travel Services

U.S. Minerals Management Service

CAMP DENNIS E & SANDRA K	3934 SYCAMORE LOOP	ANCHORAGE	AK	99504
CAMP JAS A III	PO BOX 520977	BIG LAKE	AK	99652
CAMP MICHELLE A	PO BOX 520907	BIG LAKE	AK	99652
CAMPBELL KATHLEEN B	11061 BOW CIR	ANCHORAGE	AK	99515
CAMPBELL ALVIN C & MARY L	5329 E 24TH ST	ANCHORAGE	AK	99504
CAMPBELL BARBARA J	8545 RANGEVIEW AVE	ANCHORAGE	AK	99504
CAMPBELL CARROLL J & T E	PO BOX 521166	BIG LAKE	AK	99652
Campbell Craig	P.O. Box 5800	Ft. Richardson	AK	99505-08
CAMPBELL DEBRA S	19179 BIRCHWOOD LOOP RD	CHUGIAK	AK	99567
CAMPBELL JAS W	PO BOX 93387	ANCHORAGE	AK	99509
CAMPBELL JEANIE R	PO BOX 233746	ANCHORAGE	AK	99523
CAMPBELL JEFFREY L & J M	511 MARY CIR	ANCHORAGE	AK	99515
CAMPBELL LONNIE T & ANNA	151 JELINEK PL	ANCHORAGE	AK	99504
Campbell McKie	P.O. Box 25526	Juneau	AK	99802-55
CAMPBELL MICHAEL J&JESSIE	2425 W 67TH AVE	ANCHORAGE	AK	99502
Campbell Paul	P.O. Box 2749	Palmer	AK	99645
CAMPBELL TOM & NANCY P S	PO BOX 940441	HOUSTON	AK	99694
CAMPBELL WM E	PO BOX 520066	BIG LAKE	AK	99652
CAMPBELL WM T III	PO BOX 825	WILLOW	AK	99688
CANADAY JOHN T & ANN Z	3223 CASSIUS CT	ANCHORAGE	AK	99508
CANADY PELGRUM J	PO BOX 200772	ANCHORAGE	AK	99520
CANE BEVERLY	PO BOX 521198	BIG LAKE	AK	99652
CANGE JEANNINE L CAMPBELL	19041 SARICHEF LOOP	EAGLE RIVER	AK	99577
CANNON ROBT R & SANDY L	13410 VERN DR	ANCHORAGE	AK	99516
CANTEBURY ROBBIL	PO BOX 101557	ANCHORAGE	AK	99510
Canterbury Charles M	Attn Aprv Fr po	Ft Richardson	AK	99505
CAPITAN THOS P	PO BOX 584	WILLOW	AK	99688
CAPO LOIS NICHOLS	127 HUNTER AVE	SLEEPY HOLLOW NY	NY	10591
CARBAUGH TRENTON	PO BOX 110424	ANCHORAGE	AK	99511
CARDEN JAS R& ELIZABETH M	7110 BEAUMONT CIR	ANCHORAGE	AK	99502
CARDEN WALTER W & LYN ANN	7310 DURENDA CIR	ANCHORAGE	AK	99507
CARDWELL KEN R & GAY NEL	5035 RIVERTON AVE	ANCHORAGE	AK	99516
CARESS WALTER H	6808 SPRUCE ST	ANCHORAGE	AK	99507
Carl Seaver	PO Box 877171	Wasilla	AK	99654
Carle Moe	PO Box 521107	BIG LAKE	AK	99652
CARLE ROBT C & KAREN L	PO BOX 110938	ANCHORAGE	AK	99511
Carlos Joe	16244 Poppy Lane	Eagle River	AK	99577-80
CARLSON BOK H	3803 GREENLAND DR	ANCHORAGE	AK	99503
CARLSON BRADLEY B & M A	PO BOX 221286	ANCHORAGE	AK	99522
CARLSON CADE	9710 HILLSIDE DR	ANCHORAGE	AK	99507
CARLSON ERIK J & ANGELA R	PO BOX 761	WILLOW	AK	99688
CARLSON FLOYD J SR & M Z	1412 COLUMBINE ST	ANCHORAGE	AK	99508
CARLSON JOEL E & CLARA E	PO BOX 670488	CHUGIAK	AK	99567

CARLSON RICK C&KATHLEEN M	7529 OLD HARBOR AVE	ANCHORAGE	AK	99504
CARLSON ROBT A	2548 POST RD	ANCHORAGE	AK	99501
CARLSON STEVEN ERNEST	7807 ARLENE ST	ANCHORAGE	AK	99502
CARLSON TED R & CYNTHIA Y	PO BOX 671829	CHUGIAK	AK	99567
CARLTON MICHAEL N & VICKI	1700 ADONIS DR	ANCHORAGE	AK	99515
CARLYLE JAS D & SABRINA	PO BOX 202114	ANCHORAGE	AK	99520
CARMAN CHARLENE G	2610 KELSAN CIR	ANCHORAGE	AK	99508
CARMAN ERIC & ANGELA	13321 VERN DR	ANCHORAGE	AK	99516
Carmony Wayne	P.O. Box 2929	Palmer	AK	99645
CARNEY DAN'L G	411 WINFIELD CIR	ANCHORAGE	AK	99515
CARP DOUGLAS A & WANDA M	416 FISCHER AVE	ANCHORAGE	AK	99518
CARPENTER GREGORY W & K D	909 BENCH CT	ANCHORAGE	AK	99504
Carr Matt	222 W. 7th Avenue, Room 537, Box 19	Anchorage	AK	99513
CARR MICHAEL H	5911 FIJI ST	ANCHORAGE	AK	99507
CARR THOS A	18240 MEADOW CREEK DR	EAGLE RIVER	AK	99577
CARR WESLEY E JR	PO BOX 866	WILLOW	AK	99688
Carricaburu Jill	290 E. Herring Avenue	Wasilla	AK	99654
CARRICK PATRICIA LYNN	HC 89 BOX 459	WILLOW	AK	99688
CARRILLO LIDWINO V & M R	7107 E 17TH AVE	ANCHORAGE	AK	99504
CARROLL LAWRENCE J & S L	PO BOX 521666	BIG LAKE	AK	99652
Carrow John	2925 DeBar Rd	Anchorage	AK	99508
CARSE SUSAN H	750 W 2ND AVE5	ANCHORAGE	AK	99510
CARSON JARRETT W & KITLISA	19143 KR ST	CHUGIAK	AK	99567
Carte Katherine	367 N Valley Way	Palmer	AK	99645
CARTER BRADLEY W & L L	PO BOX 520898	BIG LAKE	AK	99652
CARTER CARRIE LEE	4002 HARDING DR	ANCHORAGE	AK	99517
CARTER FAUSTIN W	PO BOX 671325	CHUGIAK	AK	99567
CARTER JANET	6440 E NORTHERN LGTS BLVD	ANCHORAGE	AK	99504
CARTER JEFFREY A & KARIN L	2016 STEEPLE	ANCHORAGE	AK	99516
CARTER MICHAEL W	PO BOX 1162	WILLOW	AK	99688
Carter Patrick	1940 N. Driftwood Circle	Palmer	AK	99645
CARTER PERRY W & JEANNIE L	921 WOODMAR PL	ANCHORAGE	AK	99515
CARTER RANDY & LEANNE	3166 CABBELL AIRSTRIP RD	ANCHORAGE	AK	99504
CARVALHO STEPHEN	PO BOX 520672	BIG LAKE	AK	99652
CARVER SKY	609 N ST	ANCHORAGE	AK	99501
CASLER CHRISTINE ANN	19050 CITATION RD	EAGLE RIVER	AK	99577
CASQUEIRA MICHAEL ROY	PO BOX 520896	BIG LAKE	AK	99652
CASSEL ENOLA B	1840 CRESCENT DR	ANCHORAGE	AK	99508
CASSIDY BRENT L & JUDY K	PO BOX 232	WILLOW	AK	99688
CASSIDY BRIAN & JOANNE L	PO BOX 938	WILLOW	AK	99688
CASTLE AGNES G	HC 89 BOX 240	WILLOW	AK	99688
CASTLE DEAN & SHERRIE	PO BOX 520866	BIG LAKE	AK	99652
CASTRO YOLANDA	PO BOX 521354	BIG LAKE	AK	99652



CASWELL LAWRENCE F & DIANN	PO BOX 520699	BIG LAKE	AK	99652
CATERINICHIO NINA	PO BOX 770651	EAGLE RIVER	AK	99577
CATHERINE R BURTON	10137 N RAINBOW SHORES RD	WILLOW	AK	99688
Cattanach Richard	8005 Schoon Street, Ste 100	Anchorage	AK	99418-30
CAUDY JUDY	PO BOX 841	WILLOW	AK	99688
CAUSEY JASON	2909 W 32ND AVE	ANCHORAGE	AK	99517
CAVANUAGH RANDALL S	8833 LAKEHURST	ANCHORAGE	AK	99502
CAVITT DAVID W	27639 PARAMOUNT DR	ANCHORAGE	AK	99577
CAYWOOD PAULA M	PO BOX 774042	EAGLE RIVER	AK	99577
CAZAC GUS	3700 W 61ST AVE	ANCHORAGE	AK	99502
CEBULA CRAIG	PO BOX 13165	TRAPPER CREEK	AK	99683
CEDERBERG ARTHUR	6956 WHITEHALL ST	ANCHORAGE	AK	99502
CEDERBERG DELBERT	6218 NIELSON WAY	ANCHORAGE	AK	99518
CEDROS PATRICK M & BONNIE L	PO BOX 566	WILLOW	AK	99688
CEHULA JAS D	18555 OSPREY CIR	ANCHORAGE	AK	99516
CERNA MERCINDO M H & MARGIE	519 LYNWOOD DR	ANCHORAGE	AK	99518
CETINCELIK ATTILA	PO BOX 521045	ANCHORAGE	AK	99652
CETLINSKI THOS	12821 GAIL ST	BIG LAKE	AK	99515
CHAIKA MADONNA	4940 NATRONA AVE	ANCHORAGE	AK	99516
CHAMBERLAIN FRANK A	PO BOX 521888	BIG LAKE	AK	99652
CHAMBERLAIN PETER & MARY C	PO BOX 520991	BIG LAKE	AK	99652
CHAMBERLIN ERIC L	1170 S HYER RD	PALMER	AK	99645
CHAMOT ELZBIETA	5907 CORDOVA ST	ANCHORAGE	AK	99518
CHANDLER LEON H & TYRA	7000 LAKE O THE HILLS CIR	ANCHORAGE	AK	99516
CHANEK PAUL D	21035 COUNTRY VIEW DR	CHUGIAK	AK	99567
CHANEY CHAS	PO BOX 308	WILLOW	AK	99688
CHANG IAN Y K	9500 INDEPENDENCE DR #700	ANCHORAGE	AK	99507
CHANG TOM K L & SUSAN H	8920 BOULDER BAY CIR	ANCHORAGE	AK	99507
CHAPMAN BRAD L	6702 FOOTHILL CIR	ANCHORAGE	AK	99504
CHAPMAN LARSON EDWARD E.	3939 WESTWOOD DR	ANCHORAGE	AK	99517
CHAPSON MARK A	PO BOX 521828	BIG LAKE	AK	99652
Charles Steve & Marian	PO Box 76	WILLOW	AK	99688
Charles Wade	26526 W. Beryozova	Willow	AK	99688
CHARRON PAUL D	PO BOX 353	WILLOW	AK	99688
CHASE WILLIAM J	7450 WADE CIR	ANCHORAGE	AK	99518
CHASSE JENNIFER Y	5831 TONGA ST	ANCHORAGE	AK	99507
CHECK SHANNON	HC 89 BOX 476	WILLOW	AK	99688
CHEEK RONALD E & NANCY M	1340 CROSS RD	ANCHORAGE	AK	99515
Chenault Charles	State Capitol, Room 505	Juneau	AK	99801-11
CHERRIER CURTIS R	PO BOX 202109	ANCHORAGE	AK	99520
CHERVEN PAUL	23000 EAGLE GLACIER LOOP	EAGLE RIVER	AK	99577
CHESBRO JAS & PATRICIA R	PO BOX 1093	PALMER	AK	99645
CHESNUT CHERYL L	8440 ABBOTT LOOP	ANCHORAGE	AK	99507

Associated General Contractors of Alaska

Alaska State Legislature

CHESNUT DAVID E	7126 TIMOTHY ST	ANCHORAGE	AK	99502
CHESO LAWRENCE L	PO BOX 210795	ANCHORAGE	AK	99521
CHILD ROBT A	2300 D ST	ANCHORAGE	AK	99503
CHILDERS BOBBY E	3700 W 86TH	ANCHORAGE	AK	99502
CHILDERS RICHARD E	4925 E 43RD	ANCHORAGE	AK	99508
CHILDRESS DAVID S	PO BOX 521023	BIG LAKE	AK	99652
CHILES DONALD G & EDRIE R	3100 DOS CIR	ANCHORAGE	AK	99507
CHISUM DONAL MJARL & G D	330 L ST # 200	ANCHORAGE	AK	99501
CHLUPACH ROBT S	PO BOX 931	WILLOW	AK	99688
CHO WON KIL & POK HYONG	2211 WASHINGTON AVE	ANCHORAGE	AK	99515
CHOMYK-DANIELS NATALIE	205 W 22ND	ANCHORAGE	AK	99503
CHOWDHURY NAIMUL HASAN	2421 TRISHA AVE	ANCHORAGE	AK	99516
CHOY ALLEN I REV TR	7830 OLD SEWARD HWY	ANCHORAGE	AK	99518
Chris Grabowski	PO Box 940183	Houston	AK	9994
Chris Gregoroff	H.C. 01 Box 2530	Glennallen	AK	99588
Chris Gregoroff	H.C. 01 Box 2530	Glennallen	AK	99588
CHRIS WASSILLIE & DONNA J	PO BOX 521352	BIG LAKE	AK	99652
CHRISTAL JOHN M & S	17440 SPAIN	ANCHORAGE	AK	99516
CHRISTANSON JAS W&SANDI L	8235 SUNDI DR	ANCHORAGE	AK	99502
CHRISTENSEN CRAIG N&NAOMI	17546 TEKLANIKA DR	EAGLE RIVER	AK	99577
CHRISTENSEN DOROTHY GAYLE	PO BOX 111462	ANCHORAGE	AK	99511
CHRISTENSEN MAUREEN	2836 N CIRCLE	ANCHORAGE	AK	99507
CHRISTENSEN JESS & LESLIE	PO BOX 521221	BIG LAKE	AK	99652
CHRISTENSON KEVIN SCOTT	PO BOX 770424	EAGLE RIVER	AK	99577
Christian Hartley	PO Box 810	Willow	AK	99688
CHRISTIANSEN CAROL	HC02 BOX 7693-8	PALMER	AK	99645
CHRISTIANSEN KELVIN H	3921 IONA CIR	ANCHORAGE	AK	99507
CHRISTIANSON GWEN G	2011 SALEM CT	ANCHORAGE	AK	99508
CHRISTIANSON JAS H	PO BOX 244953	ANCHORAGE	AK	99524
CHRISTIANSON MELFORD W&J	4047 E 88TH AVE	ANCHORAGE	AK	99507
CHRISTIE BEVERLY	PO BOX 190537	ANCHORAGE	AK	99519
CHRISTIE REGINALD J	501 W NORTHERN LTS BLVD	ANCHORAGE	AK	99503
CHRISTINE ROBT W & SANDRA	PO BOX 670203	CHUGIAK	AK	99567
CHRISTMAN MICHAEL & JOYCE	721 JACK ST	ANCHORAGE	AK	99515
CHRISTMAN PATRICIA A	PO BOX 230027	ANCHORAGE	AK	99523
CHRISTOFFERS ROBT E & M R	3015 WILEY POST AVE	ANCHORAGE	AK	99517
CHRISTOPHER L SANNER	10360 EVERGREEN ST	ANCHORAGE	AK	99507
Christopher Pete	P.O. Box 2690	Palmer	AK	99645
CHRISTOPHER ROGERS	12031 GINAMI	ANCHORAGE	AK	99516
CHRISTOPHER T SPOERHASE	12940 RIDGEWOOD DR	ANCHORAGE	AK	99516
CHRISTY JOHN II & RACHEL	2141 LORD BARANOF DR	ANCHORAGE	AK	99517
CHRISTY KATHRYN E	PO BOX 520471	BIG LAKE	AK	99652
CHRISTY WM E III	7540 WADE CIR	ANCHORAGE	AK	99518

Mat-Su Miners Baseball

CHRYST MIKE & SALLY A	PO BOX 341	WILLOW	AK	99688
CHUDNOFSKY CHRISTOPHER	PO BOX 940391	HOUSTON	AK	99694
CHUDNOFSKY DAVID	PO BOX 940013	HOUSTON	AK	99694
CHUMLEY MARY E	2936 E 42ND AVE	ANCHORAGE	AK	99508
CHUNG YU SHIN & YONG HYE	3419 STARBOARD LN	ANCHORAGE	AK	99516
CHURCH ARTHUR A JR&BONNIE	PO BOX 1044	WILLOW	AK	99688
CHURCH DAVID D&MICHELLE R	2141 S CHURCH ST	PALMER	AK	99645
Church Michelle	2141 Church Street	Palmer	AK	99645
CHYA SERENITY	PO BOX 520595	BIG LAKE	AK	99652
CIALDINI JANET M	1101 S LOBO ST	WASILLA	AK	99654
CICCARALLI ROBT & SANDRA	4190 CRANOG ST	ANCHORAGE	AK	99502
CINGEL JOHN	822 CHERRY ST	ANCHORAGE	AK	99504
CINTULA BRANDON J & JULIE	3220 KNIK AVE	ANCHORAGE	AK	99517
CINTULA JOHN L	3520 W 30TH AVE	ANCHORAGE	AK	99517
CISMOSKI DAN'L J & TAMIKO	13701 WESTWIND DR	ANCHORAGE	AK	99516
Cissna Sharon	716 W. 4th Avenue, Ste 360	Anchorage	AK	99501
Cissna Sharon	State Capitol, Room 420	Juneau	AK	99801--11
CIZEK JOS E & JENNIFER J	23916 CHANDELLE DR	CHUGIAK	AK	99567
CLAIBORNE ROBT	4908 E 43RD AVE	ANCHORAGE	AK	99508
Clapp Steve	P.O. Box 100035	Anchorage	AK	99510
CLARK DELORES M	PO BOX 507	WILLOW	AK	99688
Clark Dennis	10928 Eagle River Road, #228	Eagle River	AK	99577
Clark Dennis	P.O. Box 2889	Palmer	AK	99645
CLARK DOUGLAS R	PO BOX 940224	HOUSTON	AK	99652
CLARK FILLIPE G & JODY H	2411 LAIRD CIR	ANCHORAGE	AK	99516
CLARK GERALD K	8885 EAGLE PLACE LOOP	EAGLE RIVER	AK	99577
CLARK HAROLD B& ROBERTA L	PO BOX 940182	HOUSTON	AK	99694
CLARK JOS R & CHRISTINE M	PO BOX 773633	EAGLE RIVER	AK	99577
CLARK KURT R	3408 BRUCE LN	ANCHORAGE	AK	99517
CLARK LISA S	1100 S HYER RD	PALMER	AK	99645
CLARK MANUEL W & STACEY E	PO BOX 607	WILLOW	AK	99688
CLARK RAYMOND E&CYNTHIA A	HC 89 BOX 481	WILLOW	AK	99688
CLARK TRON T & BOBBIE A	8240 BERRY PATCH DR	ANCHORAGE	AK	99502
Clark-Newman Terrie	535 W. Evergreen	Palmer	AK	99645
CLAUDIA A CLARE	113 W NORTHERN LGTS BLVD	ANCHORAGE	AK	99503
Claus Donna	P.O. Box 770361	Eagle River	AK	99577
CLAUSEN DONALD H	18924 SOKOLOF CIR	EAGLE RIVER	AK	99577
CLAY GERALD PAUL	PO BOX 586	WILLOW	AK	99688
CLAYTON ARTHUR JAS	16639 ELEONORA ST	EAGLE RIVER	AK	99577
CLAYTON MICHAEL J & K J	17019 KIYONA CT	EAGLE RIVER	AK	99577
CLEARY MICHAEL & JANICE	3352 WILEY POST LOOP	ANCHORAGE	AK	99517
Cleary Steve	P.O. Box 101093	Anchorage	AK	99510-10
CLEM WM J & MARY C	10504 UNITY LN	POTOMAC	MD	20854

CLEMENS BRUCE D & JEAN M	PO BOX 108	WILLOW	AK	99688
CLEMENSEN JAS K & HELEN J	PO BOX 521430	BIG LAKE	AK	99652
Clemensen Jim	P O BOX 521430	BIG LAKE	AK	99652
CLEMENSEN Jim	PO BOX 241	PALMER	AK	99645
CLEMENS KEVIN Q	PO BOX 190468	ANCHORAGE	AK	99519
CLICK DORIS BRIDGET	3880 S PENINSULA DR	BIG LAKE	AK	99652
Cliff Newell	4111 Galactica Drive	Anchorage	AK	99517
CLIFT SUSAN	8990 RENDON DR	ANCHORAGE	AK	99507
CLIFTON VAN & JEAN	7120 CLAIRMONT CIR	ANCHORAGE	AK	99507
CLIFTON WM H & NANCY J	5353 CAPE SEVILLE	ANCHORAGE	AK	99516
CLINCH RICHARD L & GAIL D	PO BOX 772613	EAGLE RIVER	AK	99577
CLINTON LENORE M	1500 ZARVIS PL	ANCHORAGE	AK	99508
CLOE JANE B	2320 CANARY CT	ANCHORAGE	AK	99515
Close Rex J.	HC03 BOX 8444E	PALMER	AK	99645
Cloud James	PO Box 196127 MAC K3212-051	Anchorage	AK	99519-61
CLOUGH TIMOTHY P	3630 O'MALLEY RD	ANCHORAGE	AK	99507
CLOUSER MAURICE L	14100 SPECKING AVE	ANCHORAGE	AK	99515
CLOWERS DEAN J & LORI L	14414 RIVERTON CT	ANCHORAGE	AK	99516
CLUFF LANCE & MARGORITA	4821 BLACKSTONE CIR	ANCHORAGE	AK	99507
CLUFF LANCE A & GLORIA G	24821 JESSE LEE CT	CHUGIAK	AK	99567
CLUFF LYNN R	7016 WHITEHALL ST	ANCHORAGE	AK	99502
CLUFF MICHAEL G & WANDA	6976 WHITEHALL ST	ANCHORAGE	AK	99502
COATES STEVEN F & STACY R	PO BOX 770573	EAGLE RIVER	AK	99577
COATNEY KATHLEEN K TR	1805 STRATFORD CT	ANCHORAGE	AK	99508
COATS JAS W & BEVERLY J	4809 BUCKINGHAM WAY	ANCHORAGE	AK	99503
COBAUGH THOMAS W	3014 SUNFLOWER ST	ANCHORAGE	AK	99508
COBB RICHARD G & JANELLE	PO BOX 771342	EAGLE RIVER	AK	99577
COBBAN MICHAEL L & JOANNE	PO BOX 521098	BIG LAKE	AK	99652
COBURN MARK A & SALLY L	18828 MOOSE PL	CHUGIAK	AK	99567
COCHRAN ANDREA M	731 ASH PL	ANCHORAGE	AK	99501
Cochran Patricia Longley	429 L Street	Anchorage	AK	99501
Cochrane Phil	PO Box 196612 MB 3-2	Anchorage	AK	99519-66
COCKRUM LARRY L & CHARLENE	HC 89 BOX 245	WILLOW	AK	99688
COE CHAS W & CAROLYN M	2740 SCARBOROUGH DR	ANCHORAGE	AK	99504
COGDILL GARY D & F M	PO BOX 520732	BIG LAKE	AK	99652
Coghili, Jr. John	State Capitol, Room 204	Juneau	AK	99801-11
COHEN MONIQUE EBONY	7410 AUGUSTINE DR	ANCHORAGE	AK	99504
Colberg Jim	P.O. Box 336	Palmer	AK	99645
Colberg Talis	P.O. Box 110300	Juneau	AK	99811
COLBRY DONALD G JR	9040 ELIM ST	ANCHORAGE	AK	99507
COLE CHRIS D	2515 WINTER RIDGE CT	ANCHORAGE	AK	99507
COLE DAVID W	PO BOX 670312	CHUGIAK	AK	99567
Cole Kathleen	P.O. Box 520366	Big Lake	AK	99652

Wells Fargo Bank Alaska

Alaska Native Science Commission  
BP Exploration (Alaska), Inc.

Alaska State Legislature

Alaska Dept. of Law

Jolt Construction Company



Cook Dorothy	Native Village of Eklutna	26339 Eklutna Village Road	Chugiak	AK	99567
COOK GUY D		PO BOX 212272	ANCHORAGE	AK	99521
COOK KENNETH H&FRIEDERIKE		21818 LOWER CANYON DR	EAGLE RIVER	AK	99577
COOK MARGARET A		PO BOX 240263	ANCHORAGE	AK	99524
COOK MELVIN & ANN E		PO BOX 726	WILLOW	AK	99688
COOK RICHARD E&PATRICIA L		PO BOX 230711	ANCHORAGE	AK	99523
COOK WM D & MARY A		19328A MONASTERY DR	EAGLE RIVER	AK	99577
COOPER ANTHONY J		PO BOX 111848	ANCHORAGE	AK	99511
COOPER DAVID E& KATRINA N		1337 GARDEN ST	ANCHORAGE	AK	99508
COOPER DONALD E & TONIA		HC 89 BOX 542	WILLOW	AK	99688
COOPER KAY A		PO BOX 520257	BIG LAKE	AK	99652
COOPER KENNETH W & GAIL M		1326 W 26TH AVE	ANCHORAGE	AK	99503
Cooper Larry J.	Wells Fargo Bank Alaska, N.A.	301 W. Northern Lights Blvd, Ste. 212, #K3212-02	Anchororage	AK	99503
COOPER MARK A		PO BOX 521386	BIG LAKE	AK	99652
COOPER STEVE E B& DOROTHY		PO BOX 90493	ANCHORAGE	AK	99509
COOPER STEVE E B& DOROTHY		PO BOX 520454	BIG LAKE	AK	99652
COOPER VIRGINIA		9900 WILLIAM JONES CIR	ANCHORAGE	AK	99515
COPE KIM		PO BOX 521746	BIG LAKE	AK	99652
COPE RYAN JOHN		PO BOX 2198	BETHEL	AK	99559
COPELAND BONNIE S		1900 W 46TH	ANCHORAGE	AK	99517
COPELAND DOLLAS L		2900 BRITTANY DR	ANCHORAGE	AK	99504
COPELIN TOM		PO BOX 521085	BIG LAKE	AK	99652
COPENHAVER JOHN TAL		PO BOX 670455	CHUGIAK	AK	99567
COPPOLA FRED & GENEVIEVE		9413 CANTON LOOP	ANCHORAGE	AK	99515
CORBIN HENRY		PO BOX 13176	TRAPPER CREEK	AK	99683
CORDERO AMADO A& ANA M R		2100 CANNONEER CIR	ANCHORAGE	AK	99507
COREY MICHAEL D & DAYNA R		9010 SPRUCE RUN CIR	ANCHORAGE	AK	99507
CORIANO CARLOS		240 W 33RD AVE	ANCHORAGE	AK	99503
CORNELIUS BARRY		PO BOX 1124	WILLOW	AK	99688
CORNFORTH DENNIS& REBECCA		PO BOX 333	WILLOW	AK	99688
CORNING FRANCIS J JR		PO BOX 1045	WILLOW	AK	99688
Cornwall Daniel	Alaska State Library	P.O. Box 110571	Juneau	AK	99811-05
CORPUZ FRITZ P		PO BOX 202566	ANCHORAGE	AK	99520
Correira Linda	Susitna River Lodging	Box 786	Talkeetna	AK	99676
CORTES RICARDO		3614 E 16TH AVE	ANCHORAGE	AK	99510
Corvart Jo Nell		2260 Tasha Drive	Anchororage	AK	99502
COSPER CHAS R & SUSAN I		1404 KINNICKINICK ST #A	ANCHORAGE	AK	99508
COSSANO LESTER W& BETTY J		PO BOX 670796	CHUGIAK	AK	99567
COSTELLO PIERRE		3801 MCMAHON DR	ANCHORAGE	AK	99516
Costello Tim	TiCon Restaurant Equipment	P.O. Box 681	Talkeetna	AK	99676
Costello Zepf Trisha	Talkeetna Roadhouse	Box 604	Talkeetna	AK	99676
COTTINI MARC J. & MARINA		PO BOX 210863	ANCHORAGE	AK	99521
COTTRELL JAS A IV & D B		10100 CURVI ST	ANCHORAGE	AK	99518

COTY JOHN W SR & HELEN E	3241 W 80TH AVE	ANCHORAGE	AK	99502
COUGHLIN MICHAEL T & SANYA	PO BOX 886	WILLOW	AK	99688
COUILLARD DON	PO BOX 110269	ANCHORAGE	AK	99511
Coullahan Patrick	2700 Gambell Street, Ste 200	Anchorage	AK	99503
COUNTER DERIC S & REBEKAH	8860 PLUNGE CREEK CIR	EAGLE RIVER	AK	99577
COURTNEY GERALD L	PO BOX 520338	BIG LAKE	AK	99652
COURTNEY JAS A & LAURIE B	PO BOX 130	WILLOW	AK	99688
COURTNEY JAS A SR&MARIE K	PO BOX 306	WILLOW	AK	99688
COUSINEAU NATHAN C	16563 FARM AVE	EAGLE RIVER	AK	99577
COVARRUBIAS RAYMOND O & D	4027 BIRDSONG DR	EAGLE RIVER	AK	99577
COVEY JACOB & RACHEL	HC 89 BOX 272	WILLOW	AK	99688
COVINGTON L R & JIMMIE L	1072 RYLAND PIKE NE	HUNTSVILLE	AL	35811
COWAN JAS R & JANE A	8380 MAJESTIC DR	ANCHORAGE	AK	99504
COWAN ROBT C & CAROL J	6915 RABBIT CREEK RD	ANCHORAGE	AK	99516
Cowdery John	716 W. 4th Avenue, Ste 500	Anchorage	AK	99501
Cowdery John	State Capitol, Room 101	Juneau	AK	99801-11
COWDERY PAMELA	HC 89 BOX 101	WILLOW	AK	99688
COX DIANN	PO BOX 773291	EAGLE RIVER	AK	99577
COX DION & LAURA	PO BOX 241123	ANCHORAGE	AK	99524
COX GARY D	369 PAULINE ST	ANCHORAGE	AK	99504
Cox Linda	261 Anna Street	Palmer	AK	99645
COX RICHARD & DIANN	PO BOX 773291	EAGLE RIVER	AK	99577
Cox Robert	3111 C St Ste 500	Anchorage	AK	99503
Cox Ron	P.O. Box 871963	Wasilla	AK	99687
COX WM C & SARITA L	PO BOX 670010	CHUGIAK	AK	99567
COYLE AGNES A	3326 WESLEYAN DR	ANCHORAGE	AK	99508
COZAD CHARLES E & E A	PO BOX 521142	BIG LAKE	AK	99652
CRABB MICHAEL D & LIAN M	9100 GRANITE PLACE	ANCHORAGE	AK	99507
CRAIG DONALD E JR	PO BOX 940112	HOUSTON	AK	99694
CRAIG ELLA H	1416 BIRCHWOOD ST	ANCHORAGE	AK	99508
CRAIG TROY R	PO BOX 521712	BIG LAKE	AK	99652
CRANFORD BILLY JOHN	PO BOX 520264	BIG LAKE	AK	99652
CRAPPS JOAN H	3570 HOLLYBERRY CIR	ANCHORAGE	AK	99507
CRAVENS DAVID W	PO BOX 222575	ANCHORAGE	AK	99522
CRAVENS RON	8231 SPRUCE	ANCHORAGE	AK	99507
CRAWFORD DAVID M	2347 E 86TH CT	ANCHORAGE	AK	99507
Crawford Harry	716 W. 4th Avenue, Ste 540A	Anchorage	AK	99501
Crawford Harry	State Capitol, Room 426	Juneau	AK	99801-11
Crawford John	HC01 Box 6050L	Palmer	AK	99645
CRAWFORD TIM R JR	8020 PINEBROOK CIR	ANCHORAGE	AK	99507
CRAY MICHAEL G & GWEN	3000 WILEY POST AVE	ANCHORAGE	AK	99517
CRISWELL COLE R	20516 LUCAS AVE	EAGLE RIVER	AK	99577
CRITTENDEN ROYAL C	941 W 71ST AVE	ANCHORAGE	AK	99518

Alaska State Legislature  
 Alaska State Legislature  
 Alaska State Legislature  
 Alaska State Legislature  
 Pippel Insurance  
 Alaska State Legislature  
 Alaska State Legislature  
 Pippel Insurance

Future Interests  
 Petro Marine Services  
 City of Wasilla

CROCKETT JACK L & MARILYN			4155 APOLLO DR	ANCHORAGE	AK	99504
CROCKETT JAS F			2141 CHANDALAR DR	ANCHORAGE	AK	99504
Crockett Marilyn	Alaska Oil & Gas Association		121 W Fireweed Ln Ste 207	Anchorage	AK	99503
CROCKETT ROBT J & LYNN M			6533 W DIMOND BLVD	ANCHORAGE	AK	99502
CROFUT PATI			2308 JEFFERSON AVE	ANCHORAGE	AK	99517
CROMWELL RUFUS C JR & A R			3302 WOODLAND PARK DR	ANCHORAGE	AK	99517
CRONER VIRGINIA K			13650 W UPPER BIRCH DR	BIG LAKE	AK	99652
CRONIN JOHN M JR & E L			4107 RASPBERRY RD	ANCHORAGE	AK	99502
CRONQUIST HAL			13140 IRIS WAY	EAGLE RIVER	AK	99577
Crosby Norman & Judy	Crosby Construction		HC2 Box 7813H	Palmer	AK	99645
CROSBY ROBT L JR			PO BOX 772773	EAGLE RIVER	AK	99577
CROSBY WARREN WATSON			PO BOX 520318	BIG LAKE	AK	99652
Cross Glenda	Key Bank of Alaska		1150 South Colony Way	Palmer	AK	99645
CROSS WM J & VALERIE J			1054 BEECH LN	ANCHORAGE	AK	99501
CROSSLIN WM L			PO BOX 520385	BIG LAKE	AK	99652
CROW RICHARD E JR & K A			4101 LEYDEN RD	ANCHORAGE	AK	99516
CROWDER JOHN EDW			PO BOX 520348	BIG LAKE	AK	99652
CROWELL JOHN			3360 CHAPARRAL CIR	ANCHORAGE	AK	99502
CROWLEY GERI L			2511 TRADEWIND DR	ANCHORAGE	AK	99516
Crowther George			P.O. Box 1605	Palmer	AK	99645
CROWTHER PAUL R			5039 VANCE DR	ANCHORAGE	AK	99508
CROZIER KRISTIAN N			PO BOX 337	TOK	AK	99780
CROZIER STEPHEN B & JOYCE			13301 MCCABE CIR	ANCHORAGE	AK	99516
CRUM CONSTANCE			909 CHUGACH DR	ANCHORAGE	AK	99503
CRUM JAS A & LOUNETTE			2841 CONCORD LANE	ANCHORAGE	AK	99502
Crusey Dave & Carolyn			PO Box 871667	Wasilla	AK	99687
Cruz Dave	Cruz Construction Inc.		3852 N. Clark-Wolverine Rd	Palmer	AK	99645
CUBBEDGE JILL			PO BOX 102455	ANCHORAGE	AK	99510
CUCULLU DAVID W & WENDY G			PO BOX 110264	ANCHORAGE	AK	99511
CUDDY GRETCHEN H			2439 KARLUK ST	ANCHORAGE	AK	99508
CULLERS VIRGINIA			HC 89 BOX 538	WILLOW	AK	99688
CULVER CHAS E & SANDRA M			PO BOX 110238	ANCHORAGE	AK	99511
CULVER DEBORAH M			11740 MARY AVE	ANCHORAGE	AK	99515
CULVERWELL MELVIN			PO BOX 1164	CHUGIAK	AK	99567
CUMMINGS SUSAN J			1910 OLYMPIC DR	ANCHORAGE	AK	99515
CUMMINS KIMBERLY L			PO BOX 521336	BIG LAKE	AK	99652
CUNIFF BERNARD J & MARY J			8451 RIDGEWAY AVE	ANCHORAGE	AK	99504
CUNNINGHAM BRADLEY S & NIK			8148 SEACLIFF ST	ANCHORAGE	AK	99502
CUNNINGHAM LELAND C			7508 EAST BROOK	ANCHORAGE	AK	99504
CUNNINGHAM LLOYD F			2400 BANBURY DR	ANCHORAGE	AK	99504
CURIALE JOS & SHIRLEY			PO BOX 520913	ANCHORAGE	AK	99504
CURRAN DAVID & MYRNA			2449 MARITIME LOOP	BIG LAKE	AK	99652
CURRAN EMERY C & SHERRI M			3240 E 64TH AVE	ANCHORAGE	AK	99507



CURRAN ROBT K JR & LISA V	801 W 86TH AVE	ANCHORAGE	AK	99515
CURREY KIRK & KEIL CAROLYN	12439 GOLDEN EAGLE DR	EAGLE RIVER	AK	99577
CURRIER CRAIG J	PO BOX 91463	ANCHORAGE	AK	99509
CURRIER MARLAN L	PO BOX 520401	BIG LAKE	AK	99652
CURRO JAS J & ANN S	630 W 91ST AVE	ANCHORAGE	AK	99515
CURRY MICHAEL E	PO BOX 672170	CHUGIAK	AK	99567
CURRY STEVEN R	3810 ARCTIC BLVD	ANCHORAGE	AK	99503
CURTIS ALLEN F	PO BOX 240993	ANCHORAGE	AK	99524
CURTIS ANNE MARIE E	PO BOX 773445	EAGLE RIVER	AK	99577
CURTIS CALVIN R	PO BOX 520800	BIG LAKE	AK	99652
Curtis Cheryl	P.O. Box 3954	Palmer	AK	99645
CURTIS JAS D. & KATHLEEN A.	3701 EASTWIND DR	ANCHORAGE	AK	99516
CURTIS JENNIFER	2420 MAYLEN CIR	ANCHORAGE	AK	99516
CURTIS RICHARD W & CHERI K	2060 CONSTITUTION DR	EAGLE RIVER	AK	99577
Curtis William	P.O. Box 395	Palmer	AK	99645
CYPHERS CHRISTOPHER D&T L	420 L ST	ANCHORAGE	AK	99501
CZAJKOWSKI ALAN J & P A	143 NANOOK CIR	ANCHORAGE	AK	99504
CZAMY SHARRON L	10156 W STANLEY DR	WASILLA	AK	99654
DABELSTEEN SUSAN	4047 JAMES DR	ANCHORAGE	AK	99504
DAEMS KEVIN & SELENA	PO BOX 54	WILLOW	AK	99688
DAFOE AARON & MARILYN	PO BOX 940384	HOUSTON	AK	99694
DAGDAG EDUARDO C & M N	7410 HUCKLEBERRY CIR	ANCHORAGE	AK	99502
DAGNON DALE	PO BOX 520190	BIG LAKE	AK	99652
DAHL MARCUS L	2009 MORE ST	ANCHORAGE	AK	99504
DAHLEN THOS N & MARDELLE L	PO BOX 624	WILLOW	AK	99688
DAHLMANN ZONA G MILES	19972 BIRCHWOOD LOOP RD	CHUGIAK	AK	99567
Dahlstrom Nancy		Eagle River	AK	99577
Dahlstrom Nancy	10928 Eagle River Rd, Ste 238	Juneau	AK	99801-11
DAHMS DAVID J & LAURIE E	State Capitol, Room 409	PALMER	AK	99645
DALE JOHN A & MARILYN M	12168 E SCOTT RD	BIG LAKE	AK	99652
DALE ROBT P & LORETTA M	PO BOX 520322	ANCHORAGE	AK	99518
DALE SHARON K	525 E 76TH AVE	ANCHORAGE	AK	99516
DALE WM R & MELINDA A	4340 BIRCHBARK AVE	ANCHORAGE	AK	99516
DALLMAN ALBERT H & P M	5911 ROMANIA DR	ANCHORAGE	AK	99516
DALTON RONNIE	PO BOX 672485	CHUGIAK	AK	99567
DAM JEFFREY N & PATRICIA	160 SHELLY MARIE CIR	ANCHORAGE	AK	99515
Dan Mayfield	PO BOX 201327	ANCHORAGE	AK	99520
Dan Robertson	P.O. Box 520705	Big Lake	AK	99652
Dan Robertson	11301 Snowline Dr	ANCHORAGE	AK	99507
Dana Hills	11301 Snowline Dr	ANCHORAGE	AK	99507
DANIEL ROBT R	PO Box 1062	WILLOW	AK	99688
DANIELS JANET	1500 W 33RD	ANCHORAGE	AK	99503
DANKWORTH WM	PO BOX 142342	ANCHORAGE	AK	99514
	2320 LOREN CIR	ANCHORAGE	AK	99516

Alaska State Legislature  
Alaska State Legislature

DANNEHY BARRY P & KAREN R	12105 GINAMI ST	ANCHORAGE	AK	99516
DANNER FRANK A	PO BOX 92729	ANCHORAGE	AK	99509
DARINGER MARK N	PO BOX 212003	ANCHORAGE	AK	99521
DARNELL DARYL D& PAMELA A	PO BOX 520414	BIG LAKE	AK	99652
DARNELL V CLIFFORD	PO BOX 520955	BIG LAKE	AK	99652
DARWIN AUDREY ELLEN	PO BOX 520956	BIG LAKE	AK	99652
DATTA JENNIFER L	PO BOX 112483	ANCHORAGE	AK	99511
DAULT JAS M	PO BOX 91016	ANCHORAGE	AK	99509
DAULT MICHAEL J	1101 LOBO ST	WASILLA	AK	99654
DAVENPORT CLYDE L	PO BOX 797	WILLOW	AK	99688
DAVENPORT ELAINE JOHANNA	1101 146TH ST SE	MILL CREEK	WA	98012
DAVENPORT GENE ALLEN	PO BOX 911	WILLOW	AK	99688
DAVENPORT GRACE	PO BOX 210363	ANCHORAGE	AK	99521
DAVID W SCHACHT	PO BOX 670	WILLOW	AK	99688
David Wurts	6404 Ross Rd	Morrison	CO	80465
DAVIDSON WM D	HC 89 BOX 464	WILLOW	AK	99688
DAVIES BRENT	12841 MARINER DR	ANCHORAGE	AK	99515
DAVIS ANDREW L & ALANA L	PO BOX 772004	EAGLE RIVER	AK	99577
Davis Bettye	State Capitol, Room 11	Juneau	AK	99801-11
DAVIS BRIAN JAY	PO BOX 521442	BIG LAKE	AK	99652
DAVIS CINDY	PO BOX 200952	ANCHORAGE	AK	99520
DAVIS DOUGLAS R & NANCY G	6820 ROUND TREE DR	ANCHORAGE	AK	99507
DAVIS GAYLE L	7911 NORTHWIND AVE	ANCHORAGE	AK	99504
DAVIS HARRELL E	PO BOX 240427	ANCHORAGE	AK	99524
DAVIS JAMES E & LUCILLE A	2801 MERGANSER AVE	ANCHORAGE	AK	99516
DAVIS JERRY C & SUZANE R	1730 REBEL RIDGE DR	ANCHORAGE	AK	99504
DAVIS LARRY & EVELYN M	PO BOX 730	WILLOW	AK	99688
DAVIS MARK R	6910 TANAINA DR	ANCHORAGE	AK	99502
DAVIS MICHAEL DEAN	2520 KILKENNY CIR	ANCHORAGE	AK	99504
DAVIS VIRGLE W & SUE A	7041 ARLENE ST	ANCHORAGE	AK	99502
DAVISON ROY B & PEGGY E	3201 RABBIT CREEK RD	ANCHORAGE	AK	99516
DAWSON RICHARD R & ANNE S	PO BOX 93054	ANCHORAGE	AK	99509
DAY STEVEN G	3910 MARQUIS WAY	ANCHORAGE	AK	99502
DAYTON KEVIN R	PO BOX 940293	HOUSTON	AK	99694
DAYTON MARY L	12540A OLD GLENN HWY	EAGLE RIVER	AK	99577
DE BOER ROLAND O & G M	6426 BLACKBERRY ST	ANCHORAGE	AK	99502
DE GUZMAN REBECCA C	2230 NASH CIR	ANCHORAGE	AK	99508
Dean Davidson	10502 Lakes O' the Su	WILLOW	AK	99688
Dean Davidson	10502 Lakes O' the Su	WILLOW	AK	99688
Dean Farley & Theresa	PO BOX 85	WILLOW	AK	99688
Dean Farley & Theresa	PO BOX 85	WILLOW	AK	99688
Dean Heather	222 W. 7th Avenue, Room 537, Box 19	Anchorage	AK	99513-75
Deanna Swaney	2049 S. Horseshoe Lake	BIG LAKE	AK	99652

Alaska State Legislature

U.S. Environmental Protection Agency

DEARTH GARY	8200 WISTERIA ST	ANCHORAGE	AK	99502
Debbie Mosley	12441 Turks Turn Street	ANCHORAGE	AK	99516
Debbie Mosley	12441 Turks Turn Street	ANCHORAGE	AK	99516
DEBEAULIEAU RON D	PO BOX 521193	BIG LAKE	AK	99652
Debois Richard	P.O. Box 2461	Palmer	AK	99645
Debra Hulging-Pizak	3840 Campbell Airstrip Rd.	Anchorage	AK	99504
DeBusk Dawn	5751 E. Mayflower Ct	Wasilla	AK	99654
DECKER CHAS & GLADYS	2805 BREEZEWOOD DR	ANCHORAGE	AK	99517
DECKER GERALD	PO BOX 208	WILLOW	AK	99688
DECKER-BROWN KAREN L	2200 SHORE DR	ANCHORAGE	AK	99515
DEDEKER ERVIN L	PO BOX 703	WILLOW	AK	99688
DEDRICK CAROL M	PO BOX 672208	CHUGIAK	AK	99567
DEES L V JOE	PO BOX 670267	CHUGIAK	AK	99567
DEETER JANELLE	12320 HERITAGE RD	ANCHORAGE	AK	99516
DEGUZMAN AVELINO H & R M	3006 BETTLES BAY LOOP	ANCHORAGE	AK	99515
DEHEER ROGER & JULIE	PO BOX 112333	ANCHORAGE	AK	99511
DEISHL KSENIA	3604 WILLIAMS ST	ANCHORAGE	AK	99503
DEITZ DON R	2021 BELMONT DR	ANCHORAGE	AK	99517
Dejong Jackie	248 E. Dahlia Street	Palmer	AK	99645
DEKEYSER JOHN & GEORGIA	2259 ST ELIAS DR	ANCHORAGE	AK	99517
DEKONING GARY F.	1524 NELCHINA ST	ANCHORAGE	AK	99501
DEKREON MICHAEL J & R J	600 E 76TH	ANCHORAGE	AK	99518
DELANEY FRANK E & SANDIE	6314 WHISPERING LOOP	ANCHORAGE	AK	99504
DELANO JOHN H & LOUISE J	2270 JENNISON CIR	ANCHORAGE	AK	99508
DELAPP JOHN R & TINA D	13101 ELMORE RD	ANCHORAGE	AK	99516
DELARIES RANDAL & WENDY	1200 W DIMOND	ANCHORAGE	AK	99515
DELATORRE ELIZABETH	2922 SUMMER SUN CT	ANCHORAGE	AK	99507
DELEON CARLOS R & KAREN L	3971 ALITAK BAY CIR	ANCHORAGE	AK	99515
DELGADO GEO G& SHARLENE K	5386 SILLARY CIR	ANCHORAGE	AK	99508
DELNEGRO ROSIANN	1120 GOLDENBERRY AVE	ANCHORAGE	AK	99515
DELO HOWARD O	PO BOX 520707	BIG LAKE	AK	99652
DELONGCHAMP ROGER D	12910 SILVER SPRUCE	ANCHORAGE	AK	99516
DELUCIA ANTHONY & SANDRA M	PO BOX 812	WILLOW	AK	99688
Delucia Tony	17034 Eagle River Loop, Suite 203	Eagle River	AK	99577
DELVALLE TIMOTHY D& TOYIA	PO BOX 211293	ANCHORAGE	AK	99521
DEMOSKI BENJAMIN D & AMY	PO BOX 672114	CHUGIAK	AK	99567
DEMOSKI DAVID J & MARY L	PO BOX 688	WILLOW	AK	99688
DEMETZ MICHAEL D	PO BOX 1010	WILLOW	AK	99688
Denkewalter Geri	P.O. Box 433	Talkeetna	AK	99676
Denkewalter Geri	P.O. Box 688	Talkeetna	AK	99676
DENMAN TODD A & ERIKA A	2070 BELMONT DR	ANCHORAGE	AK	99517
DENN PETER M & LEANN M	11450 BIRCH RD	ANCHORAGE	AK	99516
DENNEHY MARK V & MARY M	13131 WHALER DR	ANCHORAGE	AK	99516

DENNEY JOS & MICHELLE A	PO BOX 672133	CHUGIAK	AK	99567
DENNIS CHANDRA R	PO BOX 103544	ANCHORAGE	AK	99510
DENNIS GAYE E	1014 JAMESTOWN RD	SEQUIM	WA	98382
DENNIS STEVEN E&CHARLOTTE	209 FAWN CT	ANCHORAGE	AK	99515
DENNY SAMUEL A & LYNDA	1180 E SNOW HILL AVE	WASILLA	AK	99654
DEPEW LAMBERT SLAY	1908 HILLCREST DR	ANCHORAGE	AK	99517
DePriest Tiny	HC05 Box 9735	Palmer	AK	99645-95
DESCHAINÉ DONNA J	14041 SABINE ST	ANCHORAGE	AK	99516
DESJARDINS NORMAN G & A L	3500 KACHEMAK PL	ANCHORAGE	AK	99515
DESJARLAIS JOS & TASHA	14919 EVERGREEN ST	ANCHORAGE	AK	99516
DESONIA MATHEW T	PO BOX 163	WILLOW	AK	99688
DESPRES DAN'L M	PO BOX 770603	EAGLE RIVER	AK	99577
DEVEREAUX ROBERT S&LETTIE	PO BOX 670517	CHUGIAK	AK	99567
DeVilbiss Larry	1800 Glenn Hwy, Suite 12	Palmer	AK	99645
DeVilbiss Larry	2300 N. Aurora Lane	Palmer	AK	99645
DEVORE BILLIE	PO BOX 200422	ANCHORAGE	AK	99520
DEVORE DON L	7408 FLORENCE CIR	ANCHORAGE	AK	99507
Devries Edna	P.O. Box 1027	Palmer	AK	99645
DEWANE MARY G STOCKTON	11001 RIDECREST DR	ANCHORAGE	AK	99516
DEWEIL RICHARD W	PO BOX 520362	BIG LAKE	AK	99652
DEWITT GORDON & MARY	1705 STRATFORD CT	ANCHORAGE	AK	99508
DEY AARON & BARBARA J	2960 DARTMOUTH DR	ANCHORAGE	AK	99508
DEYKES WAYNE R & MARILYN	24845 TEAL LOOP	CHUGIAK	AK	99567
DHAMALE GANESH	PO BOX 90853	ANCHORAGE	AK	99509
DIAMOND JOHN & HUI MO	9702 TAKI CIR	EAGLE RIVER	AK	99577
Diane Duskey	PO Box 521902	Big Lake	AK	99652
DICKENS BILLY & ILSE	PO BOX 742	WILLOW	AK	99688
DICKERSON MAHALA A	1330 E 15TH AVE	ANCHORAGE	AK	99501
DICKINSON CHARLENE	PO BOX 242622	ANCHORAGE	AK	99524
DICKINSON GLORIA L	2438 SUSITNA DR	ANCHORAGE	AK	99517
DICKINSON TERRY A & C	PO BOX 521456	BIG LAKE	AK	99652
DICKMAN TIMOTHY G	PO BOX 521329	BIG LAKE	AK	99652
DIEL WM R & MARY E	13300 VERN DR	ANCHORAGE	AK	99516
DIEMER BRADLEY	PO BOX 110051	ANCHORAGE	AK	99511
DIEMER SUSAN M	5326 WANDERING DR	ANCHORAGE	AK	99502
DIETRICH KRYSTAL LORRAINE	PO BOX 1154	TALKEETNA	AK	99676
Dietrick Larry	P.O. Box 111800	Juneau	AK	99811
DIETSCH TODD M	PO BOX 221041	ANCHORAGE	AK	99522
DIETZ MICHAEL D& MICHELLE	PO BOX 521184	BIG LAKE	AK	99652
DIGGINS BENNY R	7805 HIGHLANDER DR	ANCHORAGE	AK	99518
DIGGINS DON Y	2931 MEADOW WOOD	ANCHORAGE	AK	99516
DILLARD MICHAEL	21718 PEG ST	CHUGIAK	AK	99567
DILLEY BRITTANY LOU ANN	2836 N BACKHAUS ST	HOUSTON	AK	99694

Tiny Moose Farm

Alaska Dept. of Natural Resources  
Mat-Su Borough School Board

Devries & Associates

Alaska Dept. of Environmental Conservation

DILLEY JOHN T	3531 BRICE LN	ANCHORAGE	AK	99517
DILLON EDW L & SUZANANE Y	9431 DUNDEE CIR	ANCHORAGE	AK	99502
DINELLO FRANK A	PO BOX 520712	BIG LAKE	AK	99652
DISOTELL CARL A	PO BOX 770210	EAGLE RIVER	AK	99577
DISELBRETT DAVID P	4741 GRUMMAN ST	ANCHORAGE	AK	99504
DITTLINGER BRET W	6321 CHEVIGNY ST	ANCHORAGE	AK	99502
DITTMAN ERIC W & SHELLEY	PO BOX 14	WILLOW	AK	99688
DIVELBISS RICHARD E&KAREN	7740 BIG SPRUCE CIR	ANCHORAGE	AK	99502
DIVENS MICHAEL G	6006 MILEY DR	ANCHORAGE	AK	99504
DIXON KEVIN G	3338 STRAWBERRY RD	ANCHORAGE	AK	99502
DIXON WALTER MARK	4630 REKA DR	ANCHORAGE	AK	99508
DJM/J	7423 E 36TH AVE	ANCHORAGE	AK	99504
DOBBS ARMONDO T SR	PO BOX 110927	ANCHORAGE	AK	99511
DOBKOWSKI MARC E & JEAN A	PO BOX 520397	BIG LAKE	AK	99652
DOBKOWSKI MARC E & JEAN A	PO BOX 520397	BIG LAKE	AK	99652
DOBSON DENNIS J & S L	7110 MCLIN CIR	ANCHORAGE	AK	99507
DOBSON DONALD H	7120 OLD SEWARD HWY	ANCHORAGE	AK	99518
DOBSON GARLAND J & ANNA M	PO BOX 50	WILLOW	AK	99688
DOBSON KEITH	11071 Mtn. Lake Drive	ANCHORAGE	AK	99516
DOBSON KEITH	11071 Mtn Lake Drive	ANCHORAGE	AK	99516
DOBSON KEITH	11071 Mtn. Lake Drive	ANCHORAGE	AK	99516
DOBSON STEVEN S & JUDY A	12001 LILAC DR	ANCHORAGE	AK	99516
DOCTOR REME C	1579 WINTERGREEN ST	ANCHORAGE	AK	99508
Dodd C Shay	1081 W. Robin's Song Ave.	Wasilla	AK	99654
Dodd C Shay	1081 W. Robin's Song Ave.	Wasilla	AK	99654
DODGE GEO P & JOANNE E	PO BOX 520357	BIG LAKE	AK	99652
DODGE GEO P III	PO BOX 520427	BIG LAKE	AK	99652
DODSON GLENDA L	PO BOX 90339	ANCHORAGE	AK	99509
DOIG RUSSELL C	12401 BEACHCOMBER DR	ANCHORAGE	AK	99515
DOISHER ELI G	PO BOX 520091	BIG LAKE	AK	99652
DOLENCE JOHN A	6301 DOWNEY-FINCH DR	ANCHORAGE	AK	99516
DOLEZAL CLAUDE & PATRICIA	911 W 72ND AVE	ANCHORAGE	AK	99518
DOMINGO LYDIA F	PO BOX 631	BARROW	AK	99723
Dompeling Eric	4710 Business Park Blvd, Ste 44	Anchorage	AK	99503
Don Hepler	HC 34, Box 2195	Wasill	AK	99654
Don Keller	21128 Bowers Lane	Chugiak	AK	
DONAHUE DAVID A & JESSIE	3803 IMAGE DR	ANCHORAGE	AK	99504
DONAHUE JOS A JR	PO BOX 243376	ANCHORAGE	AK	99524
DONAHUE MARK A	6701 HOLLY LN	ANCHORAGE	AK	99502
DONALD JO AN D	PO BOX 520461	BIG LAKE	AK	99652
DONALD MICHAEL D	PO BOX 521473	BIG LAKE	AK	99652
DONALDSON GARY	PO BOX 231184	ANCHORAGE	AK	99523
DONATELLO JEAN M	PO BOX 1246	ANCHORAGE	AK	99510

Baker Oil Tools

DONCHAK DOUGLAS	4651 REKA DR	ANCHORAGE	AK	99508
DONIGHT ORION	829 E 13TH AVE	ANCHORAGE	AK	99501
DONLAN JEFF M	3741 GARY COOPER CIR	ANCHORAGE	AK	99507
Donna Quante	17438 N. Heights Place	WILLOW	AK	99688
DONNELLY GARY L & SHERRI K	8621 JUPITER DR	ANCHORAGE	AK	99507
DONOHUE JOHN F & SUSAN L	PO BOX 521404	BIG LAKE	AK	99652
Doogan Mike	4800 Cambridge Way	Anchorage	AK	99503
DOOLEY KIMBERLY J	PO BOX 520167	BIG LAKE	AK	99652
DOOLEY THOS S	2565 FORAKER DR	ANCHORAGE	AK	99517
DOOROS GEO P & MARILYN M	7640 ROVENNA ST	ANCHORAGE	AK	99518
Dooros Jennifer	1651 Betula Circle	Anchorage	AK	99507
DORAN MICHELE L CHALIFOUR	3811 KNIK AVE	ANCHORAGE	AK	99517
DORGAN KAREN R	6700 FOOTHILL DR	ANCHORAGE	AK	99504
DORONDO KAREN	6218 CAMROSE DR	ANCHORAGE	AK	99504
DORRIS JASON	2500 BROOKE DR	ANCHORAGE	AK	99517
Dorsey Gary	8271 S. Ben Hur Drive	Palmer	AK	99645
DOSS GARY L & CAROLA	6035 STAEDEM DR	ANCHORAGE	AK	99504
DOSS JESSE EUGENE	HC 89 BOX 373D	WILLOW	AK	99688
DOSSMAN CORRIS	8301 E 20TH AVE	ANCHORAGE	AK	99504
DOTY GEORGE W & PEGGY A	PO BOX 92591	ANCHORAGE	AK	99509
DOUBLE DENNIS & HEATHER	PO BOX 521413	BIG LAKE	AK	99652
DOUBRAVA PAUL J	PO BOX 520812	BIG LAKE	AK	99652
DOUCK PATRICK J	PO BOX 771812	EAGLE RIVER	AK	99577
Doug Smole	2996 S. Horseshoe Lake	Big Lake	AK	99652
Doug Smole	2996 S. Horseshoe Lake	Big Lake	AK	99652
DOUGHERTY NICHOLAS D	PO BOX 671782	CHUGIAK	AK	99567
DOUGHTY MARK W	109 W 22ND AVE	ANCHORAGE	AK	99503
Douglass John	800 E. Lynn Martin Drive	Palmer	AK	99645
DOUGLASS KATHY L	HC 89 BOX 451	WILLOW	AK	99688
DOWLING JAS K	1426 HYDER ST	ANCHORAGE	AK	99501
DOWNES DANA	19128 W APPALACHIAN TRL	BIG LAKE	AK	99652
DOWNEY MICHAEL A	PO BOX 544	WILLOW	AK	99688
DOWNEY SHEILA	9320 STRATHMORE DR	ANCHORAGE	AK	99502
DOWNING DAN'L J & DINAH J	PO BOX 967	WILLOW	AK	99688
DOWNING KYLE M	13773 W KLONDIKE DR	BIG LAKE	AK	99652
DOWNING RICHARD ELLSWORTH	PO BOX 15	WILLOW	AK	99688
DOWNING THOS R & KATHLEEN	741 OLD KLATT RD	ANCHORAGE	AK	99515
DOWNS DANA M	PO BOX 521130	BIG LAKE	AK	99652
DOYLE FRANKLIN R & GRACE E	PO BOX 521435	BIG LAKE	AK	99652
DOYLE KEVIN G	PO BOX 242271	ANCHORAGE	AK	99524
Doyle Robert	501 N. Gulikana	Palmer	AK	99645
DOYLE VALERIE	9250 BUDDY WERNER DR	ANCHORAGE	AK	99516
DRAHOSH JEREMY L	PO BOX 521644	BIG LAKE	AK	99652

Alaska State Legislature

South Knik River Community Council

Alaska Job Corps

Mat-Su Borough School District

DRAPER JOHN	PO BOX 1768	PALMER	AK	99645
DRAUGHN WM H & DOROTHY A	PO BOX 670444	CHUGIAK	AK	99567
DRAYTON CANDICE D	3330 CREEKSIDE DR	ANCHORAGE	AK	99504
DRECHSLER PAUL	PO BOX 521286	BIG LAKE	AK	99652
Dreher Darryl	P.O. Box 3713	Palmer	AK	99645
DRESDEN JOS E & ALICIA L	PO BOX 521803	BIG LAKE	AK	99652
DREW LEON E JR & LORETTA I	1350 BALFOUR DR	ANCHORAGE	AK	99515
Drewry Jay	P.O. Box 672372	Chugiak	AK	99567
DREYER HAROLD	9600 ARLENE DR	ANCHORAGE	AK	99502
DROST IAN D	2221 MULDOON	ANCHORAGE	AK	99504
DROZDOWSKI JOHN M	PO BOX 397	WILLOW	AK	99688
DRUM DOUGLAS A	7137 OLD SEWARD HWY	ANCHORAGE	AK	99518
DRUM LINDA A	19619 CRABTREE ST	CHUGIAK	AK	99567
DRUMMOND CHRISTINE R	7701 E 34TH AVE	ANCHORAGE	AK	99504
Duane Mathes	11838 Hagion ShoresDrive	Willow	AK	99688
Duane Mathes	PO Box 777	Willow	AK	99688
DUARTE JAYELENE K	4434 PARSONS AVE	ANCHORAGE	AK	99508
DUBAY JUSTIN T	PO BOX 101668	ANCHORAGE	AK	99510
DUBEY ELIZABETH	7746 BOUNDARY AVE	ANCHORAGE	AK	99504
DUBIE GERALD & BEVERLY J	PO BOX 111631	ANCHORAGE	AK	99511
DUBOIS GREGORY	1409 W 47TH AVE	ANCHORAGE	AK	99508
DUCKWORTH TERESA	9599 BRAYTON DR	ANCHORAGE	AK	99507
DUCLOS JACK B & JENNIFER	3234 LAKE PARK CIR	ANCHORAGE	AK	99517
DUCLOS PAUL JR & SARA H	PO BOX 520631	BIG LAKE	AK	99652
DUCSAY BRIAN	3701 W 42ND AVE	ANCHORAGE	AK	99517
DUDLEY TIM	13351 ALEX CIR	EAGLE RIVER	AK	99577
DUENAS MARIA D	PO BOX 202041	ANCHORAGE	AK	99520
DUFFY JAS E	265 C CHILKOOT AVE	FORT RICHARDS	AK	99505
Duffy John	350 E. Dahlia Avenue	Palmer	AK	99645
DUFT JEFF M	PO BOX 141252	ANCHORAGE	AK	99514
DUGAN KEVIN	1701 BURLINGTON ST	ANCHORAGE	AK	99508
DUGDALE WM J & DANA K	3058 BRITTANY PL	ANCHORAGE	AK	99504
DUGHMAN TIM A & DIANE L	4140 GALATICA DR	ANCHORAGE	AK	99517
DUHOX MARY A	6711 GLOUCESTER PL	ANCHORAGE	AK	99504
DUKE RHONALD J & KATHERINE	PO BOX 230408	ANCHORAGE	AK	99523
DUKES JIM & SONYA	PO BOX 274	WILLOW	AK	99688
DULEY JAS PATRICK	PO BOX 1041	WILLOW	AK	99688
DULL ARNOLD R	PO BOX 1513	BETHEL	AK	99559
DULL ARVIN D & CONNIE E	PO BOX 1727	BETHEL	AK	99559
DUMMANN ERNEST H & DEBRA	6721 ST IVES PL	ANCHORAGE	AK	99504
DUNCAN DONALD WAYNE	PO BOX 240425	ANCHORAGE	AK	99524
DUNCAN HILDUR	12510 HACE ST	ANCHORAGE	AK	99515
DUNCAN JEFFERY M & K A	PO BOX 521024	BIG LAKE	AK	99652

Butte Community Council

Global Distributors Int'l Inc.

Matanuska-Susitna Borough

DUNHAM CALVIN C & FRANCES	8211 SUMMERSET DR	ANCHORAGE	AK	99518
DUNHAM DENNIS & FLORENCE	PO BOX 90062	ANCHORAGE	AK	99509
DUNHAM SEAN & LISA	PO BOX 424	WILLOW	AK	99688
DUNLAP SCOTT	HC 89 BOX 394	WILLOW	AK	99688
DUNLOP JAS C	2948 WILL ROGERS PL	ANCHORAGE	AK	99517
DUNN DAVID E & BARBARA J	21811 SHELTERING SPRUCE	CHUGIAK	AK	99567
DUNN KENNETH M JR&HEATHER	681 N SHENANDOAH DR	PALMER	AK	99645
DUNN PETER WALLACE	6514 LAKEWAY DR	ANCHORAGE	AK	99502
DUNN RUBY D	HC 89 BOX 252-D	WILLOW	AK	99688
DUNN WAYNE E & DARLENE P	18808 FISH HATCHERY RD	EAGLE RIVER	AK	99577
DUNPHY CARRIELEE	PO BOX 521694	BIG LAKE	AK	99652
DUPLESSE MICHAELA J	9200 GLENNHAVEN DR	ANCHORAGE	AK	99502
DUPONT GAIL VICTOR EST	1121 CEDAR DR	ARCATA	CA	95521
DUPPS MICHAEL D & BARBARA	PO BOX 395	WILLOW	AK	99688
Durant Patrick	13817 Malaspina	Eagle River	AK	99 577
DURNILL GLADYS	1107 N EAST ST	KEWANEE	IL	61443
DURR ROBT L JR & ANNA L	239 ZAPPA PL	ANCHORAGE	AK	99504
DUSEK DIANE & George	PO BOX 521902	BIG LAKE	AK	99652
DUVALL AMY L	7110 TRAVIS CIR	ANCHORAGE	AK	99507
DUWA LINDA C TELLEZ	3320 E 42ND AVE	ANCHORAGE	AK	99508
DYE KIMBERLEY A	12720 SCHOONER DR	ANCHORAGE	AK	99515
DYSERT DONALD J & JOY	1045 E WILDER RD	BAY CITY	MI	48706
Dyson Fred	1035 W WINTER AVE	WASILLA	AK	99654
Dyson Fred	10928 Eagle River Rd, Ste 238	Eagle River	AK	99577
E.H. Petraeus	State Capitol, Room 121	Juneau	AK	99801-11
EAKER LAWRENCE W& TAMMY K	PO Box 241843	ANCHORAGE	AK	99524
EARL ADAM G	11525 W HORIZON DR	WASILLA	AK	99654
Earl Ramsey	PO BOX 222133	ANCHORAGE	AK	99522
Easley Paula	POB 190666	Anchorage	AK	99519
EASSA MARY E	2134 Crataegus Ave Ste 400	Anchorage	AK	99508
EASTBERG EDWIN A	PO BOX 521244	BIG LAKE	AK	99652
EASTER LOUIS C	PO BOX 520014	BIG LAKE	AK	99652
EASTLAKE CHAS	PO BOX 203103	ANCHORAGE	AK	99520
EASTON JOHN & MIRIAM K	PO BOX 670286	CHUGIAK	AK	99567
EASTWOOD SUSAN M	1715 SANYA CIR	ANCHORAGE	AK	99508
EATON BRENT P & MARI C	11384 FAIRLANE DR S	SOUTH LYON	MI	48178
EATON MARY F	2670 BROOKSTONE LOOP	ANCHORAGE	AK	99515
EATON VICTORIA	PO BOX 112466	ANCHORAGE	AK	99511
EBERLE TERENCE F	3236 LATOUCHE ST	ANCHORAGE	AK	99508
ECKERT RICHARD L & SALLY	10500 PROSPECT DR	ANCHORAGE	AK	99516
ECKHOFF PATRICIA C	8621 PECK AVE	ANCHORAGE	AK	99504
ECKMAN ANDREW S & PAULA M	PO BOX 865	WILLOW	AK	99688
	PO BOX 521523	BIG LAKE	AK	99652



ECKMANN HERBERT & KATHRYN Ed McCain	7801 SCHUSS CIR PO BOX 27	ANCHORAGE WILLOW	AK AK	99516 99688
EDDY CHAS H & MARY C Ede Ella	PO BOX 747 3201 C St Ste 604	WILLOW Anchorage	AK AK	99688 99503
EDLEN ROBT K EDEN KARSTEN & SYDNEY	PO BOX 111270 3383 E 20TH AVE	ANCHORAGE ANCHORAGE	AK AK	99511 99508
EDER JOHN J & NAN L EDER PAT	PO BOX 520410 PO BOX 520189	BIG LAKE BIG LAKE	AK AK	99652 99652
EDER ROBT O EDER ROBT O & RITA R	1345 RUDAKOF STE 201 4151 RASPBERRY RD	ANCHORAGE ANCHORAGE	AK AK	99508 99502
EDGE LL ROBT J EST Edmonds Melinda	PO BOX 707 P.O. Box 1563	WILLOW Palmer	AK AK	99688 99645-15
EDMONDS WAYNE A & DONNA EDMONDSON DONNA	5936 CAMROSE DR 3911 EASTWOOD LOOP	ANCHORAGE ANCHORAGE	AK AK	99504 99504
EDMONSTON DOUGLAS A EDMONDS BRENT	6056 DONCASTER DR 3735 YOUNG ST	ANCHORAGE ANCHORAGE	AK AK	99504 99508
EDW ARNOLD EDWARD STRABEL	1201 DENALI ST P O BOX 628	ANCHORAGE WILLOW	AK AK	99501 99688
EDWARDS BERNIE MAX EDWARDS EVELYN C	PO BOX 671890 PO BOX 670409	CHUGIAK CHUGIAK	AK AK	99567 99567
EDWARDS GAIL S EDWARDS JAS P	PO BOX 521575 2616 W 66TH AVE	BIG LAKE ANCHORAGE	AK AK	99652 99502
EDWARDS JOHN JAMES Edwards Sam	PO BOX 520545 P.O. Box 919	BIG LAKE Palmer	AK AK	99652 99645
EDWARDS STEPHEN D&SANDY R EDWARDS STEVEN R	621 LIGHTHOUSE DR HC 01 BOX 6315	ANCHORAGE PALMER	AK AK	99515 99645
EDWARDS WILLIE J EGAN JOHN N	13421 TAHOE CIR PO BOX 4485	ANCHORAGE PALMER	AK AK	99516 99645
EGEA CONCEPCION EGELAND JOAN	416 PRICE ST 131 SHELLY MARIE CIR	ANCHORAGE ANCHORAGE	AK AK	99508 99515
EGELUS PATRICK BYRON EHLERS DIETER A	2950 S SKY RANCH LOOP 716 N BRAGAW ST	ANCHORAGE ANCHORAGE	AK AK	99645 99508
Ehmann Butch Ehrhart Roy	P.O. Box 1769 Attn: APVR-RDL-OSV 977 Davis Hwy	Palmer Ft. Richardson	AK AK	99645 99505-70
EICKMAN TIMOTHY H EID MARK F	1076 COUNTY RD 2920 5140 E 112TH AVE	MT PLEASANT ANCHORAGE	TX AK	75686 99516
Eilo Kurt EISENMAYER CURT E	4820 Leah Court 2810 KRISTEN AVE	Anchorage ANCHORAGE	AK AK	99508 99507
EISERT GREGORY A & STACEY EISNER BART B	3031 AMBER BAY LOOP 3809 CAMPBELL AIRSTRIP RD	ANCHORAGE ANCHORAGE	AK AK	99515 99504
EKSTRAND LARS EST ELAM WM R & FRANCES Y	13201 ALPINE DR 1416 W 23RD AVE	ANCHORAGE ANCHORAGE	AK AK	99516 99503
ELBRADER TORY M	PO BOX 520163	BIG LAKE	AK	99652

ELDER CHRIS D	PO BOX 520777	BIG LAKE	AK	99652
ELDRED STEPHEN D& BARBARA	PO BOX 456	WILLOW	AK	99688
ELDRIDGE MICHAEL L & P P	13421 BRANT WAY	ANCHORAGE	AK	99515
Elementary Sherrod	421 N Gulkana	Palmer	AK	99645
ELEY GARY L & VERNA V	PO BOX 617	WILLOW	AK	99688
ELEY GARY L & VERNA V	PO BOX 617	WILLOW	AK	99688
Elison Glenn	6400 Andover Drive	Anchorage	AK	99516
Elkins Jessie	1101 E. North Star	Wasilla	AK	99654
ELLEGE RONALD G	4631 O'MALLEY RD	ANCHORAGE	AK	99507
Ellen Wolf	PO Box 371	Talkeetna	AK	99676
ELLERS ROBERT A	20453 LUCAS AVE	EAGLE RIVER	AK	99577
ELLIOT SETH V	PO BOX 2533	PALMER	AK	99645
ELLIOTT BART S	PO BOX 520573	BIG LAKE	AK	99652
ELLIOTT DAVID K	1500 CACHE DR	ANCHORAGE	AK	99507
ELLIOTT GERRIE	PO BOX 4024	PALMER	AK	99645
ELLIOTT ROBT E III	PO BOX 469	WILLOW	AK	99688
ELLIOTT STEPHEN V JR& V L	5723 E MORNING MIST DR	PALMER	AK	99645
ELLS CINDY W	2520 LAIRD CIR	ANCHORAGE	AK	99516
ELLS DOUGLAS LEROY II	PO BOX 200954	ANCHORAGE	AK	99520
Ellis Johnny	716 W. 4th Avenue, Ste 440	Anchorage	AK	99501
Ellis Johnny	State Capitol, Room 9	Juneau	AK	99801-11
ELLS LORRAINE	8531 E 20TH AVE	ANCHORAGE	AK	99504
ELLS SHELLEY D	4021 RESURRECTION DR	ANCHORAGE	AK	99504
Ellis Susan	541 S Cobb, Suite 205	Palmer	AK	99645
ELLITHORPE SILAS	PO BOX 288	WILLOW	AK	99688
ELLSWORTH JOHN C & J L	9424 NOBLEWOOD	ANCHORAGE	AK	99515
ELMORE CHAS B & PAMELA L	PO BOX 63	WILLOW	AK	99688
ELZIG BYRON	PO BOX 151	WILLOW	AK	99688
EMBL Y RONALD W II& JODY L	8028 CRANBERRY ST	ANCHORAGE	AK	99502
EMERICK DOUGLAS T	10737 TIMOTHY DR	CONIFER	CO	80433
EMERSON EVERETT W	830 JAY CIR	ANCHORAGE	AK	99504
EMMENEGGER WERNER	PO BOX 520018	BIG LAKE	AK	99652
EMMI John E	1015 S CHECK ST	WASILLA	AK	99654
EMMI ERNEST J.	21650 GRAYBILL ST	CHUGIAK	AK	99567
EMMI JOHN E.	21650 GRAY BILL ST	CHUGIAK	AK	99567
EMORY JAS D & JOAN L	2203 W 48TH AVE	ANCHORAGE	AK	99517
ENG TERESA A	17237 BEARPAW CIR	EAGLE RIVER	AK	99577
ENGBRETSEN MARK E& ANDREA	PO BOX 520433	BIG LAKE	AK	99652
ENGL E KENNETH M & KYONG	641 CLIPPER SHIP CT	ANCHORAGE	AK	99515
ENGLISH DWIGHT E & RHONDA	PO BOX 521471	BIG LAKE	AK	99652
ENGLISH STANLEY P&CANDICE	3640 CLAY PRODUCTS DR	ANCHORAGE	AK	99517
ENGLUND DESMOND D	4704 MALIBU RD	ANCHORAGE	AK	99517
ENGLUND SUSAN M	PO BOX 94	WILLOW	AK	99688

The Conservation Fund  
Wasilla Planning Commission

Alaska State Legislature  
Alaska State Legislature

The Ellis Group inc.

ENGSTROM DENNIS G & E J	7140 WHITEHALL ST	ANCHORAGE	AK	99502
ENSMINGER CURT	PO BOX 2391	PALMER	AK	99645
ENTERS JAS S	7940 KIANA CIR	ANCHORAGE	AK	99507
EPPLER TIMOTHY WILFRED	PO BOX 520676	BIG LAKE	AK	99652
EPPS VERNON A & BETTY J	PO BOX 505	TALKEETNA	AK	99676
Erbey Ken	635 E. Cedar Avenue	Palmer	AK	99645
ERHART LEWIS	3345 A LAKESHORE DR	ANCHORAGE	AK	99517
Eric Miknich	12000 Gillette Drive	ANCHORAGE	AK	99516
Erickson Jim	PO Box 877294	Wasilla	AK	99687
ERICKSON BRYCE & JULIE	PO BOX 111128	ANCHORAGE	AK	99511
ERICKSON CAMERON L	12531 ALPINE DR	ANCHORAGE	AK	99516
ERICKSON DAN M & PAMELA A	16216 SEVILLE PARK CIR	ANCHORAGE	AK	99516
ERICKSON HOWARD & DOROTHY	13388 E VERDA DR	PALMER	AK	99645
ERICKSON LEE A & ROBYN L	PO BOX 3992	PALMER	AK	99645
Erickson Rich	P.O. Box 872847	Wasilla	AK	99687
ERICKSON ROGER D	PO BOX 520635	BIG LAKE	AK	99652
ERICKSON TRYGVE & DIXIE	2034 CRATAEGUS AVE	ANCHORAGE	AK	99508
Erik Johnson	6711 E. David H. Circle	Palmer	AK	99645
Erik Johnson	6711 E. David H. Circle	Palmer	AK	99645
Erin McLarnon, President	PO Box 858	WILLOW	AK	99688
EROVICK BRAD A	11310 POLAR DR	ANCHORAGE	AK	99515
ERVING CLAUDE & DONNA	3811 GUNWALE CIR	ANCHORAGE	AK	99516
ESADA JOHN A JR	19736 S MITKOF LOOP	EAGLE RIVER	AK	99577
ESTABROOK GERALDINE T	2525 GAMBELL ST	ANCHORAGE	AK	99503
ESTELLE RICHARD	3020 N GLENN HWY	PALMER	AK	99645
ESTES JOAN L	PO BOX 520363	BIG LAKE	AK	99652
ESTES MICHAEL R & JANEY	PO BOX 521367	BIG LAKE	AK	99652
EUBANK MICHAEL	PO BOX 220543	ANCHORAGE	AK	99522
EUBANKS W SCOTT	PO BOX 2832	PALMER	AK	99645
Eunice Joanne	P.O. Box 3501	Palmer	AK	99645
Eunice Sandra	P.O. Box 3501	Palmer	AK	99645
EUWER ARCHIE NELSON	PO BOX 2081	PALMER	AK	99645
EVANS DARBY T	PO BOX 242062	ANCHORAGE	AK	99524
EVANS DIANA	1300 W 7TH AVE	ANCHORAGE	AK	99501
EVANS MAYFIELD	PO BOX 140905	ANCHORAGE	AK	99514
EVANS MICHAEL	PO BOX 604	WILLOW	AK	99688
EVANS MICHAEL H & JANET L	1701 S MIDDLE MESA DR	PALMER	AK	99645
EVANS THERESA K	PO BOX 391	WILLOW	AK	99688
EVERS MARK S & BEVERLY M	16670 RIVER HGTS LOOP	EAGLE RIVER	AK	99577
EVERTSON DUANE R	PO BOX 520824	BIG LAKE	AK	99652
Ewing Mark	477 S. Boundary Street	Wasilla	AK	99654
Eyler Lisa	619 E. Ship Creek Avenue, Ste 321	Anchorage	AK	99501
EZELL WENDELL L & BONNIE E	2011 S JENSEN RD	PALMER	AK	99645

EZELLE EDW M	1551 S MIDDLE MESA DR	PALMER	AK	99645
Faber Wanda	PO BOX 201147	Anchorage	AK	99520
FAGLIE AZEL M JR	PO BOX 521298	BIG LAKE	AK	99652
FAHRNI JANET K	PO BOX 521383	BIG LAKE	AK	99652
FAHY JOHN G	1579 VALARIAN ST	ANCHORAGE	AK	99508
FAIKS JANET Q	PO BOX 521152	BIG LAKE	AK	99652
Faiks Jim	BOX 521152	BIG LAKE	AK	99652
FAIR LOWELL & HEATHER	645 G ST #100-579	ANCHORAGE	AK	99501
FAIRBANKS NORMAN L & MARY	7217 BAILEY DR	ANCHORAGE	AK	99502
FAIRBANKS ROBT & MARY	2600 BRITTANY DR	ANCHORAGE	AK	99504
FAIRCHILD JOHN C	PO BOX 521101	BIG LAKE	AK	99652
Fairclough Anna	P.O. Box 771112	Eagle River	AK	99577
FAIRFIELD RUSSELL D & VERA	PO BOX 521785	BIG LAKE	AK	99652
FAIST JOHN D & JANIS L	788 N TARA ST	HOUSTON	AK	99694
FAIT GARY	PO BOX 520781	BIG LAKE	AK	99652
Fall Jim	333 Raspberry Road	Anchorage	AK	99518
FALLER DAVID A & JULIE A	5326 A ST	ANCHORAGE	AK	99518
FAN TONY CHING	20511 PHILADELPHIA WAY	EAGLE RIVER	AK	99577
FANNON DEBRA L & HERRICK D	7321 CHRISTOPHER CIR	ANCHORAGE	AK	99507
FANNON JOSHUA F	PO BOX 521344	BIG LAKE	AK	99652
Fannon Link	P.O. Box 940027	Houston	AK	99694
FANNON LINK C	PO BOX 520978	BIG LAKE	AK	99652
FARAGO STEVEN W	PO BOX 883	WILLOW	AK	99688
FARIAS RALPH JR	PO BOX 430	WILLOW	AK	99688
FARKAS GERARD L & JULIE A	4817 WESLEYAN DR	ANCHORAGE	AK	99508
FARMER DAVID	711 LIGHTHOUSE CT	ANCHORAGE	AK	99515
FARMER RICHARD F & CHERYL	PO BOX 520548	BIG LAKE	AK	99652
FARR JAS A	PO BOX 201839	ANCHORAGE	AK	99520
FARRAR TERRY G & AMY S	433 C 7TH ST	FORT RICHARDS	AK	99505
FARRELL L LAWRENCE	10611 ABBOTT LOOP RD	ANCHORAGE	AK	99507
FARRELL RICKY J & BRANDY	6861 RABBIT CREEK	ANCHORAGE	AK	99516
FARRINGTON SARA J SISLER	317 TAYLOR	ANCHORAGE	AK	99508
FAST BILL B & BONNIE MAE	3807 BARBARA DR	ANCHORAGE	AK	99517
FAULK DAVID G & BONNIE J	8401 BRAYTON DR STE 200	ANCHORAGE	AK	99507
FAULKNER WINTHROP S & A H	9004 JEWEL TERRACE ST	ANCHORAGE	AK	99502
FAUNCH DONALD STEVE	4033 IONA CIR	ANCHORAGE	AK	99507
Faussett Tom	HC02 Box 7741-M	Palmer	AK	99645
FAZEKAS LASZLO & LASZLONE	2708 W 67TH AVE	ANCHORAGE	AK	99502
FEATHERS JAS W JR	9200 INDEPENDENCE	ANCHORAGE	AK	99507
FEDOR MARK S	PO BOX 1070	WILLOW	AK	99688
FEE ROBT P	PO BOX 919	PALMER	AK	99645
FEEBECK WM H & SUSAN A	2320 TASHA DR	ANCHORAGE	AK	99502
FEENEY MICHAEL P & ROBIN M	1810 GREENDALE DR	ANCHORAGE	AK	99504

FEGERT ROBT D & SUSAN K	HC 89 BOX 461	WILLOW	AK	99688
FEIL MICHAEL C	PO BOX 170	WILLOW	AK	99688
FELDER HEATHER E	PO BOX 200621	ANCHORAGE	AK	99520
FELL WILLIAM R & TINA A	7322 SETTER DR	ANCHORAGE	AK	99502
FELLAND JAS J & LORI A	6120 E DEARBORN DR	PALMER	AK	99645
FELT ALLAN V & BARBARA M	PO BOX 233133	ANCHORAGE	AK	99523
FELTS HOWARD L	PO BOX 1314	PALMER	AK	99645
FENERTY DENNIS G&HARRIETT	2330 INNES CIR	ANCHORAGE	AK	99515
FERGUSON DAVID B & A L	2624 W 66TH AVE	ANCHORAGE	AK	99502
FERGUSON MICHAEL D	PO BOX 676	WILLOW	AK	99688
FERGUSON THOS A	4203 W DIMOND BLVD	ANCHORAGE	AK	99502
FERGUSON TIMOTHY C	2008 E 4TH AVE	ANCHORAGE	AK	99501
FERNANDEZ ERNEST	501 E 73RD AVE	ANCHORAGE	AK	99518
FERNANDEZ JOS T	PO BOX 521563	BIG LAKE	AK	99652
FERRETT DOUGLAS A & LINDA	PO BOX 521129	BIG LAKE	AK	99652
FERRI JOHN	PO BOX 521662	BIG LAKE	AK	99652
FERRIS JUSTIN C	6749 DEBARR RD	ANCHORAGE	AK	99507
FERRIS MICHAEL S	5322 SHORECREST DR	ANCHORAGE	AK	99502
FERRY GEO F	16750 E HUNTLEY RD	PALMER	AK	99645
FERRY JAS D & GERALDINE H	PO BOX 285	WILLOW	AK	99688
FESLER CURTIS W & A S	6645 MCGILL WAY	ANCHORAGE	AK	99502
FESLER DORATHY JEAN	PO BOX 521022	BIG LAKE	AK	99652
FICKES CARL JAS & CATHY D	9510 SELKIRK DR	ANCHORAGE	AK	99515
FICKLIN BRAD & AMY	PO BOX 520808	BIG LAKE	AK	99652
FIDLER THOMAS F	556 N CAPPY ROACH RD	PALMER	AK	99645
FIEDLER LINWOOD W & K M	PO BOX 135	WILLOW	AK	99688
FIELD JUDITH A	2700 JEFFORD PL	ANCHORAGE	AK	99517
FIELD ROBT C	PO BOX 88	WILLOW	AK	99688
FIELDING DAVID M	PO BOX 91810	ANCHORAGE	AK	99509
FIELDING Jim	PO BOX 770538	EAGLE RIVER	AK	99577
FIELDING JIM H	PO BOX 770538	EAGLE RIVER	AK	99577
FIGUEROA BILLY J	PO BOX 161	WILLOW	AK	99688
FIKE BRETT & STEPHANIE	HC 89 BOX 711	WILLOW	AK	99688
FIKE REV TR	3311 EVERGREEN ST	ANCHORAGE	AK	99504
FIKES BILLY L & VIRGINIA	PO BOX 521134	BIG LAKE	AK	99652
FILKILL DAVID B	PO BOX 1014	WILLOW	AK	99688
FILLMORE R DEAN	PO BOX 244793	ANCHORAGE	AK	99524
FINCHER JAS M & CAROL A	9411 ABBOTT LOOP RD	ANCHORAGE	AK	99507
FINEGAN THOS E & NINA L	1107 RIL CIR	ANCHORAGE	AK	99504
FINK MATTHEW L	2008 HILLCREST CIR	ANCHORAGE	AK	99503
FINK THOS A	1350 W 23RD AVE	ANCHORAGE	AK	99503
Finkenbinder Travis	P.O. Box 13021	Trapper Creek	AK	99683
FINN THOS D	7670 PLEASURE VIEW CIR	ANCHORAGE	AK	99507

Trapper Creek Community Council

FINNEGAN TIM & THERESA K	6101 MOOSE MEADOW LN	ANCHORAGE	AK	99516
FIRMIN WM A & SUSAN M	PO BOX 962	WILLOW	AK	99688
FISCHER DAVID E & BONNIE J	PO BOX 230903	ANCHORAGE	AK	99523
FISCHER ERNST W	5432 NORTHERN LGTS BLVD	ANCHORAGE	AK	99508
FISCHER JOS F & DAWN M	1611 GEORGE BELL CIR	ANCHORAGE	AK	99515
FISCHER KELLY CALLISON	11850 PORTAGE CIR	ANCHORAGE	AK	99515
Fischer Nancy	P.O. Box 410	Palmer	AK	99645
FISCHER WARREN	HC 89 BOX 280	WILLOW	AK	99688
FISH GORDON H	PO BOX 520513	BIG LAKE	AK	99652
FISH JAS G & KAREN Y	7821 PORSCHE	ANCHORAGE	AK	99502
FISH THOS	PO BOX 346	PALMER	AK	99645
FISHBACK STEVE & JAN	540 L ST	ANCHORAGE	AK	99501
FISHER DOUGLAS E	PO BOX 520990	BIG LAKE	AK	99652
FISHER EDW	PO BOX 521464	BIG LAKE	AK	99652
FISHER ROBT L & PAMELA E	PO BOX 4517	PALMER	AK	99645
FISHER STEVEN ROBT	PO BOX 520209	BIG LAKE	AK	99652
FISHER TIMOTHY S & DELLA R	4838 NEWCASTLE WAY	ANCHORAGE	AK	99503
FISK JASON J	3100 WAR PL	ANCHORAGE	AK	99517
FISK LEIGH	3485 SHERRIE ST	ANCHORAGE	AK	99504
FITES DEBORAH W	PO BOX 520109	ANCHORAGE	AK	99652
FITZGAIRELD CHAS V	PO BOX 732	WILLOW	AK	99688
FITZGAIRELD CLAIRE R	PO BOX 248	WILLOW	AK	99688
FITZGERALD DAVID A	13155 STEPHENSON ST	ANCHORAGE	AK	99515
FITZGERALD KEVIN	1525 N ST	ANCHORAGE	AK	99501
FITZMAURICE ANDREA	4450 CORDOVA ST	ANCHORAGE	AK	99503
FLAHERTY BRENDAN A & C S	7750 E SAPHIRE RD	PALMER	AK	99645
FLAHERTY MICHAEL J & DIANA	19614 1ST ST	EAGLE RIVER	AK	99577
FLANAGAN ROBT F	8510 FLAMINGO DR	ANCHORAGE	AK	99502
FLATLEY PAULA BARRETT	19986 BIRCHWOOD LOOP RD	CHUGIAK	AK	99567
Fleek Adrienne	222 W. 7th Avenue, Room 537, Box 19	Anchorage	AK	99501
FLEISCHHACKER RONALD R	10428 CHAIN OF ROCK	EAGLE RIVER	AK	99577
FLEMING JOHN T & NANCY	1310 HEIDI CIR	ANCHORAGE	AK	99518
FLEMING WAYNE	PO BOX 618	PALMER	AK	99645
FLETCHER ROBT & RUBY	PO BOX 52111	BIG LAKE	AK	99652
FLETCHER ROBT L	4122 HOOD CT	ANCHORAGE	AK	99517
FLEWELLING KENNETH B	PO BOX 413	WILLOW	AK	99688
FLORES DAN'L J	PO BOX 230061	ANCHORAGE	AK	99523
FLORIO SHAWN A & RONA I	2020 SHORE DR	ANCHORAGE	AK	99515
FLOWERS BERNARD E	8219 NADINE ST	ANCHORAGE	AK	99507
FLOWERS CARLA JEAN	10588 6330 RD	ANCHORAGE	AK	99507
FLOWERS CARLA JEAN	3121 E 84TH AVE	MONTRORSE	CO	81401
FLOWERS DELMAR	PO BOX 520818	ANCHORAGE	AK	99507
FLOYD JAS & TAMI	501 N. Gulikana	BIG LAKE	AK	99652
Floyd Kim		Palmer	AK	99645

Borealis Business Services

U.S. Environmental Protection Agency

Mat-Su Borough School District

FLOYD VERNON E & KAZUKO	PO BOX 190491	ANCHORAGE	AK	99519
FLUG FREDERICK C JR	PO BOX 521733	BIG LAKE	AK	99652
FLYNN CHAS P	1519 HIDDEN LN	ANCHORAGE	AK	99501
Foerster Cathy	333 West 7th Avenue, Suite 100	Anchorage	AK	99501
FOGEL HELEN M	5951 KEYANN CIR	ANCHORAGE	AK	99504
FOLAND MARY A	7156 TERRY PL	ANCHORAGE	AK	99502
FOLEY HAROLD C JR & SUSAN	223 PEPPERTREE CIR	ANCHORAGE	AK	99504
FOLEY JAS W & SUE L	5308 COPE ST	ANCHORAGE	AK	99518
Foley Jeff	301 Callista Ct Ste A	Anchorage	AK	99518-30
FOLEY KENITH M & DEBRA J	1181 N BALSAM CIR	WASILLA	AK	99654
FOLLETT RICHARD H&SHIRLEY	903 W NORTHERN LGTS BLVD	ANCHORAGE	AK	99503
FOLSOM AARON A	PO BOX 4552	PALMER	AK	99645
FOLSOM MICHAEL D& ANDRINA	PO BOX 520361	BIG LAKE	AK	99652
FOLTA RUSSELL & PHYLLIS H	1786 MORNINGTIDE CT	ANCHORAGE	AK	99501
FONTAINE KENNETH & DONNA	PO BOX 520446	BIG LAKE	AK	99652
Foo Stanley	4720 Business Park Blvd Ste G25	Anchorage	AK	99503
Foo Stanley	19328 South Mitkof Loop	Eagle River	AK	99577
FOOTE MICHAEL D & DOLORES	PO BOX 3698	PALMER	AK	99645
FORBUSH KYLE R & LISA A	PO BOX 521516	BIG LAKE	AK	99652
FORD DAVID G. & JANICE A.	4272 LOWER KOGRU DR	EAGLE RIVER	AK	99577
FORD JANICE A	1404 KINNIKNICK	ANCHORAGE	AK	99508
FOREMAN ED W	1028 BAXTER RD	PUEBLO	CO	81006
Foreman Paul	333 W. 4th Avenue, Ste 307	Anchorage	AK	99510
FORREST MISTI	HC 87 BOX 470	WILLOW	AK	99688
FORREST SCOTT	3641 HAZEN CIR	ANCHORAGE	AK	99515
Forschen Garland	P.O. Box 940027	Houston	AK	99694
FORSGREN RICHARD E & A K	PO BOX 65	WILLOW	AK	99688
FORSLAND DAVID C & JODI R	2242 KNOLL CIR	ANCHORAGE	AK	99501
FORSTNER EDW	PO BOX 521114	BIG LAKE	AK	99652
FORSYTH JEREMY A & J W	1735 BARTLETT DR	ANCHORAGE	AK	99507
FORSYTH RONALD G & L L	4798 MILLS DR	ANCHORAGE	AK	99508
FORTHUN STEVEN J& DONNA L	2632 GLACIER ST	ANCHORAGE	AK	99508
FORTIER SAMUEL J	6800 SEQUOIA CIR	ANCHORAGE	AK	99516
FORTNER JON & WENDY	14232 SNOWDRIFT WAY	ANCHORAGE	AK	99515
FORTUNY MIKEL R & RENE	PO BOX 112846	ANCHORAGE	AK	99511
FOSTER DIANE L	PO BOX 1303	PALMER	AK	99645
FOSTER EARLE C TR	PO BOX 243132	ANCHORAGE	AK	99524
FOSTER GARY	PO BOX 220706	ANCHORAGE	AK	99522
FOSTER JOS E & CRYSTAL M	PO BOX 520533	BIG LAKE	AK	99652
FOSTER KEVIN E & SUSAN A	PO BOX 1057	WILLOW	AK	99688
FOSTER STEVE W & TRACY L	6927 LASER DR	ANCHORAGE	AK	99504
Foster William	P O BOX 955	PALMER	AK	99645
FOSTER William T	P O BOX 4787	PALMER	AK	99645

FOUNTAIN JAS GREGG	7210 HUNTSMAN	ANCHORAGE	AK	99518
FOURNIER WALTER K	4211 FLORINA ST	ANCHORAGE	AK	99508
FOUSE DONALD J & SUSAN B	2310 ALBION CIR	ANCHORAGE	AK	99515
FOWLER H WINTHROP JR	5100 WOODBRIDGE DR	ANCHORAGE	AK	99516
FOWLER ROGER & LINDA	PO BOX 1025	WILLOW	AK	99688
Fox Dale	Alaska Cabaret, Hotel, Restaurant & Retailers Assn	Anchorage	AK	99518
Fox Jerry	1111 E. 80th Avenue, Ste 3	Anchorage	AK	99518
FOX JOHN W.	5610 Silverado Way, Ste A1	ANCHORAGE	AK	99523
FOX WM B & DALENE	PO BOX 142	PALMER	AK	99645
FRACHEY SEVEN J & JOHNA K	PO BOX 757	WILLOW	AK	99688
FRAHM MICHAEL J & MARY LOU	PO BOX 1083	WILLOW	AK	99688
FRANK DAVID	765 W 70TH	ANCHORAGE	AK	99518
FRANK KEVIN J & PAMELA B	1909 N SALEM LOOP	ANCHORAGE	AK	99508
FRANK LEROY H	PO BOX 198	WILLOW	AK	99688
FRANK PETER W	PO BOX 3594	PALMER	AK	99645
FRANKLIN JEREMY P	13962 FIRE CREEK TRL	EAGLE RIVER	AK	99577
FRANKS MARK S	6720 E 11TH	ANCHORAGE	AK	99504
FRANTZICH CARL M & MARILYN	PO BOX 808	PALMER	AK	99645
Fraser Carol	425 G Street, Suite 210	Anchorage	AK	99501
FRASIER RICHARD	PO BOX 414	WILLOW	AK	99688
FRAY JONATHAN C & KRISTIE J	PO BOX 521594	BIG LAKE	AK	99652
FRAZIER DEBORAH K	PO BOX 940396	HOUSTON	AK	99694
FRAZIER KIM ROBERT	3740 BEEMAN CIR	ANCHORAGE	AK	99507
FRAZIER RANDY B & ANNE M	3940 TURNAGAIN BLVD E	ANCHORAGE	AK	99517
FREAD WILLIAM E	1981 S JENSEN RD	PALMER	AK	99645
FREDENHAGEN JAS S & V J	2720 VALLEY FORGE CIR	ANCHORAGE	AK	99502
FREDERICKS DAWN	12950 SE 246TH ST	KENT	WA	98031
FREDERICKS GLENN WILFRED	1400 VIRGINIA CT	ANCHORAGE	AK	99501
FREDERICKS SUSAN ANN	PO BOX 1354	UNALASKA	AK	99685
Frederickson Teresa	P.O. Box 1772	Palmer	AK	99645
FREDRICK JAS S & LINDA D	4580 MONTROSE CIR	ANCHORAGE	AK	99515
FREDRICKSON SHELLEY ANN	832 W 19TH AVE	ANCHORAGE	AK	99503
FREEMAN JACK & DEBORAH	2721 E 144TH AVE	ANCHORAGE	AK	99516
Freeman Ken	P.O. Box 196601	Anchorage	AK	99519
FREEMAN LAMAR	PO BOX 523	WILLOW	AK	99688
FREESE ROBT C	5811 E BIDEFORD BLVD	PALMER	AK	99645
FRELIN MICHEL	9106 ARLENE ST	ANCHORAGE	AK	99502
French bob	685 Birch St	ANCHORAGE	AK	99501
FRENCH COBIE R	19800 KIRK AVE	EAGLE RIVER	AK	99577
French Hollis	716 W. 4th Avenue, Ste 420	Anchorage	AK	99501
French Hollis	State Capitol, Room 504	Juneau	AK	99801-11
FRICK WM	PO BOX 521521	BIG LAKE	AK	99652
FRIDERICI JAS	9451 WEST LAKE DR	EAGLE RIVER	AK	99577



FRIDLEY DAVID L	PO BOX 520042	BIG LAKE	AK	99652
FRIEDMAN JEFFREY A	1534 D ST	ANCHORAGE	AK	99501
FRIENDSHUH MICHAEL J	1231 E WOODCREST DR	WASILLA	AK	99654
FRIES CAROLE S MILLER	PO BOX 112034	ANCHORAGE	AK	99511
FRICTHER DAVID L	3450 NEWCOMB DR	ANCHORAGE	AK	99508
FRITTS WM K JR & MICHELLE	PO BOX 5416	FORT RICHARDS	AK	99505
FRITZ CHRISTINE	PO BOX 1341	PALMER	AK	99645
FRITZ MICHAEL G&CAROL LE	PO BOX 18	PALMER	AK	99645
FROESCHLE JERRY D & S M	4040 APOLLO DR	ANCHORAGE	AK	99504
FROHLING KEVIN K	6250 N LOSSING RD	PALMER	AK	99645
FROMMER JOHN AARON	7001 VIBURNUM DR	ANCHORAGE	AK	99507
FROST DONALD W & SHARON T	13210 W KING ARTHUR DR	HOUSTON	AK	99694
FROST KELLY J & JILL E	PO BOX 521515	BIG LAKE	AK	99652
FROST ROBT & VELDA DEANNA	524 W QUIJK SILVER CIR	PALMER	AK	99645
Frost Steven	P.O. Box 940027	Houston	AK	99694
FROST STEVEN W & HEIDI J	PO BOX 940132	HOUSTON	AK	99694
FRY ROBT F & SANDRA R	PO BOX 521673	BIG LAKE	AK	99652
FRY STEVEN & MARIA	618 W 75TH AVE	ANCHORAGE	AK	99518
FRYE IVY	PO BOX 4059	PALMER	AK	99645
FRYE STEVE	PO BOX 210123	ANCHORAGE	AK	99521
FRYE WESLEY R JR& CANDACE	14600 SANDERLIN ST	ANCHORAGE	AK	99516
FRYER HENRY M	PO BOX 1181	TALKEETNA	AK	99676
FRYSLIE BRIAN	2301 MCKINLEY AVE	ANCHORAGE	AK	99517
FUCHS HELMUT G. & LINDA	9441 PONDEROSA DR	ANCHORAGE	AK	99507
Fuhrer Ron	4100 Spenard Road	Anchorage	AK	99517
FULLER JEFFERY E	PO BOX 197	WILLOW	AK	99688
FULLER JERRY W	3400 W 86TH AVE	ANCHORAGE	AK	99502
FULLER MICHAEL J & K A	4900 SOUTHAMPTON DR	ANCHORAGE	AK	99503
FULLER RALPH L	PO BOX 521627	BIG LAKE	AK	99652
FULLER RICHARD D&KRISTI H	6310 KALMIA	ANCHORAGE	AK	99507
FULLER ROBT B & LORILEE K	5511 LITTLE TREE ST	ANCHORAGE	AK	99507
FULLER SHANNON L	13547 W KLUTINA DR	BIG LAKE	AK	99652
FULLER VIRGINIA A	5606 TONGA ST	ANCHORAGE	AK	99507
FULLERTON ADINE A	PO BOX 746	WILLOW	AK	99688
FULTON LARRY D & BRENDA L	2041 S DECAMP CIR	PALMER	AK	99645
FUNK GAIL SCHEI	3721 W 67TH AVE	ANCHORAGE	AK	99502
FUNKHOUSER PETE J & G A	640 OCEANVIEW DR	ANCHORAGE	AK	99515
FURBUSH DOROTHY B	PO BOX 110245	ANCHORAGE	AK	99511
FYFE WARREN R	PO BOX 403	WILLOW	AK	99688
GAFFEY RICHARD	PO BOX 521825	BIG LAKE	AK	99652
GAGE BRIAN L	PO BOX 520440	BIG LAKE	AK	99652
GAGE WILLIAM L & K M	5066 BUCKINGHAM WAY	ANCHORAGE	AK	99503
GAINES CARL A	227 N GULKANA ST	PALMER	AK	99645

City of Houston

Anchorage Education Association



GARDNER JOHN M& KATHERINE Gardner Paul	PO BOX 4401	PALMER	AK	99645
Garley Sandra	580 Briar Drive	Wasilla	AK	99654
Garlock Clark & Betty	290 E. Hering Avenue	Wasilla	AK	99654
GARLOCK THOS	16500 Spruce Street, #723	Palmer	AK	99645
GARNER POL SUK HONG	8430 WISTERIA	ANCHORAGE	AK	99502
GARNER RICHARD WM	2830 PELICAN DR	ANCHORAGE	AK	99515
GARNESS DAVID A & SHIRLEY	3001 WIDGEON LN	ANCHORAGE	AK	99508
GAROUTTE KIRK E	2614 SPRUCEWOOD ST	ANCHORAGE	AK	99508
GARRETT DENNIS	6831 ROVENNA ST	ANCHORAGE	AK	99518
GARRETT WALTER D	PO BOX 520481	BIG LAKE	AK	99652
GARRISON GEO F	3761 WINTERSET DR	ANCHORAGE	AK	99508
GARRISON JASH & M R	7059 FAIRWEATHER PARK LP	ANCHORAGE	AK	99518
GARRISON ROY M	2260 LAKE GEORGE DR	ANCHORAGE	AK	99504
GARROD ARTHUR T & JANET L	3719 LUNAR DR	ANCHORAGE	AK	99504
GARROUTTE VINCE& ANGELA L	6991 E CONNECT CT	PALMER	AK	99645
Gary Stromberg	3020 CARRIAGE DR	ANCHORAGE	AK	99507
GARZA JOE & LULU	10101 SLALOM DRIVE	ANCHORAGE	AK	99507
GASQUE EDW M & KAREN S	5016 SETON DR	ANCHORAGE	AK	99508
GASQUE MURRY M	13961 JARVI DR	ANCHORAGE	AK	99515
GASTON RALPH D & KAREY R	PO BOX 520208	BIG LAKE	AK	99652
GATES DANIEL	PO BOX 520197	BIG LAKE	AK	99652
GATES GERALD L & MICHELLE	3403 DORBRANDT ST	ANCHORAGE	AK	99503
GATES JOHN F	2928 SEAWIND DR	ANCHORAGE	AK	99516
GATES NATHANIEL D & WENDY	2744 ILLIAMNA AVE	ANCHORAGE	AK	99517
Gatto Carl	301 MARINER DR	ANCHORAGE	AK	99515
GAUTHIER RAYMOND A	State Capitol, Room 411	Juneau	AK	99801--11
GAWRELUK WM & ALICE A	8324 WILCOX ST	ANCHORAGE	AK	99502
GAY ROGER H	6570 ASKELAND DR	ANCHORAGE	AK	99507
GEARING STEVE R & VICKI L	PO BOX 521215	BIG LAKE	AK	99652
GEARRING WALTER L JR	PO BOX 520632	BIG LAKE	AK	99652
GEARY DENNIS O & JENNIFER	4755 S MCKECHNIE LOOP	PALMER	AK	99645
GEE DONALD E	PO BOX 242753	ANCHORAGE	AK	99524
GEE ROBT F	8220 HENRY CIR	ANCHORAGE	AK	99507
Geeting Doug	PO BOX 110868	ANCHORAGE	AK	99511
GEIERMANN LOUIS J	P.O. Box 42	Talkeetna	AK	99676
GEIGER ARNOLD A & THERESA	PO BOX 521284	BIG LAKE	AK	99652
GEIGER JODI LYNNE COLON	6300 TRAPPERS TRAIL RD	ANCHORAGE	AK	99516
GEIGER ROBT GEO	PO BOX 423	WILLOW	AK	99688
GEIL DANIEL G	3531 E 42ND AVE	ANCHORAGE	AK	99508
GEN TEAMSTERS LOC UN #959	1226 HIGH ST	AUBURN	CA	95603
Gentile John	520 E 34TH AVE	ANCHORAGE	AK	99503
GEO W. MURPHY	20133 Eagle River Road	Eagle River	AK	99577
	PO BOX 1003	WILLOW	AK	99688

GEORGE DAVID S & CAROLE F	16922 KIYONA CIR	EAGLE RIVER	AK	99577
GEORGE HOWARD	12608 FLORENCE DRIVE	WILLOW	AK	99688
GEORGE HOWARD	12608 FLORENCE DRIVE	WILLOW	AK	99688
GERAGHTY ANN	PO BOX 521753	BIG LAKE	AK	99652
GERARD BARBARA	P O BOX 520815	BIG LAKE	AK	99652
GERE SCOTT L & MONICA D	7901 SCHUSS CIR	ANCHORAGE	AK	99507
GERIK MICHAEL A	PO BOX 110710	ANCHORAGE	AK	99511
GEROW SEZEN M	7135 WHITEHALL CT	ANCHORAGE	AK	99502
GERSTENFELD SOL L	PO BOX 140326	ANCHORAGE	AK	99514
GETTY ALFRED W III& KEY E	18201 SPAIN DR	ANCHORAGE	AK	99516
GETZ LEON C & NANCY C	7036 HENDERSON LOOP	ANCHORAGE	AK	99507
GEUSS ARTHUR	PO BOX 93270	ANCHORAGE	AK	99509
Gholson Carolyn	P.O. Box 2153	Palmer	AK	99645
GIANCOLI RICHARD	1651 S AUDIE ST	PALMER	AK	99645
GIBSON JON D	8003 PIONEER DR	ANCHORAGE	AK	99504
GIBSON MARY ANN	128 E 45TH	ANCHORAGE	AK	99503
GIBSON ROGER	16440 BLACK BEAR CIR	ANCHORAGE	AK	99516
GIELAROWSKI BOBBIE	PO BOX 521112	BIG LAKE	AK	99652
GIELAROWSKI S J REV TR	11751 MARY AVE	ANCHORAGE	AK	99515
GIELOW CHAS & MARY	PO BOX 3448	PALMER	AK	99645
GIFFORD THOS M	6040 STAEDEM DR	ANCHORAGE	AK	99504
Gilbert Alisa	1057 W. Fireweed Lane, Ste 204	Anchorage	AK	99503
Gilbert Jim	184 E. 53rd Avenue	Anchorage	AK	99518
GILBERT JOHN H JR	PO BOX 520337	BIG LAKE	AK	99652
GILES CLIFFORD THOS	PO BOX 520241	BIG LAKE	AK	99652
GILL DAVE W & KELLEY R	PO BOX 521477	BIG LAKE	AK	99652
GILL GREG	2208 TURNAGAIN PKY	ANCHORAGE	AK	99517
GILL LARRY S & DIANE C	2600 TURNAGAIN PKY	ANCHORAGE	AK	99517
GILL ROBT R & HELEN A	1851 FALCON CIR	ANCHORAGE	AK	99504
GILL RUSSELL A& BARBARA J	2124 DOUGLAS DR	ANCHORAGE	AK	99517
GILLESPIE BRADLEY & MARY B	PO BOX 940075	HOUSTON	AK	99694
GILLESPIE BRIAN M&DIXIE L	20890 OBERG RD	CHUGIAK	AK	99567
GILLESPIE WADE R&FRIEDA L	PO BOX 521064	BIG LAKE	AK	99652
GILLIOM JON LYNN	8511 HIDDEN VIEW CT	ANCHORAGE	AK	99504
GILMAN ZACHARY F	PO BOX 671326	CHUGIAK	AK	99567
GILMORE PATRICK H & VIKKI	610 BOUNTY DR	ANCHORAGE	AK	99515
GILROY JAS T	PO BOX 520622	BIG LAKE	AK	99652
Gilroy Steve	PO BOX 141	TALKEETNA	AK	99676
GILSON RONALD V & DONNA L	PO BOX 672084	CHUGIAK	AK	99567
GIOFFRE FRANK	PO BOX 770926	EAGLE RIVER	AK	99577
GIRARD JON MEGLER	PO BOX 770787	EAGLE RIVER	AK	99577
GIRARD WM E & DIANE E	8801 TEMPEST CIR	ANCHORAGE	AK	99507
GIROUX SANDRA D	6001 MILEY ST	ANCHORAGE	AK	99504

Alaska Wood Crafts

American Cancer Society, Anchorage  
Udelhoven Oilfield System Services

GISSEL CHRISTOPHER J	PO BOX 588	WILLOW	AK	99688
GITTLEIN DAVID A& LOREE J	PO BOX 521643	BIG LAKE	AK	99652
GITTLEIN RICHARD	PO BOX 521201	BIG LAKE	AK	99652
GLASER DONALD E & IRENE S	PO BOX 127	WILLOW	AK	99688
GLASS-NIELSON ROYLYN	PO BOX 240403	ANCHORAGE	AK	99524
Glavinovich Paul	PO Box 112816	Anchorage	AK	99511
GLAZIER THOS E JR	PO BOX 231703	ANCHORAGE	AK	99523
GLEASON CANDACE M	PO BOX 1335	PALMER	AK	99645
Gleason David & DeAnn	PO Box 871007	Wasilla	AK	99687
Glen Dave	P.O. Box 3223 P	Palmer	AK	99645
GLINES NEWELL H & K J	PO BOX 520373	BIG LAKE	AK	99652
GLINES NEWELL H& KATHLEEN	PO BOX 521402	BIG LAKE	AK	99652
GLOVER THEO G	9200 INDEPENDENCE DR #203	ANCHORAGE	AK	99507
GMYR LAWRENCE A	PO BOX 520375	BIG LAKE	AK	99652
GOARD KEVIN T & JANET	1340 AIRPORT HEIGHTS DR	ANCHORAGE	AK	99508
GODBEE DERRAL	PO BOX 595	WILLOW	AK	99688
GODBY CINDY	PO BOX 245	TALKEETNA	AK	99676
GODHARDT GEO E	PO BOX 521535	BIG LAKE	AK	99652
GODING DAVE	PO BOX 521518	BIG LAKE	AK	99652
GODWIN SCOTT & KELLY	247B MELODY PL	ANCHORAGE	AK	99504
GOERING BRENT M	19252 TRAILBAY DR	EAGLE RIVER	AK	99577
GOFF PRISCILLA M	PO BOX 1351	PALMER	AK	99645
GOLD MINERS LODGE	PO BOX 307	WILLOW	AK	99688
GOLDBAR WENDY R	11 CEDAR HILLS DR	PINE HAVEN	WY	82721
GOLDBECK LONN M	PO BOX 173	WILLOW	AK	99688
GOLDEN JOHNNY & TOBI	2820 GREENSCREEK CIR	ANCHORAGE	AK	99516
GOLYAN NIKOLAY N& LUDMILA	PO BOX 231005	ANCHORAGE	AK	99523
GONZALES MARK L & EDITH	5030 GARLAND CIR	ANCHORAGE	AK	99508
GONZALES OSCAR	PO BOX 232562	ANCHORAGE	AK	99523
GONZALEZ STEPHEN L&MARY E	4931 OMEGA CIR	ANCHORAGE	AK	99516
GOOCEY ALBERT DAVID	3309 IOWA ST	ANCHORAGE	AK	99517
Good Natalie	P.O. Box 3658	Palmer	AK	99645
GOODE ANNETA F	12740 RIDGEWOOD DR	ANCHORAGE	AK	99516
GOODE JOHN F III	PO BOX 244	WILLOW	AK	99688
GOODENOUGH CHAS	PO BOX 614	WILLOW	AK	99688
GOODMAN IVAN M & LEONA M	1701 TWINING DR	ANCHORAGE	AK	99504
Goodman Roy & Ann	P.O. Box 2883	Palmer	AK	99645
GOODMAN WARD C	PO BOX 190681	ANCHORAGE	AK	99519
GOODNIGHT HARRY C	16212 BERTHOD WAY	ANCHORAGE	AK	99577
GOODRICH GERALD B&CARLA M	4961 BYRD LN	EAGLE RIVER	AK	99515
GOODRICH KATHYRINE LYNCH	2824 E 88TH AVE	ANCHORAGE	AK	99504
Goodrich Monte & Georgia	1226 S. Chugach St.	Palmer	AK	99645
Goodrich Suzanne	19118 Jamie Dr	Eagle River	AK	99577

Grasshopper Adventures

Palmer Machinery Company

GOODWIN JACKY	PO BOX 396	WILLOW	AK	99688
GOODWIN SHANNON O	1181 N PINION DR	WASILLA	AK	99654
GORDON MARK	1013 TYONEK DR	ANCHORAGE	AK	99501
GORDON PATRICK M & MARY	PO BOX 520325	BIG LAKE	AK	99652
GORIOUSHKINA SVETLANA M	PO BOX 670817	CHUGIAK	AK	99567
GORREBECK DENNIS W	1209 ENGLISH BLUFF CT	BRANDON	FL	33511
Gorski Susan	P.O. Box 770353	Eagle River	AK	99577
GOSE TERRY	4700 MELVIN AVE	ANCHORAGE	AK	99517
GOSSE THOS R	PO BOX 520794	BIG LAKE	AK	99652
Gott Katherine	4501 Diplomacy Drive	Anchorage	AK	99501
GOTTSTEIN ROBT	1433 W 9TH AVE	ANCHORAGE	AK	99501
GOUGH BRUCE A & CYNTHIA M	7705 CANDYWINE CIR	ANCHORAGE	AK	99507
GOULD PEG	PO BOX 90156	ANCHORAGE	AK	99509
GRABOWSKI CAROLYN A	PO BOX 940183	HOUSTON	AK	99694
GRAEBER JULIA L	1261 E WOODCREST DR	WASILLA	AK	99654
GRAHAM CHAD R	PO BOC 502	HOUSTON	AK	99694
GRAHAM CONDON L & YON S	2725 HAVITUR WAY	ANCHORAGE	AK	99504
GRAHAM DAVID S	PO BOX 515	WILLOW	AK	99688
GRAHAM DEBORAH	PO BOX 521202	BIG LAKE	AK	99652
GRAHAM DOUG	PO BOX 113083	ANCHORAGE	AK	99511
GRAHAM FRED T & LINDA S	PO BOX 940014	HOUSTON	AK	99694
GRAHAM GLEN & LINDA L	PO BOX 671768	CHUGIAK	AK	99567
GRAHAM JOHN D & LAUREL C	1401 S PIONEER DR	PALMER	AK	99645
Graham Stu	P.O. Box 4558	Palmer	AK	99645
GRAHAM TROY L & MAUREEN	PO BOX 521454	BIG LAKE	AK	99652
GRAHAM WM A & TAMA M	1265 BANNISTER DR	ANCHORAGE	AK	99508
GRAJOLA JOANNE VANGRASS	4430 WRIGHT ST	ANCHORAGE	AK	99507
GRAMSE SHANNON	2147 SUNRISE DR	ANCHORAGE	AK	99508
GRANATH PHIL	PO BOX 2683	PALMER	AK	99645
GRANBERRY SJE	3335 LAKE PARK CIR	ANCHORAGE	AK	99517
GRANDSTAFF JOHN M	PO BOX 202862	ANCHORAGE	AK	99520
GRANGER WM A	13810 JARVI DR	ANCHORAGE	AK	99515
GRANT GARY A	10736 HIGH BLUFF DR	EAGLE RIVER	AK	99577
GRANT GLENDA L	4009 E 9TH AVE	ANCHORAGE	AK	99508
Grant Henderson	3302 W. 81st. Ave.	Anchorage	AK	99502
Grant Henderson	3302 W. 81st. Ave.	Anchorage	AK	99502
GRANT MICHAEL L	PO BOX 520352	BIG LAKE	AK	99652
GRANT RICK D	PO BOX 112484	ANCHORAGE	AK	99511
GRANTLAND JESSE S	8640 E BLUE MOUNTAIN LN	PALMER	AK	99645
GRASS RUSSELL E	529 E 15TH AVE	ANCHORAGE	AK	99501
Grasse Margaret	P.O. Box 773336	Eagle River	AK	99577
GRATRIX KRIS & SHARON	1327 W 27TH AVE	ANCHORAGE	AK	99503
GRAU LEONARD J JR	715 PEARL DR	ANCHORAGE	AK	99518

Chugiak-Eagle River Chamber of Commerce

Southcentral Foundation

A Creative Reflection

GRAVES GREGORY D	PO BOX 521397	BIG LAKE	AK	99652
GRAVES MARK H	PO BOX 241382	ANCHORAGE	AK	99524
GRAVES MICHAEL T	PO BOX 520815	BIG LAKE	AK	99652
Graves Mike	P O BOX 520815	BIG LAKE	AK	99652
GRAVES MIKE	16547 E. SMITH ROAD	PALMER	AK	99645
GRAY A KATHLEEN	602 N FLOWER ST	ANCHORAGE	AK	99508
GRAY CAROL JEAN	PO BOX 100491	ANCHORAGE	AK	99510
GRAY KELSEY	2132 MISTY GLEN CIR	ANCHORAGE	AK	99502
GRAY KEVIN & DAWN A	6900 CALLAWAY CIR	ANCHORAGE	AK	99516
Gray Robert	P.O Box 112323	Anchorage	AK	99511
GRAYBILL DELORES M	PO BOX 670358	CHUGIAK	AK	99567
GRAYDON ANITA S	PO BOX 763	WILLOW	AK	99688
GREEN BENJAMIN A	PO BOX 521355	BIG LAKE	AK	99652
GREEN CECELIA G	PO BOX 210495	AUKE BAY	AK	99821
GREEN CHAS D & LYNNE L	PO BOX 670894	CHUGIAK	AK	99567
GREEN CLIFFORD J	PO BOX 670443	CHUGIAK	AK	99567
Green Curtis	836 E. 15th Ave.	Anchorage	AK	99501
GREEN ELLEN	8471 FOXLAIR CIR	ANCHORAGE	AK	99507
GREEN GREG & HEATHER	3957 GENEVA PL	ANCHORAGE	AK	99508
GREEN JUSTIN	2903 TANGLEWOOD DR	ANCHORAGE	AK	99517
GREEN KELLY M	PO BOX 520934	BIG LAKE	AK	99652
GREEN KRISTINE K	3406 W 80TH AVE	ANCHORAGE	AK	99502
Green Lyda	State Capitol, Room 516	Juneau	AK	99801-11
Green Lyda	600 E. Railroad Ave. Ste 1	Wasilla	AK	99564
GREEN RAY A & ANNA L	5751 E CAMBORNE DR	PALMER	AK	99645
GREENAWALT RICHARD G	650 W POTTER	ANCHORAGE	AK	99518
GREENHALGH DAVID F	PO BOX 770852	EAGLE RIVER	AK	99577
GREENLEE BRIAN L & NELIDA	3945 W 37TH CT	ANCHORAGE	AK	99517
GREENLEE KENNETH E	3400 BRUCE LN	ANCHORAGE	AK	99517
GREENSTREET DARREL R & M L	PO BOX 1191	PALMER	AK	99645
GREENUP GWEN M	1301 S DOWN WIND CIR	PALMER	AK	99645
GREENWOOD JASON H	PO BOX 520982	BIG LAKE	AK	99652
Gregoire Voncille	P O BOX 349	TALKEETNA	AK	99676
GREGORIN RICHARD C & F L	3821 ROBIN ST	ANCHORAGE	AK	99504
GREGORY DOUGLAS L JR	21240 MEADOW LAKE DR	CHUGIAK	AK	99567
GREGORY JAS L & SAMUELLA A	7125 OLD SEWARD HWY	ANCHORAGE	AK	99518
GREGORY R J MD	PO BOX 521512	BIG LAKE	AK	99652
Gregory Strong	3559 S. Horseshore Lake Rd.	WILLOW	AK	99688
GREIG MARK R & CONNIE A	PO BOX 520327	BIG LAKE	AK	99652
Greiner Carolyn	P.O. Box 770251	Eagle River	AK	99577
GRENIER BRIAN J&LISA M TR	PO BOX 561	WILLOW	AK	99688
GREY ANNELIESE	PO BOX 521486	BIG LAKE	AK	99652
GRIFFETH BEVERLY	PO BOX 15304	FRITZ CREEK	AK	99603

Griffin Charles	Charles Griffin CPA	P.O. Box 670	Palmer	AK	99645
Griffin Chas R & Charlotte		PO BOX 670	PALMER	AK	99645
Griffin Jane R		4625 TAFT ST	ANCHORAGE	AK	99517
Griffin Jos D & Pamela J		PO BOX 696	WILLOW	AK	99688
Griffin Kelley	MAT-SU DOG SLED COUNCIL	HC 35 BOX 53552	WASILLA	AK	99687
Griffin Michael & Gillian		2800 BROOKRIDGE CIR	ANCHORAGE	AK	99504
Griffin Oscar	Local 995/996	P.O. Box 190485	Anchorage	AK	99519
Griffin Wayne E Jr		PO BOX 101805	ANCHORAGE	AK	99510
Griffith Robt W		PO BOX 520928	BIG LAKE	AK	99652
Griffith Stephen R		2729 KARLUK	ANCHORAGE	AK	99508
Grigg Cole D & Michelyn		PO BOX 520211	BIG LAKE	AK	99652
Griggs Sharon S.		PO BOX 1087	PALMER	AK	99645
Grimes Paul S		6825 WATERFALL DR	EAGLE RIVER	AK	99577
Grinde David L		PO BOX 771702	EAGLE RIVER	AK	99577
Grippin Edw E & Clydene A		2924 KIMBERLIE CT	ANCHORAGE	AK	99508
Groendyke Fred		PO BOX 771483	EAGLE RIVER	AK	99577
Groschel Gerhard Mgr		PO BOX 104500	ANCHORAGE	AK	99510
Grogg Jennifer I		1030 W FORAKER DR	WASILLA	AK	99654
Groh John B & Mary Ellen		PO BOX 322	WILLOW	AK	99688
Gron Dahl Rodger D & Julie K		PO BOX 805	WILLOW	AK	99688
Gron Din Tamara A		PO BOX 4670	PALMER	AK	99645
Gronenthal John R & M K		PO BOX 521173	BIG LAKE	AK	99652
Gross Jeffrey G		7235 E DENELLE ST	PALMER	AK	99645
Grosz Patricia J		PO BOX 629	TALKEETNA	AK	99676
Grove Melvin B Jr & Cheryl		PO BOX 520575	BIG LAKE	AK	99652
Grover Paul R		HC 89 BOX 539	WILLOW	AK	99688
Grubaugh Yukon E		4820 KLONDIKE AVE	ANCHORAGE	AK	99508
Grubb Codie J		PO BOX 520431	BIG LAKE	AK	99652
Grube Dennis & Pauline		PO BOX 916	PALMER	AK	99645
Gruber John B		17915 TEDROW DR	EAGLE RIVER	AK	99577
Gruchow John		HC 01 BOX 6260D	PALMER	AK	99645
Gruenberg Max	Alaska State Legislature	716 W. 4th Avenue, Ste 320	Anchorage	AK	99501
Gruenberg Max	Alaska State Legislature	State Capitol, Room 112	Juneau	AK	99801-11
Grunwald Benjamin & Edith		2531 S EVENING FOG CIR	PALMER	AK	99645
Gschwind Jos R		5800 HOLDEN DR	ANCHORAGE	AK	99516
Guarderas Luis Gilberto		PO BOX 230163	ANCHORAGE	AK	99523
Guay Robt A		13500 NEPTUNE CT	ANCHORAGE	AK	99515
Guetschow Bernd C		425 G ST	ANCHORAGE	AK	99501
Gunn Bobbie Gene		HC 89 BOX 129	WILLOW	AK	99688
Gunlogson Richard W & J K		PO BOX 193	WILLOW	AK	99688
Gunnarson Peter		1407 W 45TH AVE	ANCHORAGE	AK	99503
Gunnell Scott A & Mary Kay		1251 VAUNDA AVE	WASILLA	AK	99654
Gunther Chas & Barbara		4721 E 115TH AVE	ANCHORAGE	AK	99516



GURGANUS GENE B	PO BOX 244302	ANCHORAGE	AK	99524
GURNETT MIKE L & KAREN S	PO BOX 940275	HOUSTON	AK	99694
GUSTAFSON GEORGIA L	2005 W 31ST AVE	ANCHORAGE	AK	99517
GUTCHER NORMAN K& BETTE J	5263 E BIDEFORD BLVD	PALMER	AK	99645
Guttenberg David	State Capitol, Room 13	Juneau	AK	99801-11
GUTTIERREZ JAS E	PO BOX 520908	BIG LAKE	AK	99652
GUZIEJEWSKI THOS W	4727 MILLS DR	ANCHORAGE	AK	99508
H SERVICES/PT MAC TOWER	PO BOX 201007	ANCHORAGE	AK	99520
HAAG CHIP & SHERRI	PO BOX 671763	CHUGIAK	AK	99567
HAAG LEO R & SANDRA J	4503 GARFIELD ST	ANCHORAGE	AK	99503
HAASE GARY W & CINDY	3750 FURROW CREEK RD	ANCHORAGE	AK	99516
HAASL PHILIP H & PEGGY	PO BOX 672274	CHUGIAK	AK	99567
HACKENBERGER DONNA J	2120 PAXSON DR	ANCHORAGE	AK	99504
HACKENBERGER PETER J&LORI	11000 ZELMA CIR	ANCHORAGE	AK	99516
HADAD THOS M JR	PO BOX 521922	BIG LAKE	AK	99652
HADDEN GUY H & DONNA L	7820 CRANBERRY ST	ANCHORAGE	AK	99502
HAESE WM J & ANN V	19705 DELPHIN CIR	EAGLE RIVER	AK	99577
HAGBO EDGAR D & MARSHA	PO BOX 521305	BIG LAKE	AK	99652
HAGEL F GERALD & LOUISE A	4113 WESTWOOD DR	ANCHORAGE	AK	99517
HAGER REX ALAN	4602 MOUNTAIN VIEW DR	ANCHORAGE	AK	99508
Haggard Dale & Carolyn	Box 521004	Big Lake	AK	99652
HAGGSTROM KATHY VAUDRIN	PO BOX 190407	ANCHORAGE	AK	99519
HAGLER DEREK S& ALEXANDRA	22314 WOODCLIFF CT	CHUGIAK	AK	99567
HAGMEIER JOHN C & JUDITH	2204 CLEVELAND AVE	ANCHORAGE	AK	99517
HAHN KARL A JR & CONNIE G	2136 E 36TH AVE	ANCHORAGE	AK	99508
HAHN NED E & CATHLEEN S	1423 W 13TH AVE	ANCHORAGE	AK	99501
HAHN TIFFANY M	2907 CROWS NEST CIR	ANCHORAGE	AK	99515
HAHN WOLFGANG & JANET E	PO BOX 772431	EAGLE RIVER	AK	99577
HAINSWORTH REX	12531 BRANDON ST	ANCHORAGE	AK	99515
HAKANSON DENNIS WAYNE	3624 E 17TH	ANCHORAGE	AK	99504
HAKENSON LINDA M	PO BOX 3326	PALMER	AK	99645
HALE JESSE M	1275 N HOKA HAY CIR	WASILLA	AK	99654
HALE JOHN H	PO BOX 101195	ANCHORAGE	AK	99510
HALES WM J & CATHY L	PO BOX 521274	BIG LAKE	AK	99652
HALEY CYNTHIA S	6505 AIR GUARD RD	ANCHORAGE	AK	99502
HALL AARON L & TONJA E	PO BOX 434	WILLOW	AK	99688
HALL AARON M & NAOMI D	PO BOX 520921	BIG LAKE	AK	99652
HALL CECILIA EST	PO BOX 520445	BIG LAKE	AK	99652
Hall Celia	P.O. Box 102075	Anchorage	AK	99510
HALL EVAN E & JANETTE	PO BOX 212	WILLOW	AK	99688
Hall Gary	465 Crestwood Avenue	Wasilla	AK	99654
HALL GEO D & PAMELA A	18749 1ST ST	EAGLE RIVER	AK	99577
Hall Jess	P.O. Box 1987	Palmer	AK	99645

Alaska State Legislature

U.S. Fish & Wildlife Service

Wasilla Planning Commission

Hall Quality Homes

HALL JESS L	PO BOX 1987	PALMER	AK	99645
HALL KENNETH S	PO BOX 211255	ANCHORAGE	AK	99521
HALL MICHAEL D & KATHLEEN	6557 CIMARRON CIR	ANCHORAGE	AK	99504
HALL RICHARD & VERA	PO BOX 521874	BIG LAKE	AK	99652
HALL RICHARD N	8305 HEAVENLY CIR	ANCHORAGE	AK	99502
Hall Robert	P. O. Box 871906	Wasilla	AK	99687
HALL ROBT L & THEDA L	1330 ST GOTTHARD	ANCHORAGE	AK	99508
HALL ROLAND H & NEION	7610 JASON PL	ANCHORAGE	AK	99502
HALL RONALD R & BARBARA A	PO BOX 670955	CHUGIAK	AK	99567
HALL TRAVIS	PO BOX 1027	BARROW	AK	99723
Hall Walt	750 Cope	Palmer	AK	99645
HALL WM O & LINDA D	8320 E 20TH AVE	ANCHORAGE	AK	99504
HALLER RICHARD A	PO BOX 110731	ANCHORAGE	AK	99511
HALLIFAX TRAVIS J & LESLIE	PO BOX 737	WILLOW	AK	99688
HALSETH JASON L & SUSAN L	PO BOX 801	WILLOW	AK	99688
HALSTEAD DEAN R	PO BOX 552	WILLOW	AK	99688
Halter Vern	P. O. Box 389	Willow	AK	99688
Halter Vern	P. O. Box 389	Willow	AK	99688
Halverson Ellen	PO Box 874151	Wasilla	AK	99687
HALVERSON RITA	PO BOX 520532	BIG LAKE	AK	99652
HAMACHER FLORENCE	3535 PARSONS AVE	ANCHORAGE	AK	99504
HAMANN DENNIS W & LORI A	1200 S OAT ST	WASILLA	AK	99654
HAMBACH ANDREA K	PO BOX 882	WILLOW	AK	99688
HAMBRICK MIKE J	PO BOX 520180	BIG LAKE	AK	99652
HAMIL JOHN T II & ANGELIA	22719 ROBINSON RD	CHUGIAK	AK	99567
HAMILTON CHARLES S	4440 EDINBURGH DR	ANCHORAGE	AK	99515
HAMILTON DONNA D OLSON	PO BOX 1953	PALMER	AK	99645
Hamilton Dorothy	257 Crestwood Ave	Wasilla	AK	99654
Hamilton John	511 E. Steel Loop Road	Palmer	AK	99645-66
HAMILTON RICHARD M & P I	PO BOX 252	WILLOW	AK	99688
HAMILTON STEPHEN A	PO BOX 143373	ANCHORAGE	AK	99514
Hamlin Jason	3000 A Street, Ste 100	Anchorage	AK	99503
HAMM ERNEST A JR & EVELYN	2612 ARLINGTON DR	ANCHORAGE	AK	99517
HAMM WM PAUL	4831 RETRIEVER CIR	ANCHORAGE	AK	99502
Hamming Darrin	655 N. Natasha	Palmer	AK	99645
HAMMON JACK F	2310 PAXTON DR	ANCHORAGE	AK	99501
HAMMOND CHERYL L	464 N CHUGACH ST	PALMER	AK	99645
HAMMOND DAVID R & KYLEE J	3040 B WENDY'S WAY	ANCHORAGE	AK	99517
HAMMONDS KENNETH R JR&J A	PO BOX 0181	WILLOW	AK	99688
HAMMONS JAS W & LAZELL L	PO BOX 1615	PALMER	AK	99645
HAMON THOS V	1122 CHESTER PARK DR	DULUTH	AK	55812
HAMRICK A WESLEY&YVONNE M	PO BOX 521328	BIG LAKE	AK	99652
HAMRICK TIMOTHY & PEGGY	131 W PENNINGTON PL	WASILLA	AK	99654

Hamrick Wes & Yvonne	P.O. Box 521328	Big Lake	AK	99652
HANAS RICHARD L & SUSAN L	3531 HILAND DR	ANCHORAGE	AK	99504
HANCOCK BENJAMIN	4600 DELONG DR	ANCHORAGE	AK	99502
HANCOCK HOWARD W III	PO BOX 242143	ANCHORAGE	AK	99524
HAND THOS	645 G ST STE 100	ANCHORAGE	AK	99501
HANEL MICHAEL P	8171 ROVENNA ST	ANCHORAGE	AK	99518
HANELINE ROBT L	14451 CHAWA CIR	ANCHORAGE	AK	99516
HANEY ROBT	PO BOX 521015	BIG LAKE	AK	99652
HANGLIN MARK S & JACQUELYN	PO BOX 521212	BIG LAKE	AK	99652
HANK HARRY	PO BOX 521378	BIG LAKE	AK	99652
HANK JOHN F	HC 89 BOX 706	WILLOW	AK	99688
Hanley Mark	3201 C St., Ste. 603	Anchorage	AK	99503
HANNA RICHARD T & CHERYL M	7331 AUGUSTINE DR	ANCHORAGE	AK	99504
HANNAH TONEY A & LINDA M	1711 WOO BLVD	ANCHORAGE	AK	99515
HANNAM THOS	PO BOX 2786	PALMER	AK	99645
HANNAMAN REBECCA ZABORAC	4207 IRENE DR	ANCHORAGE	AK	99504
HANNON CHARLIE A	PO BOX 521122	BIG LAKE	AK	99652
HANNON CHARLOTTE	1031 E DUNEDIN ST	WASILLA	AK	99654
HANNON HARLAND & SANDRA	PO BOX 222484	ANCHORAGE	AK	99522
HANNON KEVIN & SHERRY	PO BOX 706	WILLOW	AK	99688
HANNON PETER A	8701 SHEBANOF AVE	ANCHORAGE	AK	99507
HANRATH ROSEMARY K	HC 89 BOX 484	WILLOW	AK	99688
HANSEN ALLAN B	901 CORAL LN	ANCHORAGE	AK	99515
HANSEN BJARNE J	6452 BLACKBERRY AVE	ANCHORAGE	AK	99502
HANSEN DEAN A	4440 VIEW CIR	ANCHORAGE	AK	99507
HANSEN ETHAN C & CHRISTINE	PO BOX 521498	BIG LAKE	AK	99652
HANSEN PHYLLIS E	PO BOX 3865	PALMER	AK	99645
HANSEN WALLACE A	PO BOX 520422	BIG LAKE	AK	99652
HANSMEIER KURT G	PO BOX 112005	ANCHORAGE	AK	99511
HANSMEYER PAUL C	1900 WALDRON DR	ANCHORAGE	AK	99507
Hanson Brad	325 E. Dolphin	Palmer	AK	99645
HANSON JEFFREY R	385 STANDISH	ANCHORAGE	AK	99504
HANSON JEREMIAH T	11123 216TH ST E	GRAHAM	WA	98338
HANSON JERRY A & JEANNINE	PO BOX 410	WILLOW	AK	99688
HANSON MARY A	17724 CHILKAT CT	EAGLE RIVER	AK	99577
HANSON ROBT F	4991 E 5TH AVE	ANCHORAGE	AK	99508
HANSON SHAWN S	9241 KIRKWALL CIR	ANCHORAGE	AK	99502
HANSON THOS ALAN	5109 LIONHEART DR	ANCHORAGE	AK	99508
HANSON WAYNE BRIGHAM	PO BOX 17	WILLOW	AK	99688
HANSON WM K & BONNIE K	PO BOX 37	WILLOW	AK	99688
HAR SUN KYUNG	2531 BROOKHILL	ANCHORAGE	AK	99516
HARALSON KELLY E & LYNN M	2140 S CHURCH ST	PALMER	AK	99645
HARBOUR BRANDON G & ANGELA	610 LORI DR	ANCHORAGE	AK	99504

Anadarko Petroleum Corp.

City of Palmer

HARDELAND TOMAS S	14671 E OUTER SPRINGER LP	PALMER	AK	99645
HARDING BRUCE C & SANDRA T	16702 YELLOWSTONE CIR	EAGLE RIVER	AK	99577
HARDING RITA LORENE	PO BOX 364	WILLOW	AK	99688
HARDING ROGER F	1217 ELM ST	SANDPOINT	ID	83864
HARDMAN JOHN & RANEY	PO BOX 773521	EAGLE RIVER	AK	99577
HARDMAN TRACY L	PO BOX 520838	BIG LAKE	AK	99652
HARDY FRANK C	PO BOX 201903	ANCHORAGE	AK	99520
Hare Robert	PO Box 521623	Big Lake	AK	99652
HARE ROBT & LUELLA	PO BOX 521623	BIG LAKE	AK	99652
HARGRAVES DARIN R & JULIE	9823 DINAAGA DR	EAGLE RIVER	AK	99577
HARLAN SUSAN A	PO BOX 90713	ANCHORAGE	AK	99509
HARMAN EDW	7061 LOWELL CIR	ANCHORAGE	AK	99502
HARMAN LINDA TERRI	PO BOX 521920	BIG LAKE	AK	99652
HARMON JAS E & EMILY E	PO BOX 520527	BIG LAKE	AK	99652
HARMON JERALD & BEVERLY	4322 CONE PL	JUNEAU	AK	99801
HARNE STEVEN ERNEST	HC 52 BOX 8570	INDIAN	AK	99540
HARNED LYNN M	PO BOX 521178	BIG LAKE	AK	99652
HARNISH NEVA P	2231 DAYBREAK CT	ANCHORAGE	AK	99501
HARPEL KAY E	12341 FREEDOM RD	ANCHORAGE	AK	99516
HARPER CATHERINE M	4178 APOLLO	ANCHORAGE	AK	99504
HARPER JORGE GLEN	12011 SHORE CIR	ANCHORAGE	AK	99515
HARPER LAWRENCE ERIC	5540 CAPE SEVILLE	ANCHORAGE	AK	99516
HARPER PATRICIA G	PO BOX 110496	ANCHORAGE	AK	99511
Harpster James A & Annette	PO BOX 2846	PALMER	AK	99645
HARREN RICHARD L	1240 E LAKESHORE AVE	WASILLA	AK	99654
HARRIMAN C LYN & BETTY L	PO BOX 220483	ANCHORAGE	AK	99522
HARRINGTON BENJAMIN & HOPE	14644 W LAKE RIDGE DR	EAGLE RIVER	AK	99577
HARRINGTON VICKY L	PO BOX 671883	CHUGIAK	AK	99567
HARRIS BERNARD TODD	8305 BEARBERRY ST	ANCHORAGE	AK	99502
HARRIS CHAS D & DIANNE K	701 W 8TH ST	ANCHORAGE	AK	99501
HARRIS DONALD W	PO BOX 670622	CHUGIAK	AK	99567
HARRIS DOROTHY I	3136 COTTONWOOD ST	ANCHORAGE	AK	99508
HARRIS GERARD J SR & K K O	PO BOX 521736	BIG LAKE	AK	99652
Harris John	716 W. 4th Avenue, Ste 300	Anchororage	AK	99501
Harris John	State Capitol, Room 208	Juneau	AK	99801--11
Harris Karen	950 South Trunk Road	Palmer	AK	99645
HARRIS KRISTIAN J	PO BOX 203253	ANCHORAGE	AK	99520
HARRIS LAWRENCE W	12673 DEWEY ST	LOS ANGELES	CA	90066
HARRIS LAWRENCE W	12673 DEWEY ST	LOS ANGELES	CA	90066
HARRIS ROBT D	PO BOX 521182	BIG LAKE	AK	99652
HARRIS ROBT L	517 MUMFORD ST	ANCHORAGE	AK	99508
HARRIS TAMMY Y T	5580 SAPPHIRE LOOP	ANCHORAGE	AK	99504
HARRIS TANYA M F	8911 DEWBERRY ST	ANCHORAGE	AK	99502

Alaska State Legislature  
 Alaska State Legislature  
 Alaska Garden Gate B&B

Harrison Denece	Big Lake Chamber of Commerce	P.O. Box 520067	AK	99652
HARRISON STEPHEN C & T J		HC 89 BOX 332	AK	99688
HARROLD-JONES TARRIM		7700 E SAPPHIRE RD	AK	99645
HARRY JOHN W & MARY L		6735 FREEBIRD CIR	AK	99507
HART SHARON		PO BOX 520558	AK	99652
HARTER CHAS W		PO BOX 904	AK	99688
HARTIG ALICE R IRP		2555 MARITIME LOOP	AK	99515
Hartig Lawrence	Hartig Rhodes Hoge & Lekisch	717 K St Ste 201	AK	99501
HARTIG PAUL E & PAMELA A		HC 01 BOX 6177	AK	99645
HARTLEY CHRISTIAN M		PO BOX 810	AK	99688
HARTLEY-MCKEOWN VIRGIE		PO BOX 793	AK	99688
HARTLIEB GORDON W&SUZAN L		3419 GRISSOM CIRCLE	AK	99517
Hartly Mandy Jo		PO BOX 490	AK	99676
HARTMAN DAVID H		4901 HARTMAN CIR	AK	99507
HARTMAN JESS B		16169 E SMITH RD	AK	99645
HARTUNG TERESA		1750A HERITAGE CIR	AK	99645
HARTWIG RICHARD & IRENE		4620 E 115TH AVE	AK	99516
HARTZELL RANDOLPH G & K S		PO BOX 697	AK	99688
HARVEY ERIC E		PO BOX 771026	AK	99577
HARVEY WILMA E		1710 BEAVER PL	AK	99504
HASPI BEKIM		9810 WHITEFISH CIR	AK	99515
HASSINGER PETER J		PO BOX 725	AK	99688
HASTIE BRYAN E		4204 GREENLAND DR	AK	99517
HASTINGS JACKSON		106 SWEETGALE CT	AK	99518
HATCHER CHARLENE CSTDN		922 KATHY PL	AK	99504
HATLEY JAS M & CRYSTAL K		HC 89 BOX 467	AK	99688
HATTENBURG RUSSEL & CARMEN		1970 S WHITING CIR	AK	99645
Haugen Al	Machinists & Aerospace Workers Local 601-Air Tr	8141 Schoon St., Suite 201	AK	99501
HAVELOCK ERIC & JUDITH		2819 WILEY POST AVE	AK	99517
HAWK KENNETH E		PO BOX 103274	AK	99510
Hawker Mike		716 W. 4th Avenue, Ste 620	AK	99501
Hawker Mike	Alaska State Legislature	State Capitol, Room 502	AK	99801-11
HAWKINS CHRISTOPHER L	Alaska State Legislature	2221 MULDOON RD	AK	99504
HAWKINS ERNEST F		PO BOX 521176	AK	99652
HAWKINS JASON M		PO BOX 1723	AK	99645
HAWKINS JOS T & JULIE G		642 S ALASKA ST	AK	99645
HAWK-JETTON FLORENCE F		PO BOX 520568	AK	99652
HAWLEY CLAIR E		PO BOX 240187	AK	99524
HAY CALVIN C & MARGARET D		PO BOX 111468	AK	99511
HAYDEN JACK M & SHERRI A		PO BOX 771801	AK	99577
HAYDEN M SHANNON		2420 HIGHGATE CIR	AK	99507
HAYE RENE M		2508 BERRYMAN LN	AK	99502
HAYES DONALD K		17228 N JUANITA LOOP	AK	99577

HAYES DONALD R	PO BOX 521586	BIG LAKE	AK	99652
HAYES GLENN F	3651 E 20TH AVE	ANCHORAGE	AK	99508
HAYES MYUNG CHA	511 E 45TH AVE	ANCHORAGE	AK	99503
HAYES ROBT L JR & JANET	1507 VASSAR DR	ANCHORAGE	AK	99508
HAYNES CHRISTIAN	HC 89 BOX 395	WILLOW	AK	99688
HAYNES JANICE L	PO BOX 671005	CHUGIAK	AK	99567
HAYNES KENNETH ROBT & G A	1843 SCENIC WAY	ANCHORAGE	AK	99501
HAYNES MERLON N & LISA M	PO BOX 940282	HOUSTON	AK	99694
Haynes Ronald	340 N Bonanza St	Palmer	AK	99645
HAZELTON HANS & LISA	1640 WICKERSHAM DR	ANCHORAGE	AK	99507
HAZLITT CHRISTOPHER C	PO BOX 3557	PALMER	AK	99645
HEAD HENRY P	1400 W BENSON BLVD #400	ANCHORAGE	AK	99503
HEAD ROGER D & BONNIE	PO BOX 765	WILLOW	AK	99688
HEADLE BRUCE C & MARY M	23111 MELANA CIR	CHUGIAK	AK	99567
HEAFER MICHAEL A	12341 FREEDOM RD	ANCHORAGE	AK	99516
HEAIRET WM A & HELEN L	PO BOX 521403	BIG LAKE	AK	99652
Heal Larry	P.O. Box 2730	Palmer	AK	99645
HEAL LARRY E & EDITH H	PO BOX 2730	PALMER	AK	99645
HEAPS KENNETH LEE	2711 A GLACIER ST	ANCHORAGE	AK	99508
HEATER LARRY G & LUCILLE	PO BOX 371	WILLOW	AK	99688
HEATH ROSELYNN MAE	1101 W 71ST CT	ANCHORAGE	AK	99518
Heatwole Mike	P.O. Box 196660	Anchorage	AK	99519
Heaven F. LeRoi	300 N. Boundary, Suite B	Palmer	AK	99645
HEAVILIN ROBT E	1420 GWENN DR	ANCHORAGE	AK	99515
HEBERT KELLY H& NATALIE S	9725 POSEIDON	ANCHORAGE	AK	99515
HEBERT MARIE	PO BOX 601	WILLOW	AK	99688
HEBERT RONALD J	410 B N BLISS	ANCHORAGE	AK	99508
HEBNES THOS J & CLAUDIA C	2321 SUES WAY	ANCHORAGE	AK	99516
HEDGES JOHN R JR	16445 ELEANORA ST	EAGLE RIVER	AK	99577
HEFFERN GENE R & SANDRA J	3850 KNIK AVE	ANCHORAGE	AK	99517
HEFFERNAN LOUIS J III&L K	PO BOX 833	WILLOW	AK	99688
Hegna Joe	19634 Delphin Circle	Eagle River	AK	99577
Hegna Joe	19634 Delphin Circle	Eagle River	AK	99577
HEHL ADAM & LINDA	7836 KIANA CIR	ANCHORAGE	AK	99507
HEHNLIN JEAN M&DONNELLY R	201 FAWN CT	ANCHORAGE	AK	99515
HEIDBRINK GARY T	PO BOX 1043	WILLOW	AK	99688
Heidi Jenkins	PO Box 171	WILLOW	AK	99688
HEIDY BOYD M & LINDA C	PO BOX 831	PALMER	AK	99645
HEIER DAVID F	2770 S SKY RANCH LOOP	PALMER	AK	99645
Heinrich Steve	HC89 Box 8575	Talkeetna	AK	99676
HEINRICH TOMMY G JR & J Y	2113 W 47TH AVE	ANCHORAGE	AK	99517
HEINRICH TOMMY GENE & M J	6724 CHEVIGNY	ANCHORAGE	AK	99502
HEINZ SUSAN A	3000 PORCUPINE TRL	ANCHORAGE	AK	99516

Allstate Insurance Co.

Alyeska Pipeline Service Co.  
Wasilla, Knik, Willow Historic Society

Alyeska Pipeline Service Company  
MWH Engineering

Y Community Council

HEISE JON & MELISSA	PO BOX 677	WILLOW	AK	99688
HELIE JOHN R & RENEE M	8511 ATKINS PL	ANCHORAGE	AK	99507
HELLMAN OWEN R	13301 SHELBURNE RD	ANCHORAGE	AK	99516
HELM RICHARD & CHANTELL	PO BOX 671575	CHUGIAK	AK	99567
HELMERICKS JEFF T & SUSAN	621 AIRPORT RD	PALMER	AK	99645
HELMS TEAGUE A	PO BOX 773265	EAGLE RIVER	AK	99577
HEMBREE LARRY J& CHERYL M	3900 E 67TH AVE	ANCHORAGE	AK	99507
HEMMELGARN PAUL	HC 89 BOX 407	WILLOW	AK	99688
HEMMER CARL J & CYNTHIA L	PO BOX 471	WILLOW	AK	99688
HENDERSHOT JAS & ALIDA	PO BOX 190929	ANCHORAGE	AK	99519
HENDERSON CARL M	1321 WINTERGREEN ST	ANCHORAGE	AK	99508
HENDERSON ERIC T & JULIA	PO BOX 2501	PALMER	AK	99645
HENDERSON HAROLD J JR & G	1075 S CHECK ST	WASILLA	AK	99654
HENDERSON MICHAEL ROY	8301 FRANK ST	ANCHORAGE	AK	99518
HENDERSON RICHARD E & P S	8641 VIGOR CIR	ANCHORAGE	AK	99504
HENDERSON STEVEN M& SUSAN	3520 WENTWROTH ST	ANCHORAGE	AK	99508
HENDERSON SUSAN J	12 GREENBRIER DR	MISSOULA	MT	59802
HENDERSON WM H & MARGOT E	PO BOX 1623	PALMER	AK	99645
HENDLEY JAS H& JENNIFER L	19816 DRIFTWOOD BAY DR	EAGLE RIVER	AK	99577
HENDRICKS R LEE& JOSLYN A	PO BOX 520187	BIG LAKE	AK	99652
HENDRICKS RUNAR L	1551 LATOUCHE ST	ANCHORAGE	AK	99501
HENDRICKSON GRANT A	7320 N ESTHER DR	PALMER	AK	99645
HENDRICKSON JOHN W	3830 CLAY PRODUCTS	ANCHORAGE	AK	99517
HENDRIX CARLA J	PO BOX 111357	ANCHORAGE	AK	99511
HENG ERIC HUNG HAW	1335 I ST	ANCHORAGE	AK	99501
HENKEL THOS A	7870 E FIREWEED	PALMER	AK	99645
HENLEY DAVID W	2311 W 83RD AVE	ANCHORAGE	AK	99502
HENLEY DELLARIE G	HC 89 BOX 525	WILLOW	AK	99688
Henn CATHERINE R	1219 H ST	ANCHORAGE	AK	99501
HENNIGH GARY L	1846 SUNRISE DR	ANCHORAGE	AK	99508
HENNINGS MARK J & KERRY S	9420 SUGAR CIR	ANCHORAGE	AK	99516
HENNINGSEN BARBARA J	6207 E 34TH AVE	ANCHORAGE	AK	99504
HENRICHS LARRY R	PO BOX 370	WILLOW	AK	99688
HENRY PATRICK O & LORI M	620 CLIPPERSHIP CT	ANCHORAGE	AK	99515
Henry W	P. O. Box 585	Talkeetna	AK	99676
HENRY WALTER J & JANET E	2042 CRATAEGUS	ANCHORAGE	AK	99508
HENSEL JOHN D & GLORIA J	1700 S PIONEER DR	PALMER	AK	99645
HENSLEY OVERTON B	3620 PENLAND PKY	ANCHORAGE	AK	99508
HENSON ERNEST	3545 S. GROWNWALDT STREET	BIG LAKE	AK	99652
HENSON ERNEST	3545 S. GROWNWALDT STREET	BIG LAKE	AK	99652
HENSON ERNEST L & LILA L	PO BOX 520629	BIG LAKE	AK	99652
HEPLER LUCY P	2419 BROOKE DR	ANCHORAGE	AK	99517
HERITAGE JAS F & SUSAN M	8118 BLACKBERRY ST	ANCHORAGE	AK	99502

HERMAN FREDERICK D & VICKI	PO BOX 355	WILLOW	AK	99688
HERMANN JEREMY & NICHOLE	6531 TESHILAR DR	ANCHORAGE	AK	99507
HERMES KURT B	2641 PUFFIN POINT CIR	ANCHORAGE	AK	99507
HERRINGTON MARK & KHRISTY	6957 LASER DR	ANCHORAGE	AK	99504
HERSCHBACH CLARENCE A	PO BOX 521084	BIG LAKE	AK	99652
HERSCHBACH DOUGLAS L	PO BOX 520986	BIG LAKE	AK	99652
HERSCHBACH THOS C & LISA	PO BOX 521143	BIG LAKE	AK	99652
HERSCHBERGER GLEN & FANNIE	HC 89 BOX 466	WILLOW	AK	99688
HERSHMAN TERRY A	PO BOX 348	PALMER	AK	99645
HERTER PATRICK C	4321 RABBIT CREEK RD	ANCHORAGE	AK	99516
HERTZ RICHARD P & E J	PO BOX 90998	ANCHORAGE	AK	99509
HESS DAGMAR H	PO BOX 510	WILLOW	AK	99688
HESS TOM S	PO BOX 521855	BIG LAKE	AK	99652
HESS WM A JR	7010 FREDERICKS DR	ANCHORAGE	AK	99504
HESSER KARL	7431 TYONE CT	ANCHORAGE	AK	99504
HESSER REBECCA	5300 HILAND RD	EAGLE RIVER	AK	99577
HESSON DON	PO BOX 243434	ANCHORAGE	AK	99524
HESSON JOHN A & SANDRA K	3512 KNIK AVE	ANCHORAGE	AK	99517
HESTER FRANCIS L & HELEN	PO BOX 851	WILLOW	AK	99688
HESTER JAS R	17941 TONSINA CT	EAGLE RIVER	AK	99577
HESTERBERG DEBRA	2560 S EVENING FOG CIR	PALMER	AK	99645
HETRICK JAMES R	PO BOX 520372	BIG LAKE	AK	99652
HEUBNER WM F	4721 NEWCASTLE WAY	ANCHORAGE	AK	99503
HEUSSER RICHARD V & AMY M	1424 GARDEN ST	ANCHORAGE	AK	99508
HEUTON RONALD JOE JR	PO BOX 895	WILLOW	AK	99688
HEWITT CHAS A & MARY BETH	4229 BIRCH RUN DR	ANCHORAGE	AK	99507
HICKEL PETER J & LAUREL J	3737 KNIK AVE	ANCHORAGE	AK	99517
HICKEL RAYMOND E	6242 TYRE CIR	ANCHORAGE	AK	99502
Hickel Roger	11001 Calaska Circle	Anchorage	AK	99515
HICKLIN JAN L.	PO BOX 3674	PALMER	AK	99645
HICKMAN MARGARET A	1936 WILDWOOD LN	ANCHORAGE	AK	99517
HICKS JOHN S & KRISTI LYN	PO BOX 596	WILLOW	AK	99688
HIDALGO CECILIA M	PO BOX 110374	ANCHORAGE	AK	99511
HIEBERT ROBIN A	PO BOX 73	WILLOW	AK	99688
HIGGINBOTHAM JEFFREY K	PO BOX 520552	BIG LAKE	AK	99652
Higgins Caroline	2511 Tidewater Rd.	Anchorage	AK	99501
HIGHT MICHAEL D & DEVEN K	25151 MEADOWRIDGE CT	CHUGIAK	AK	99567
HIGHTOWER MATTHEW	7441 TRENTON LN	ANCHORAGE	AK	99502
HILBISH GEORGEJANA M	5766 TINIAN ST	ANCHORAGE	AK	99507
HILBRECHT ARNOLD L & NANCY	PO BOX 256	WILLOW	AK	99688
HILDE LEE	PO BOX 770234	EAGLE RIVER	AK	99577
HILDERBRAND B JOS II	PO BOX 477	BARROW	AK	99723
Hilderbrant Grant	333 Raspberry Road	Anchorage	AK	99518

Roger Hicckel Contracting

Totem Ocean Trailer Express

Alaska Dept. of Fish & Game



HILL A SABRINA	2503 CHURCHILL DR	ANCHORAGE	AK	99517
HILL BRETT R & EMILY A	PO BOX 366	PALMER	AK	99645
HILL DEOLE & KALLEAGH D	PO BOX 111104	ANCHORAGE	AK	99511
HILL DIANA C TR TRE	6030 GLENKERRY DR	ANCHORAGE	AK	99504
HILL HARRY J	PO BOX 190591	ANCHORAGE	AK	99519
HILL JAMES R	9336 BLACKBERRY	ANCHORAGE	AK	99515
HILL KATHRYN	811 N KLEVIN ST	ANCHORAGE	AK	99508
HILL KENNETH L & G LAURINE	1226 W 10TH AVE	ANCHORAGE	AK	99501
HILL LORIE JO	PO BOX 521076	BIG LAKE	AK	99652
HILL MARK A & ROBIN R	PO BOX 241951	ANCHORAGE	AK	99524
HILL STEWART EDW	PO BOX 940012	HOUSTON	AK	99694
HILL YOLANDA	12301 JOHN'S RD	ANCHORAGE	AK	99515
HILLBORN JOHN R	17915 LACY DR	EAGLE RIVER	AK	99577
HILLEMEIER MARK K	PO BOX 520966	BIG LAKE	AK	99652
HILLIER FRANK O	PO BOX 538	WILLOW	AK	99688
HILLMAN FREDERICK	1685 STANTON AVE	ANCHORAGE	AK	99508
HILLS DANA L	PO BOX 1062	WILLOW	AK	99688
HILTON TERRY L & E C	6220 E DEARBORN DR	PALMER	AK	99645
HIMAN PATRICK L	PO BOX 672635	CHUGIAK	AK	99567
HIMES ELWOOD M & SHERRY L	1405 NELCHINA ST	ANCHORAGE	AK	99501
HIMMELRIGHT JOHN I & K E	7510 SOLARSET CIR	ANCHORAGE	AK	99507
HINCHEY VIDA FAE EST	841 I ST	ANCHORAGE	AK	99501
HINDERMANN THOMAS D	PO BOX 521446	BIG LAKE	AK	99652
HINES BENNIE G JR & MARSHA	PO BOX 521153	BIG LAKE	AK	99652
HINES LINDA L	PO BOX 940465	HOUSTON	AK	99694
HINGST ALLEN LEE	18139 TONSINA CT	EAGLE RIVER	AK	99577
HINZ LYNETTE ANNE MORENO	6407 E 31ST	ANCHORAGE	AK	99504
HIPSAK MARVIN & SHIRLEY	16604 WOODS POINTE CIR	ANCHORAGE	AK	99516
HIRSCH DALE H & CINDY A	PO BOX 521472	BIG LAKE	AK	99652
Hirschmann Randi	HC-31 Box 5274	Wasilla	AK	99687
HITCHCOCK STEVE	PO BOX 111835	ANCHORAGE	AK	99511
Hite Kevin	P.O. Box 240405	Anchorage	AK	99524-24
HITE KEVIN E & CYNTHIA D	8050 SUMMERSET	ANCHORAGE	AK	99518
HITE TIMOTHY J & DEBORA K	PO BOX 671291	CHUGIAK	AK	99567
HIX TERRY C & KATSUKO	11008 MOLLER DR NW	GIG HARBOR	WA	98332
HIXSEN MICHAEL L	PO BOX 92925	ANCHORAGE	AK	99509
HJALSETH JAS C & R M	1255 S GEIGER ST	TACOMA	WA	98465
HJELLEN GILBERT E & S	13351 REEF PL	ANCHORAGE	AK	99515
HOBBS ERNEST LLOYD	4919 WESLEYAN DR	ANCHORAGE	AK	99508
HOBSON LARRY	PO BOX 520421	BIG LAKE	AK	99652
HOCHSTEIN SPENCE L & BONNY	1342 MATTERHORN WAY	ANCHORAGE	AK	99508
HODGE CAMPBELL	PO BOX 520592	BIG LAKE	AK	99652
HODGSON PATRICK	PO BOX 788	WILLOW	AK	99688

Alaska State Snowmobile Association

HODSON JANE	1530 THUJA ST	ANCHORAGE	AK	99507
HODSON LOYD M	3811 CROSSON CIR	ANCHORAGE	AK	99517
HODSON MELVIN L & JEAN	PO BOX 568	WILLOW	AK	99688
HOEFER GLEN W & JOYCE A	4331 TIKISHLA ST	ANCHORAGE	AK	99504
HOEFER BRIAN GEO	5320 COUNTRY CLUB LN	ANCHORAGE	AK	99516
HOENACK KARL & JENNIE	5320 EMMANUEL AVE	ANCHORAGE	AK	99508
HOEY DAVID C & DEANNA R	10918 NORTH FLEET DR	ANCHORAGE	AK	99515
HOEY DAVID C & DEANNA R	10918 NORTH FLEET DR	ANCHORAGE	AK	99515
HOFBAUER RUSTY F	PO BOX 5414	FORT RICHARDS	AK	99505
HOFBAUER THOS	PO BOX 5604	FORT RICHARDS	AK	99505
HOFF STEPHEN & CORA	PO BOX 521905	BIG LAKE	AK	99652
HOFFMAN JULIA A	432 IDAHO ST	ANCHORAGE	AK	99504
HOFFMAN THOS M & ALEXIS M	2810 GREENSCREEK CIR	ANCHORAGE	AK	99516
HOGAN KLINTON & TINA	5431 WINDFLOWER CIR	ANCHORAGE	AK	99507
HOGATE EUGENE J & TANDY S	PO BOX 521631	BIG LAKE	AK	99652
HOGLE RAYMOND E	PO BOX 635	WILLOW	AK	99688
HOGUE BEVERLY J	PO BOX 273	TALKEETNA	AK	99676
HOHN MARY ELIZABETH	PO BOX 927	WILLOW	AK	99688
HOHNSTEIN ROBT G & J E	PO BOX 244941	ANCHORAGE	AK	99524
HOLBERT NITA B	6812 CUTTY SARK ST	ANCHORAGE	AK	99502
HOLBROOK BRYAN LEE	8011 BRIARWOOD ST	ANCHORAGE	AK	99518
HOLCOMB MELISSA	PO BOX 520003	BIG LAKE	AK	99652
HOLCOMB SEAN N	PO BOX 4168	PALMER	AK	99645
HOLCOMB WM H	PO BOX 902	WILLOW	AK	99688
HOLDER DAVID W & ANNE M	9750 E LEE ANN DR	PALMER	AK	99645
HOLDIMAN ANNE M	1534 NICHOLS ST	ANCHORAGE	AK	99508
HOLE JAS D	PO BOX 1501	PALMER	AK	99645
HOLEN KEITH A	PO BOX 521581	BIG LAKE	AK	99652
HOLL LUDMILA	2518 RIDGE POINTE DR	ANCHORAGE	AK	99515
HOLLADAY JOEL D	PO BOX 520199	BIG LAKE	AK	99652
HOLLAND MARY	2701 INGRA ST	ANCHORAGE	AK	99508
HOLLAND SABRIETA R	2299 S FALK LN	PALMER	AK	99645
Holler Doug	521 W. Lake View Avenue	Wasilla	AK	99654
HOLLIE JOS K & GWENDOLYN	PO BOX 521028	BIG LAKE	AK	99652
HOLLINGSHEAD MICHCA RUTH	PO BOX 940374	HOUSTON	AK	99694
Hollis Nancy	PO Box 3095	Anderson	AK	99744
HOLLOWELL VICKI L	PO BOX 143324	ANCHORAGE	AK	99514
HOLLSTEIN GREGORY E	705 MULDOON RD	ANCHORAGE	AK	99504
HOLMBERG KIM-MARIE S	PO BOX 3233	PALMER	AK	99645
HOLMBERG MATTHEW P& KELLY	PO BOX 94001	HOUSTON	AK	99694
HOLMES CRAIG W	4121 URSA	ANCHORAGE	AK	99517
Holmes Doyle	350 East Dahlia Avenue	Palmer	AK	99645
HOLMES DOYLE E	PO BOX 0009	WILLOW	AK	99688

City of Wasilla

Denali Borough School Board

Holmes Eileen	Denali Borough	P. O. Box 3012	Anderson	AK	99744
Holmes Lindsey	Alaska State Legislature	4149 Hood Court	Anchorage	AK	99517
HOLT BUDD D SR		PO BOX 327	WILLOW	AK	99688
Holt Frank	Frank Holt's Small Engine	HC04 Box 9639	Palmer	AK	99645
HOLT WESLEY DEAN & ROBIN		PO BOX 520647	BIG LAKE	AK	99652
HOLT WM J		1200 W DIMOND BLVD	ANCHORAGE	AK	99515
HOLTAN JOHN C & P L O		1112 P ST	ANCHORAGE	AK	99501
Holycross Jim	City of Wasilla	290 E. Herning Avenue	Wasilla	AK	99654
HOLZMAN LINDA LOU		9701 BUDDY WERNER DR	ANCHORAGE	AK	99516
HONDA KAREN A		2118 LORD BARANOF DR	ANCHORAGE	AK	99517
HONEYBEE LK AERO PK OWNER		PO BOX 957	WILLOW	AK	99688
HOOD ALAN BRIAN		6008 DEBARR RD	ANCHORAGE	AK	99504
HOOD RICHARD L & ERMA J		7530 WADE CIR	ANCHORAGE	AK	99518
HOOD STEVE		13029 NE 70TH DR	KIRKLAND	WA	98033
HOOK DENISE MICHELLE		PO BOX 520308	BIG LAKE	AK	99652
HOOKER WAYNE W		11900 RAINBOW AVE	ANCHORAGE	AK	99516
Hoolley Stan	Iditarod Trail Sled Dog Race	P. O. Box 874502	Wasilla	AK	99687
HOOVER DOUGLAS JOHN		PO BOX 520480	BIG LAKE	AK	99652
HOPE JOAN F		PO BOX 2311	PALMER	AK	99645
HOPKINS RICHARD L		PO BOX 137	WILLOW	AK	99688
HOPKINSON HAYWARD K&COSBY		PO BOX 520345	BIG LAKE	AK	99652
Hopler Mark		19750 Driftwood Bay Drive	Eagle River	AK	99577
HOPPE GREGORY M & KAREN M		PO BOX 520636	BIG LAKE	AK	99652
HOPPER JAMES		1509 W 32ND AVE	ANCHORAGE	AK	99503
HOPPER MAURICE E		17711 STEAMBOAT DR	ANCHORAGE	AK	99516
HORAK JEFFREY & JONI		15850 RASMUSSEN ST	ANCHORAGE	AK	99577
HORN SANDRA J		HC 89 BOX 88	EAGLE RIVER	AK	99577
Horn Steve	Alaska Dept. of Transportation & Public Facilities	PO Box 196900	WILLOW	AK	99688
HORNE MICHAEL J		6132 FARPOINT DR	Anchorage	AK	99519-69
HORNER HAROLD L & J D		PO BOX 200985	ANCHORAGE	AK	99507
HORNSBY ROBT		PO BOX 5494	ANCHORAGE	AK	99520
HORNUNG RICHARD M		6156 E VISIONS CREST BLVD	FORT RICHARDS	AK	99505
HORNUNG-GREEN N G & N D		2551 S HORNUNG RD	PALMER	AK	99645
Hort Dan	Long Rifle Lodge	HC03 Box 8445 H	PALMER	AK	99645
HORTON ANDREW CHARLES		PO BOX 4925	Palmer	AK	99645
HORTON TAMMY J		341 E BENSON	PALMER	AK	99645
HORVATH DAN'L L&AVERILL A		PO BOX 4725	ANCHORAGE	AK	99503
HOSACK CHAS R & DALEANN		9340 EMERALD DR	PALMER	AK	99645
HOSHAW DONALD J & SUSAN E		7756 BRENTWOOD DR	ANCHORAGE	AK	99502
HOSKINS RANDALL & MICHELE		2330 LOREN CIR	ANCHORAGE	AK	99502
HOSMAN D B SOLBERG V J		2300 BARROW ST	ANCHORAGE	AK	99516
HOUGHTALING BERT W		PO BOX 521626	ANCHORAGE	AK	99503
HOUK MITCHELL T		10201 E PALMER-WASILLA	BIG LAKE	AK	99652
			PALMER	AK	99645

HOUSE SUSAN J	16244 COPPER MOUNTAIN CIR	EAGLE RIVER	AK	99577
Houser COLLEEN D	12510 BEACHCOMBER	ANCHORAGE	AK	99515
HOUSER TONY L & MARILYN E	PO BOX 1996	PALMER	AK	99645
HOUSTON DALE A & DEANNA K	5511 CRAIGER CT	ANCHORAGE	AK	99507
HOUSTON DAVID S	517 W NORTHERN LIGHTS	ANCHORAGE	AK	99503
HOUT FRANK W	2156 FOREST PARK DR	ANCHORAGE	AK	99517
HOUT JOERENE	6801 TERESA CIR	ANCHORAGE	AK	99516
HOVANEC GARY L	PO BOX 240886	ANCHORAGE	AK	99524
HOVDEN GEO E & ANGELA M	3537 N POINT DR	ANCHORAGE	AK	99502
HOVLAND AURORA S	1890 PARKWAY DR	ANCHORAGE	AK	99504
HOWARD BRIAN K & DANIELLE	280 PEPPERTREE LOOP	ANCHORAGE	AK	99504
Howard Cliff	P.O. Box 587	Talkeetna	AK	99676
HOWARD GEO G JR & MARY G	PO BOX 3093	PALMER	AK	99645
Howard George Jr.	12608 Florence Dr.	WILLOW	AK	99688
HOWARD JEFFREY R	PO BOX 520661	BIG LAKE	AK	99652
HOWARD JOHN CHRISTOPHER	14614 FIRE LAKE DR	EAGLE RIVER	AK	99577
HOWARD RANDALL H&JENNIFER	920 JOHAM CIR	ANCHORAGE	AK	99515
HOWARD TERRY L	11328 QUEST CIR	ANCHORAGE	AK	99515
HOWARD WM W	7500 E SAPHIRE RD	PALMER	AK	99645
HOWE STEPHEN L & SHERRALEE	19314 BIRCHWOOD LOOP RD	CHUGIAK	AK	99567
HOWELL JEREMY S	PO BOX 241942	ANCHORAGE	AK	99524
HOWELLS RONALD A & SUSAN A	8400 DUBEN AVE	ANCHORAGE	AK	99504
HOWSE NORMAN R	10650 SE BULL RUN RD	SANDY	OR	97055
HOYT GORDON S & JUDITH C	PO BOX 770947	EAGLE RIVER	AK	99577
HOYT HAROLD W & CAROL A	PO BOX 520487	BIG LAKE	AK	99652
HOYT ROSEMARY	PO BOX 521724	BIG LAKE	AK	99652
Hoyt Virginia	501 N. Guikana	Palmer	AK	99645
HRUBES SHANE DUANE	4511 CASSIN	ANCHORAGE	AK	99507
HUBBARD SAMUEL A & TAMMY R	PO BOX 521428	BIG LAKE	AK	99652
HUBBLE CHRIS	801 E 100TH AVE	ANCHORAGE	AK	99515
HUBBLE CHRIS & CHRISTIN M	412 ORCHID CIR	ANCHORAGE	AK	99515
HUBER FRANCIS J & ANNA TRES	501 W 20TH AVE	ANCHORAGE	AK	99503
HUBER SCOTT	3840 MIDVALE CIR	ANCHORAGE	AK	99507
HUDDLESTON DONELLA A	1651 S MIDDLE MESA DR	PALMER	AK	99645
Hudson Cliff	P.O. Box 648	Talkeetna	AK	99676
Hudson Ken	350 E. Dahlia Avenue	Palmer	AK	99645-64
HUDSPETH AARON S	PO BOX 940413	HOUSTON	AK	99694
HUDSPETH DAVID K & P M	2700 NATHANIEL CT	ANCHORAGE	AK	99517
HUEBNER JOHN W & KAY E	8831 JULIANA ST	ANCHORAGE	AK	99502
HUESTIS ERIC & TASHEENA	PO BOX 683	WILLOW	AK	99688
HUETTL JAMES A & JANET R	3731 ROALD AMUDSEN AVE	ANCHORAGE	AK	99517
HUETTL SONJA	1750 KARLUK ST	ANCHORAGE	AK	99501
HUEY KENT D & MARY A	6001 AZALEA DR	ANCHORAGE	AK	99516

Mat-Su Borough School Board

Hudson Air Service, Inc.  
Matanuska-Susitna Borough

HUEZO JESUS A			PO BOX 101476	99510	ANCHORAGE	AK
Huffines Eleanor		The Wilderness Society	430 W. 7th Avenue, Ste 210	99501	Anchorage	AK
HUFFMAN GEO L & DEBORAH I			PO BOX 520881	99652	BIG LAKE	AK
HUFFMAN JAS L & DEBRA J			9930 ANNETTE CIR	99577	EAGLE RIVER	AK
HUFFMAN PAUL A JR			PO BOX 521819	99652	BIG LAKE	AK
HUFMAN JOS D & MURIEL V			4000 EDINBURGH DR	99515	ANCHORAGE	AK
HUGGETT TRAVIS J& DESIREE			6630 E 8TH AVE	99504	ANCHORAGE	AK
Huggins Charlie		ALASKA STATE LEGISLATURE	604 E. Railroad Avenue	99654	WASILLA	AK
Huggins Charlie		Alaska State Legislature	604 E. Railroad Avenue	99801-11	Juneau	AK
Huggins Charlie		ALASKA STATE LEGISLATURE	PO BOX 230912	99654	WASILLA	AK
HUGHES GENE MICHAEL			9501 PONDEROSA DR	99523	ANCHORAGE	AK
HUGHES GEO W & NANCY A			1806 CLEVELAND AVE	99516	ANCHORAGE	AK
HUGHES JAS S			PO BOX 672222	99517	ANCHORAGE	AK
HUGHES JOHN C & LORINE J			PO BOX 520204	99567	CHUGIAK	AK
HUGHES KENNETH L&JENNETTA			1592 COFFEY LN	99652	BIG LAKE	AK
HUGHES MARY K			3627 DUNKIRK DR	99501	ANCHORAGE	AK
HUGHES ROBT M & MARGARET			3111 C ST # 505	99502	ANCHORAGE	AK
HULEN A DOUGLAS & HELEN M			PO BOX 2033	99503	ANCHORAGE	AK
HULL KEITH R & NANCY C			2607 SOLOMON	99645	PALMER	AK
HULL THOS H			P.O. Box 520901	99507	ANCHORAGE	AK
Huls Troy			PO BOX 111796	99652	Big Lake	AK
HULSE CASANDRA RENEE			2011 Constitution Drive	99511	ANCHORAGE	AK
Hultten Kurt			7031 JOSEPH ST	99577	Eagle River	AK
HUMBLE KATHI M			4601 SOUTHPARK BLUFF DR	99518	ANCHORAGE	AK
HUME JOHN D & JILL D			PO BOX 521172	99516	ANCHORAGE	AK
HUMES MICHAEL R			8380 S TALLENTA SPUR RD	99652	BIG LAKE	AK
HUMMEL ALFRED			385 S FLOWER ST	99676	TALKEETNA	AK
HUMMEL ELIZABETH F			19019 CRYSTAL ISLAND DR	99502	ANCHORAGE	AK
HUMPHREY CHRIS L & LYNN A			2925 WILL ROGERS	99577	EAGLE RIVER	AK
HUMPHREY GARY & LORI			22419 GLACIER VIEW DR	99517	ANCHORAGE	AK
HUMPHREY MICHAEL C & A M			PO BOX 520090	99577	EAGLE RIVER	AK
HUMPHREYS CHAD E & LISA E			705 S WILLIWAU DR	99652	BIG LAKE	AK
HUNSUCK ALAN SCOTT			7333 TARSUS DR	99645	PALMER	AK
HUNSUCK WM E & PAMELA			PO BOX 77	99502	ANCHORAGE	AK
HUNT DAVID & SANDY			6924 E 6TH AVE	99688	WILLOW	AK
HUNT HOWARD J			8508 LASSEN CIR	99504	ANCHORAGE	AK
HUNT JAS W JR & JACKIE K			PO BOX 190635	99577	EAGLE RIVER	AK
HUNT KENNETH			601 W 5TH AVE	99519	ANCHORAGE	AK
HUNT MAUREEN E			P.O. Box 3691	99501	ANCHORAGE	AK
Hunt Patti			4235 MCLEAN PL	99645	Palmer	AK
HUNT RODNEY D			7600 BERRY CIR	99504	ANCHORAGE	AK
HUNT SAM D & DIANE L			PO BOX 770464	99502	ANCHORAGE	AK
HUNTER BONNIE JEANNE				99577	EAGLE RIVER	AK

HUNTER DARRELL & LIZ	PO BOX 940076	HOUSTON	AK	99694
HUNTER KEVIN	2901 MADISON WAY	ANCHORAGE	AK	99508
HUNTER TERRY L & ANN M	17617 LAOANA CIR	EAGLE RIVER	AK	99577
HUNTLEY ROBT H& BARBARA L	3801 CENTERPOINT DR	ANCHORAGE	AK	99503
HUNTSMAN HENRY COLE	438 MUMFORD ST	ANCHORAGE	AK	99508
Huppert Paul	P. O. Box 969	Palmer	AK	99645
HURLBUT FRANK	2003 ROOSEVELT DR	ANCHORAGE	AK	99517
HURSH JOYCE A	PO BOX 141293	ANCHORAGE	AK	99514
HURU SUSAN M	PO BOX 240301	ANCHORAGE	AK	99524
HUSEMAN NORMA J	2008 JAMESTOWN CIR	ANCHORAGE	AK	99507
HUSSAIN SAFDAR	4300 ARCTIC BLVD	ANCHORAGE	AK	99503
HUSTEAD CLIFFORD& KATHRYN	8140 CRANBERRY ST	ANCHORAGE	AK	99502
HUSTON BERNARDETTE A	237 W 23RD AVE	ANCHORAGE	AK	99503
HUTCHINSON GUIDA	1615 HIDDEN LN	ANCHORAGE	AK	99501
HUTCHINSON ROBT H & DIANNE	PO BOX 229	WILLOW	AK	99688
Hutmacher Bill	555 Cordova Street	Anchorage	AK	99501
HYATT JOHN S & BARBARA L	PO BOX 3957	PALMER	AK	99645
HYDE RONALD B JR	1948 BRANDILYN CIR	ANCHORAGE	AK	99516
HYDER DENNIS C	PO BOX 848	WILLOW	AK	99688
IBARRA FELIPE V JR & EMMA	PO BOX 141352	ANCHORAGE	AK	99514
IBRAHIM MOHAMED	2103 MISTYBROOK CIR	ANCHORAGE	AK	99502
IHLE TOSHIKO K	5207 COPE ST	ANCHORAGE	AK	99518
ILNITSKIY SERGEY I	5139 E 42ND AVE	ANCHORAGE	AK	99508
IMHOF PETER F JR	PO BOX 671993	CHUGIAK	AK	99567
IMIG ERIC A	2611 FAIRBANKS ST	ANCHORAGE	AK	99503
IMLACH THOS D	PO BOX 100431	ANCHORAGE	AK	99510
Imm Teresa	3900 C St Ste 801	Anchorage	AK	99503-59
INESS KEVIN J & LISA L	PO BOX 940143	HOUSTON	AK	99694
INGA DOUGLAS V & VIOLA L	PO BOX 520720	BIG LAKE	AK	99652
INGALDSON WM H & SANDRA J	4120 TAZLINA AVE	ANCHORAGE	AK	99517
INGALLS DONALD R & LOIS	605 BARROW ST	ANCHORAGE	AK	99501
Ingalls Hal	8240 Petersburg St.	Anchorage	AK	99507
INGALLS HAL	6780 LAUDEN CIR	ANCHORAGE	AK	99502
INGRAHAM KATHLEEN G	PO BOX 497	WILLOW	AK	99688
INGRAM RONALD	PO BOX 532	WILLOW	AK	99688
ION DAVID L	PO BOX 940271	HOUSTON	AK	99694
IPARRAGUIRRE DAVID L	1545 S HOYT	ANCHORAGE	AK	99508
Irene Norcross	PO Box 242	WILLOW	AK	99688
IRIS BENJAMIN W & KAREN E	14409 DON CIR	EAGLE RIVER	AK	99577
ISABELLE DELIDO A& MARY S	PO BOX 220021	ANCHORAGE	AK	99522
ISHIKI RICKEY H	1908 W HILLCREST DR	ANCHORAGE	AK	99517
ISRAEL EDW L	2015 HILLCREST DR	ANCHORAGE	AK	99518
ISRAEL JESSE F	1180 E GOLDFENDALE	WASILLA	AK	99654

Palmer Produce

Alaska Dept. of Environmental Conservation

Arctic Slope Regional Corporation

Denali Drilling

ITHAL SHERRY A	5346 EMMANUEL AVE	ANCHORAGE	AK	99508
IVERSON PHILIP L & AUDREY	2235 CRATAEGUS AVE	ANCHORAGE	AK	99508
IVES DIANA L	900 W 5TH AVE # 525	ANCHORAGE	AK	99501
IVEY G BILBO & ELAINE E	3211 W 80TH AVE	ANCHORAGE	AK	99502
JACK BONNIE L	1063 W 20TH AVE	ANCHORAGE	AK	99503
Jack Hammon	2310 Paxson	Anchorage	AK	99501
JACKMAN CORY	PO BOX 1096	TALKEETNA	AK	99676
JACKS MARCELLA PEGGY	740 OLD KLATT RD	ANCHORAGE	AK	99515
JACKSON ANTHONY D & KERRI	8640 MUIR CT	ANCHORAGE	AK	99504
JACKSON BENNETT J & LISA M	6116 AUSTRIA DR	ANCHORAGE	AK	99516
JACKSON FRANK JR & MAXINE	3011 W 31ST AVE	ANCHORAGE	AK	99517
JACKSON JAS E & CATHY L	PO BOX 92301	ANCHORAGE	AK	99509
JACKSON JUSTIN K	7946 PORT OXFORD DR	ANCHORAGE	AK	99507
Jackson Karleen	P.O. Box 110601	Juneau	AK	99811-06
Jackson Kevin	HC02 Box 7764	Palmer	AK	99645
JACKSON MARK G & SARAH J	PO BOX 3155	PALMER	AK	99645
JACKSON WARRENE & B C	6967 FOUNTAIN DR	ANCHORAGE	AK	99502
JACOBS BRADY PATRICK P	8200 E 6TH AVE	ANCHORAGE	AK	99504
JACOBS DALE J	PO BOX 673	WILLOW	AK	99688
JACOBS MICHEL J	PO BOX 467	WILLOW	AK	99688
JACOBS SYLVIA S	PO BOX 313	WILLOW	AK	99688
JACOBS WYATT E	PO BOX 943	WILLOW	AK	99688
JACOBSEN JOHN J	1703 NUNAKA DR	ANCHORAGE	AK	99504
JACOBSON DUSTIN L	4437 SAN ROBERTO	ANCHORAGE	AK	99508
JACOBSON SCOTT	230 W ALBRECHT AVE	PALMER	AK	99645
JACOBSON STAN	PO BOX 521667	BIG LAKE	AK	99652
JACOBSON WM C	2221 DUVOY CT	ANCHORAGE	AK	99502
JACOBUS KENNETH P PC	310 K ST	ANCHORAGE	AK	99501
JAEGER CRAIG & MICHELE	22846 MYRTLE DR	EAGLE RIVER	AK	99577
JAEGER RAYMOND A & S D	PO BOX 90541	ANCHORAGE	AK	99509
JALBERT WAYNE	17616 LAOANA CIR	EAGLE RIVER	AK	99577
James & Florence Pitcher	18226 S. Guenrsey Rd.	Wasilla	AK	99687
JAMES ANN M	1451 S MIDDLE MESA DR	PALMER	AK	99645
JAMES DENA	PO BOX 243442	ANCHORAGE	AK	99524
JAMES EDWARD M	3911 PAWN PL	ANCHORAGE	AK	99508
JAMES GREGORY BYRON	6930 E LOWER MESA DR	PALMER	AK	99645
James Huston	P.O. Box 9	Willow	AK	99688
JAMES LYNDA RAE	1634 W 13TH AVE	ANCHORAGE	AK	99501
JAMES MARLISE A	25034 THUNDERBIRD DR	CHUGIAK	AK	99567
JAMES MARY L	7728 CANDYWINE CIR	ANCHORAGE	AK	99507
James Miller	26546 West Beryozova Dr.	Willow	AK	99683
James Norcross	P O BOX 242	WILLOW	AK	99688
James Pitcher	18226 S. Guenrsey Rd.	Wasilla	AK	99687

Alaska Dept. of Health & Social Services  
Jackson Answering & Business

Willow Area Community Council

JAMES ROBT I & ROBERTA B	18921 SUTWIK CIR	EAGLE RIVER	AK	99577
JAMES ROGER & SUSAN	1253 PIKE AVE	PORT CHARLOT	FL	33948
JAMESON CARMENE	PO BOX 201913	ANCHORAGE	AK	99520
Jamie West	PO Box 534	WILLOW	AK	99688
JAMMES DOUGLAS M & E J	18928 WAR ADMIRAL DR	EAGLE RIVER	AK	99577
JAMROSE JAS	HC 89 BOX 430	WILLOW	AK	99688
Janice Strong	PO Box 875169	Wasilla	AK	99687
JANICEK KAREN	8820 TEMPEST CIR	ANCHORAGE	AK	99507
Janis Sharon	1011 E. Tudor	Anchorage	AK	99503
JANIS-TUCKER MARY M	1725 BRINK DR	ANCHORAGE	AK	99504
JANKE PAUL A & JILL R	8551 JUPITER DR	ANCHORAGE	AK	99507
JANKOWSKI THERESE H	HC 89 BOX 432	WILLOW	AK	99688
JANNECK TIMOTHY W	8950 SHEBANOF AVE	ANCHORAGE	AK	99516
JANSEN JAS H & VICKI A	4111 ENDEAVOR CIR	ANCHORAGE	AK	99515
Jansen Sara	231 W. EVERGREEN AVE.	PALMER	AK	99645
JANUARY KEVIN R&CATHERINE	4110 DEBARR RD	ANCHORAGE	AK	99508
JANULEK MILAN	PO BOX 335	WILLOW	AK	99688
JANUSIEWICZ SEAN A	PO BOX 91060	ANCHORAGE	AK	99509
JARDINE KEITH	PO BOX 771695	EAGLE RIVER	AK	99577
JARRETT DAVID L	PO BOX 203106	ANCHORAGE	AK	99520
JARVIS DIANE	7618 TARSUS	ANCHORAGE	AK	99502
JARVIS TROY A	5400 WOODSHIRE CIR	ANCHORAGE	AK	99516
JARVIS TROY A & CECILIA M	10137 N RAINBOW SHORES RD	WILLOW	AK	99688
Jason Kuehn	1 Wood Sorrel	Littleton	CO	80127
JASPER FRANK J & PATRICIA	310 HIGH VIEW DR	ANCHORAGE	AK	99515
Jassal Parminder	P. O. Box 771012	Eagle River	AK	99577
Jeanine St.John	PO Box 978	WILLOW	AK	99688
Jeanine St.John	PO Box 978	WILLOW	AK	99688
Jeff Fuller	PO Box 197	WILLOW	AK	99688
JEFFERY GERALD C& ROBIN S	PO BOX 520888	BIG LAKE	AK	99652
JEFFORD JERRY G	1421 L ST	ANCHORAGE	AK	99501
JEFFREY MATTHEW C	6605 MCGILL WAY	ANCHORAGE	AK	99502
JEFFREY ROBIN S	PO BOX 520878	BIG LAKE	AK	99652
JEHLEN PATRICK M&MARY ANN	PO BOX 598	WILLOW	AK	99688
JELLICH KATHLEEN	4308 MARS DR	ANCHORAGE	AK	99507
Jen Mark	222 W. 7th Avenue, Room 537, Box 19	Anchorage	AK	99513-75
JENKINS HEIDI RUH	PO BOX 171	WILLOW	AK	99688
JENKINS JOHN A & NANCY J	PO BOX 520827	BIG LAKE	AK	99652
JENKINS ROBT D	13317 BRANT WAY	ANCHORAGE	AK	99515
JENKINS RONALD HOWARD	PO BOX 417	WILLOW	AK	99688
JENKINS SHANNON J	PO BOX 520065	BIG LAKE	AK	99652
Jenne Rose	PO BOX 19	TALKEETNA	AK	99676
Jenne Rose	P. O. Box 525	Talkeetna	AK	99676

U.S. Fish & Wildlife Service

City of Palmer

U.S. Environmental Protection Agency

MSB PLANNING COMMISSION  
Three Rivers Accommodations



JENNINGS GEO H R&ANDREA B	PO BOX 376	WILLOW	AK	99688
JENNINGS MARIE ILENE	3656 BISQUIER DR	ANCHORAGE	AK	99508
JENNINGS STEPHANIE K	PO BOX 520585	BIG LAKE	AK	99652
JENNINGS WM F & DAISY L	16250 OLD GLENN HWY	CHUGIAK	AK	99567
JENNISON BRENT W	PO BOX 92772	ANCHORAGE	AK	99509
JENSEN CHRIS R & JEAN M	1551 S TANYA CT	PALMER	AK	99645
JENSEN GRACE A	1301 E WOODCREST DR	WASILLA	AK	99654
JENSEN GRETHE & ANDREAS	PO BOX 92167	ANCHORAGE	AK	99509
JENSEN JEFF & SANDY	PO BOX 110629	ANCHORAGE	AK	99511
JENSEN JEFFREY J & SANDRA	PO BOX 100629	ANCHORAGE	AK	99511
JENSEN MICHAEL & KATHARINA	6450 DOWNEY FINCH DR	ANCHORAGE	AK	99516
JENSEN MYRON A & SHIZUE F	3330 W 31ST AVE	ANCHORAGE	AK	99517
JENSEN PAUL J & LAURIE K	PO BOX 102	WILLOW	AK	99688
JENSEN SHERRIE L	6721 REEDYKE CIR	ANCHORAGE	AK	99507
JENSEN WAYNE L	10211 LEE ST	EAGLE RIVER	AK	99577
JERDE CHAS & SHARI M	2304 W 46TH AVE	ANCHORAGE	AK	99517
JEREMIAS PAUL & JOANN	HC 89 BOX 269	WILLOW	AK	99688
JERMAIN WM K	3000 A ST	ANCHORAGE	AK	99503
JERNIGAN TRACIE LYNN	PO BOX 937	WILLOW	AK	99688
JERUE CARL & CHERIA	690 S LOWER RD	PALMER	AK	99645
Jessee Jeff	550 W. 7th Avenue, Ste 1820	Anchorage	AK	99501
JESTICE PATRICIA A	HC 89 BOX 384	WILLOW	AK	99688
JETCO	2631 W 100TH	ANCHORAGE	AK	99515
JEZ GERALD & KRISTAN	PO BOX 774376	EAGLE RIVER	AK	99577
JIGLIOTTI JOHN & OLGA	2020 MULDOON RD	ANCHORAGE	AK	99504
JILLSON ROBT C JR	PO BOX 520964	BIG LAKE	AK	99652
JILWAN ASSIA Y & DAAD A	4640 KENT ST	ANCHORAGE	AK	99503
Jim Faiks	PO Box 521152	Big Lake	AK	99652
Jim Huston	PO BOX 38	WILLOW	AK	99688
Jim Huston	PO Box 38	WILLOW	AK	99688
JIMENEZ STEVEN NOEL	PO BOX 92035	ANCHORAGE	AK	99509
JINGCO ANGELA C	8175 KRONOS DR	ANCHORAGE	AK	99502
Joan Bryner	PO Box 247	WILLOW	AK	99688
JOBE JOHN P	PO BOX 221051	ANCHORAGE	AK	99522
Joe Gauna	7421 Silver Birch	Anchorage	AK	99502
Joe Gauna	7421 Silver Birch	Anchorage	AK	99502
JOHANKNECHT PETER J & L E	7700 EVANDER DR	ANCHORAGE	AK	99518
JOHANSEN MATTHEW & INNA B	PO BOX 242081	ANCHORAGE	AK	99524
John Himmelright	7510 Solarset Circle	Anchorage	AK	99507
JOHN JAS E	10215 GOODNEWS CIR	ANCHORAGE	AK	99515
John Scudder	8421 Stacey Circle	ANCHORAGE	AK	99507
John Scudder	8421 Stacey Circle	ANCHORAGE	AK	99507
John Strassenburgh	PO Box 766	Anchorage Talkeetna	AK	99676

Alaska Mental Health Trust Authority

John Strsenburgh	PO Box 766	Talkeetna	AK	99676
Johns Ken	406 W. Fireweed Lane, Ste 201	Anchorage	AK	99503
JOHNS MARBETH	1912 LOGAN ST	ANCHORAGE	AK	99508
JOHNSEN KRAG A& JOLIE N S	6840 SERENITY CIR	ANCHORAGE	AK	99502
JOHNSON ALICE S	1536 I ST	ANCHORAGE	AK	99501
JOHNSON B GIL & SONDRAG	1691 CRESCENT DR	ANCHORAGE	AK	99508
JOHNSON BRIAN & MICHELLE	7400 E DENELLE ST	PALMER	AK	99645
JOHNSON CASEY F	PO BOX 142372	ANCHORAGE	AK	99514
Johnson Craig	12200 Timberlane Drive	Anchorage	AK	99515
JOHNSON CRAIG A	PO BOX 521300	BIG LAKE	AK	99652
JOHNSON DALLAS & SUSAN	6700 GUNNISON DR	ANCHORAGE	AK	99516
JOHNSON DAN'L E	8801 BOOM CIR	ANCHORAGE	AK	99502
JOHNSON DAVID & DENISE	5900 BEVERLY DR	ANCHORAGE	AK	99516
JOHNSON DAVID & JANE CAREY	5970 YUKON RD	ANCHORAGE	AK	99516
JOHNSON DAVID J	PO BOX 520417	BIG LAKE	AK	99652
JOHNSON DAVID L	PO BOX 809	WILLOW	AK	99688
Johnson Dennis	11401 Old Glenn Hwy	Eagle River	AK	99577
JOHNSON ERIC H & SUSAN B	9634 VICTOR RD	ANCHORAGE	AK	99515
JOHNSON EUNICE M	PO BOX 241121	ANCHORAGE	AK	99524
JOHNSON EVERETT & YONG	3760 IMAGE DR	ANCHORAGE	AK	99504
JOHNSON FAMILY TR UTA	3546 N POINT DR	ANCHORAGE	AK	99502
JOHNSON FRED E	PO BOX 521170	BIG LAKE	AK	99652
JOHNSON GERARD F & CHERYL	691 CEDAR PARK CIR	ANCHORAGE	AK	99515
JOHNSON GROVER L&ELAINE M	PO BOX 1334	CHUGIAK	AK	99567
JOHNSON HILDEGARD	1045 P ST	ANCHORAGE	AK	99501
JOHNSON JAS A	PO BOX 521312	BIG LAKE	AK	99652
JOHNSON JAS F & ELLEN W	1943 ASHLEY COVE CIR	ANCHORAGE	AK	99516
JOHNSON JASON J	PO BOX 672604	CHUGIAK	AK	99567
JOHNSON JEFFREY P & VERA L	4681 SOUTHPARK BLUFF DR	ANCHORAGE	AK	99516
JOHNSON JEREMY	PO BOX 670667	CHUGIAK	AK	99567
JOHNSON JOHNVA A	13749 HUNTERWOOD LN	EAGLE RIVER	AK	99577
JOHNSON LANCE K	PO BOX 113196	ANCHORAGE	AK	99511
JOHNSON LELAND A & JANE W	PO BOX 110333	ANCHORAGE	AK	99511
JOHNSON LYDIA A	3903 SPENARD RD	ANCHORAGE	AK	99517
JOHNSON MARSHAL W	HC 89 BOX 202	WILLOW	AK	99688
JOHNSON MICHAEL N&KAREN M	5854 KATAHDIN DR	ANCHORAGE	AK	99502
JOHNSON RALPH C & ANITA M	1221 GARY ST	WENATCHEE	WA	98801
JOHNSON RANDY M & ANN M	4189 CHECKMATE DR	ANCHORAGE	AK	99508
JOHNSON RAYMOND W & P	6035 SPRUCE MEADOWS LOOP	ANCHORAGE	AK	99507
JOHNSON RAYNOLD & A L	22424 N BIRCHWOOD LOOP	CHUGIAK	AK	99567
JOHNSON REV. DONALD L.	6065 PROMINENCE POINTE DR	ANCHORAGE	AK	99516
JOHNSON RICHARD D	PO BOX 520696	BIG LAKE	AK	99652
JOHNSON RICHARD R&KARROLL	PO BOX 674	WILLOW	AK	99688

Ahtna Inc.

Alaska State Legislature

Picture This Art Gallery

JOHNSON ROBT H M & TERESA	16547 BETTY ST	ANCHORAGE	AK	99516
JOHNSON ROBT J & LINDA L	PO BOX 520768	BIG LAKE	AK	99652
JOHNSON ROSALIE L	3121 NORM CIR	ANCHORAGE	AK	99507
JOHNSON RYAN G	PO BOX 4619	PALMER	AK	99645
JOHNSON SHAWN R & TINA M	PO BOX 422	WILLOW	AK	99688
JOHNSON SIDNEY L & JEANNE	14220 SUNVIEW DR	ANCHORAGE	AK	99515
JOHNSON STEVEN F&ROSALIND	PO BOX 929	WILLOW	AK	99688
JOHNSON THEO E & JOY E	6105 KENSINGTON DR	ANCHORAGE	AK	99504
JOHNSON THOS P	PO BOX 520157	BIG LAKE	AK	99652
JOHNSON TIMOTHY R	PO BOX 220064	ANCHORAGE	AK	99522
JOHNSON TODD ELLIS	1061 W NUGGET AVE	WASILLA	AK	99654
JOHNSON TRINA M	PO BOX 92475	ANCHORAGE	AK	99509
JOHNSON VINCENT M	PO BOX 671067	CHUGIAK	AK	99567
JOHNSON WALTER C	PO BOX 230981	ANCHORAGE	AK	99523
JOHNSTON BENJAMIN E & C J	PO BOX 791	PALMER	AK	99645
JOHNSTON DENNIS D&NANCY C	PO BOX 940101	HOUSTON	AK	99694
JOHNSTON GARY THOS	HC 89 BOX 180	WILLOW	AK	99688
JOHNSTON GREGORY W& KATHY	7551 E BETTER DUCK CIR	PALMER	AK	99645
JOHNSTON HAROLD L & SYDNEY	10927 N FLEET DR	ANCHORAGE	AK	99515
JOHNSTON HERBERT B & SUSAN	8701 BELL	ANCHORAGE	AK	99507
JOHNSTON LARRY	17944 BEAUJOLAIS DR	EAGLE RIVER	AK	99577
JOHNSTON LORI	PO BOX 202428	ANCHORAGE	AK	99520
JOHNSTON MICHAEL D & R	PO BOX 111334	ANCHORAGE	AK	99511
JOHNSTON ROBT ANDREW	1500 S LOWER CIR	PALMER	AK	99645
JOHNSTON SHAWN P & LAURA J	7300 BIG MOUNTAIN DR	ANCHORAGE	AK	99516
JOHNSTON THOS S	9150 CHIPWOOD CIR	ANCHORAGE	AK	99507
JOHNSTONE STOWELL R	3210 SEAWIND DR	ANCHORAGE	AK	99516
JOKELA KURT	PO BOX 670046	CHUGIAK	AK	99567
JOKELA RALPH B & MARY ANN	PO BOX 850	WILLOW	AK	99688
JOLLEY TOM & KATHY	10244 SILVERTIP CIR	EAGLE RIVER	AK	99577
JOLLY MATTHEW TROY	4641 SANDY BEACH DR	ANCHORAGE	AK	99502
JON & CHERYL FUGLESTAD	12618 N HAGION SHORES DR	WILLOW	AK	99688
Jon Fray	Mi 4.2 Echo Lake rd	Big Lake	AK	99652
Jon Fray	Mi 4.2 Echo Lake rd	Big Lake	AK	99652
JONAS JAS R	PO BOX 520201	BIG LAKE	AK	99652
Jones - ELG Elenor	PO BOX 93509	ANCHORAGE	AK	99509
JONES ANNA M MATHIS DOSS	2610 W 29TH	ANCHORAGE	AK	99517
JONES BILL L & MICHELLE R	11806 WILDERNESS DR	ANCHORAGE	AK	99516
JONES BRIEN W	PO BOX 543	WILLOW	AK	99688
JONES CAROL M THEODORE	3634 BONIFACE PKWY	ANCHORAGE	AK	99504
JONES CLIFFORD L	400 D ST STE 110	ANCHORAGE	AK	99501
JONES CLINTON P	PO BOX 520054	BIG LAKE	AK	99652
JONES DARRYL D	PO BOX 940146	HOUSTON	AK	99694

JONES DONNA R	823 W 53RD AVE	ANCHORAGE	AK	99518
JONES ELIZABETH A	PO BOX 141133	ANCHORAGE	AK	99514
JONES F LELAND & CAROLYN	20628 PHILADELPHIA WAY	EAGLE RIVER	AK	99577
JONES GENE & DARYL D	PO BOX 1287	PALMER	AK	99645
JONES GREGORY L & KAREN L	4720 SILVER SPRINGS CIR	ANCHORAGE	AK	99507
JONES JANET LEE	PO BOX 231124	ANCHORAGE	AK	99523
JONES JAS E	7921 LITTLE DIPPER AVE	ANCHORAGE	AK	99504
JONES JULIAN	4334 E 5TH AVE	ANCHORAGE	AK	99508
JONES KEVIN L	PO BOX 221762	ANCHORAGE	AK	99522
JONES MARGARET E	11801 MARY AVE	ANCHORAGE	AK	99515
Jones Melton	P.O. Box 1302	Palmer	AK	99645
JONES MICHAEL S&STEPHANIE	PO BOX 520861	BIG LAKE	AK	99652
JONES NATHANIEL L&LILLIAN	5432 E NORTHERN LGTS BLVD	ANCHORAGE	AK	99508
JONES RICHARD L & PAULA M	2330 N GREEN FOREST DR	PALMER	AK	99645
JONES ROBT M	PO BOX 521016	BIG LAKE	AK	99652
JONES ROY S	PO BOX 2414	PALMER	AK	99645
JONES STANLEY VERELL	8937 EAGLE PLACE LP	EAGLE RIVER	AK	99577
JONES STEVEN E	PO BOX 13356	TRAPPER CREEK	AK	99683
JONES TOMMALINE	PO BOX 940241	HOUSTON	AK	99694
JONES TONY	1427 AIRPORT HEIGHTS DR	ANCHORAGE	AK	99508
JONES TRAVIS M	PO BOX 521067	BIG LAKE	AK	99652
JONES WM G & EDNA P	2233 SORBUS WAY	ANCHORAGE	AK	99508
JONES-ELG ELINOR I	PO BOX 93509	ANCHORAGE	AK	99509
JONROWE MICHAEL D & DEEDEE	PO BOX 272	WILLOW	AK	99688
JORDAN	PO BOX 3185	PALMER	AK	99645
JORDAN	PO BOX 3185	PALMER	AK	99645
Jordan James	4107 Laurel Street	Anchorage	AK	99508
JORDAN NICOLENE	PO BOX 2572	PALMER	AK	99645
JORDAN RENEE L.	1691 S ABBY BLVD	WASILLA	AK	99654
JORDAN S GAR	PO BOX 3185	PALMER	AK	99645
JORDET JASON A	3311 MAX CIR	ANCHORAGE	AK	99507
JORGENSEN RICHARD J & K D	3020 BROOKRIDGE CIR	ANCHORAGE	AK	99504
JOSAITIS CURTIS J	PO BOX 670967	CHUGIAK	AK	99567
JOURDIAN DONALD	PO BOX 233253	ANCHORAGE	AK	99523
JOVANOV RODENKO R & MARIA	2646 SECLUSION DR	ANCHORAGE	AK	99504
JOY AUSTIN J JR& REGENA S	PO BOX 111073	ANCHORAGE	AK	99511
JUDD LAWRENCE T & JOSIE L	1025 GOLDENDALE DR	WASILLA	AK	99654
JUDGE TERRY & KATHY LYNN	8050 PIONEER	ANCHORAGE	AK	99504
Judy	HC 34, Box 2293	Wasilla	AK	99654
Judy	HC 34, Box 2293	Wasilla	AK	99654
JUDY LARRY M SR&VERSTOVIA	PO BOX 521773	BIG LAKE	AK	99652
Judy Tymick	HC 34 Box 2596	Wasilla	AK	99654
JULIAN JAS V	PO BOX 520128	BIG LAKE	AK	99652

Valley Locksmith

Alaska State Medical Association

JULSEN KURT D	1201 AUTUMN LN	ANCHORAGE	AK	99504
JUNE KIM	PO BOX 167	WILLOW	AK	99688
JUNGWIRTH JOS S & RICHELLE	5264 CAPE SEVILLE DR	ANCHORAGE	AK	99516
JUNTUNEN CARL I JR	4601 REKA DR	ANCHORAGE	AK	99508
JURASEK NICK	PO BOX 243282	ANCHORAGE	AK	99524
JURASEK THOS J & LISA A	5373 E BIDEFORD BLVD	PALMER	AK	99645
JURIK ANDREW & MINDY L	2246 HANNING BAY CIR	ANCHORAGE	AK	99515
JURKOWSKI GREG A & LISA H	10846 ANVIK CIR	EAGLE RIVER	AK	99577
JURY JEROME D & JANET M	13047 RIDGEVIEW	ANCHORAGE	AK	99516
K S & P EICHHOLZ TR	PO BOX 141822	ANCHORAGE	AK	99514
KAAS VIKI L	517 W 12TH AVE	ANCHORAGE	AK	99501
KAER SUSAN A	PO BOX 101113	ANCHORAGE	AK	99510
KAERCHER DAVID DALE & P L	3061 AMBER BAY LOOP	ANCHORAGE	AK	99515
KAERCHER JOHN M & TERRA S	11000 KASILOF BLVD	ANCHORAGE	AK	99507
KAHN HARVEY W & JEANNETTE A	18811 K & R ST	CHUGIAK	AK	99567
KAIN DONNA K	PO BOX 520757	BIG LAKE	AK	99652
KAIN LEISL A	PO BOX 521636	BIG LAKE	AK	99652
KAISER HENRY S JR & E L	1555 ALDER DR	ANCHORAGE	AK	99508
KAISER PATRICIA ANNE	3732 DEBORAH LN	ANCHORAGE	AK	99504
KAISER ROBT JOE	17145 EAGLE RIVER RD	EAGLE RIVER	AK	99577
KALAMARIDES GEO MICHAEL	1013 E 27TH AVE	ANCHORAGE	AK	99508
KALAMARIDES JOS A & P J	2528 CAPTAIN COOK DR	ANCHORAGE	AK	99517
KALBFLEISCH DEAN F & D D	3206 SHERRIE ST	ANCHORAGE	AK	99504
KALE STEVEN E	PO BOX 520027	BIG LAKE	AK	99652
KAMHOLZ PHEBE A	PO BOX 521055	BIG LAKE	AK	99652
KAMINSKI KATHRYN	PO BOX 3196	ANDERSON	AK	99744
KAMMERMEYER PATRICK M	PO BOX 2483	PALMER	AK	99645
KAMPEN LARRY & CAROL	4760 SPORTSMAN DR	ANCHORAGE	AK	99502
KAMSTRA WALTER W & ATTA E	910 BENCH CT	ANCHORAGE	AK	99504
KAN LI	PO BOX 2886	PALMER	AK	99645
KANADY RANDALL B & JUDY R	8015 FROSTLINE CT	ANCHORAGE	AK	99507
Kane Carol G.	Alaska Association of Secondary School Principals P.O. Box 2889	Palmer	AK	99645
KANE WM D & CAROL G	PO BOX 520502	BIG LAKE	AK	99652
Kangas Larry & Betty	Box 831	Big Lake	AK	99652
KAPPER STEPHAN & JACQUELINE	3410 SOUTHBUFF CIR	ANCHORAGE	AK	99515
KAPUNIAI KATHRYN F	HC 89 BOX 107	WILLOW	AK	99688
KARA ALLAN J	1330 W 79TH AVE	ANCHORAGE	AK	99516
Karabelnikoff Sally	7435 Old Harbor Road	Anchorage	AK	99504
KARABELNIKOFF SALLY A	7435 OLD HARBOR RD	ANCHORAGE	AK	99504
KARDELL JUANITA K	7813 DOVER AVE	ANCHORAGE	AK	99504
KARDYS BARBARA L	3430 E 65TH AVE	ANCHORAGE	AK	99507
KAREEN EDW L & PATRICIA A	3800 E HUFFMAN RD	ANCHORAGE	AK	99516
Karen Nugen-Logan	PO Box 871545	WASILLA	AK	99687

Karen Nugen-Logan	PO Box 871545	AK	99687
KARI PHILLIP J & LAURIE A	1125 GRANTHAM RD	AK	99654
KARMUN HARRY C JR	1025 W LONE CUB DR	AK	99654
KASACHEV GRIGORY	PO BOX 230412	AK	99523
KASICA RICHARD F & MELINDA	10240 SEXTANT CIR	AK	99515
KASKA CRISTY A	12711 OLD GLENN HWY	AK	99577
KASMAR CHAS D & MARILYN	2608 NATHANIEL CT	AK	99517
KASPER RICHARD J & RITA V	PO BOX 520507	AK	99652
KASPER TIMOTHY J & KIM	PO BOX 521451	AK	99652
KASS KELLY & JENNIFER	10244 GOODNEWS CIR	AK	99515
KASSNER JAY E	300 W 123RD AVE	AK	99515
KATASSE CONSTANCE C	5000 KENAI AVE	AK	99508
Kathy Huston	P.O. Box 1027	AK	99688-10
KATZENBERGER DOUGLAS W	PO BOX 521538	AK	99652
KAUFMAN JOS	PO BOX 671016	AK	99567
KAVANAUGH GRACE	8130 WOODGREEN CIR	AK	99518
KAWABE EIKO	424 EAST MANOR AVE	AK	99501
KEEN GARLEN & JOYCE V	800 N HYER RD	AK	99645
KEENE GERALD P	PO BOX 958	AK	99688
KEETON ROBERT C III	3141 E 112TH AVE	AK	99516
KEHL RYAN & ERIN	11440 BROWDER AVE	AK	99507
KEHL MICHAEL & DONNA	1930 E 66TH	AK	99507
KEHRER ROBT S	4132 E 3RD AVE	AK	99508
KEIDA RICHARD P&JEANNETTE	PO BOX 116	AK	99688
KEIRN REX E JR	1126 HARLEY TRAIL	AK	99701
KEISOR CHAS R & SHIRLEY J	PO BOX 201932	AK	99520
KEISOR HAROLD H & SHIRLEY	6088 SPRUCE MEADOWS LOOP	AK	99507
KEISOR JAS F	3209 WILEY POST LOOP	AK	99517
Keith Dobson	11071 Mtn Lake Drive	AK	99516
Keith Gordon	4111 Aviation Avenue	AK	99519
KELLAR NICHOLAS L & DONNA	12841 MIDORI DR	AK	99516
Kellard James	P O BOX 101	AK	99676
Kellard James J. & Susan P.	P.O. Box 101	AK	99676
Kellard Suzy	P.O. Box 1009	AK	99676
KELLER ARTHUR L & JANE E	850 N TINA LN	AK	99645
Keller Diane	290 E. Herming Avenue	AK	99654
KELLER DIANE K	1120 N CRAIG STADLER LOOP	AK	99654
KELLER DONALD D	21128 BOWERY LN	AK	99567
KELLER EDW L	PO BOX 940383	AK	99694
Keller Wes	600 E. Railroad Ave. Ste 1	AK	99564
KELLEY ALVIN S & JUDY A	7132 PECK AVE	AK	99504
KELLEY JAMES P & HEIDI L	PO BOX 814	AK	99688
KELLEY LEONARD T	821 N STREET	AK	99501

Willow Area Community Council

Alaska Dept. of Transportation/Public Facilities

Museum of Northern Adventure  
Talkeetna Gifts & Collectibles

City of Wasilla

Alaska State Legislature

KELLEY MARGARET A	HC 89 BOX 348	WILLOW	AK	99688
Kelley Ray	PO Box 675	WILLOW	AK	99688
KELLY ADELANE M	PO BOX 203175	ANCHORAGE	AK	99520
KELLY DARRELL V	PO BOX 12	WILLOW	AK	99688
KELLY DAVID G	3761 CHINIAK BAY DR	ANCHORAGE	AK	99517
KELLY DONTE L & SHIRLEY J	3419 NEWCOMB DR	ANCHORAGE	AK	99508
Kelly Frank	P.O. Box 770548	Eagle River	AK	99577
Kelly Griffin	HC35 Box 53557	Wasilla	AK	99654
KELLY JOHN J & DIANE J	3920 BRYANT RIDGE PL	ANCHORAGE	AK	99504
Kelly Jr. Daniel	581 BRIAR DR	WASILLA	AK	99654
Kelly Michael	State Capitol, Room 434	Juneau	AK	99801-11
KELLY TIMOTHY J	7441 MARGARET CIR	ANCHORAGE	AK	99518
KELM SCOTT & MARY	30100 EKLUND AVE	EAGLE RIVER	AK	99577
KEMPTON THOS G	337 W 12TH AVE	ANCHORAGE	AK	99501
Ken Hiltiker	10510 Lone Tree Drive	Anchorage	AK	99507
Ken Ludy	5800 Yadon Dr.	Wasilla	AK	99654
KENDALL DENNIS H& BARBARA	7823 STEWART MOUNTAIN DR	EAGLE RIVER	AK	99577
KENDRICK CARMEN K	PO BOX 520810	BIG LAKE	AK	99652
KENISON GEO & SHARON	PO BOX 520944	BIG LAKE	AK	99652
KENISON LAURA A	PO BOX 210656	ANCHORAGE	AK	99521
KENISON STEPHEN G&DONNA M	PO BOX 521755	BIG LAKE	AK	99652
KENLEY DOUGLAS	9806 E NORTHSTAR CIR	PALMER	AK	99645
KENNARD RICHARD W & S A	PO BOX 201704	ANCHORAGE	AK	99520
KENNEDY EDWARD & PATRICIA	2802 BORLAND DR	ANCHORAGE	AK	99517
KENNEDY GREG R & CRYSTAL	20255 CONSTITUTION DR	EAGLE RIVER	AK	99577
KENNEDY R DUKE& BARBARA M	PO BOX 90514	ANCHORAGE	AK	99509
KENNELLY NEIL R	2805 DAWSON ST #101	ANCHORAGE	AK	99503
Kenneth M Whedbee	450 Bazil Circle	BIG LAKE	AK	99652
KENNON JAS M	18551 CITATION RD	EAGLE RIVER	AK	99577
Kenshalo Richard	1740 S. Chugach Street	Palmer	AK	99645
KENT JOHN F & CAROLYN C	1754 MORNINGTIDE CT	ANCHORAGE	AK	99501
Kent Lynn	555 Cordova St.	ANCHORAGE	AK	99501
KENT THOS W & CATHERINE M	8201 HENRY CIR	Anchorage	AK	99507
KENYON DOUGLAS R& BARBARA	8121 WILLIWA AVE	ANCHORAGE	AK	99504
KEOUGH DANNY R	6460 ASKELAND DR	ANCHORAGE	AK	99507
KEOWN RAYMOND M JR & P L	PO BOX 3904	PALMER	AK	99645
KEPLER JUSTIN	PO BOX 520444	BIG LAKE	AK	99652
KERKOVE GREGORY J & KAREN	1321 KINNIKINNICK	ANCHORAGE	AK	99508
KERR MICAH A	PO BOX 36	WILLOW	AK	99688
KERR RICHARD & LINDA	PO BOX 190284	ANCHORAGE	AK	99519
KERSHAW GREGORY J	3821 CHINIAK BAY DR	ANCHORAGE	AK	99515
Kerttula Beth	State Capitol, Room 430	Juneau	AK	99801
Kerttula Jalmar	P.O. Box 1009	Palmer	AK	99645

KESSLER MICHAEL D	PO BOX 665	WILLOW	AK	99688
KETCHUM DANA B	PO BOX 649	WILLOW	AK	99688
KETTERER EDW R & FRANCES	3909 TURNAGAIN BLVD E	ANCHORAGE	AK	99517
Kevin Gray	9000 Callaway Circle	Anchorage	AK	99507
Kevin Hite	8050 Summerset Drive	Anchorage	AK	99612
Kevin Hite	8050 Summerset Drive	Anchorage	AK	99518
KEY STEVEN A	801 N LANE ST	ANCHORAGE	AK	99508
KHOKHORINA TATIANA J	10600 STROGANOF DR	ANCHORAGE	AK	99507
KIDD MUREL J & LINDA L	PO BOX 1434	ANCHORAGE	AK	99645
KIDRICK STEVE	PO BOX 520062	PALMER	AK	99645
KIESLER DIETER K	PO BOX 25	BIG LAKE	AK	99652
Kiger Bill	PO BOX 25	WILLOW	AK	99688
KIKUCHI ROY M & ANGELINA	550 W. 7th Avenue, Ste 1340	Anchorage	AK	99501
KIKUCHI SAM J	2536 LOVEJOY DR	ANCHORAGE	AK	99508
KILLEN JAMES JR	PO BOX 520268	BIG LAKE	AK	99652
KILLION THEA	8286 OPAL DR	ANCHORAGE	AK	99502
KILLORAN NANCY	433 E DOWLING RD	ANCHORAGE	AK	99518
KILPATRICK RANITA L	17767 SANCTURAY DR	EAGLE RIVER	AK	99577
KILPATRICK TONY	546 W 19TH AVE	ANCHORAGE	AK	99503
KIM EUI GYUN & SUN OKE	1028 E 5TH AVE	ANCHORAGE	AK	99501
KIM GYUNG S & MYUNG SOON	2131 CRATAEGUS AVE	ANCHORAGE	AK	99508
KIM TONY	3434 COTTONWOOD ST	ANCHORAGE	AK	99508
Kim Woodbury	16664 BRIARCLIFF RIDGE	ANCHORAGE	AK	99516
KIM YOUNG HYON & SONG CHA	PO Box 520957	BIG LAKE	AK	99652
KIMBALL DAVID A	15612 BRIDGE VIEW DR	ANCHORAGE	AK	99516
KIMBALL GARY L	400 E 24TH AVE	ANCHORAGE	AK	99503
KIMBALL MADELINE R COLE	9210 VANGARD DR	ANCHORAGE	AK	99507
KIMBLEY C A WHITE G L	PO BOX 178	WILLOW	AK	99688
KIMM JACK	811 CEDAR ST	ANCHORAGE	AK	99501
KIMPTON RANDY L & MACEY JO	PO BOX 100007	ANCHORAGE	AK	99510
Kincaid Janet	5207 SHORECREST DR	ANCHORAGE	AK	99515
KINCAID KEN E	P.O. Box 118	Palmer	AK	99645
KINCAID TERRY M & BRIDGET	PO BOX 2444	PALMER	AK	99645
KINDRED EVAN L & LINDA B	10336 LE DOUX LN	EAGLE RIVER	AK	99577
KINDRED ROBERT O&JANENE M	PO BOX 41	WILLOW	AK	99688
King Bob	200 BONNIE JEAN CT	ANCHORAGE	AK	99515
KING CURTIS A & RUTH A	P.O. Box 3869	Palmer	AK	99645
KING DAVID A & BONNIE J	9225 GEESE CIR	EAGLE RIVER	AK	99577
KING DONALD E	PO BOX 670089	CHUGIAK	AK	99567
KING DWIGHT G	PO BOX 104398	ANCHORAGE	AK	99510
KING GARY W	919 W EDINBOROUGH DR	PALMER	AK	99645
KING JACK C & CAROLE A	12715 BERESFORD DR	STERLING HGTS MI	MI	48313
KING MARILYNN ROSE	4030 HAMPTON DR	ANCHORAGE	AK	99504
	PO BOX 520743	BIG LAKE	AK	99652

Alaska Dept. of Natural Resources

Colony Inn

Bob King Design



KING MURIEL L	1605 W 13TH AVE	ANCHORAGE	AK	99501
KING ONRAL IRENE	8107 SPRING ST	ANCHORAGE	AK	99518
KING RUSSELL L & DONNA L	15750 E JUPITER AVE	PALMER	AK	99645
KING STEVE E	PO BOX 670361	CHUGIAK	AK	99567
KING THOS H & SARAH L	4141 TAHOE	ANCHORAGE	AK	99515
KINNEAR ROBT & JOHANNA	3902 STARGAZER CIR	ANCHORAGE	AK	99516
KINNEY JOS & PATRICIA	HC 89 BOX 452	WILLOW	AK	99688
KINNEY MICHAEL G	PO BOX 520726	BIG LAKE	AK	99652
KINNEY RODNEY P JR & B D	10144 LEE ST	EAGLE RIVER	AK	99577
KINNEY ROGER H & SUE A	6483 VILLAGE PKWY	ANCHORAGE	AK	99504
Kipke CARL P	13126 BEACH CIR	ANCHORAGE	AK	99515
KIRBY BRYAN	12725 E KAYE MARIE CT	PALMER	AK	99645
Kircher Michael	641 N. 3rd Street	Palmer	AK	99645
KIRCHNER DEBRA R	7752 DOVER AVE	ANCHORAGE	AK	99504
KIRCHNER MICHAEL F&CONNIE	8840 BANNO CIR	ANCHORAGE	AK	99502
KIRK BRIAN & KRISTI	18322 KANTISHNA DR	EAGLE RIVER	AK	99577
KIRKLAND CLIFFORD A & C M	2737 GLACIER ST	ANCHORAGE	AK	99508
KIRN MERVIN & GLADYS O	PO BOX 4444	PALMER	AK	99645
KISSEE STEPHEN A& LINDA D	11219 TERRACE HILLS DR	EAGLE RIVER	AK	99577
KISSELL AL & NATALIA	3560 HOLLYBERRY CIR	ANCHORAGE	AK	99507
Kitka Julie	1577 C Street, Suite 300	Anchorage	AK	99501
KITSOS WM G	PO BOX 520295	BIG LAKE	AK	99652
KIZANIS REBECCA R	11822 NE 159TH ST	BOTHELL	WA	98011
Klapperich John	2200 E. Parks Hwy	Wasilla	AK	99654
KLAWITTER MIKE R	PO BOX 2021	PALMER	AK	99645
KLAWUNDER MARILYN S	13040 HILLSIDE DR	ANCHORAGE	AK	99516
KLAYUM SCOTT L	PO BOX 671722	CHUGIAK	AK	99567
Klebesadel Lucy	16547 E. SMITH ROAD	PALMER	AK	99645
KLEBS GARY & MARY	1852 BUCCANEER PL	ANCHORAGE	AK	99501
Klebs Mike	1107 E. 72nd Avenue	Anchorage	AK	99518
KLEIN DAVID T	1201 N INJUN JOE CIR	WASILLA	AK	99654
KLEIN KEITH A & MYLINDA A	6290 E ALTRA DR	PALMER	AK	99645
Klein Kimberly	333 Raspberry Rd	ANCHORAGE	AK	99518
Klein Kimberly	333 Raspberry Rd	ANCHORAGE	AK	99518
KLEINENBERG ALBERTUS H& A	PO BOX 521020	BIG LAKE	AK	99652
KLEVEN JUDITH ANN	18905 BEVERLY AVE	CHUGIAK	AK	99567
KLEY JOS & KARLA	2771 SEAFARER LOOP	ANCHORAGE	AK	99516
KLINE PHILLIP D & CAROLYN	4812 BLACKSTONE CIR	ANCHORAGE	AK	99507
KLINE RALPH L & PAMELA A	PO BOX 488	WILLOW	AK	99688
KLINK SVETLANA	3940 SYCAMORE LOOP	ANCHORAGE	AK	99504
KLINSKI KEN N & NANCY J	PO BOX 190163	ANCHORAGE	AK	99519
KLIPPEL KEVIN C	PO BOX 773391	EAGLE RIVER	AK	99577
KLOEP DAVID P	PO BOX 520687	BIG LAKE	AK	99652

Palmer Planning & Zoning Commission

Alaska Federation of Natives

KMBQ Radio

Klebs Mechanical, Inc.

KLORA JEFFREY S Kluberton Tom	Mat-Su Borough Assembly	106 HIGH MEADOWS DR P.O. Box 200	FLORENCE Talkeetna	CO	81226
KLUEVER SCOTT L & JENNIFER		PO BOX 771011	EAGLE RIVER	AK	99676
KLUNDER KEN		P O BOX 2883	PALMER	AK	99577
KNAPP DAVID H & JOANNA M		1648 STANFORD DR	ANCHORAGE	AK	99645
KNAPP FRANK E & K A		PO BOX 78	PALMER	AK	99508
KNAPP HERMANN J		104 MULDOON RD	ANCHORAGE	AK	99645
KNAPP JAS C		PO BOX 520530	BIG LAKE	AK	99504
KNAPP MICHAEL R		PO BOX 520263	BIG LAKE	AK	99652
KNAPP-ROBINSON DENISE L		PO BOX 112842	ANCHORAGE	AK	99652
KNAUF FRANK E		PO BOX 520912	BIG LAKE	AK	99511
KNAUFF RANDALL WELLINGTON		16400 BLACK BEAR CIR	ANCHORAGE	AK	99652
KNICELEY EARNIE C & MAVIS		9241 GROVER DR	ANCHORAGE	AK	99516
Knickerbocker Glen	Unit Company	620 E. Whitney Road	Anchorage	AK	99516
KNIGHT GASTON N & JANET W		5306 N STAR ST	ANCHORAGE	AK	99501-12
KNIGHT JAS W JR		PO BOX 241344	ANCHORAGE	AK	99518
KNIGHT JIMMIE E		1272 S MOUNT BAKER AVE	ANCHORAGE	AK	99524
KNIGHT KENNETH R & SANDRA		PO BOX 112687	CAMANO ISLAND WA	WA	98282
KNIGHT KEVIN & ROXANNE		6930 VIBURNUM DR	ANCHORAGE	AK	99511
KNIGHT KIRK		25673 BENDILENT	ANCHORAGE	AK	99507
KNOTT VICTOR		14114 ROCKY RD	EAGLE RIVER	AK	99577
Knowles Bruce		PO Box 823206	ANCHORAGE	AK	99516
KNOWLES KIM P		2231 KNOLL CIR	ANCHORAGE	AK	99501
KNOWLES PAT		8427 MENTRA ST	ANCHORAGE	AK	99518
KNOWLES THOS B & ROSE M		PO BOX 457	WILLOW	AK	99688
KNOWLTON STANLEY B & M.F.		10031 FARPOINT CIR	ANCHORAGE	AK	99507
KNOX WM V & SHARON C		2040 INNES CIR	ANCHORAGE	AK	99515
KNUDSEN CHRIS A		PO BOX 521852	BIG LAKE	AK	99652
KNUDSEN PETER W&KATHRYN F		8050 CHIPPER TREE CIR	ANCHORAGE	AK	99507
Knudson Kip	Tesoro Alaska Petroleum Company	PO Box 196272	Anchorage	AK	99519
KNUJEPPEL ROGER & YOSHIKO		18942 CITATION RD	EAGLE RIVER	AK	99577
KOBETICH STEVEN L		PO BOX 520995	BIG LAKE	AK	99652
KOCH BARRY		3962 REFLECTION DR	ANCHORAGE	AK	99504
KOCUREK ERIC M & JENNIFER		2600 CAPTAIN COOK DR	ANCHORAGE	AK	99517
KOEHLER KURT A & CATHERINE		PO BOX 671026	CHUGIAK	AK	99567
KOEHLER MARK A		PO BOX 521545	BIG LAKE	AK	99652
KOENIG GERARD & DANNILEE		1846 CINDY LEE LN	ANCHORAGE	AK	99507
KOENIG JAS E		8174 LLOYD DR	ANCHORAGE	AK	99502
KOHFIELD BYRON D & V L		3026 BRITTANY PL	ANCHORAGE	AK	99504
KOJIN ANANY		106 ELM AVE	ERSKINE	MIN	56535
KOLBECK RONALD B & HELEN		2003 W 48TH AVE	ANCHORAGE	AK	99517
KOLBERG HARVEY A & J		17290 E THREE SISTERS DR	ANCHORAGE	AK	99645
KOLBERG ROSE ANN		1901 S KAYE MARIE DR	PALMER	AK	99645

KOLDEWAY JOHN M	9400 EMERALD ST	ANCHORAGE	AK	99502
KOLEROK LAURA	PO BOX 670053	CHUGIAK	AK	99567
Kolivosky Melinda	P. O. Box 305	Palmer	AK	99645
KOLIVOSKY MICHAEL C & M E	PO BOX 0305	PALMER	AK	99645
KONG YOUNG O	7950 CRESCENT HILL DR	ANCHORAGE	AK	99507
Koning CATHLEEN S	1201 AUTUMN LN	ANCHORAGE	AK	99504
KONJEVICH JOHN	6543 ROSEWOOD ST	ANCHORAGE	AK	99518
KONNO MICHIIRO & SATOKO	HC 89 BOX 260	WILLOW	AK	99688
Kontz Mike	1617 Industrial	Palmer	AK	99645
Kookesh Albert	State Capitol, Room 7	Juneau	AK	99801-11
KOONTZ DARRELL E	4210 DOROTHY DR	ANCHORAGE	AK	99504
KOONTZ EVELYN W	7417 HENNINGS WAY	ANCHORAGE	AK	99504
KOPCZENSKI ANDREW C	HC 89 BOX 259	WILLOW	AK	99688
KOPPLIN PAUL R	6040 TRAPPERS TRAIL RD	ANCHORAGE	AK	99516
Kops Braun	HC02-7726	Palmer	AK	99645
KORDEWICK TERRENCE E & K A	7300 SETTER DR	ANCHORAGE	AK	99502
KORDUS TREVIS K & JULIE A	1190 N SAM SNEAD LOOP	WASILLA	AK	99654
KOREVEC SCOTT EDW & LINDA Z	3400 HILAND DR	ANCHORAGE	AK	99504
KORMAN DARRELL E & C	PO BOX 521411	BIG LAKE	AK	99652
KORNMANN JOHN & DIXIE	PO BOX 521621	BIG LAKE	AK	99652
KORPI DAVID CHAS	PO BOX 822	WILLOW	AK	99688
KORPI DAVID CHAS	PO BOX 822	WILLOW	AK	99688
KORPI JOHN H	14001 SPECKING AVE	ANCHORAGE	AK	99515
KORTE STEVEN W	PO BOX 862	WILLOW	AK	99688
KOSIK DAVID J & LAURIE	4851 LEAH CT	ANCHORAGE	AK	99508
Koskela Greg	1534 W. Lake Lucille Drive	Wasilla	AK	99654
KOTEEEN ELEANOR	PO BOX 111465	ANCHORAGE	AK	99511
KOTLAREK KENNETH J&LOIS J	19515 FIRST ST	EAGLE RIVER	AK	99577
KOUSAL KIRK K	PO BOX 1081	WILLOW	AK	99688
KOVAC NINA M	210 E 11TH AVE	ANCHORAGE	AK	99501
KOVACEVICH TERRENCE J&V A	207 DAILEY AVE	ANCHORAGE	AK	99515
KOVAR MARK F & PRISCILLA	PO BOX 4097	PALMER	AK	99645
KOWAL PAUL	PO BOX 331	WILLOW	AK	99688
KOWALKE ROBT J & SHELLIE	PO BOX 174	WILLOW	AK	99688
KOWALSKI DARRYL J& TERESA	PO BOX 402	WILLOW	AK	99688
KOWALSKI PHILLIP & EVA M	PO BOX 520349	BIG LAKE	AK	99652
KOZLER RICHARD	1420 INLET PL	ANCHORAGE	AK	99501
Kragness Leonard	1817 Parkside Drive	Trapper Creek	AK	99501-57
KRAMER WM L & CATHI A	PO BOX 521783	BIG LAKE	AK	99652
KRASSELT KENNETH	4950 NOTTINGHAM WAY	ANCHORAGE	AK	99503
KRAUSE RICHARD J & KAY M	PO BOX 222121	ANCHORAGE	AK	99502
Kreger Ron	HC-31 Box 5184	Wasilla	AK	99654
KREIN OLIVER A & CAROL S	4241 WARWICK DR	ANCHORAGE	AK	99508

KRETSINGER STEVEN	411 FALKE CT	ANCHORAGE	AK	99504
KREUN STEVEN C & MARGARET	4671 SOUTHPARK BLUFF DR	ANCHORAGE	AK	99516
KRICHKO DEBRA JEAN STOLT	1529 P ST	ANCHORAGE	AK	99501
KRIGER MICHAEL D	1800 DIOMEDE ST	ANCHORAGE	AK	99504
KRIZMAN HENRY E & BARBARA	16500 CHASEWOOD LN	ANCHORAGE	AK	99516
KROPIDLOWSKI KENNETH L & L	3112 GLENN DON DR	ANCHORAGE	AK	99504
KRUGER IRA L & KATHERINE M	PO BOX 940202	HOUSTON	AK	99694
KRUSE DAN P & GRACE L	9900 SLALOM DR	ANCHORAGE	AK	99516
KRUSE RICHARD	7940 EVANDER DR	ANCHORAGE	AK	99518
KRUSE TIMOTHY & ERIN	12573 W LOOKING GLASS DR	HOUSTON	AK	99694
KRUSELL JEFF	6711 CUTTY SARK ST	ANCHORAGE	AK	99502
Krzewinski Thomas	1750 Abbott Rd Ste 200	Anchorage	AK	99507
KTUU TV 2/GOOSEBAY	701 E TUDOR RD	ANCHORAGE	AK	99503
KUCERA ALMA LOUISE	PO BOX 520790	BIG LAKE	AK	99652
KUDLOCK GEO JR	PO BOX 520162	BIG LAKE	AK	99652
KUEHLER RANDALL L & LAURI	1080 SERRANO DR	WASILLA	AK	99654
KUEHN GARY J & WINDELL L	1203 S CENTER POINT DR	WASILLA	AK	99654
KUEST LOUIS JOHN III	PO BOX 240083	ANCHORAGE	AK	99524
KUFEL MARK C	3880 CARAVELLE DR	ANCHORAGE	AK	99502
Kuhn Gary	809 Chugach	Palmer	AK	99645
KULIEVAN JAS P & MARY C	18547 MAN O WAR RD	EAGLE RIVER	AK	99577
KUMPULA JOEL E & GINGER R	HC 89 BOX 329	WILLOW	AK	99688
KUNSELMAN DAVID W EST	1600 W 11TH AVE # 27	ANCHORAGE	AK	99501
KUPHALDT HAROLD J	6543 LINDEN DR	ANCHORAGE	AK	99502
KURANI KURBAN	PO BOX 92413	ANCHORAGE	AK	99509
KURPIUS GARY L	2053 EASTRIDGE DR	ANCHORAGE	AK	99501
KURPJUWEIT NICHOLAS A	PO BOX 940415	HOUSTON	AK	99694
KURTH PATRICIA D	17041 ARIES CT	ANCHORAGE	AK	99516
KURTZ DAVID L & MARIANN W	5901 TRAPPERS TRL RD	ANCHORAGE	AK	99516
KURTZ HELEN	2125 N OLD GLENN HWY	PALMER	AK	99645
KURTZ MARJIL SMITH	7010 CHAD ST	ANCHORAGE	AK	99518
KURZEJESKI ADAM & RONA	1815 MOUNT YUKLA CIR	EAGLE RIVER	AK	99577
KUSE TONY & DEBRA J	PO BOX 240693	ANCHORAGE	AK	99524
KUSH MICHAEL & KATHERYN	12510 ESTUARY CIR	ANCHORAGE	AK	99516
KUTZNER JOHN A SR	PO BOX 521052	BIG LAKE	AK	99652
KVALHEIM Mary	1001 E PULLMAN DR	WASILLA	AK	99654
L. Jacobson	PO Box 343	WILLOW	AK	99688
LABATE BLANCHE	1321 KARLUK ST	ANCHORAGE	AK	99501
LABNO THOS M SR	2841 KEMPTON HILLS DR	ANCHORAGE	AK	99516
LABUFF HATTIE T	PO BOX 202293	ANCHORAGE	AK	99520
LACEY WADE C & BARBARA J	12341 AUDUBON CIR	ANCHORAGE	AK	99516
LaChapelle Francis	505 E Bluff Dr	Anchorage	AK	99501-11
LACHER RANDY K	1175 SNOW HILL AVE	WASILLA	AK	99654

Golder Associates, Inc.

USKH

MSB ASSEMBLY MEMBER

AT&T Alascom

LACK JAS A	2619 JUNEAU ST	ANCHORAGE	AK	99517
LACKEY CONRAD E	PO BOX 520663	BIG LAKE	AK	99652
LACKEY HAROLD L JR & YONG	1620 WATERWOOD CIR	ANCHORAGE	AK	99507
LACKEY JAS	541 TOGIAK CIR	ANCHORAGE	AK	99503
LACY NEAL T & GARY W	PO BOX 2664	PALMER	AK	99645
LADD SHANNA	PO BOX 773433	EAGLE RIVER	AK	99577
LADEGARD RICKI C PLATT	251 S HOYT	ANCHORAGE	AK	99508
LADNER ROBT	730 E 120TH	ANCHORAGE	AK	99515
LAFFEY DONNA G	PO BOX 490	WILLOW	AK	99688
LAFLEUR PEARLA M	HC 89 BOX 376	WILLOW	AK	99688
LAFOURNAISE JOHN J&DALE A	1328 O STREET	ANCHORAGE	AK	99501
LAFRAMBOISE BETTYE JAYNE	8130 LOGANBERRY ST	ANCHORAGE	AK	99502
Lagrone Terry & Peggy	3731 W. 80TH AVE.	ANCHORAGE	AK	99502
LAIRD KEVIN B	8045 LLOYD DR	ANCHORAGE	AK	99502
LAIRD MICHAEL S	1705 MCKINLEY AVE	ANCHORAGE	AK	99517
Laird Paul	360 W. Benson Blvd., Ste 200	Anchorage	AK	99503
LAIRD PAUL A & BETH L	4218 UPPER KOGRU DR	EAGLE RIVER	AK	99577
LAIRD VERNON H& SHIRLEY J	3866 RICHARD EVELYN BYRD	ANCHORAGE	AK	99517
LAKEY CLEMENT ALVIN	PO BOX 4746	PALMER	AK	99645
LALKY ASI	3600 OREGON DR	ANCHORAGE	AK	99517
LAMB BRUCE	PO BOX 520335	BIG LAKE	AK	99652
LAMB EDGAR L JR & L L	3630 N SPRUCE GROUSE CIR	PALMER	AK	99645
LAMBE DAVID PAUL	10900 LAKE OTIS PKWY	ANCHORAGE	AK	99516
LAMORE DAVID A & SHERRY C	7141 OLD HARBOR RD	ANCHORAGE	AK	99504
LAMOREUX REX & CHRISTINE	4103 HAYES ST	ANCHORAGE	AK	99503
LAMOY DEXTER W & BETSY A	11A WOOD HILL RD	OKANOGAN	WA	98840
LAMPL LINDA	3705 ARCTIC BLVD	ANCHORAGE	AK	99503
LAMPRECHT DON & KAREN TR	9201 GROVER DR	ANCHORAGE	AK	99507
LAMSON ORVILLE F & NANCY	PO BOX 521335	BIG LAKE	AK	99652
Lance Barve	HC 89 Box 248	Willow	AK	99688
Lance Brian	222 West 7th Avenue, Room 517	Anchorage	AK	99513
LANCE SYLVIA A	PO BOX 2884	PALMER	AK	99645
Lance Wilson	PO Box 940065	Houston	AK	99694
LAND WALTER L & LORETTA L	PO BOX 954	TALKEETNA	AK	99676
LANDERS CHAS & LINDA	22300 GLACIER VIEW	EAGLE RIVER	AK	99577
LANDES PAUL E & WENDY R	16409 SANDPIPER DR	ANCHORAGE	AK	99516
LANDIS TERENCE P	PO BOX 5478	FORT RICHARDS	AK	99505
LANDMESSER ROBT R & C K	19332 FIRST ST	EAGLE RIVER	AK	99577
LANE ALLAN L	331 EGAVIK DR	ANCHORAGE	AK	99503
LANE ROBT P	9510 ARLENE DR	ANCHORAGE	AK	99502
LANE RYAN & JOY P	PO BOX 3858	PALMER	AK	99645
LANEY JUDY L	8621 E 20TH AVE	ANCHORAGE	AK	99504
LANEY SAMMY W SR	711 ASH PL	ANCHORAGE	AK	99501

Alaska Support Industry Alliance

National Marine Fisheries Services

LANG THERESIA A	2117 BELAIR DR	ANCHORAGE	AK	99517
LANGBERG MORRIS D&THERESA	711 W 86TH AVE	ANCHORAGE	AK	99515
LANGMADE DALE	PO BOX 2912	BETHEL	AK	99559
LANIER JAS F	3200 PROVIDENCE DR	ANCHORAGE	AK	99508
LANKFORD SHEILA MAY	816 OCEANVIEW DR	ANCHORAGE	AK	99515
LANKFORD STEVE & PATRICIA	1326 1/2 MEDFRA	ANCHORAGE	AK	99501
LANNET ERNEST G	PO BOX 110821	ANCHORAGE	AK	99511
LANSZING AYL A C	5901 E 6TH AVE	ANCHORAGE	AK	99504
LAPHAM BRUCE & DIANNE	PO BOX 158	WILLOW	AK	99688
LAPIENE SCOTT C & PATI	PO BOX 771728	EAGLE RIVER	AK	99577
LAPORTE STEVE M&CYNTHIA L	PO BOX 520334	BIG LAKE	AK	99652
LAPP THOS A	2311 JEFFERSON AVE	ANCHORAGE	AK	99517
LAPP THOS A	10611 ABBOTT LOOP RD	ANCHORAGE	AK	99507
LAPPEGARD RICKY WAYNE	PO BOX 102535	ANCHORAGE	AK	99510
LARAUX CHAS R & MELISSA R	PO BOX 2653	BETHEL	AK	99559
LARAUX MARY ANN	PO BOX 465	BETHEL	AK	99559
LARK LYLE B & DELILAH E	PO BOX 521468	BIG LAKE	AK	99652
LARRABEE LAMONT S & E P	11701 RAINBOW AVE	ANCHORAGE	AK	99516
Larrabee Patrick W	P O BOX 4691	PALMER	AK	99645
LARRABEE PATRICK W	P O BOX 1804	PALMER	AK	99645
LARSEN CORY B	1400 W 25TH AVE	ANCHORAGE	AK	99503
LARSEN ROBERT J TRE	2602 W 32ND AVE	ANCHORAGE	AK	99517
LARSON BRIAN E & CHERYL K	9108 GLORALEE ST	ANCHORAGE	AK	99502
Larson Harold	Box 847, Gold Creek via	Talkeetna	AK	99676
Larson Helga	P.O. Box 1869	Palmer	AK	99645
LARSON JUANITA P	PO BOX 92	WILLOW	AK	99688
LARSON LAURA J	6621 IMLACH DR	ANCHORAGE	AK	99502
LARSON LAVERNE	PO BOX 520365	BIG LAKE	AK	99652
LARSON LEON ROYCE & C D J	PO BOX 111223	ANCHORAGE	AK	99511
LARSON LITA	PO BOX 521242	BIG LAKE	AK	99652
Larson Marge	500 W. International Airport Rd, Suite A	Anchorage	AK	99518-11
LARSON MARIANNE	PO BOX 516	WILLOW	AK	99688
LARSON MARTIN L & MYRNA T	PO BOX 626	WILLOW	AK	99688
LARSON RICHARD V	12515 RYA RD	ANCHORAGE	AK	99516
LARSON VICKIE L	8410 RAINTREE CIR	ANCHORAGE	AK	99507
LARUE LANCE	PO BOX 2553	PALMER	AK	99645
LASELLE WARREN & RUTH	127 HAMLET RD	BRANSON	MO	65616
LASHER WM J & PEGGY J	PO BOX 112044	ANCHORAGE	AK	99511
LASKY MICHAEL A JR&BARI E	PO BOX 520850	BIG LAKE	AK	99652
LASSMAN GERRY L	PO BOX 92941	ANCHORAGE	AK	99509
LASTUFKA ANTHONY B&MARY J	PO BOX 112054	ANCHORAGE	AK	99511
LATTO JAS & JULIANNE	10251 VALLEY PARK DR	ANCHORAGE	AK	99507
LAUDON SCOTT L	2819 DAWSON ST	ANCHORAGE	AK	99503

Gold Creek-Susitna  
Mat-Su Borough Planning Commission

American Lung Association of Alaska

LAUGHLIN DANNY	605 W TUDOR RD	ANCHORAGE	AK	99501
LAUGHLIN STEVEN D&MAPSITA	7161 ARLENE ST	ANCHORAGE	AK	99502
LAUNER ADAM H & CAROLYN A	8121 JESTERS PL	ANCHORAGE	AK	99518
Laura Bedard	PO BOX 564	WILLOW	AK	99688
Laura Bedard	13971 N. Michigan St	Willow	AK	99518
LAURANCE JOHN D III	400 W 121ST AVE	ANCHORAGE	AK	99515
LAURIE VIVIAN C	PO BOX 1148	PALMER	AK	99645
LAURIN CHRISTINA JENNIFER	9245 COMMONS PL	ANCHORAGE	AK	99502
LAURIN KEVIN & KRISTIN	19010 SOKOLOF CIR	EAGLE RIVER	AK	99577
LAURIN MARK R	8301 BRIARWOOD STE 201	ANCHORAGE	AK	99518
LAUTARET AARON P	9131 CATHEDRAL PL	ANCHORAGE	AK	99507
LAUTRUP JOEL W & DAVELLE A	5794 E MORNING MIST DR	PALMER	AK	99645
LAUWERS CALVIN D & LESLIE	7154 LOWELL CIR	ANCHORAGE	AK	99502
LAVALLEE ROBT J & K S	1075 SUSITNA DR	WASILLA	AK	99654
LAVIGNE ROBT W & JUDY L	7641 BETHANY CIR	ANCHORAGE	AK	99507
LAVIGUEUR MICHAEL A	1126 E MCKEE LN	WASILLA	AK	99654
LAWALTER JEROME A	PO BOX 354	PALMER	AK	99645
Lawer David	P.O. Box 100720	Anchorage	AK	99510-07
LAWHEED JAS H & CHARLOTTE	103 RAINBOW DR	OXNARD	CA	93033
LAWRENCE FAWN JACKSON	6630 LAKEWAY DR	ANCHORAGE	AK	99502
LAWRENCE GEO R & CHARITY D	1135 W HOLIDAY DR	WASILLA	AK	99654
LAWSON EUGENE & MARY A	624 N PINE ST	ANCHORAGE	AK	99508
LAWSON JASON R	1430 MAJELLA CIR	ANCHORAGE	AK	99515
Lawton Joe	2075 Glenn Hwy	Palmer	AK	99645
LAWTON JOS	549 E CARIBOU AVE	PALMER	AK	99645
Lay Jolie	Attn: APVR-RDL-OSV 977 Davis Hwy	Ft. Richardson	AK	99505-70
LAYMAN CHAS D & MARY M	PO BOX 520030	BIG LAKE	AK	99652
LEA SHAUN	614 W 87TH AVE	ANCHORAGE	AK	99515
Leach James	P.O. Box 520682	Big Lake	AK	99652
LEACH JAS B III DVM & S A	PO BOX 520683	BIG LAKE	AK	99652
LEACH NORMA M	4010 MERRILL DR	ANCHORAGE	AK	99517
LEAF JOHN R E & NAMIKO	2931 LEXINGTON CIR	ANCHORAGE	AK	99502
LEARY MAURA A	5861 E CAMBORNE DR	PALMER	AK	99645
LEARY RICHARD & MICHELLE	8110 WELLSLEY CT	ANCHORAGE	AK	99507
Leathard Pete	3601 C St., Ste. 1000	Anchorage	AK	99503
LEAVENS EUGENE R	1933 FOREST PARK DR	ANCHORAGE	AK	99517
LEBARON KAYLEN	PO BOX 230414	ANCHORAGE	AK	99523
LeClair John	2347 Azurite Court	Anchorage	AK	99507
LECLAIR ROCK R & ROCHELLE	PO BOX 2763	PALMER	AK	99645
LEDERHOS DONALD D & KAREN	8110 RESURRECTION DR	ANCHORAGE	AK	99504
LEE ANDY A	5706 FIJI ST	ANCHORAGE	AK	99507
LEE BLAINE K & CINDY D	PO BOX 3194	PALMER	AK	99645
LEE CARLA JANE	PO BOX 1676	PALMER	AK	99645

First National Bank

Alaska State Fair

US Army AK Directorate of Logistics

Big Lake Susitna Veterinary

VECO Corporation

Chadux

LEE CODY	2171 IVAN DR	ANCHORAGE	AK	99507
LEE HARRY R & MARY A	8701 SPENDLOVE DR	ANCHORAGE	AK	99516
LEE HARRY X	6609 WHISPERS LOOP	ANCHORAGE	AK	99504
LEE JAE WON	3501 ALAMOSA DR	ANCHORAGE	AK	99502
LEE JEAN E OGILVIE	3115 SHELDON JACKSON ST	ANCHORAGE	AK	99508
LEE JEREMY S & LYNN M	2358 S TACKETTS CIR	PALMER	AK	99645
Lee John	P.O. Box 2409	Palmer	AK	99645
LEE KAO & YOJUA	3317 TARWATER AVE	ANCHORAGE	AK	99508
LEE MELVIN H	10130 NANTUCKET LOOP	ANCHORAGE	AK	99507
LEE MIKEL E	8449 E 20TH AVE	ANCHORAGE	AK	99504
LEE XAI	734 NORTHLAND ST	ANCHORAGE	AK	99508
LEE ZACK W & GRACE A	PO BOX 281	WILLOW	AK	99688
LEEMAN PATRICIA	PO BOX 1644	BETHEL	AK	99559
LEFEBVRE RICHARD A	1921 SUNRISE DR	ANCHORAGE	AK	99508
LEFEVRE PATRICIA N	2811 E 48TH AVE	ANCHORAGE	AK	99507
LEFFINGWELL MICHAEL & M A	PO BOX 521453	BIG LAKE	AK	99652
LEFOR JOHN D & AMY L	1846 COLUMBINE ST	ANCHORAGE	AK	99508
LEHE THOS R JR & HEATHER	2876 S CHARMING VALLEY	PALMER	AK	99645
LEIF MARK H	PO BOX 200247	ANCHORAGE	AK	99520
LEIGH ALAN F & PATRICIA A	PO BOX 670137	CHUGIAK	AK	99567
LEIGH TIMOTHY F & KATHLEEN	PO BOX 803	WILLOW	AK	99688
LEISER STEPHEN S & CLOVIS	PO BOX 210254	ANCHORAGE	AK	99521
LEKANDER LANCE	2260 VANDERBILT CIR	ANCHORAGE	AK	99508
LEKISCH PETER A	1403 P ST	ANCHORAGE	AK	99501
Leland Marilyn	703 W. Tudor Road, Suite 200	Anchorage	AK	99503
LELLE DONALD J & WENDI	PO BOX 615	WILLOW	AK	99688
LEMAITRE MICHAEL P & PEGGY	18637 MT PLOVER CIR	ANCHORAGE	AK	99516
LEMM SHIRLEY A SNELL	PO BOX 133	ANCHOR POINT	AK	99556
LENHART DENNIS E & DONNA	2839 S CHARMING VALLEY	PALMER	AK	99645
LENORMAN DAVID B & STACY B	410 KAYAK DR	ANCHORAGE	AK	99515
LENORMAN MICHAEL J & M K	3423 W 100TH AVE	ANCHORAGE	AK	99515
LENT CHAS T & JENNIFER K	PO BOX 209	WILLOW	AK	99688
LENTZ PAUL B & JEANNE M	PO BOX 4855	PALMER	AK	99645
LEONARD FRANCES J	4147 ABBOTT RD	ANCHORAGE	AK	99507
LEONARD JAMIE	HC 89 BOX 247	WILLOW	AK	99688
LEONARD MICHAEL L & KORRIN	PO BOX 6373	PALMER	AK	99645
LEONE GEORGETTE E	PO BOX 520177	BIG LAKE	AK	99652
Leppanen Barb	P.O. Box 364	Sutton	AK	99674-04
LeRoi & Margaret Heaven	HC 31 Box 5113	Wasilla	AK	99654
LERVAAG ROBT W & JOAN D	4421 EDINBURGH DR	ANCHORAGE	AK	99515
LESH NANCY L	1828 BOOTLEGGERS COVE DR	ANCHORAGE	AK	99501
LESHER LAURENCE A	PO BOX 521693	BIG LAKE	AK	99652
LESLIE MARLENE D	3700 CLAY PRODUCTS DR	ANCHORAGE	AK	99517

Alaska Power Association

Sutton/Alpine Civic Club



LETASKY CAROL L	5990 E GERSHMEL LOOP	PALMER	AK	99645
LEUER DAVID J & MARELDINE	2955 BASS ST	ANCHORAGE	AK	99507
Leveen Larry S.	P O BOX 415	PALMER	AK	99645
LEVINSON ANNA L	PO BOX 689	WILLOW	AK	99688
Lewanski Jerry	555 W. 7th Avenue, Ste 1380	Anchorage	AK	99501
LEWIS DAVID S & A G	3120 BROOKVIEW ST	ANCHORAGE	AK	99504
LEWIS JAS F & J'LENE M	PO BOX 772423	EAGLE RIVER	AK	99577
Lewis Jim	P.O. Box 772423	Eagle River	AK	99577
LEWIS JOHN E	PO BOX 2866	PALMER	AK	99645
LEWIS JOHN GLEN	PO BOX 211712	ANCHORAGE	AK	99521
LEWIS LEONARD N	1041 C S 2974	BLANCHARD	OK	73010
LEWIS LINDA	9011 EMERALD DR	ANCHORAGE	AK	99502
LEWIS LYLE D & LANELLE C	7643 E 20TH AVE	ANCHORAGE	AK	99504
LEWIS MARK	1175 FILLMORE ST	DENVER	CO	80206
LEWIS MARK A & KATHLEEN A	3431 PURDUE ST	ANCHORAGE	AK	99508
LEWIS NED WM	3801 MCCAIN LOOP	ANCHORAGE	AK	99503
LEWIS RICHARD J	344 DEERFIELD DR	ANCHORAGE	AK	99515
LEWIS ROBT T	PO BOX 521268	BIG LAKE	AK	99652
Lewis Sharon	1617 Industrial	Palmer	AK	99645
LEWIS STEPHEN A	PO BOX 520874	BIG LAKE	AK	99652
LEWIS VICKI L	9821 SPRING HILL DR	ANCHORAGE	AK	99507
LEYDON THOS F & ROSEANNE M	8527 EMERALD ST	ANCHORAGE	AK	99502
LEYDON TOM	3300 W DIMOND BLVD	ANCHORAGE	AK	99502
L'HUILLIER DONALD C	P O BOX 101	TALKEETNA	AK	99676
LIBBY LAWRENCE W & WILDA M	2525 W 69TH AVE	ANCHORAGE	AK	99502
Liebersbach David	P.O. Box 5750	Ft. Richardson	AK	99505-57
LIEBSACK EDW WM	2205 LINCOLN AVE	ANCHORAGE	AK	99517
Liggett Jay	240 W. 5th Avenue, Ste 114	Anchorage	AK	99501
LIGHT JOHN	4300 NATRONA AVE	ANCHORAGE	AK	99516
LIKAR JOHN	PO BOX 520594	BIG LAKE	AK	99652
LILJA NELS A & GLORIA M	PO BOX 520774	BIG LAKE	AK	99652
LILLEY HORACE EDW	PO BOX 106	UNALAKLEET	AK	99684
LILLY JOHN E & CYNTHIA Y	PO BOX 521224	BIG LAKE	AK	99652
LILLY ROBT E & CARA L	PO BOX 520396	BIG LAKE	AK	99652
LIM GUAT-NGOH	PO BOX 521862	BIG LAKE	AK	99652
Limani Medzit	800 W. Evergreen Street	Palmer	AK	99645
LIMBERG DAVID L	PO BOX 520202	BIG LAKE	AK	99652
LINCOLN LAURIE J	3910 IMAGE DR	ANCHORAGE	AK	99504
Lind Destry	633 E. 81st Avenue	Anchorage	AK	99518
LIND FRED F	6422 REED LN	ANCHORAGE	AK	99502
LIND MARC & BRENDA	PO BOX 670182	CHUGIAK	AK	99567
Linda	1950 Bay View Dr.	Wasilla	AK	99654
Linda	1950 Bay View Dr.	Wasilla	AK	99654

Alaska Dept. of Natural Resources

Kitchens, Ect.

Alaska Div. Homeland Security & Emergency Manag

National Park Service

Limani's Bar & Grill

Consolidated Enterprises

Organization	Address	City	Zip
Linda Oxley, Chair	PO Box 1027	WILLOW	99688
LINDAU JAS C	PO BOX 231656	ANCHORAGE	99523
LINDBECK MICHAEL C	2813 SNUG HARBOR CIR	ANCHORAGE	99507
LINDBERG MICHAEL J	23215 NORTHWOOD DR	CHUGIAK	99567
LINDBERG TOM W & WENDY	951 JOHAM CIR	ANCHORAGE	99515
LINDBLOM DAWSON L & Y K	17142 FOOTHILL AVE	EAGLE RIVER	99577
LINDEEN MICHAEL & KAREN	PO BOX 232764	ANCHORAGE	99523
LINDEMULDER RONALD B	5450 E 99TH AVE	ANCHORAGE	99516
LINDGREN RICH & TINA	11751 MARY AVE	ANCHORAGE	99515
LINDGREN RICHARD J & L W	PO BOX 520567	BIG LAKE	99652
LINDHOLM STEPHANIE L	1050 BALSAM CIR	WASILLA	99654
Lindman CHAD R & MELISSA M	1206 OXFORD DR	ANCHORAGE	99503
LINDNER CURTIS W	18930 WHIRLAWAY RD	EAGLE RIVER	99577
LINDOW WM P&LINDOW THOS D	8220 OPAL DR	ANCHORAGE	99502
LINDQUIST DAVID L & SUSAN	PO BOX 670016	ANCHORAGE	99567
LINDSEY DAVE & ELIZABETH	13001 SHELBURNE RD	ANCHORAGE	99516
LINK ARNOLD G	3447 ABBOTT RD	ANCHORAGE	99507
LINK ARNOLD G JR	2220 DUVOY CT	ANCHORAGE	99502
LINK JAS A & LINK ELAINE M	8118 FRANK ST	ANCHORAGE	99518
LINK SHARON	13126 BAY CIR	ANCHORAGE	99515
LINKHART STEVEN E& TERESA	PO BOX 195	WILLOW	99688
LINKOVICH GERALD M	2224 CULVER PLACE	ANCHORAGE	99503
Linn K. Allen	P O BOX 276	PALMER	99645
LINNELL DENNIS R	6967 TOWN & COUNTRY PL	ANCHORAGE	99502
LINVILLE JEANNIE L DAVIS	3352 W 82ND AVE	ANCHORAGE	99502
LIPPS HERBERT L & JOANN R	2311 TASHA DR	ANCHORAGE	99502
LIPPS THOS & ANITA	5021 CURRIN CIR	ANCHORAGE	99516
LIPSE KEITH & LINDA	PO BOX 520976	BIG LAKE	99652
LIRETTE JOHN L & BECKY JO	PO BOX 671109	CHUGIAK	99567
LISKA KENT B & KATHLEEN M	3516 PATTERSON	ANCHORAGE	99504
LISSNER MINA	7360 E DENELLE ST	PALMER	99645
LIST JEROME O	3340 PROVIDENCE DR	ANCHORAGE	99508
LISTER JAS F	6940 STANLEY DR	ANCHORAGE	99518
LISTON-CUNNINGHAM VANESSA	HC 89 BOX 482	WILLOW	99688
LITTLE ALBERT C & JANIE	PO BOX 521292	BIG LAKE	99652
LITTLE KENNETH L JR	PO BOX 821	WILLOW	99688
LITTLE WM R & ELIZABETH L	1250 PINECONE CIR #37	WASILLA	99654
LITTLETON ROBERT B	506 NATHAN	ANCHORAGE	99518
Lively Brit	259 South Alaska Street	Palmer	99645
LIVENGOOD ALLAN L & T S	3004 W 29TH AVE	ANCHORAGE	99517
LIZOTTE RAYMOND H & ELLEN	PO BOX 81	WILLOW	99688
LLANEZA MANUEL G & L L	PO BOX 190853	ANCHORAGE	99519
LLEWELLYN KAREN	3712 JAMES DR	ANCHORAGE	99504

Willow Area Community Organization

Mapmakers Alaska

LLOYD BRIAN E	3906 COSMOS DR	ANCHORAGE	AK	99517
LLOYD BRUCE & MELISSA Lochner Dick	18631 LITTLE CAPE CIR 10593 Sun Bean Drive	EAGLE RIVER Eagle River	AK AK	99577 99577
LOCKARD LANCE C & LARRELL	PO BOX 111513	ANCHORAGE	AK	99511
LOCKARD LANCE C II	3434 S PENINSULA DR	BIG LAKE	AK	99652
LOCKARD LANCE C JR	PO BOX 212923	ANCHORAGE	AK	99521
LOCKER PHILLIP JR & JANICE	6790 ROUND TREE DR	ANCHORAGE	AK	99516
LOCKHART JOHNNIE C	6521 GREENWOOD ST	ANCHORAGE	AK	99518
LOEFFLER GEOFFREY S&SUSAN	PO BOX 521421	BIG LAKE	AK	99652
LOESCHE DONALD E&ROMOLA A Loew John	PO BOX 520156 HC04-9166	BIG LAKE Palmer	AK AK	99652 99645
LOEW JOHN A	15091 E LAZY LADY LN	PALMER	AK	99645
LOEW WM P & ELAINE J	PO BOX 111888	ANCHORAGE	AK	99511
LOEWE MATTHEW F	PO BOX 232491	ANCHORAGE	AK	99523
LOEWEN RICK	14735 PARK HILLS DR	ANCHORAGE	AK	99516
LOFGREN CLINTON D	5101 E 142ND AVE	ANCHORAGE	AK	99516
LOFLAND DAVE K& CATHERINE	PO BOX 43	WILLOW	AK	99688
LOFTIN PATRICIA LANDIS	HC 89 BOX 208	WILLOW	AK	99688
LOGAN PHILLIP M	6961 WINDSOR PL	ANCHORAGE	AK	99502
Logan Rebecca	360 W. Benson Blvd., Ste 200	Anchorage	AK	99503
LOGAN THOS & GRACE	PO BOX 520253	BIG LAKE	AK	99652
LOGAN VERNON DAVID	PO BOX 521365	BIG LAKE	AK	99652
LOGSDON BERT	8901 E KIVA WAY	PALMER	AK	99645
LOHR EDW M	3915 MARTAGON CIR	ANCHORAGE	AK	99516
LOHR GARY D	PO BOX 770503	EAGLE RIVER	AK	99577
LOMONACO MICHAEL S	PO BOX 203206	ANCHORAGE	AK	99520
Long Carl Rembly and Ms. Petrina	P.O. Box 140144	Anchorage	AK	99514
LONG CHARLIE	3606 THOMPSON AVE	ANCHORAGE	AK	99508
LONG CHESTER K & MARY ANN	9200 CAMPBELL TERRACE DR	ANCHORAGE	AK	99515
LONG JOHN M & JUANNA R	PO BOX 3683	PALMER	AK	99645
LONG NORWOOD A	1835 BANNISTER RD	ANCHORAGE	AK	99508
LONG STANLEY A & F K	3436 UPLAND DR	ANCHORAGE	AK	99504
LONGACRE DOUG & SUZETTE P	12600 RIDGEWOOD RD	ANCHORAGE	AK	99516
LONGLET JOYCE	3510 GLENN-DON CIR	ANCHORAGE	AK	99504
LONGMIRE LANCE P	12161 GALENA CIR	ANCHORAGE	AK	99507
Longnecker Larry	2075 Glenn Hwy.	Palmer	AK	99645
LONGORIA CARMEN	4010 MACINNES ST	ANCHORAGE	AK	99508
LONGTIN WENDY	5460 S BODENBURG LOOP	PALMER	AK	99645
LOOPSTRA JOHN F & H M	2970 DARTMOUTH DR	ANCHORAGE	AK	99508
LOPEZ GUY & TONI	2030 SHORE DR	ANCHORAGE	AK	99515
LORDS DENNIS R& LUCINDA R	PO BOX 675	WILLOW	AK	99688
Lorenzen Dr. Peter	11901 Business Blvd #B108	Eagle River	AK	99577
LORENTZEN LESLIE O & JEAN	9100 ARLON	ANCHORAGE	AK	99507

Associated Builders & Contractors

Alaska State Fair

Alaska Chiropractic and Therapy

LORENZEN DONNA	PO BOX 520081	BIG LAKE	AK	99652
LORENZEN JOE	PO BOX 521497	BIG LAKE	AK	99652
Lori Okuley	2531 Horseshoe Lake Road	Big Lake	AK	99652
Lori Tipton	PO Box 113396	Anchorage	AK	99511
Lori Tipton	PO Box 113396	Anchorage	AK	99511
LOTT DAULTON M&VIRGINIA D	PO BOX 521856	BIG LAKE	AK	99652
Lounsbury Loren	705 W. 6th Avenue	Anchorage	AK	99501
LOUNSBURY RICHARD R & B C	601 W 20TH AVE	ANCHORAGE	AK	99503
Lovas Thomas	5840 Azalea Dr	Anchorage	AK	99516-43
LOVE KATHERINE R	PO BOX 2733	PALMER	AK	99645
LOVE WAYNE P & DONNA	PO BOX 520729	BIG LAKE	AK	99652
Lovett John	222 W. 7th Avenue, # 14	Anchorage	AK	99501
LOVETT JOHN	PO BOX 110688	ANCHORAGE	AK	99511
LOWBER JOHN & MARY CAY	3342 SOUTHBLUFF CIR	ANCHORAGE	AK	99515
LOWE HOPE L	2934 EMORY ST	ANCHORAGE	AK	99508
LOWE JACK W	1225 ELEGANTE LN	ANCHORAGE	AK	99501
LOWE SHARRON E	PO BOX 520400	BIG LAKE	AK	99652
LOWMAN GLENN C	1310 P ST	ANCHORAGE	AK	99501
LOWTHER VIOLET M	823 VALLEY ST	ANCHORAGE	AK	99504
LOY JEANELLE E	3921 TRURO DR	ANCHORAGE	AK	99507
LOYD MARILYN	PO BOX 520409	BIG LAKE	AK	99652
LOYER RITA D	14000 E OUTER SPRINGER LP	PALMER	AK	99645
LUCAS LAURIE ANN	4031 A LUNAR DR	ANCHORAGE	AK	99504
LUCAS MICHAEL R & CATHY S	PO BOX 1741	BETHEL	AK	99559
LUCERO GILBERT M & DIANA	7550 E SAPHIRE RD	PALMER	AK	99645
LUCIANO GINA L	3331 W 31ST	ANCHORAGE	AK	99517
Lucier Panu	421 W. 1st Avenue, Suite 300	Anchorage	AK	99501
LUCORE JEFFRIE T	PO BOX 2554	PALMER	AK	99645
LUCUS ROBT LEROY	1200 N LUCUS RD	WASILLA	AK	99654
LUCZYCKI JAS M& REBECCA A	317 GAYLENE CIR	ANCHORAGE	AK	99504
LUDWIG NEIL N& SHAUNDRA L	1020 S EDITH CT	CHANDLER	AZ	85249
LUERAS ROBT E & LINDA J	8017 CRANBERRY ST	ANCHORAGE	AK	99502
LUEVANO JACOB A	PO BOX 1753	PALMER	AK	99645
LUFFBERRY J RANDALL & D L	263 S ALASKA ST	PALMER	AK	99645
LUITEN KURT D	2451 HIGHGATE CIR	ANCHORAGE	AK	99502
LUKIN MAX & EMILY	PO BOX 2692	PALMER	AK	99645
Lulie Williams, President	PO Box 240573	ANCHORAGE	AK	99501
LUM HELEN	PO BOX 670887	CHUGIAK	AK	99567
LUND JOYCE A	1720 BURLINGTON ST	ANCHORAGE	AK	99508
LUNSFORD WARREN C&DEBORAH	PO BOX 100228	ANCHORAGE	AK	99510
LUTH WM A & NATALIE J	PO BOX 428	WILLOW	AK	99688
LUTHER ERIC L	PO BOX 278	WILLOW	AK	99688
LUTHER HOWARD C & DIANA M	PO BOX 521137	BIG LAKE	AK	99652





MANNING TERRENCE & TERESE	PO BOX 671888	CHUGIAK	AK	99567
MANRIQUE JOHANNES J & P L	1831 S HIGH RD	PALMER	AK	99645
MANSON DAN'L J & ANGELA J	8124 E 4TH AVE	ANCHORAGE	AK	99504
MANUAL WILMA M	PO BOX 509	BETHEL	AK	99559
MARAKAMI MARK H	PO BOX 521088	BIG LAKE	AK	99652
Marc Okuley	P.O.Box 520563	Big Lake	AK	99652
MARCHANT FREDERICK T & D D	PO BOX 723	WILLOW	AK	99688
MARCINEK THADDEUS & V F	4301 UPPER KOGRU PL	EAGLE RIVER	AK	99577
MARCINIAK GARY S&EVELYN E	PO BOX 111261	ANCHORAGE	AK	99511
Marcy Coman, President	PO Box 520067	BIG LAKE	AK	99652
MARGESON FLOYD E JR & S M	PO BOX 520452	BIG LAKE	AK	99652
Marie Sorka	PO Box 849	WILLOW	AK	99688
Marjorie Bellringer	25655 Buckshot Drive	Palmer	AK	99645
MARKET ANN T	PO BOX 91287	ANCHORAGE	AK	99509
MARKLE STEVEN LON	4909 E 43RD	ANCHORAGE	AK	99508
MARKS WILLIAM E & DONNA L	PO BOX 329	WILLOW	AK	99688
MARLOW BRENDA L SIEFKER	7960 UPPER O'MALLEY RD	ANCHORAGE	AK	99507
MARMAN DAN'L J & MARGARET	12161 GALENA CIR	ANCHORAGE	AK	99507
MARONEY RUTH L.	6325 ROSEMONT DR	ANCHORAGE	AK	99516
MAROTTA DANNY L JR	PO BOX 520664	BIG LAKE	AK	99652
MAROULIS JOHN& ANNA MARIE	1621 STANTON AVE	ANCHORAGE	AK	99508
MARPLE MATTHEW ALLEN	PO BOX 940427	HOUSTON	AK	99694
MARQUARDT A JOHN& DIANE G	PO BOX 92621	ANCHORAGE	AK	99509
MARQUARDT JAY J	PO BOX 91494	ANCHORAGE	AK	99509
MARQUEZ CHERYL L	2405 N TAHITI LOOP	ANCHORAGE	AK	99507
MARQUEZ JAS	PO BOX 521393	BIG LAKE	AK	99652
MARQUEZ JOSE L & CARMEN E	321 VILLA CIR	ANCHORAGE	AK	99504
MARR IVAN	PO BOX 521382	BIG LAKE	AK	99652
Marrs Carl Marrs	2239 Sorbus Way	Anchorage	AK	99508
MAPSH JAS J & D'AVA A	1806 TERREBONNE LOOP	ANCHORAGE	AK	99502
MARSH JERAMI DEAN	PO BOX 202136	ANCHORAGE	AK	99520
Marsh Ken	PO BOX 13011	TRAPPER CREEK	AK	99683
MARSHALL EILEEN V	1131 M CT	ANCHORAGE	AK	99501
MARSHALL PAMELA ANN	3334 DISCOVERY BAY DR	ANCHORAGE	AK	99515
MARSHALL SHERALL L JR & S	19846 E HENKINS RD	ANCHORAGE	AK	99567
MARSHALL STANLEY K & C L	3621 E 66TH AVE	ANCHORAGE	AK	99507
MARTENS DANA EDMUND	PO BOX 110490	ANCHORAGE	AK	99511
MARTENS JERRY C & RONDA L	11901 GILLETTE DR	ANCHORAGE	AK	99516
MARTENS LEONARD M & BETTY	12611 BEACHCOMBER DR	ANCHORAGE	AK	99515
MARTENS PAULINE R	2104 ARCADIA DR	ANCHORAGE	AK	99517
MARTIN DENNIS A & JUDY L	6920 CHAD ST	ANCHORAGE	AK	99518
Martin Eiv	9373 Hiland Drive	Eagle River	AK	99577
MARTIN JOHN PHILLIP	4236 IRENE DR	ANCHORAGE	AK	99504

Big Lake Chamber of Commerce

Southfork Community Council

MARTIN JOHN T	655 N SMITH RD	PALMER	AK	99645
MARTIN JON S & BRENDA M	4615 S MCKECHNIE LOOP	PALMER	AK	99645
MARTIN RICHARD W&TERESA A	PO BOX 670114	CHUGIAK	AK	99567
MARTIN SAMUEL A	7624 CHAIMI LOOP	ANCHORAGE	AK	99504
MARTIN TANJA	PO BOX 4478	PALMER	AK	99645
MARTIN TIM R & TINA F	6801 SAMUEL CT	ANCHORAGE	AK	99516
MARTINEZ ARMANDO & SANDRA	PO BOX 520203	BIG LAKE	AK	99652
MARTINEZ KIM	2540 SEBRING CIR	ANCHORAGE	AK	99516
MARTINSON DAVID A& CHERYL	9016 SNOWY OWL CIR	ANCHORAGE	AK	99507
MARTONE FRANK P	1720 ABBOTT RD	ANCHORAGE	AK	99507
MARTUSHEFF MARK	PO BOX 223	ANCHOR POINT	AK	99556
Marushack Joseph	P.O. Box 100360	Anchorage	AK	99510
MARVIN FREDERICK L & L	1030 DENALI ST	ANCHORAGE	AK	99501
MARVIN GEORGE & CHRISTINA	1940 PATTERSON	ANCHORAGE	AK	99504
Mary Anderson	PO Box 870854	Wasilla	AK	99687
Mary Anderson	PO Box 870854	Wasilla	AK	99687
Mary Miller	10661 Cutter Circle	Anchorage	AK	99515
MASKER WILLARD H & ANNE E	11153 KASKANAK DR	EAGLE RIVER	AK	99577
MASON GARY L & AUDREY G	18540 UPPER MCCRARY	EAGLE RIVER	AK	99577
MASON JACOB J	PO BOX 143495	ANCHORAGE	AK	99514
MASON JOHN D & RUTH S	2350 SAINT ELIAS DR	ANCHORAGE	AK	99517
MASON ROBT E & LENA D	2926 CAMPBELL AIRSTRIP RD	ANCHORAGE	AK	99504
MASSEY W C	1200 N KIMBALL AVE	SOUTHLAKE	TX	76092
MASSIE DOUGLAS ABRAHAM	PO BOX 1109	WILLOW	AK	99688
Masteller Mark	PO BOX 965	PALMER	AK	99645
MASUDA KEISO	6500 E 12TH AVE	ANCHORAGE	AK	99504
MATCHETT HELEN P	3542 ALEXANDER AVE	ANCHORAGE	AK	99508
MATHENY CAROL R	HC 89 BOX 204	WILLOW	AK	99688
MATHES DUANE	11838 Hagion ShoresDrive	Willow	AK	99688
MATHES DUANE	11838 Hagion ShoresDrive	Willow	AK	99688
MATHES DUANE E & WILMA J	PO BOX 777	WILLOW	AK	99688
MATHES GARY T	PO BOX 22273	ANCHORAGE	AK	99522
MATHEWS ROBT J	1807 ROOSEVELT DR	ANCHORAGE	AK	99517
MATHIESON EDW	1007 N LAVENTURE RD	MT VERNON	WA	98237
Mathis Joe	235 E. 8th Avenue, Suite #200	Anchorage	AK	99501
MATHIS JOE B	HC 89 BOX 543	WILLOW	AK	99688
MATSON JESSICA S	1130 S CORKEY ST	WASILLA	AK	99654
MATLOCK KEVIN PAUL	PO BOX 671048	CHUGIAK	AK	99567
MATLOCK TIMOTHY K	11833 E HELEN DR	PALMER	AK	99645
MATSUTANI OSAMU H&GRACE D	3540 E 142ND AVE	ANCHORAGE	AK	99516
MATTHEWS CURTIS D	PO BOX 520785	BIG LAKE	AK	99652
MATTHEWS DAN'L R& CAROLYN	8939 E LEXINGTON ST	PALMER	AK	99645
Matthews David	301 W Northern Lights Ste 300	Anchorage	AK	99503

ConocoPhillips

MSB PLANNING COMMISSION

American Red Cross of Alaska

H.C. Price Co.



MATTHEWS DAVID J&VICTORIA Matthews Gary	PO BOX 1173	PALMER	AK	99645
MATTHEWS MAURICE K	4048 Laurel St # 203	Anchorage	AK	99508
MATTHEWS ROSEMARY	2804 W NORTHERN LGTS BLVD	ANCHORAGE	AK	99517
MATTHEWS SUZAN R TITUS	PO BOX 520807	BIG LAKE	AK	99652
MATTHEWS W W JR & DONNA	6739 AIR GUARD RD	ANCHORAGE	AK	99502
MATTHEWS WM M JR	8916 GLORALEE ST	ANCHORAGE	AK	99502
Matthias Karen	PO BOX 91216	ANCHORAGE	AK	99509
MATTHESEN BARRY	310 K Street, Suite 220	Anchorage	AK	99501
MATTINGLEY BERNARD & J A	2860 SEAFARER LOOP	ANCHORAGE	AK	99516
MATTIX STEPHEN L	6200 PETERSBURG ST	ANCHORAGE	AK	99507
MATTSON BRUCE H & KAREN A	PO BOX 520528	BIG LAKE	AK	99652
MATVEEV GEO & HELENA	PO BOX 233	WILLOW	AK	99688
MAURER RICK & JENNIFER	5571 ALORA LOOP	ANCHORAGE	AK	99504
MAXWELL CRAIG A & L A	6913 CAPE LISBURNE	ANCHORAGE	AK	99504
MAXWELL EDDIE JR & SUSAN	PO BOX 520173	BIG LAKE	AK	99652
MAXWELL GLENN A	PO BOX 242573	ANCHORAGE	AK	99524
MAXWELL LOU ANNE	PO BOX 520753	BIG LAKE	AK	99652
MAXWELL ROBT	1780 THUJA AVE	ANCHORAGE	AK	99507
MAXWELL TAL	PO BOX 670435	CHUGIAK	AK	99567
MAY BARBARA J RODERICK	521 74TH AVE	ANCHORAGE	AK	99518
MAY JAMES R	2410 BRITTANY CIR	ANCHORAGE	AK	99504
MAY JAMES R	2340 CASEY CUSACK LOOP	ANCHORAGE	AK	99515
MAY JEFFERY	9810 HOMESTEAD TRL	ANCHORAGE	AK	99516
MAY LELAND & REBECCA	7521 ROVENNA ST	ANCHORAGE	AK	99518
MAY WARNER T & CAROLINE E	9540 N GOVERNMENT CRK	PALMER	AK	99645
MAYER LOIS ALDEA L/E	7615 CHAIMI LOOP	ANCHORAGE	AK	99514
MAYEUR CLINT & TERI-JO	1234 N EL CASON ST	VISALIA	CA	93291
MAYFIELD DAN	PO BOX 672406	CHUGIAK	AK	99567
MAYFIELD JAS D& CATHERINE	P.O. Box 520705	BIG LAKE	AK	99652
MAYFIELD SANDRA K	2471 CHANDALAR DR	ANCHORAGE	AK	99504
MAYNARD MYRNA M	PO BOX 142454	ANCHORAGE	AK	99514
MAYO CHRISTINE	2237 FOREST PARK DR	ANCHORAGE	AK	99517
Mayo Desi	PO BOX 200093	ANCHORAGE	AK	99520
MAYO LARRY R	1740 S. Chugach Street	Palmer	AK	99645
MAYO LIDA P	PO BOX 520883	BIG LAKE	AK	99652
MAZE CONNIE L& BOWLUS R K	PO BOX 868	WILLOW	AK	99688
MCALAEVEY GERALD J & D L	PO BOX 520242	BIG LAKE	AK	99652
MCALLEN CHARLIE	18027 TONSINA CT	EAGLE RIVER	AK	99577
MCALLEN DAVID E & JIMI A	2422 TULANE ST	ANCHORAGE	AK	99504
MCALLEN DON & CAROL	351 TURPIN ST	ANCHORAGE	AK	99504
MCALLEN JAMES J & BETH A	4831 KLONDIKE AVE	ANCHORAGE	AK	99508
MCALLISTER JEFFREY L & G H	11755 WILDERNESS DR	ANCHORAGE	AK	99516
	9001 JERNON ST	ANCHORAGE	AK	99515

Alaska School Activities Association

Canadian Consulate

Matanuska Telephone Association

MCALPINE BRADLEY E	PO BOX 520110	BIG LAKE	AK	99652
MCBRIDE A DALE & GLORIA	18848 MAN O WAR RD	EAGLE RIVER	AK	99577
MCBRIDE BRANDON C	7017 HENDERSON LOOP	ANCHORAGE	AK	99507
MCBRIDE JONATHAN D & J D	HC 89 BOX 352	WILLOW	AK	99688
MCBRIDE KEVIN M & KATHLEEN	1140 W BOLD PEAK CIR	WASILLA	AK	99654
MCBRIDE SHANNON K	PO BOX 3811	PALMER	AK	99645
MCBRYDE BRETT M & LINDA M	18710 UPPER MCCRARY RD	EAGLE RIVER	AK	99577
MCCABE MICHAEL W	HC 89 BOX 473	WILLOW	AK	99688
MCCAIN CAROLYN L	PO BOX 574	WILLOW	AK	99688
MCCAIN GEO L & KAREN A	2510 KILKENNY CIR	ANCHORAGE	AK	99504
MCCALL LINDA	PO BOX 520251	BIG LAKE	AK	99652
MCCARROLL ALAN W	PO BOX 521840	BIG LAKE	AK	99652
MCCARROLL JUSTIN G & R I	PO BOX 520154	BIG LAKE	AK	99652
MCCART DICKIE R & JANET M	9440 BASHER DR	ANCHORAGE	AK	99507
MCCARTER ROBT F JR & L M	23046 LIVE ALDER	CHUGIAK	AK	99567
MCCARTNEY EDW ALLEN	7601 DOVER AVE	ANCHORAGE	AK	99504
MCCARTNEY MICHAEL & JULIE	PO BOX 3802	PALMER	AK	99645
MCCARTY A MARLENE	1115 W 20TH AVE	ANCHORAGE	AK	99503
MCCASKELL JAS S	177-10 MICHEAL CT	ANCHORAGE	AK	99504
MCCASLIN PETER J & KIMBERLY	PO BOX 520035	BIG LAKE	AK	99652
MCCELLAND DAVID	12225 CHAPPEL RD	ARLINGTON	WA	98223
MCCLELLAND CRAIG & CHARLENE	8235 ELEUSIS DR	ANCHORAGE	AK	99502
MCLINTOCK DIXIE BARRETT	102 BARANOF ST	SITKA	AK	99835
MCLINTOCK LLOYD J & C K	PO BOX 521441	BIG LAKE	AK	99652
MCCOLL JESSICA L	PO BOX 483	WILLOW	AK	99688
MCCOMB STEVE L	3305 DORIS PL	ANCHORAGE	AK	99518
MCCOMISKEY JOHN P & LORAIN	7821 CRESCENT HILL CIR	ANCHORAGE	AK	99507
MCCOMISKEY ROBT & JANET E	201 E MANOR AVE	ANCHORAGE	AK	99501
MCCONARTY RHONDA RAE	18517 CULROSS CIR	EAGLE RIVER	AK	99577
MCCONKEY DAN'L	2900 E 142ND ST	ANCHORAGE	AK	99516
MCCONNELL TERRY KARYLANN	11631 BIRCH TRAIL CIR # B	ANCHORAGE	AK	99515
MCCOOL JAS R	PO BOX 521246	ANCHORAGE	AK	99652
MCCORMICK JERALD D & NAOM	2001 BOREALIS DR	BIG LAKE	AK	99652
MCCORMICK TAMMY M	4101 E 112TH AVE	ANCHORAGE	AK	99503
MCCOTTER CHAD C & KIMBERLY	3926 E BRENTWOOD CIR	ANCHORAGE	AK	99516
MCCOWAN MELVIN K & SONJA A	HC 89 BOX 250	ANCHORAGE	AK	99502
MCCOY BERNARD N SR	9825 GRAVINA CIR	WILLOW	AK	99688
MCCOY DAN'L & GRACE	PO BOX 201136	EAGLE RIVER	AK	99577
McCoy Joan	P.O. Box 93484	ANCHORAGE	AK	99520
MCCOY TERRY	PO BOX 13045	Anchorage	AK	99509
MCCRACKEN JON	3301 W 31ST AVE	TRAPPER CREEK	AK	99683
MCCUBREY QUENTIN M & J J	13540 WINDWARD CT	ANCHORAGE	AK	99517
MCCULLOUGH MARK D	HC 89 BOX 438	ANCHORAGE	AK	99516
		WILLOW	AK	99688

Nexus Northwest

MCCUMBER MYRON L & NORMA M	1740B S RAGOSA CIR	PALMER	AK	99645
MCCURRY CORNELIUS	PO BOX 112826	ANCHORAGE	AK	99511
MCDANIEL JAY J	PO BOX 520174	BIG LAKE	AK	99652
MCDANIEL ROBERT J	4700 TAFT ST	ANCHORAGE	AK	99517
MCDONALD BERRY L & DONNA L	PO BOX 520245	BIG LAKE	AK	99652
MCDOLE DAN'L A & JACQUELYN	PO BOX 233467	ANCHORAGE	AK	99523
MCDONALD DEREK S & VICKY K	PO BOX 520961	BIG LAKE	AK	99652
MCDONALD EDWARD & BETTIE	7211 SANAK CIR	ANCHORAGE	AK	99504
McDonald Harry	1800 E 1st Ave	Anchorage	AK	99501-18
MCDONALD JOHN C & SANDY J	PO BOX 940215	HOUSTON	AK	99694
MCDONNELL MICHAEL D & J J	PO BOX 520555	BIG LAKE	AK	99652
McDonough Dan	P.O. Box 1205	Palmer	AK	99645
MCDONOUGH KEVIN L	PO BOX 1137	WILLOW	AK	99688
MCDUGALL DARRIN L & P L	PO BOX 940161	HOUSTON	AK	99694
MCDOWELL DOROTHY L	HC 89 BOX 144	WILLOW	AK	99688
MCELROY CHRISTOPHER D	PO BOX 230116	ANCHORAGE	AK	99523
MCEWEN DON C III	2350 E 68TH AVE	ANCHORAGE	AK	99507
MCGARY JACK R & GERALDINE	5928 CAMDEN CIR	ANCHORAGE	AK	99504
MCGEE BONNIE L	4812 SUNDI DR	ANCHORAGE	AK	99502
McGee Kevin	1736 Cara Loop	Anchorage	AK	99515
MCGEE KIRK	16551 CHASEWOOD	ANCHORAGE	AK	99516
McGehee Michelle	350 E. Dahlia Avenue	Palmer	AK	99645
MCGHAN COREY ALAN	PO BOX 520772	BIG LAKE	AK	99652
MCGHUEY ARLENE M	6640 IMLACH DR	ANCHORAGE	AK	99502
McGill Jessica	PO Box 2054	Palmer	AK	99645
MCGINNIS DAVID	2080 N HEMMER RD	PALMER	AK	99645
MCGOWAN DAN'L P	PO BOX 220661	ANCHORAGE	AK	99522
MCGOWEN GERALD L	PO BOX 520620	BIG LAKE	AK	99652
MCGRATH MICHAEL C	333 E HARVARD	ANCHORAGE	AK	99501
MCGRATH PATRICIA A	PO BOX 520692	BIG LAKE	AK	99652
MCGRATH THOS A	301 N TAYLOR	ANCHORAGE	AK	99508
MCGRAW PATRICIA A	PO BOX 15212	FRITZ CREEK	AK	99603
MCGREGOR WAYNE	1301 IVY CIR	WASILLA	AK	99654
MCGUIRE DELLA P	PO BOX 521465	BIG LAKE	AK	99652
MCGUIRE FRANK R	PO BOX 241233	ANCHORAGE	AK	99524
McGuire Lesli	716 W. 4th Avenue, Ste 430	Anchorage	AK	99501
MCINNIS JANICE K	4721 BECHAROF ST	ANCHORAGE	AK	99507
MCINTIRE BETTY J	6931 MONTAGNE CIR	ANCHORAGE	AK	99507
MCINTYRE JESSE C	4229 ROLLINS DR	ANCHORAGE	AK	99508
MCKAY CAROL J	PO BOX 221669	ANCHORAGE	AK	99522
MCKAY DAVID M	6231 S AIRPARK PL	ANCHORAGE	AK	99502
MCKEE CHAS EDISON	202 E NORTHERN LIGHTS	ANCHORAGE	AK	99503
MCKEE JOHN MATTHEW & G L	PO BOX 405	WILLOW	AK	99688

MCKEE WM H & RUTH L	PO BOX 190026	ANCHORAGE	AK	99519
MCKEEVER JOHN S	PO BOX 230666	ANCHORAGE	AK	99523
MCKEEVER MIKE	BOX 70 MARCUS ST	EAGLE RIVER	AK	99577
MCKEEVER RON	PO BOX 671741	CHUGIAK	AK	99567
MCKEEVER T A	2024 LOUSSAC DR	ANCHORAGE	AK	99517
MCKELLAR GARY L & TAMBRA G	11816 WILDERNESS DR	ANCHORAGE	AK	99516
MCKENZIE BRADY S&MARJORIE	3321 E 84TH AVE	ANCHORAGE	AK	99507
McKenzie Don	P.O. Box 772922	Eagle River	AK	99577
MCKEOWN JOS P	PO BOX 140342	ANCHORAGE	AK	99514
MCKIERNAN JOHN E & MARIA A	PO BOX 5358	FORT RICHARDS	AK	99505
MCKILLOP WM H & CARLENE K	1179 E MCKEE CT	WASILLA	AK	99654
MCKINNEY HERBERT SR & J L	11015 CANGE ST	ANCHORAGE	AK	99516
MCKINNEY LEONARD F	1103 SOUTHAMPTON DR	ANCHORAGE	AK	99503
MCKINNIS DOUGLAS L & WENDY	6601 E LONESOME DR	PALMER	AK	99645
MCLAIN EDW A & ANN G	6910 GEMINI DR	ANCHORAGE	AK	99504
MCLAIN MICHAEL C	PO BOX 2616	PALMER	AK	99645
MCLAIN TIMOTHY J & MICHELE	7743 EASTBROOK DR	ANCHORAGE	AK	99504
MCLANE DAVID A & MARGARET	11431 OLD GLENN HWY	EAGLE RIVER	AK	99577
MCLANE PEGGY J	3904 LOIS DR A	ANCHORAGE	AK	99517
MCLAREN MARJORIE P	2431 SPINDRIFT CIR	ANCHORAGE	AK	99515
MCLARNON PAUL J & ERIN S	PO BOX 940401	HOUSTON	AK	99694
McLean Mike	P.O. Box 699	Palmer	AK	99645
MCLEAN ROBT C	6341 ROSE HIP CIR	ANCHORAGE	AK	99507
MCLEAN WALT	PO BOX 1005	WILLOW	AK	99688
MCLENDON LINDA C	1906 TWINING DR	ANCHORAGE	AK	99504
MCLEOD SCOTT A & KAREN L	8400 SKYHILL DR	ANCHORAGE	AK	99502
MCLEOD SHAWN D & HONNEN M	PO BOX 671671	CHUGIAK	AK	99567
MLOUD BECKY	3707 SPENARD RD	ANCHORAGE	AK	99503
MCMAHILL LARRY J & JO ANN	PO BOX 940302	HOUSTON	AK	99694
MCMAHON READ JEFFREY M & CYNTHIA L	8637 CROSS POINTE LOOP	ANCHORAGE	AK	99504
MCMEEKIN ANDREW J	PO BOX 521561	BIG LAKE	AK	99652
MCMILLAN JAS A & VALERIE A	PO BOX 242721	ANCHORAGE	AK	99524
MCMULLIN DEBORAH A TRE	PO BOX 231743	ANCHORAGE	AK	99523
MCMURRAY TOBIN A	5785 E BIDEFORD BLVD	PALMER	AK	99645
MCPHAIL LEBRON P & C Y	PO BOX 449	PALMER	AK	99645
MCPHERSON PATRICK A & C D	1124 LEATHERLEAF LOOP	WASILLA	AK	99654
MCPHETERS WM & JUDITH	1304 BIRCHWOOD ST	ANCHORAGE	AK	99508
MCQUEEN JOHN W JR & M E	920 W DIMOND BLVD	ANCHORAGE	AK	99515
MCRILL MARY L	1148 W LONE CUB DR	WASILLA	AK	99654
MCRBERTS PATRICIA A	PO BOX 2246	PALMER	AK	99645
McRoberts Paul	PO BOX 521282	BIG LAKE	AK	99652
MCRBERTS PAUL	P O BOX 415	PALMER	AK	99645
MCRBERTS PAUL D	PO BOX 521282	BIG LAKE	AK	99652

RE/MAX of Eagle River

Argus Fire Protection

MCRBERTS RYAN M & ALICIA	PO BOX 521645	BIG LAKE	AK	99652
MCSORLEY GEO P JR&THERESA	1180 BALSAM CIR	WASILLA	AK	99654
MCVEIGH STEVE L	341 BONIFACE PKY	ANCHORAGE	AK	99504
MCVITTY WM ROBT JR	5200 BISHOPS CASTLE CIR	ANCHORAGE	AK	99516
MEADE THOMAS R	PO BOX 112473	ANCHORAGE	AK	99511
MEADOWS CHRISTIANNE	PO BOX 770273	EAGLE RIVER	AK	99577
MEARS GLEN A	6781 ROUND TREE DR	ANCHORAGE	AK	99516
MEDINA PRIMITIVO & MAGNA	7310 BIGLERVILLE CIR	ANCHORAGE	AK	99507
MEE EDWARD E & JEANNE E	14505 E DOC MCKINLEY	PALMER	AK	99645
MEEHAN PATRICK	1241 W 27TH AVE	ANCHORAGE	AK	99503
MEFODIEV ALEXEY	825 LATOUCHE ST	ANCHORAGE	AK	99501
MEIER LEROY A & DOLORES C	PO BOX 4416	PALMER	AK	99645
MEIER STEVE	2806 BORLAND DR	ANCHORAGE	AK	99514
MEISLER MARTIN J& PAULA F	3801 KNIK AVE	ANCHORAGE	AK	99517
MEISTER JAS K	1434 I ST	ANCHORAGE	AK	99501
MELANI MORRIS A & ALBERTA	PO BOX 520628	BIG LAKE	AK	99652
MELICKIAN ARTHUR H	404 E ARCTIC AVE	PALMER	AK	99645
Meiolo Michael	10928 Eagle River Road, #228	Eagle River	AK	99577
MELLOT THOS R & SHELLY A	3111 DOS CIR	ANCHORAGE	AK	99507
Melton Marsha	P.O. Box 891	Palmer	AK	99645
MELVILLE JADINE	3810 W 74TH AVE	ANCHORAGE	AK	99502
Menard Curt	350 E. Dahlia Avenue	Palmer	AK	99645
Menard Linda	3060 N. Lazy Eight Court, Ste 2, PMB 777	Wasilla	AK	99654
Menard Steve	PMB 257 Box 875910	Wasilla	AK	99687
Mendenhall, P. E. James W.	2471 Belmont Dr.	Anchorage	AK	99517
MENDIOLA RAYMOND C	PO BOX 1237	PALMER	AK	99645
MENDOZA FERDINAND & EVA	2953 SECLUSION COVE	ANCHORAGE	AK	99515
MENO JENNIFER R	2325 CASEY CUSACK LOOP	ANCHORAGE	AK	99515
MENSE MIKE	919 DAVID PL	ANCHORAGE	AK	99501
MERCHANT MARK A	20576 ICE FALL DR	EAGLE RIVER	AK	99577
MERCHANT ROB M& DELORES K	1801 E DOWLING RD	ANCHORAGE	AK	99507
MERCULIEF PATIENCE P	5811 RADCLIFF DR	ANCHORAGE	AK	99504
MERRELL BRUCE D& SHARON S	3301 MADISON WAY	ANCHORAGE	AK	99508
MERRICK AUGUSTINE J&KELLY	20892 ICEFALL DR	EAGLE RIVER	AK	99577
MERRIGAN FRANCIS F SR & P	7401 E JIM COTTRELL CIR	PALMER	AK	99645
MERRILL ART	PO BOX 869	WILLOW	AK	99688
MERRILL CHAS	PO BOX 873	WILLOW	AK	99688
MERRILL JERRY R	PO BOX 597	WILLOW	AK	99688
MERRILL MARK S	PO BOX 103	WILLOW	AK	99688
MERRILL MONTE R & HEATHER	1910 S TERRACE CT	PALMER	AK	99645
MERRITT GLENN R	PO BOX 521210	BIG LAKE	AK	99652
MERSEREAU DAVID	4832 HUNTER DR	ANCHORAGE	AK	99502
MESA FRANCES T A	PO BOX 521478	BIG LAKE	AK	99652

UAA Chugiak-Eagle River Campus  
 Colony Inn Too  
 Matanuska-Susitna Borough  
 Mat-Su Borough School Board  
 City of Wasilla

MESICK KIMBERLY RODERICK	1741 BURLINGTON ST	ANCHORAGE	AK	99508
MESTAS DENNIS M & TERRY A	13340 REEF PL	ANCHORAGE	AK	99515
MESTAS DENNIS PC PPST	880 N ST	ANCHORAGE	AK	99501
METCALF RODNEY D	654 HARP CIR	EAGLE RIVER	AK	99577
METHONEN GWENDOLYN	2635 COTTONWOOD ST	ANCHORAGE	AK	99508
METHONEN MINA M	PO BOX 521373	BIG LAKE	AK	99652
METHVEN SCOTT ALLEN	717 O ST	ANCHORAGE	AK	99501
Metva Marty	635 W. Nelson Avenue	Wasilla	AK	99654
METRO EDW G	PO BOX 520685	BIG LAKE	AK	99652
MEW MARK T & DIANE Y	3923 BIRDSONG DR	EAGLE RIVER	AK	99577
MEYER JEAN V	10230 INDEPENDENCE	CHATSWORTH	CA	91311
Meyer Kevin	716 W. 4th Avenue, Ste 310	Anchorage	AK	99501
Meyer Kevin	State Capitol, Room 515	Juneau	AK	99801-11
MEYER KEVIN G & MARTY A	4020 WINCHESTER LOOP	ANCHORAGE	AK	99507
MEYER STUART J & ELAINE K	PO BOX 520255	BIG LAKE	AK	99652
MEYER TRISTAN D & ALANA M	705 QUARTERMASTER RD	FORT RICHARDS	AK	99505
MEYER WM J	6941 HUNT AVE	ANCHORAGE	AK	99504
MEYERS ALBERT F & MARION	2438 E 27TH AVE	ANCHORAGE	AK	99508
MEYERS ANN	2437 BONIFACE PKWY	ANCHORAGE	AK	99504
MEYHOFF CHAD	4130 ENDEAVOR CIR	ANCHORAGE	AK	99515
MEZA DAN'L	8901 FOREST VILLAGE DR	ANCHORAGE	AK	99502
MEZA DAN'L W	PO BOX 521517	BIG LAKE	AK	99652
MICELI SUN & Anthony	3343 S SOUTHLUFF	ANCHORAGE	AK	99515
MICHAEL CHAS P & MARY JANE	2421 WELLINGTON CIR	ANCHORAGE	AK	99517
MICHEL ANITA L	5318 KEUKA CT	ANCHORAGE	AK	99508
MICHLIG MARIAN L	PO BOX 520096	BIG LAKE	AK	99652
MIDDLETON TIMOTHY & JOYCE	717 M ST	ANCHORAGE	AK	99501
MIDYETT RICHARD E JR & P A	PO BOX 210	WILLOW	AK	99688
MIELKE FRANK G	PO BOX 670287	CHUGIAK	AK	99567
MIELKE PHILLIP D & YUK LAN	PO BOX 520158	BIG LAKE	AK	99652
Mihayl Victor	PO Box 2630	Palmer	AK	99645
Mike Huske	7031 Whitehall	Anchorage	AK	99502
Mike Whedbee	450 Brazil Circle	Big Lake	AK	99652
MIKE WHITLATCH	P O BOX 125	WILLOW	AK	99688
MIKEL KOKO	441 W 92ND AVE	ANCHORAGE	AK	99502
MIKKELSEN TIMOTHY O	1260 N RAINBOW PARK DR	WASILLA	AK	99654
MIKKELSON RICHARD	8720 JEWEL TERRACE CIR	ANCHORAGE	AK	99502
MILLAR RICHARD A & DIANE L	10270A E STRAND DR	PALMER	AK	99645
MILLARD JOHN	PO BOX 521211	BIG LAKE	AK	99652
MILLEN KELLY M	200 W 34TH	ANCHORAGE	AK	99503
MILLER ARCHIE L & DIANE M	432 E MANOR	ANCHORAGE	AK	99501
MILLER BRETT R & MARY JANE	PO BOX 521475	BIG LAKE	AK	99652
Miller Christie	Alaska Dept. of Community & Economic Developm 550 West 7th Avenue, Suite 1770	Anchorage	AK	99501

MILLER CHRISTOPHER R & L A Miller David	Federal Highway Administration	5010 NOTTINGHAM WAY 709 W. 9th St., Room 851 430 WOLF DR	ANCHORAGE Juneau EAGLE RIVER	AK AK AK	99503 99802 99577
MILLER DAVID L & CECILY A		PO BOX 940175	HOUSTON	AK	99694
MILLER DONALD P & SUSAN M		2505 ARCTIC BLVD	ANCHORAGE	AK	99503
MILLER FRANK & FRANCINE		PO BOX 940127	HOUSTON	AK	99694
MILLER GARY L & DEBORAH C		PO BOX 1018	WILLOW	AK	99688
MILLER JAMIE & MISTY		10661 CUTTER CIR	ANCHORAGE	AK	99515
MILLER JAS R & MARY E		PO BOX 520727	BIG LAKE	AK	99652
MILLER JESSE J & RHAWNIE M		3114 N ENCHANTED CIR	HOUSTON	AK	99694
MILLER JESSICA W		PO BOX 1074	WILLOW	AK	99688
MILLER JOHN M & ALICE R		8949 SAHALEE	ANCHORAGE	AK	99507
MILLER LOUIS & BABETTE		PO BOX 771398	EAGLE RIVER	AK	99577
MILLER MARTIN E & LORENA		PO BOX 520737	BIG LAKE	AK	99652
MILLER MATTHEW C		PO BOX 238	WILLOW	AK	99688
MILLER MEREDITH & LINDA L		3212 DOIL DR	ANCHORAGE	AK	99507
MILLER MICHAEL J		6549 NOTTING HILL	ANCHORAGE	AK	99504
MILLER MICHELLE M		PO BOX 521438	BIG LAKE	AK	99652
MILLER OWEN & GLORIA	Alaska Community Action on Toxics	505 W. Northern Lights Blvd., Suite 205 PO BOX 1292	Anchorage PALMER	AK AK	99503 99645
Miller Pamela		PO BOX 4987	PALMER	AK	99645
MILLER PAMELA R		18735 FISH HATCHERY RD	EAGLE RIVER	AK	99577
MILLER PAUL B		2711 WESLEYAN DR	ANCHORAGE	AK	99508
MILLER RAYMOND & TELESITA		813 West Northern Lights Blvd.	Anchorage	AK	99503
MILLER ROGER & KATHERINE		510 MELLOW PL	ANCHORAGE	AK	99508
Miller Ron		536 W 15TH AVE	ANCHORAGE	AK	99501
MILLER RONALD		17700 E CAROL ANN DR	PALMER	AK	99645
MILLER RUSSELL S		4515 N MCKEAN DR	PALMER	AK	99645
MILLER STEVEN R & TAMMY J		PO Box 357	Willow	AK	99688
MILLER WALTER L & PAULA J		PO BOX 357	WILLOW	AK	99688
Miller William & Linda A		2300 E 48TH AVE	ANCHORAGE	AK	99507
MILLER WM K & LINDA A		407 E NORTHERN LGTS BLVD	ANCHORAGE	AK	99503
MILLER WM L & PHYLLIS J		P. O. Box 240249	Anchorage	AK	99524-02
MILLHOUSE DENNIS	Governor's Council on Disabilities and Special Edu	2945 S CHARMING VALLEY LP	PALMER	AK	99645
Millie Ryan		1517 S. Industrial Way	Palmer	AK	99645
MILLIMAN DAVID K & KAREN A	H&C Feed	PO BOX 111216	ANCHORAGE	AK	99511
Mills Doug		PO BOX 212251	ANCHORAGE	AK	99521
MILLS SARAH ANNE		7453 W PARK CIR	ANCHORAGE	AK	99502
MILLS THEO C		PO BOX 202126	ANCHORAGE	AK	99520
MILLS WM J & JOHN B		PO BOX 770716	EAGLE RIVER	AK	99577
MILLS-PRICE PETER N		PO Box 116	PALMER	AK	99645
MILTON CHESTER R	Friends of Mat-Su	PO BOX 940091	HOUSTON	AK	99694
Mimi Peabody		2450 N KOSLOSKY CT	PALMER	AK	99645
MINCH MELVIN M & MYRNA					
MINDER PATRICK M & K					

MINER MARGA HAS			1841 FALCON CIR	ANCHORAGE	AK	99504
Miner Teri	Miner Designs		P.O. Box 2061	Palmer	AK	99645
Miner-Collins Jana	North Star Behavioral Health		634 S Bailey #207	Palmer	AK	99645
MINERT DOUGLAS J & SUZANNE			3411 S BLUFF CIR	ANCHORAGE	AK	99515
MINGO DAN'L R & KRISTIE M			PO BOX 222	WILLOW	AK	99688
Minich Sophie	CIRI		P.O. Box 93330	Anchorage	AK	99509
MINKLER DUANE W			1433 S INDUSTRIAL PK WAY	PALMER	AK	99645
MINOGUE JON B			2191 S AMHERST CT	PALMER	AK	99645
MINSCH MICHAEL A			PO BOX 345	WILLOW	AK	99688
MINSHULL LOU ANN SPEARS			PO BOX 521578	BIG LAKE	AK	99652
MINTER GERALD & DIANA G			PO BOX 670745	CHUGIAK	AK	99567
MINTON JAS C & LYNNE M			PO BOX 190121	ANCHORAGE	AK	99519
MIRANDA PAUL			4848 SHELKOF ST	ANCHORAGE	AK	99507
MIRAZO RENEE			3336 SUN VALLEY DR	ANCHORAGE	AK	99577
MISSIG THEO J			10265 GOODNEWS CIR	EAGLE RIVER	AK	99515
MITCHELL DENNIS CARL			127 N BLISS	ANCHORAGE	AK	99508
MITCHELL DENNIS G & LOIS I			PO BOX 935	WILLOW	AK	99688
MITCHELL ERIC & MELANIE			3237 WILEY POST LOOP	ANCHORAGE	AK	99517
MITCHELL FRANCIS B.			18227 BARONOFF AVE	EAGLE RIVER	AK	99577
MITCHELL KENT J			PO BOX 373	WILLOW	AK	99688
MITCHELL MARY			PO BOX 26	WILLOW	AK	99688
MITCHELL MICHAEL A			11720 RAINBOW AVE	ANCHORAGE	AK	99516
MITCHELL STUART A & LINDA			PO BOX 520384	BIG LAKE	AK	99652
MITCHELL THOS L			2230 CHANDALAR ST	ANCHORAGE	AK	99504
Mitchell William			P.O. Box 137	Palmer	AK	99645
MITCHELL WM B & SANDRA			PO BOX 520973	BIG LAKE	AK	99652
MITCHELLS RICHARD A JR			PO BOX 102095	ANCHORAGE	AK	99510
Mitchells Rick	AMATS		911 W. 8th Avenue, Ste 100	Anchorage	AK	99577
MITZEN VERNON J & ROBYN A			18207 KANTISHINA	EAGLE RIVER	AK	99504
MLAKAR LOUIS E & FLORENCE			7505 BOUNDARY	ANCHORAGE	AK	99694
MOBLEY WARREN W L/E			PO BOX 940402	HOUSTON	AK	99652
MOCK HARRY W			PO BOX 520612	BIG LAKE	AK	99652
MOCK LAWRENCE A & J A			10313 THUJA CIR	ANCHORAGE	AK	99507
MOCKERMAN EUGENE & ALICE			PO BOX 520442	BIG LAKE	AK	99652
MODAFFARI ANTHONY			PO BOX 520582	BIG LAKE	AK	99652
Modaffar Marilyn & Ron	Farm Loop Farm		P.O. Box 2942	Palmer	AK	99645
MOE CARL			PO BOX 521107	BIG LAKE	AK	99652
MOE EDW M & CONNIE L			707 W 21ST AVE	ANCHORAGE	AK	99503
MOE KARL E			1810 KODIAK ST	ANCHORAGE	AK	99504
MOE STACY A			PO BOX 521107	BIG LAKE	AK	99652
MOELLER JOHN C & LAURA J			7531 E 6TH	ANCHORAGE	AK	99504
MOEN CHRIS T & MARY C			2441 S COGGIN DR	PALMER	AK	99645
MOE-NEWBY JANE M			1600 BANNISTER DR	ANCHORAGE	AK	99508



MOFFITT DAN'L SCOTT Moffitt Leonard	PO BOX 521503 Box 748	BIG LAKE Palmer	AK	99652
Moffitt Tracy	P.O. Box 3337	Palmer	AK	99645
MOFFITT TRACY A&KATHRYN A	PO BOX 3337	PALMER	AK	99645
MOHR FREDERICK C JR & J L	PO BOX 110113	ANCHORAGE	AK	99511
MOISTNER KRISTIAN & S A	PO BOX 521360	BIG LAKE	AK	99652
Moistner Sharilyn	1150 S Colony Way Suite 5	Palmer	AK	99645
MOLESKY FRANK P	2701 EASTHAVEN CIR	ANCHORAGE	AK	99508
MOLINA RODOLFO C & C R	3100 AMBER BAY LOOP	ANCHORAGE	AK	99515
MONACO JOHN ALLEN ANDREW	2213 RIDGEMONT DR	ANCHORAGE	AK	99507
MONAGHAN KEVIN & DONNA	8628 EMERALD ST	ANCHORAGE	AK	99502
MONAGHAN NEIL T	1152 E MCKEE CT	WASILLA	AK	99654
MONAHAN RICHARD W JR& P A	PO BOX 1721	PALMER	AK	99645
MONDRE PAUL M	PO BOX 111094	ANCHORAGE	AK	99511
Monegan Walt	P.O. Box 1112000	Juneau	AK	99811
MONETTE GREGORY A	1707 BELLEVUE LP	ANCHORAGE	AK	99515
Money Charles	750 W. 2nd Avenue, Ste 100	Anchororage	AK	99501
MONEY KATHRYN L	PO BOX 910	ANCHOR POINT	AK	99556
MONEYMAKER FRANK & LINDA	11030 KASKANAK	EAGLE RIVER	AK	99577
MONFREY LEOLA	2510 MAYLEN CIR	ANCHORAGE	AK	99516
MONIZ CAROL	PO BOX 521884	BIG LAKE	AK	99652
MONROE KENT A	PO BOX 2189	CHUGIAK	AK	99567
MONROE LINDA	112 E DANNA AVE	WASILLA	AK	99654
MONROE PATRICA JEAN	PO BOX 520534	BIG LAKE	AK	99652
MONSEN MARTIN E & NORMA G	19637 1ST ST	EAGLE RIVER	AK	99577
MONTAGUE GARY W & C L	6675 HOLLY LN	ANCHORAGE	AK	99502
Montague Peter	1120 Vermont Avenue NW	Washington	DC	20590
MONTBRIAND ELIZABETH	209 BUNNELL ST	ANCHORAGE	AK	99508
MONTEIL WALTER E& MICHELE	12043 W SKYLINE DR	EAGLE RIVER	AK	99577
MONTERROSA ALEXANDER & K	4310 GANNETT CIR	ANCHORAGE	AK	99504
MONTGOMERY DANA L	5653 RENEE	ANCHORAGE	AK	99504
MONTGOMERY DONNIE R	PO BOX 141532	ANCHORAGE	AK	99514
MONTGOMERY JOE D & EBBA E	1048 BEECH LN	ANCHORAGE	AK	99501
MONTGOMERY RANDALL H & BA	PO BOX 520733	ANCHORAGE	AK	99652
MONTGOMERY RANDALL H&N L	PO BOX 521415	BIG LAKE	AK	99652
MONTOYA HERBERT P & KAY S	10510 E BELLA VISTA	BIG LAKE	AK	99652
MOODY DON	PO BOX 520709	SCOTTSDALE	AZ	85258
MOODY DONALD D	1536 WINTERGREEN ST	BIG LAKE	AK	99652
MOODY JAS R & ROSE M	3434 S KRYSTAL PL	ANCHORAGE	AK	99508
MOODY ROBT R & LUCY A	PO BOX 670370	BIG LAKE	AK	99652
MOODY W MICHAEL	7541 SPORTSMENS POINT CIR	CHUGIAK	AK	99567
MOODY WALTER L	4222 PIPER ST	ANCHORAGE	AK	99502
MOON HYO-YOUNG & MOO-SANG	315 KRANE DR	ANCHORAGE	AK	99508

MOONEY EDW C & HARRIET M	PO BOX 521331	BIG LAKE	AK	99652
MOONEY JAS E III & TANYA	PO BOX 521756	BIG LAKE	AK	99652
MOORE ANTHONY J	7164 LINDEN DR	ANCHORAGE	AK	99502
MOORE ASBURY C IV	3705 ARCTIC	ANCHORAGE	AK	99503
MOORE BEVERLY	PO BOX 521896	BIG LAKE	AK	99652
MOORE BRUCE A & COLLEEN J	7410 SETTER DR	ANCHORAGE	AK	99502
MOORE BRUCE O & BARBARA L	2242 ARCADIA	ANCHORAGE	AK	99517
MOORE CHAS D & FE C	PO BOX 1001	WILLOW	AK	99688
MOORE DAN'L E & DIANE M	8637 BROOKRIDGE DR	ANCHORAGE	AK	99504
MOORE DAN'L T & NANCY E	PO BOX 520509	BIG LAKE	AK	99652
MOORE DEAN	PO BOX 520987	BIG LAKE	AK	99652
MOORE GARY W & MARIA C	2635 DRAPER DR	ANCHORAGE	AK	99517
MOORE GEO J JR& DEBORAH M	4743 SYDNEY PARK DR	ANCHORAGE	AK	99516
MOORE GERALD ROBT	PO BOX 4233	PALMER	AK	99645
MOORE JAMES F	PO BOX 201854	ANCHORAGE	AK	99520
MOORE LEROY	4018.5 E 6TH AVE	ANCHORAGE	AK	99508
MOORE MARVIN J	HC 89 BOX 393	WILLOW	AK	99688
MOORE PATRICIA M	6700 DOWNEY FINCH LN	ANCHORAGE	AK	99516
MOORE RICHARD I	11530 HAWKINS LN	ANCHORAGE	AK	99516
MOORE SCOTT K	9040 AMANDA CIR	ANCHORAGE	AK	99501
MOORE TERRY L	PO BOX 111928	ANCHORAGE	AK	99511
MOORE WM N	PO BOX 4336	PALMER	AK	99645
MORAN ANGELA R	4220 E 104TH AVE	ANCHORAGE	AK	99507
MORAN DAVID K & BETTY L	9350 BASHER DR	ANCHORAGE	AK	99507
MORCK JAY D	PO BOX 3268	PALMER	AK	99645
MOREDA RONALD J & K P	PO BOX 773073	EAGLE RIVER	AK	99577
MOREHOUSE REX L	PO BOX 1047	WILLOW	AK	99688
MORELAND ROBT L SR & RUTH	PO BOX 212415	ANCHORAGE	AK	99521
MORGAN ALLEN J & WENDY M	PO BOX 996	WILLOW	AK	99688
MORGAN JUSTIN	2436 JUNEAU ST	ANCHORAGE	AK	99508
MORGAN LYLE SCOTT	PO BOX 110597	ANCHORAGE	AK	99511
MORGAN PHILLIP H	1241 W 27TH	ANCHORAGE	AK	99503
MORGAN, JR. SYLVAN (HAL)	HCO1 BOX 6204	PALMER	AK	99645
MORHAIN KENNETH A & KATHY	10230 SILVER TIP CIR	EAGLE RIVER	AK	99577
MORINO KATHRYN A& REBECCA	801 HILLTOP DR	PALMER	AK	99645
MORITZ MICHAEL B & LORI A	2241 OLYMPIC DR	ANCHORAGE	AK	99515
MORK PHYLLIS A	1321 VALARIAN ST	ANCHORAGE	AK	99508
MORMAN DALE G & JO LYNN	711 HARBOR CIR	ANCHORAGE	AK	99515
Moroneil CHRISTINA M MORONELL	10420 OLIVE LN	ANCHORAGE	AK	99515
MORRIS DENNIS J & MARILYN	PO BOX 771424	EAGLE RIVER	AK	99577
MORRIS DOUGLAS E	9825 CARLSON RD	ANCHORAGE	AK	99507
MORRIS IRVIN H	11241 ECHO ST	EAGLE RIVER	AK	99577
MORRIS JOHN R	8511 E 11TH CT	ANCHORAGE	AK	99504

MORRIS MATTHEW J	17031 CORONADO RD	EAGLE RIVER	AK	99577
MORRIS SHIRLEY A	PO BOX 520752	BIG LAKE	AK	99652
MORRISON CONNOR	108 MACE RD	GILBOA	NY	12076
MORRISON DIANE A	PO BOX 2393	PALMER	AK	99645
MORRISON ELIZABETH A	PO BOX 244604	ANCHORAGE	AK	99524
MORRISON JOHN ARNOLD	3801 E 65TH	ANCHORAGE	AK	99507
MORRISON JOHN E	1510 ELCADORE DR	ANCHORAGE	AK	99507
MORRISON KENNETH W & LORI	PO BOX 520277	BIG LAKE	AK	99652
MORRISON LANCE K	2000 E 68TH AVE	ANCHORAGE	AK	99507
MORRISON LINDA J	PO BOX 940262	HOUSTON	AK	99694
MORRISON MICHAEL V	11706 LOVELAND CIR	EAGLE RIVER	AK	99577
MORRISON SHELLY	801 AIRPORT HEIGHTS	ANCHORAGE	AK	99508
MORRISON STEVEN A	PO BOX 210313	ANCHORAGE	AK	99521
MORRISON ULA-NIEKA	1900 W 46TH AVE	ANCHORAGE	AK	99517
MORRISON-LUGO MARIA	PO BOX 672233	CHUGIAK	AK	99567
MORRISSEY KAREN	3015 MADISON WAY	ANCHORAGE	AK	99508
MORROW ROGER L	PO BOX 770112	EAGLE RIVER	AK	99577
MORROW WM E	PO BOX 520261	BIG LAKE	AK	99652
MORTENSEN KENNETH R & S D	PO BOX 940087	HOUSTON	AK	99694
MORTON CLAIRE	18619 OLD GLENN HWY	CHUGIAK	AK	99567
MORTON LAVERNE E& LINDA L	PO BOX 520227	BIG LAKE	AK	99652
MORTON RAYMOND & LOIS	PO BOX 210865	ANCHORAGE	AK	99521
MORTON ROYCE E & ROBIN M	6311 TAY CIR	ANCHORAGE	AK	99502
MOSBY W G & ANN	800 W COVILLE CIR	PALMER	AK	99645
MOSES DEXTER L & JODY	12810 TROY ST	ANCHORAGE	AK	99515
MOSS ELDRIDGE JR & SUN H	419 N BUNN ST	ANCHORAGE	AK	99508
MOJA CHUE Y	4005 PARSONS AVE	ANCHORAGE	AK	99508
MOUW JASON E B& HEATHER E	6111 SPRUCE MEADOWS LOOP	ANCHORAGE	AK	99507
MOZALEVSKIY NIKOLAY A	965 W JOSH DR	PALMER	AK	99645
MUCHA JERRY & OLIVIA	PO BOX 90793	ANCHORAGE	AK	99509
MUCKEY GREGORY L & NOREEN	PO BOX 215	WILLOW	AK	99688
MUCKEY STEVEN C	PO BOX 309	WILLOW	AK	99688
MUELLER DONAL F & J A	1684 WINTERSET DR	ANCHORAGE	AK	99508
MUELLER SCOTT & INA L	8611 MUIR CT	ANCHORAGE	AK	99504
MUFFOLETTO JOHN & LISBETH	16241 BAUGH CIR	ANCHORAGE	AK	99516
MUIR DOROTHA L	5800 TRAPPERS TRAIL RD	ANCHORAGE	AK	99516
Mulcahy Laurie	Alaska Dept. of Transportation and Public Facilities P.O. Box 196900	Anchorage	AK	99519
MULCAHY PETER J & DEBORAH	PO BOX 671846	CHUGIAK	AK	99567
Mulcahy Thomas	P.O. Box 190912	Anchorage	AK	99519-09
MULCAHY THOS P	PO BOX 190912	ANCHORAGE	AK	99512
MULESKY JAY	PO BOX 212636	ANCHORAGE	AK	99521
MULHOLLAND KEITH & S	PO BOX 672264	CHUGIAK	AK	99567
MULLER JEFF & LAUREL	7340 TICONDEROGA PL	ANCHORAGE	AK	99502

MULLER JUDITH MOE	2410 GALEWOOD ST	ANCHORAGE	AK	99508
MUNCY EVERETTE P & GENEVA	PO BOX 143134	ANCHORAGE	AK	99514
MUNCY FRANK & JACKIE	PO BOX 438	PALMER	AK	99645
MUNCY ROBT & VICTORIA	688 E EKLUTNA AVE	PALMER	AK	99645
MUNDT WALTER A & PAMELA R	6606 NOTTINGHILL DR	ANCHORAGE	AK	99504
MUNGARAY RUBEN E & D E	10920 LIVINGSTON ST	ANCHORAGE	AK	99516
Munoz Helen	P.O. Box 4013	Palmer	AK	99645
MUNSON DONALD E & VIVIAN	PO BOX 92056	ANCHORAGE	AK	99509
MUNTER JAS A & ELIZABETH	5701 PENNY CIR	ANCHORAGE	AK	99516
MURACHEV PETER S & FEVRUSA	PO BOX 521343	BIG LAKE	AK	99652
MURAK KONRAD	PO BOX 5134	FORT RICHARDS	AK	99505
MURCHIE PHILLIP A JR	PO BOX 112515	ANCHORAGE	AK	99511
Murkowski Lisa	709 Hart Senate Building	Washington	D.C.	20510
Murkowski Lisa	510 L Street, Ste 550	Anchorage	AK	99501
MURPHY ALICE M	1115 NW MARKET ST	SEATTLE	WA	98107
MURPHY CLAY E & VIRGINIA A	600 CHERRY ST	ANCHORAGE	AK	99504
MURPHY DAN'L W	PO BOX 144	WILLOW	AK	99688
MURPHY HARVEY L & JANET M	2461 BELMONT DR	ANCHORAGE	AK	99517
MURPHY MARTY W & BRENDA K	6800 CHEROKEE WAY	ANCHORAGE	AK	99504
MURPHY MICHAEL J & MARLA	20524 PHILADELPHIA WAY	EAGLE RIVER	AK	99577
MURPHY MICHAEL P & YOUNGJU	1300 S BERTHA LN	WASILLA	AK	99654
MURPHY SUSAN	9540 MORNINGSIDE LOOP	ANCHORAGE	AK	99515
MURPHY WM L & RUBY	HC 89 BOX 320	WILLOW	AK	99688
MURR ANDREW C & KANDEE L	PO BOX 520143	BIG LAKE	AK	99652
MURRAY DAVID W	2805 VALLEYWOOD DR	ANCHORAGE	AK	99517
MURRAY HELENE	1316 O ST	ANCHORAGE	AK	99501
MURRAY WM L & KATHRYN	5234 SILLARY CIR	ANCHORAGE	AK	99508
MURRELL THOS M & LINDA S	PO BOX 4727	PALMER	AK	99645
MUSBACH RONALD & CHRISANDRA	PO BOX 863	WILLOW	AK	99688
MUSCIO CHAS H	PO BOX 200648	ANCHORAGE	AK	99520
Muse Curt	2201 E Parks Hwy	Wasilla	AK	99654
MUSHAT JOE	PO BOX 231286	ANCHORAGE	AK	99523
MUSIAL RUDOLPH J	PO BOX 435	WILLOW	AK	99688
MUSICH JOS L	PO BOX 202745	ANCHORAGE	AK	99520
MYERCHIN PHILIP R & MARIE	1083 POTLATCH CIR	ANCHORAGE	AK	99503
MYERS CHRISTOPHER	6740 SAMUEL CT	ANCHORAGE	AK	99516
MYERS CLEM B. & LINDA L.	PO BOX 671689	CHUGIAK	AK	99567
MYERS CRAIG	PO BOX 521510	BIG LAKE	AK	99652
MYERS DONALD	11701 GANDER ST	ANCHORAGE	AK	99516
MYERS TIMOTHY J	PO BOX 190323	ANCHORAGE	AK	99519
Myhre Tom	1150 S. Colony Way, Suite 10	Palmer	AK	99645
Mylius Dick	W. 7th Avenue, Suite 1070	Anchorage	AK	99501
MYRICK CALVIN C & VICKI K	911 CATHY PL	ANCHORAGE	AK	99504

A-Lazy Acres B & B

United States Senate  
United States Senate

Alaska Troutfitters

Edward Jones Investments  
Alaska Dept. of Natural Resources

MYRICK DANNY EUGENE	2441 SCARBOROUGH	ANCHORAGE	AK	99504
NAANES RONALD LEE JR	732 O ST	ANCHORAGE	AK	99501
NADING CURTIS & DENA	2057 DUKE DR	ANCHORAGE	AK	99508
NAEB KEVIN D & FRANCES	PO BOX 5221	FORT RICHARDS	AK	99505
NAFFZIGER MARK A & SHAWNEY	PO BOX 520796	BIG LAKE	AK	99652
NAFFZIGER RALPH D & KAREN	PO BOX 520196	BIG LAKE	AK	99652
NAGY JAS J	HC 89 BOX 328	WILLOW	AK	99688
NALL ERNEST R & KIMBERLY S	PO BOX 524	WILLOW	AK	99688
NALLE JOS S & DAWN K	4041 N STAR ST	ANCHORAGE	AK	99503
NAMEN TERENCE J	9501 BIRCH RD	ANCHORAGE	AK	99516
Nancy Binder	PO Box 476	Willow	AK	99688
Naneng Myron	P. O. Box 219	Bethel	AK	99559
NANEZ JOSE & LUCIA A	16751 THEODORE DR	EAGLE RIVER	AK	99577
NAPAGEAK THOS EDW	PO BOX 671595	CHUGIAK	AK	99567
NARDINI TIM	PO BOX 521771	BIG LAKE	AK	99652
NASH JERRY E & MEREDITH S	1040 N SAM SNEAD LOOP	WASILLA	AK	99654
NASH LORIN L & HILDEGARD	PO BOX 210314	AUKE BAY	AK	99821
NATION DANIEL JON	3800 E 65TH AVE	ANCHORAGE	AK	99507
Nation Mary Lynn	605 W. 4th Avenue, Room G-62	Anchorage	AK	99501
NAYLOR SKIP & ALISHA L	7750 JAGUAR CIR	ANCHORAGE	AK	99502
NEAL HARRY C & GOLDENE H	1281 S VINE RD	WASILLA	AK	99654
NEELEY FRANK W & BEVERLY L	1604 E ST	ANCHORAGE	AK	99501
NEES DAVID & CHRISTINE	9141 KING DAVID DR	ANCHORAGE	AK	99507
NEESE FRED & ELFRIEDA	1511 MEDFRA ST	ANCHORAGE	AK	99501
NEGUS CHRISTOPHER M & M M	HC 89 BOX 268	WILLOW	AK	99688
NEHER KEVIN R	PO BOX 230	WILLOW	AK	99688
NEISINGER NAOMI RUTH	20121 RAVEN DR	EAGLE RIVER	AK	99577
NEITHERCOAT WM	1400 W 40TH AVE	ANCHORAGE	AK	99503
NEJA SANDRA R.	1200 I ST	ANCHORAGE	AK	99501
NELSEN BERNARD E & INGE D	2960 SEAWIND DR	ANCHORAGE	AK	99516
NELSEN GEO & SHIRLEY	6801 LEWIS PL	ANCHORAGE	AK	99507
NELSON DEAN	3309 WOODLAND PARK DR	ANCHORAGE	AK	99517
NELSON FRANCIS A & K	4201 N OAKWOOD	PALMER	AK	99645
NELSON JEFFREY J	3533 W 79TH AVE	ANCHORAGE	AK	99502
NELSON JERRI L	PO BOX 520392	BIG LAKE	AK	99652
NELSON JOHN VICTOR	836 M ST	ANCHORAGE	AK	99501
NELSON JON M & VIDA D	3521 TANGLEWOOD PL #1	ANCHORAGE	AK	99517
NELSON LES R	HC 89 BOX 317	WILLOW	AK	99688
NELSON LORI ANN	PO BOX 110357	ANCHORAGE	AK	99511
NELSON MABLE	PO BOX 194	WILLOW	AK	99688
NELSON MARCA & RENAE J	PO BOX 521833	BIG LAKE	AK	99652
NELSON MARK C	10870 KAMISHAK BAY CIR	ANCHORAGE	AK	99515
NELSON MICHAEL	1107 NW 6TH AVE	DELRAY BEACH	FL	33444

Association of Village Council Presidents

U.S. Fish & Wildlife Service

NELSON PAUL	6740 JOLLIPAN CT	ANCHORAGE	AK	99507
NELSON RONALD DEAN	330 ANNA ST	PALMER	AK	99645
NELSON VICKI R	5167 E BIDEFORD BLVD	PALMER	AK	99645
Nerland Brian	P.O. Box 100420	Anchorage	AK	99510
NESBITT JAS M JR & SUSAN	10470 HAMPTON DR	ANCHORAGE	AK	99507
NESHEIM WM H & SUE A	PO BOX 773591	EAGLE RIVER	AK	99577
NESS PAMELA L	2261 S HORNUNG RD	PALMER	AK	99645
NEUHARTH SHANE & RENEE	PO BOX 368	WILLOW	AK	99688
Neuman Mark	State Capitol, Room 432	Juneau	AK	99801-11
Neuman Mark	600 E. Railroad Ave. Ste 1	Wasilla	AK	99564
NEUMANN ROBT G& CAROLYN F	1433 W 13TH AVE	ANCHORAGE	AK	99501
NEUMANN ROSALIE	2429 TURNAGAIN PKY	ANCHORAGE	AK	99517
NEUMILLER ROBT J JR	2570 BACKHAUS ST	HOUSTON	AK	99694
NEUSCHWANDER JERRY & GAIL	PO BOX 671305	CHUGIAK	AK	99567
NEUSER BETINA JEAN	634 FISCHER AVE	ANCHORAGE	AK	99518
NEVENZEL MICHAEL G	1101 GOLDENDALE DR	WASILLA	AK	99654
NEVES BENNY L	6151 E GERSHMELOOP	PALMER	AK	99645
NEWBY THOS S & MONA LISA	PO BOX 242524	ANCHORAGE	AK	99524
NEWELL MAX	PO BOX 521000	BIG LAKE	AK	99652
NEWHALL RAY C & HELEN P	PO BOX 521258	BIG LAKE	AK	99652
NEWMAN CHRISTIAN O&MARY A	PO BOX 829	WILLOW	AK	99688
NEWMAN DAVID J	PO BOX 1036	WILLOW	AK	99688
NEWMAN RICHARD C & E R	4230 WARWICK DR	ANCHORAGE	AK	99508
NEWMAN RICK A	PO BOX 66	WILLOW	AK	99688
NEWMAN STEVEN D	PO BOX 666	WILLOW	AK	99688
Newman Ty	14926 Old Glenn Hwy	Eagle River	AK	99577-92
NEWMAN WALTER E& PATRICIA	PO BOX 98	WILLOW	AK	99688
NEWTON CAMERON W& JESSICA	PO BOX 672178	CHUGIAK	AK	99567
NEWTON CHAS A	PO BOX 1928	PALMER	AK	99645
NEWTON EARNEST L	PO BOX 1046	WILLOW	AK	99688
NEWTON HERBERT C& CAROL J	11567 WATSON RD	SAINT IGNATIUS	MT	59865
NEWTON SHARON L	7908 HIGHLANDER DR	ANCHORAGE	AK	99518
NEWTON TIMOTHY TODD	2745 NUGGET LN	ANCHORAGE	AK	99516
NGUYEN CHANH T	21017 SCENIC DR	CHUGIAK	AK	99567
NGUYEN HONG L	2720 SCARBOROUGH DR	ANCHORAGE	AK	99504
NGUYEN KHANH V	1261 S SEWARD MERIDIAN	WASILLA	AK	99654
NIBERT MICHAEL J	PO BOX 112203	ANCHORAGE	AK	99511
NICHOLS BRENT A	275 S BUNN ST	ANCHORAGE	AK	99508
NICHOLS DARRYL R	PO BOX 520616	BIG LAKE	AK	99652
NICHOLS MARVIN R& DONNA M	PO BOX 3071	PALMER	AK	99645
NICHOLS MELVIN R & SHARON	3015 SEAWIND DR	ANCHORAGE	AK	99516
NICHOLS THOS S III	1335 WINTERGREEN	ANCHORAGE	AK	99508
NICHOLSON DANIEL A	6605 NOTTING HILL DR	ANCHORAGE	AK	99504

NICKERSON ROBT M & MARION	911 LIGHTHOUSE CT	ANCHORAGE	AK	99515
Nicklie David	P.O. Box 94	Cantwell	AK	99729
Nickols Mike & Pam	P.O. Box 706	Talkeetna	AK	99676
Nicolai Matthew	301 Callista Court, Suite A	Anchorage	AK	99518-30
NICOLL ALAN L & JODY L	PO BOX 520006	BIG LAKE	AK	99652
NICOLL KYLE L & AMANDA	PO BOX 520297	BIG LAKE	AK	99652
NICOLSON JOANNE E	2280 S RUE DE LA PAIX	HOUSTON	AK	99694
Niebrugge Robert	PO Box 4187	Palmer	AK	99645
NIELSEN KNUD	9820 MAINTREE DR	ANCHORAGE	AK	99507
NIELSEN SUSAN V	PO BOX 230202	ANCHORAGE	AK	99523
NIELSON DOUGLAS J	2201 FORAKER DR	ANCHORAGE	AK	99517
NIEMANN RICHARD R&CHRISTI	532 E 73RD	ANCHORAGE	AK	99518
NIEMELA SELMA M	13000 ADMIRALTY WAY	EVERETT	WA	98204
Nies Jim	P.O. Box 727	Talkeetna	AK	99676
NIESEN GREG J & LENORA H	PO BOX 670842	CHUGIAK	AK	99567
NIGSWANDER THOS S & R S	2105 OTTER ST	ANCHORAGE	AK	99504
NIGHTINGALE GORDON J& P J	2426 REDWOOD ST	ANCHORAGE	AK	99508
NILES WM F & ERIKA M	PO BOX 110685	ANCHORAGE	AK	99511
NILSEN BRUCE & SHARON M	5920 E CAMBORNE DR	PALMER	AK	99645
Nilssen Gregory	259 S ALASKA STREET	PALMER	AK	99645
NIMS JACK A	13341 REEF PL	ANCHORAGE	AK	99515
NIMS PETER W	3533 ROBIN ST	ANCHORAGE	AK	99504
Nina Davidson	PO Box 954	WILLOW	AK	99688
NIWA KYOICHIRO & MADGE	7444 TARSUS DR	ANCHORAGE	AK	99502
Nixon Penny	5224 E. Birch Drive	Palmer	AK	99645
Nixon Penny	P O BOX 1514	WASILLA	AK	99687
NOBLETT KELLY L	PO BOX 520577	BIG LAKE	AK	99652
Noel W. Woods	PO Box 827	PALMER	AK	99645
NOLFI ADRIAN PETER	234 W 12TH AVE	ANCHORAGE	AK	99501
Nolfi Jay	P.O. Box 520309	Big Lake	AK	99652
NOLFI JULE J	PO BOX 520309	BIG LAKE	AK	99652
NOLFI-DODGE NANCY ANN	PO BOX 520667	BIG LAKE	AK	99652
NOLTE JOYCE M	7644 SNOWVIEW DR	ANCHORAGE	AK	99507
NORBERG JEFFERY A&SUSAN M	PO BOX 521156	BIG LAKE	AK	99652
NORCROSS JAMES	P O BOX 521430	BIG LAKE	AK	99652
NORCROSS JAS H & IRENE E	PO BOX 242	WILLOW	AK	99688
NORD ERIC D	PO BOX 521796	BIG LAKE	AK	99652
Nord Leroy	P.O. Box 384	Palmer	AK	99645
NORDBERG ALBERTA C& ROBIN	PO BOX 625	WILLOW	AK	99688
NORDHOF ALLEN & ANITA L	PO BOX 1066	WILLOW	AK	99688
NORDSTROM ERIC M	PO BOX 672632	CHUGIAK	AK	99567
Noreen Austermuhl	PO Box 521281	Big Lake	AK	99652
NORENE LARRY G	400 W BENSON	ANCHORAGE	AK	99503

Norman & Shirley Wakeman	PO Box 988	Willow	AK	99688
Norman John	333 West 7th Avenue, Suite 100	Anchorage	AK	99501
NORMAN PAUL L & NEESHA F	PO BOX 5467	FORT RICHARDS	AK	99505
NORMAN RANDY T & TAUNA	PO BOX 521376	BIG LAKE	AK	99652
NORMAND ARTHUR B & JULIA	PO BOX 940192	HOUSTON	AK	99694
NORRIS JOHN P	PO BOX 350	WILLOW	AK	99688
NORRIS JOHN R & DONNA L	4631 SHELBURNE PL	ANCHORAGE	AK	99516
NORRIS NATALIE A	PO BOX 33	WILLOW	AK	99688
NORRIS ROBT & ROBERTA	10897 S TUTTLE RD	WASILLA	AK	99654
NORTH LINDA	PO BOX 926	WILLOW	AK	99688
NORTHEY BEN	3221 South Circle	ANCHORAGE	AK	99507
NORTHEY BEN	3221 South Circle	ANCHORAGE	AK	99507
NORTHEY BEN A & LAURIE A	3221 SOUTH CIRCLE	ANCHORAGE	AK	99507
NORTHSTAR TERM& STEVEDORE	PO BOX 102019	ANCHORAGE	AK	99510
NORTON CHAS P	PO BOX 110973	ANCHORAGE	AK	99511
NORTON DAVID C & KIM Z	PO BOX 2742	PALMER	AK	99645
NORTON KEVIN J	12170 WOODWARD DR	ANCHORAGE	AK	99516
NORUP LEROY R & YON H	68680 BAXTER TERRACE CIR	ANCHORAGE	AK	99504
NORVELL CYNTHIA A	PO BOX 671310	CHUGIAK	AK	99567
NOTEStINE LES	13401 BAYWIND DR	ANCHORAGE	AK	99516
Notti Emil	Alaska Dept. of Commerce, Community & Economic Development	Juneau	AK	99811
NOURSE STEVEN	P.O. Box 110800	BIG LAKE	AK	99652
NOVAK STEVE J & BECKY R	PO BOX 520012	ANCHORAGE	AK	99507
NOVAK VINCENT L	6761 TESHLAR DR	ANCHORAGE	AK	99522
NOVAKOVICH STEPHEN J	PO BOX 220892	ANCHORAGE	AK	99504
NOVOTNY FRANK H	8221 PIONEER DR	ANCHORAGE	AK	99652
NOWLAND DOYLE D	PO BOX 520741	BIG LAKE	AK	99502
NOYES WM J	3221 W 80TH AVE	ANCHORAGE	AK	99502
NUNLEY TIM & SENJA	PO BOX 520791	BIG LAKE	AK	99652
NUNN BELLE	2051 S. JENSEN RD	PALMER	AK	99645
NUNNALLY JERRY C& SUSAN H	PO BOX 521361	BIG LAKE	AK	99652
NUSS CONSTANCE R VAN HORN	PO BOX 365	WILLOW	AK	99688
NUSS ROB A & SHELLEY E	521 HIGH VIEW DR	ANCHORAGE	AK	99515
Nyman Dave	421 WINFIELD CIR	ANCHORAGE	AK	99515
OAKES NATHAN J & MURIEL A	911 W. 8th Avenue, Ste 100	Anchorage	AK	99501
OAKLEY RALPHE & M I	PO BOX 520469	BIG LAKE	AK	99652
OATES JEROME D JR & E T	PO BOX 125	WILLOW	AK	99688
OATES JOHN P & JANET P	PO BOX 520298	BIG LAKE	AK	99652
OBBER MARTHA C	PO BOX 92576	ANCHORAGE	AK	99509
OBREMSKI MICHAEL A&MARCIE	PO BOX 220634	ANCHORAGE	AK	99522
O'BRIEN DANNIE A	PO BOX 1091	PALMER	AK	99645
O'BRIEN GRACE R LUTHER	PO BOX 940093	HOUSTON	AK	99694
O'Brien Murph	7621 EVANDER DR	ANCHORAGE	AK	99518
	350 E. Dahlia Avenue	Palmer	AK	99645



O'BRIEN WALTER JAS & S A	PO BOX 94046	ANCHORAGE	AK	99509
O'BRIEN WM C	PO BOX 1342	PALMER	AK	99645
O'Claray Greg	P. O. Box 21149	Juneau	AK	99802-11
O'Connell Dianne	3701 E. Tudor Rd., Ste 208	Anchorage	AK	99507
O'CONNELL RICHARD W & H J	12229 W HAZEL AVE	WASILLA	AK	99654
ODELL STEPHEN G & DEBRA A	1611 JUNEAU DR	ANCHORAGE	AK	99501
ODGERS RICHARD C	935 E 79TH AVE	ANCHORAGE	AK	99518
O'DOCHARTY BRIAN S	PO BOX 1267	BARROW	AK	99723
ODOM DANNY O & TRACI E	3101 BROOKVIEW	ANCHORAGE	AK	99504
ODOM ELIZABETH	2451 W 27TH AVE	ANCHORAGE	AK	99517
ODOM JAVIS L & DELOISE	11570 ROCKRIDGE DR	ANCHORAGE	AK	99516
O'DONNELL LEON LEE JR	PO BOX 202191	ANCHORAGE	AK	99520
O'DONOVAN LANCE E	PO BOX 521837	BIG LAKE	AK	99652
ODSATHER KENNETH L & A M	3202 WESTMAR CIR	ANCHORAGE	AK	99508
OGARD ALTON & ENA	PO BOX 210047	ANCHORAGE	AK	99521
OGSTAD JERRY & KATHY	PO BOX 1100	WILLOW	AK	99688
OH KI JUNG	8808 ROCKY COVE DR	ANCHORAGE	AK	99507
O'Hara Bill	PO BOX 520931	BIG LAKE	AK	99652
O'HARA WM P & MARGARET M	PO BOX 520310	BIG LAKE	AK	99652
O'HAVER MEL N EST	5804 MILEY DR	ANCHORAGE	AK	99504
OKESON MARK	PO BOX 4135	PALMER	AK	99645
Okonek James	P.O. Box 545	Talkeetna	AK	99676
OKULEY MARCUS J & LORI A	PO BOX 520563	BIG LAKE	AK	99652
OKULEY MARCUS J & LORI A	PO BOX 520563	BIG LAKE	AK	99652
OLDHAM KEN	3650 LAKE OTIS PKY	ANCHORAGE	AK	99508
O'LEARY DIANA FRANCES	PO BOX 202028	ANCHORAGE	AK	99520
OLIVER JAS L. & CYNTHIA J.	3705 ARCTIC BLVD	ANCHORAGE	AK	99503
OLIVER RODNEY WAYNE	PO BOX 201268	ANCHORAGE	AK	99520
OLSEN CHARLES E & SARA	19818 TOFSON ST	CHUGIAK	AK	99567
OLSEN DALE B & MARY M	3350 S SKY RANCH LOOP	PALMER	AK	99645
OLSEN GARY A & ALICE L	PO BOX 52	WILLOW	AK	99688
OLSEN LARRY W	628 E 74TH AVE	ANCHORAGE	AK	99518
OLSEN ROSELINE G	PO BOX 521432	BIG LAKE	AK	99652
OLSEN ROY O	2136 E 37TH AVE	ANCHORAGE	AK	99508
OLSON BRIAN E & TINA L	2030 COLONY LOOP	ANCHORAGE	AK	99507
OLSON CHAD MICHAEL	203 BUNNELL ST	ANCHORAGE	AK	99508
OLSON CHRISTOPHER L	15674 W MILLERS REACH RD	HOUSTON	AK	99694
Olson Dana	HC-30 Box 5438	Wasilla	AK	99654
OLSON DENISE M	3200 DISCOVERY BAY DR	ANCHORAGE	AK	99515
OLSON DENNIS & STEPHANIE	PO BOX 521327	BIG LAKE	AK	99652
Olson Don	12049 Spring Brook Drive	Eagle River	AK	99577
Olson Donny	716 W. 4th Avenue, Ste 510	Anchorage	AK	99501
Olson Donny	State Capitol, Room 560	Juneau	AK	99801-11

OLSON DOUGLAS L	1150 N GOLDEN HILLS DR	PALMER	AK	99645
OLSON ERIC A & TRACY E	11930 PORTAGE DR	ANCHORAGE	AK	99515
OLSON ETHEL ANN	PO BOX 521074	BIG LAKE	AK	99652
OLSON GARY LEE	PO BOX 221831	ANCHORAGE	AK	99522
OLSON GERALD H&JAQUELYN S	2330 S BANBURY CT	PALMER	AK	99645
OLSON GLENN A & DIANE H	2515 SUSITNA DR	ANCHORAGE	AK	99517
OLSON JOHN E	8641 BLACKBERRY ST	ANCHORAGE	AK	99502
OLSON KEN J	PO BOX 3312	PALMER	AK	99645
OLSON RHODA L	1570 S MIDDLE MESA DR	PALMER	AK	99645
OLSON RYAN L	PO BOX 1156	PALMER	AK	99645
OLSON TREVOR	PO BOX 3370	PALMER	AK	99645
O'MALLEY TIMOTHY J	PO BOX 622	WILLOW	AK	99688
O'MARY CODY L	PO BOX 878	WILLOW	AK	99688
ONDECK JUDY N	9524 WESTLAKE DR	EAGLE RIVER	AK	99577
ONDERIK PAULETTE	PO BOX 701	WILLOW	AK	99688
ONDOLA WALTER & JANET L	3400 S BUTTE RD	PALMER	AK	99645
O'NEILL BASIL E& JANET TR	1353 M ST	ANCHORAGE	AK	99501
O'NEILL DIANE	8510 E 10TH AVE	ANCHORAGE	AK	99504
O'Neill Ed	Brown Jug, Inc., Alaska's Retailer of Fine Wines, S P.O. Box 190027	Anchorage	AK	99519-00
O'Neill Gloria	3600 San Jeronimo Drive	Anchorage	AK	99508
O'NEILL JAS & KATHERINE A	1110 KAYLIN CIR	ANCHORAGE	AK	99515
O'NEILL KERI	2613 W 27TH AVE	ANCHORAGE	AK	99517
O'NEILL WM A	3550 ALAMOSA DR	ANCHORAGE	AK	99502
ONSTOTT RICK & MICHELE	PO BOX 231411	ANCHORAGE	AK	99523
OPINSKY ROBT & EDITH	1605 W 12TH AVE	ANCHORAGE	AK	99501
OPSAHL RICHARD J	3624 W 78TH AVE	ANCHORAGE	AK	99502
ORGUTT DOUGLAS LEE	PO BOX 190843	ANCHORAGE	AK	99519
Orie Orien	PO Box 520716	BIG LAKE	AK	99652
ORIEN O ANDREW & RUTH N	9701 HOMESTEAD TRL	ANCHORAGE	AK	99507
ORIGER DARIN J	PO BOX 468	WILLOW	AK	99688
ORIGER GREG A & SARA V	PO BOX 378	WILLOW	AK	99688
O'ROURKE RUTH E	PO BOX 1786	BARROW	AK	99723
OROZCO CARLOS A	7621 MARYLAND AVE	ANCHORAGE	AK	99504
ORR CYNTHIA J	9211 STRATHMORE DR	ANCHORAGE	AK	99502
ORR ROBT JAS & LUCY M	PO BOX 520484	BIG LAKE	AK	99652
ORTNER JOSE	PO BOX 95	PALMER	AK	99645
ORTWIG MICHAEL A & DANICE	4170 LAKERIDGE LN	ANCHORAGE	AK	99502
ORZECHOWSKI MICHAEL W & J	12142 WOODWAY CIR	ANCHORAGE	AK	99516
OSBORNE HELEN	9480 MORNINGSIDE LOOP	ANCHORAGE	AK	99515
OSNESS MICHAEL J	801 AIRPORT HGTS	ANCHORAGE	AK	99508
OST MATTHEW E & DIANNA H	PO BOX 92693	ANCHORAGE	AK	99509
OSWALD DONALD T & MARCIA L	PO BOX 521861	BIG LAKE	AK	99652
OSWALD MAURY A	3260 AMBER BAY LOOP	ANCHORAGE	AK	99515

OSWALD RONNIE E& PATRICIA	7011 GIBBS HILL CIR	ANCHORAGE	AK	99504
OTTLEY THOS R & ROXIE L	4640 NEWCASTLE WAY	ANCHORAGE	AK	99503
OTTO CYNTHIA L	2626 MAPLEWOOD	ANCHORAGE	AK	99508
OUDEKIRK ERIC A& SHELLEY	PO BOX 907	WILLOW	AK	99688
OUELLETTE THOS & VALERIE	PO BOX 114	WILLOW	AK	99688
OURADA DUKE A	HC 89 BOX 471	WILLOW	AK	99688
OUSLEY RONNIE	3421 ARCTURUS CIR	ANCHORAGE	AK	99517
OVERLAND JON W& KATHERINE	1075 PINION DR	WASILLA	AK	99654
OVERTON RAYMOND E & J E	2550 MELVIN AVE	ANCHORAGE	AK	99517
OWEN CHRISTINA	PO BOX 891	WILLOW	AK	99688
OWEN GRACE N	915 INGRA	ANCHORAGE	AK	99501
Owen Richard	PO Box 196601	Anchorage	AK	99519-66
OWENS CHRIS C	18910 TWENTY GRAND RD	EAGLE RIVER	AK	99577
Owner	P.O. Box 202001	Anchorage	AK	99520-20
OWNERS OF BLOCK 7 WITHIN	GENERAL DELIVERY	WILLOW	AK	99688
OWSLEY SADIE M	6907 E 16TH AVE	ANCHORAGE	AK	99504
OXLEY LINDA M	PO BOX 908	WILLOW	AK	99688
OZUNA CASEY RAYMOND	PO BOX 672065	CHUGIAK	AK	99567
PAAL JEAN M	1420 W 11TH AVE	ANCHORAGE	AK	99501
PACE WM T & CHRIS A	5966 E CUMULUS CIR	PALMER	AK	99645
PADEN BRIAN D	2620 CURLEW CIR	ANCHORAGE	AK	99502
PADIE BOB G & LINDA L	PO BOX 940046	HOUSTON	AK	99694
PADIE MARCELLA	PO BOX 940054	HOUSTON	AK	99694
PADRICK STEPHEN REX	13661 KAREN CIR	ANCHORAGE	AK	99515
PAGE CHRISTOPHER	237 N PARK	ANCHORAGE	AK	99508
PAGE PAUL DAN'L	1400 S DOWN WIND CIR	PALMER	AK	99645
PAIGE MITCHELL B & TINA C	1521 HELEN DR	ANCHORAGE	AK	99515
PAIGE-JACOBSON V REV TR	PO BOX 343	WILLOW	AK	99688
PAINTER H RUSSELL & R G	2181 BELMONT DR	ANCHORAGE	AK	99517
PALEMBAS JAS M JR	7655 E SAPHIRE RD	PALMER	AK	99645
Palin Faye	3362 NAOMI AVE	WASILLA	AK	99654
PALIN JAS D	PO BOX 201043	ANCHORAGE	AK	99520
Palin Sarah	P.O. Box 110001	Juneau	AK	99811-00
PALMATIER MARK L	531 E 46TH PL	ANCHORAGE	AK	99503
PALMATIER RICHARD & TWILA	PO BOX 1040	WILLOW	AK	99688
PALMER ALLEN E & D J	1140 PINION DR	WASILLA	AK	99654
PALMER DAVID R & E H	7000 E LOWER MESA DR	PALMER	AK	99645
PALMER FLORENCE	15400 E SWAPS LN	PALMER	AK	99645
Palmer Jim	1400 W Benson Blvd, Suite 350	Anchorage	AK	99503
Palmer Jim	10346 Stewart Drive	Eagle River	AK	99577
PALMER WM & REGINA	1826 THUNDERBIRD PL	ANCHORAGE	AK	99508
PALMQUIST DAVID E II	1404 RICHARDSON	ANCHORAGE	AK	99504
Panamaroff Thomas	4300 B St Ste 408	Anchorage	AK	99503

ExxonMobil

Alaska Sales & Service

MSB Planning Commission

Office of the Governor

The Palmer Group  
The Palmer Group

Koniag, Inc.



PATRICK INGA M			PO BOX 521807	BIG LAKE	AK	99652
Patrick Judy	Judy Patrick Photography		430 W 7th Ave Ste 220	Anchorage	AK	99501
PATRICK MONIKA			PO BOX 521705	BIG LAKE	AK	99652
Patrick Sharrock			8731 Solar Drive	Anchorage	AK	99507
PATRICK WILLIAM R			PO BOX 520413	BIG LAKE	AK	99652
PATTERSON CHRIS L			PO BOX 520510	BIG LAKE	AK	99652
PATTERSON DAVID & MARSHA			104 AMBER	CALDWELL	ID	83065
Patterson Donald			P.O. Box 162	Talkeetna	AK	99676
PATTERSON GARY W & M E			7804 LINDA LN	ANCHORAGE	AK	99518
PATTERSON JAS M & J M			1321 S BONANZA ST	PALMER	AK	99645
PATTERSON MARC S			3200 W 30TH	ANCHORAGE	AK	99517
PATTERSON MICHAEL J			810 W SECOND AVE	ANCHORAGE	AK	99501
PATTERSON RHONDA L			PO BOX 521525	BIG LAKE	AK	99652
PATTERSON SCOTT & BETTY			PO BOX 118	WILLOW	AK	99688
PATTON MICHAEL J & TERRY L			1303 S CENTER POINT DR	WASILLA	AK	99654
Paul Gardner			580 Briar dr	Wasilla	AK	99654
PAUL LANCE A			PO BOX 521514	BIG LAKE	AK	99652
PAUL MICHAEL G			PO BOX 521776	BIG LAKE	AK	99652
Paul R. DuClos Jr.			P. O. Box 520631	Big Lake	AK	99652
PAULEY VALLI R CHAPMAN			3501 BALCHEN	ANCHORAGE	AK	99517
PAULSON DEAN & MINDY			736 WINTER HAVEN ST	ANCHORAGE	AK	99504
PAULSON NATHAN & SARA			6006 W DIMOND BLVD	ANCHORAGE	AK	99502
PAVISH STEPHEN L			PO BOX 1075	WILLOW	AK	99688
PAYNE DAN'L E & EILEEN M			PO BOX 520492	BIG LAKE	AK	99652
PEABODY CHARANNE R			PO BOX 520494	BIG LAKE	AK	99652
Peabody Will	Lazy Mountain Community Council		HCO4 Box 9110	Palmer	AK	99645
PEACE TIMOTHY			PO BOX 695	WILLOW	AK	99688
PEARL BRIAN D & TERRY L			HC 89 BOX 486	WILLOW	AK	99688
PEARSON GAIL M CARTWRIGHT			1436 M ST	ANCHORAGE	AK	99501
Pearson Julie	Denali Borough School Board		PO Box 3037	Anderson	AK	99744
Pearson Leslie	Alaska Dept. of Environmental Conservation		555 Cordova Street	Anchorage	AK	99501
PEARSON LLOYD D			9499 BRAYTON DR	ANCHORAGE	AK	99507
Pease Mary Ann	MAP Consulting		11640 Woodbourne Dr.	Anchorage	AK	99516
Peck Ron	Alaska Travel Industry Association		2600 Cordova St., Ste 201	Anchorage	AK	99503-27
PECK VELMA D			1250 BOGARD RD	WASILLA	AK	99654
PEDERSEN STEVEN N			4715 BLUE HERON CIR	ANCHORAGE	AK	99507
PEDERSEN WM J & VICKI L			16704 VIRGO AVE	ANCHORAGE	AK	99516
PEDERSON HAROLD L & DIANE			925 R ST	ANCHORAGE	AK	99501
PEDERSON JOHN G			PO BOX 1908	PALMER	AK	99645
PEDERSON JUSTIN L			PO BOX 670552	CHUGIAK	AK	99567
PEDERSON WM & NANCY			PO BOX 521706	BIG LAKE	AK	99652
PEEL JAS O & LENA G			305 OSCAR ST	PALMER	AK	99645
PEET JAS V			724 FISCHER AVE	ANCHORAGE	AK	99518

Peg Watkinson	2855 S Horseshoe Lake Rd	Big Lake	AK	99652
PELCH STEPHEN D	808 S BAILEY ST # 103	PALMER	AK	99645
PELCHAT RALPH & BARBARA A	PO BOX 983	WILLOW	AK	99688
PELLETT RICHARD J & J A	PO BOX 521132	BIG LAKE	AK	99652
PELTIER DAVID & ANN	18934 ANDREANOF LOOP	EAGLE RIVER	AK	99577
Peltier Ellen	PO BOX 13176	TRAPPER CREEK	AK	99683
PELTIER ELLEN	P O BOX 955	PALMER	AK	99645
PELTIER KENNETH & AMY	PO BOX 2433	PALMER	AK	99645
Peltz Larry	222 West 7th Avenue, Room 517	Anchorage	AK	99513
PENA-GARCA MARIO ALBERTO	PO BOX 100940	ANCHORAGE	AK	99510
PENDERGRASS HIRAM E&MAIKO	PO BOX 3591	PALMER	AK	99645
PENDLETON CYNTHIA M	3201 REDOUBT CT	ANCHORAGE	AK	99517
PENMAN ROBT G & W LARAINÉ	807 W 72ND AVE	ANCHORAGE	AK	99518
PENNINGTON DENNIS L & K K	PO BOX 4386	PALMER	AK	99645
PENNINGTON MARK W&DIANE F	4806 BUCKINGHAM WAY	ANCHORAGE	AK	99503
PEPPEL THOS J	PO BOX 520691	BIG LAKE	AK	99652
PEPPERS JOHN M JR	PO BOX 230994	ANCHORAGE	AK	99523
PEPPINGER THAD A	801 AIRPORT HGTS DR	ANCHORAGE	AK	99508
PEREGRINE PROP INC	7421 WOBURN CIR	ANCHORAGE	AK	99502
PEREIRA JAMES DANIEL	PO BOX 521106	BIG LAKE	AK	99652
PEREZ RAUL A	PO BOX 565	WILLOW	AK	99688
PERHAM CRAIG JAS	18940 TALARIK ST	EAGLE RIVER	AK	99577
PERKINS CAROL J	22012 VALLEY AVE	CHUGIAK	AK	99567
PERKINS HENRY B	PO BOX 671475	CHUGIAK	AK	99567
PERKINS WM O & M JEANETTE	6171 E DEARBORN DR	PALMER	AK	99645
Perfman Randi	P.O. Box 520067	Big Lake	AK	99652
PERLMAN RANDI	PO BOX 521186	BIG LAKE	AK	99652
Perman Ira	421 W. 1st Avenue, Suite 300	Anchorage	AK	99501
PERONTO JIM & ANGELA	1770 ELCADORE DR	ANCHORAGE	AK	99507
PERRON MAVIS E	2150 BELMONT DR	ANCHORAGE	AK	99517
PERROZZI JOSEPH A& JANE I	1559 L STREET	ANCHORAGE	AK	99501
PERRY ARNOLD C&LORRAINE T	7621 E 34TH AVE	ANCHORAGE	AK	99504
PERRY DAVID P & JUNE A	5817 E UPDRAFT RD	PALMER	AK	99645
PERRY KEITH D	205 E DIMOND BLVD	ANCHORAGE	AK	99515
Perry Kris	550 W. 7th Avenue, Suite 1700	Anchorage	AK	99501
PERRY ROBERT M	PO BOX 520529	BIG LAKE	AK	99652
PERRY RONALD C & LENA	2150 IVAN DR	ANCHORAGE	AK	99507
PERSSON ERIC J & JANALEE	3120 BETTLES BAY LOOP	ANCHORAGE	AK	99515
PETERKIN JOHN R JR	4311 E 3RD AVE	ANCHORAGE	AK	99508
PETERLLA DENNIS L & K L	PO BOX 520784	BIG LAKE	AK	99652
PETERS GREGORY M	HC 89 BOX 325	WILLOW	AK	99688
PETERS JAROD L	PO BOX 221809	ANCHORAGE	AK	99522
PETERS JOHN & TERESA	PO BOX 521764	BIG LAKE	AK	99652

National Marine Fisheries Service

Big Lake Chamber of Commerce

Alaska Humanities Forum

Office of the Governor - Anchorage

PETERS KENNETH & KAREN	9900 LONE TREE DR	ANCHORAGE	AK	99507
PETERSEN CHRIS A& KAREN M	7320 BIGLERVILLE CIR	ANCHORAGE	AK	99507
PETERSEN DAWAYNE	PO BOX 521466	BIG LAKE	AK	99652
PETERSEN DONALD R & DIANA	1125 OCEANVIEW DR	ANCHORAGE	AK	99515
PETERSEN DORIS M	3550 W DIMOND BLVD	ANCHORAGE	AK	99502
PETERSEN GARY & DEBRA	2011 JARVIS	ANCHORAGE	AK	99515
PETERSEN GARY W	545 N TAYLOR ST	ANCHORAGE	AK	99508
PETERSEN IRENE	PO BOX 91000	ANCHORAGE	AK	99509
PETERSEN JAS F	12541 TANADA LOOP	ANCHORAGE	AK	99515
PETERSEN JEAN	PO BOX 521046	BIG LAKE	AK	99652
PETERSEN KELLY SUMMERS	4100 TAFT DR	ANCHORAGE	AK	99517
Petersen Robert	P.O. Box 586	Talkeetna	AK	99676
PETERSEN RONALD W&NELLY B	PO BOX 210274	ANCHORAGE	AK	99521
PETERSEN Sam	HC 01 Box 6077-P	Palmer	AK	99645-96
PETERSON GEO E	1051 N LUCILLE ST	WASILLA	AK	99654
PETERSON ALBERTA M	3351 MONTICELLO CT	ANCHORAGE	AK	99503
PETERSON AMY E	PO BOX 201117	ANCHORAGE	AK	99520
PETERSON BRIAN P	2351 S COGGIN DR	PALMER	AK	99645
PETERSON DEVERL& D LOUISE	902 LANCASTER DR	ANCHORAGE	AK	99503
PETERSON GREGORY A&PAMELA	12821 GAIL ST	ANCHORAGE	AK	99515
PETERSON JANICE M	1030 DENALI ST	ANCHORAGE	AK	99501
PETERSON JULE R&MARLENE M	13100 BISCAYNE CIR	ANCHORAGE	AK	99516
PETERSON KENT D&KRYSTAL L	8300 EXETER DR	ANCHORAGE	AK	99507
PETERSON MICHAEL D SR	11719 S DONNY BROOK DR	WASILLA	AK	99654
PETERSON PAUL	2961 DRAKE DR	ANCHORAGE	AK	99508
PETERSON ROBT L & PAULINE	5132 E 42ND AVE	ANCHORAGE	AK	99508
PETERSON THELMA C	5736 RADCLIFF DR	ANCHORAGE	AK	99504
PETITO STAN L	5010 SETON CIR	ANCHORAGE	AK	99508
PETRAEUS E H	PO BOX 241843	ANCHORAGE	AK	99524
PETRAM WM A & SONDRAL	PO BOX 529	WILLOW	AK	99688
PETRIK WM A	531 MARY CIR	ANCHORAGE	AK	99515
PETROS AARON	PO BOX 670311	CHUGIAK	AK	99567
PETRUSA TERRY J & AMY B	3225 ROBIN ST	ANCHORAGE	AK	99504
PETTIT RICKIE L & M J	13693 SUNSET VIEW	ANCHORAGE	AK	99515
PEVAN RICHARD SCOTT	PO BOX 1077	WILLOW	AK	99688
PFEIFER BARBARA S	PO BOX 770557	EAGLE RIVER	AK	99577
PFEIFER CRAIG	1859 BANNISTER ST	ANCHORAGE	AK	99508
PFEIFER RICHARD JOHN	4301 NEEDLE CIR	ANCHORAGE	AK	99508
PFEIFER SCOTT & ANNE L	9447 NOBLEWOOD ST	ANCHORAGE	AK	99515
PFEIFFER PAUL J & DIANA L	7020 LINDEN CIR	ANCHORAGE	AK	99502
PFEIFFER BRENT & JODI L	125 S VIX WAY	WASILLA	AK	99654
PFLIEGER RODNEY L & A J	7751 WANDERING DR	ANCHORAGE	AK	99502
PFLUGH GEROLD J SR	PO BOX 82	WILLOW	AK	99688

Clark-Wiltz Mining  
Arctic Pipe Inspection

PHALON PAUL R	10927 S LOSTWOOD DR	SANDY	UT	84092
PHALP WM C JR & SUSAN K	3300 W 78TH	ANCHORAGE	AK	99502
PHELAN ALBERT R & LYNDA K	5684 E MORNING MIST DR	PALMER	AK	99645
PHELPS PRISCILLA M.	PO BOX 4865	PALMER	AK	99645
PHILLIPS JOHN L	PO BOX 1189	PALMER	AK	99645
PHILLIPS KYONG S FREEMAN	7850 LOTUS DR	ANCHORAGE	AK	99502
PHILLIPS LARRY I	PO BOX 111304	ANCHORAGE	AK	99511
PHILLIPS MARK W & CAROL J	1314 BIRCHWOOD ST	ANCHORAGE	AK	99508
PHILLIPS MARTIN	PO BOX 521151	BIG LAKE	AK	99652
Phillips Michael	P.O. Box 4581	Palmer	AK	99645
PHILLIPS ROBT A & M M	PO BOX 520195	BIG LAKE	AK	99652
PHILLIPS THOS H& GLORIA L	PO BOX 593	WILLOW	AK	99688
PHIPPS JAS A & KRISTINE L	PO BOX 520097	BIG LAKE	AK	99652
PIASKOWSKI MARY I	3400 SKIPPER ST	ANCHORAGE	AK	99504
PICCARD ALAN R	3911 LARON LN	ANCHORAGE	AK	99504
PICHLER NORMAN	3301 E TUDOR RD	ANCHORAGE	AK	99507
PICHLER RON L & MARGO F	16216 ESSEX PARK DR	ANCHORAGE	AK	99516
PICKARSKI MONICA	PO BOX 520497	BIG LAKE	AK	99652
PICKENS MARVIN E	20341 PHILADELPHIA WAY	EAGLE RIVER	AK	99577
PICKERING JAS M & LAVEETA	PO BOX 520077	BIG LAKE	AK	99652
PICKERING ROBT& CHARLOTTE	PO BOX 113	WILLOW	AK	99688
PICKETT ROBT M	PO BOX 1029	PALMER	AK	99645
Piekarski Andrew	2501 Commercial Dr # 140	Anchorage	AK	99501
PIENING YONG	918 S COLONY WAY	PALMER	AK	99645
PIERCE HAROLD & TRACIE L	3327 W 30TH AVE	ANCHORAGE	AK	99517
PIFER STEVEN R & TRACY J	23108 SHELTERING SPRUCE	CHUGIAK	AK	99567
PIKE GREGORY M	5137 SILLARY CIR	ANCHORAGE	AK	99508
PINEAULT WM A	1095 DOLPHIN WAY	FAIRBANKS	AK	99709
PINKERTON FRANK W.	1101 CORDOVA ST	ANCHORAGE	AK	99501
PINKLEY DALLENE A	PO BOX 670965	CHUGIAK	AK	99567
PINNICK VIRGINIA	PO BOX 520244	BIG LAKE	AK	99652
PINSON LORNA E	3427 CORONA CIR	ANCHORAGE	AK	99517
PIPER ROSS C & MARY E	3326 JERDE CIR	ANCHORAGE	AK	99504
PIPPEL ANTHONY J & MIRIAM	PO BOX 1807	PALMER	AK	99645
PippeI Tony	P.O. Box 1807	Palmer	AK	99645
PISK JAMES & WENDY	3500 DEARMOUN RD	ANCHORAGE	AK	99516
PITCHER FLORENCE	18226 S. Guernsey Rd.	Wasilla	AK	99687
PITNER MILO D	PO BOX 798	WILLOW	AK	99688
PITTS TAMMY	PO BOX 521425	BIG LAKE	AK	99652
PLATZEK WM A	2020 MULDOON RD # 45	ANCHORAGE	AK	99508
PLAYLE CLINTON K& KAREN A	1900 S CHURCH ST	ANCHORAGE	AK	99645
Playle Linton	HC 01 Box 6219	Palmer	AK	99645
PLEW SHAWN M & CRYSTAL L	2610 BRYANT CIR	ANCHORAGE	AK	99507

Laborers Int'l Union of North America, 341

City of Palmer

Quality Moving and Storage



Pluard CHAS P & KRISTINA	1061 N JACK NICKLAUS DR	WASILLA	AK	99654
PLUMMER BRIAN J	PO BOX 242462	ANCHORAGE	AK	99524
PLUMMER GREGORY G	5811 TINIAN AVE	ANCHORAGE	AK	99507
PLUMMER RAYMOND E	11700 NORTHERN RAVEN DR	ANCHORAGE	AK	99516
PLUNKETT JAS	2750 SCARBOROUGH DR	ANCHORAGE	AK	99504
PLZAK DUANE C	3840 CAMPBELL AIRSTRIP RD	ANCHORAGE	AK	99504
POAGE ALLEN	PO BOX 945	WILLOW	AK	99688
POCK JILL H	PO BOX 520102	BIG LAKE	AK	99652
PODBIELSKI TIMOTHY H	1201 N PINECONE CIR	WASILLA	AK	99654
PODLEJSKI CHRISTINE	2525 HIDDEN RETREAT PL	ANCHORAGE	AK	99508
PODVIN DANISE W	23212 GREEN GARDEN CIR	CHUGIAK	AK	99567
PODVIN JAS K & MARY B	4841 RIDGE TOP CIR	ANCHORAGE	AK	99508
Poe Robert	900 W. 5th Avenue, #300	Anchorage	AK	99501
POHL STEVEN P	PO BOX 97	WILLOW	AK	99688
POHLAND CHAS A & MAXINE G	3410 N SHORE DR	ANCHORAGE	AK	99515
POIRIER GERALD E EST	PO BOX 1811	PALMER	AK	99645
POIROT DAN'L E	PO BOX 520168	BIG LAKE	AK	99652
POLINSKY JOS R	531 MELLOW PL	ANCHORAGE	AK	99508
POLLARD E JEAN	1406 ERMINE ST	ANCHORAGE	AK	99504
POLLARD PATRICIA L	PO BOX 520862	BIG LAKE	AK	99652
Pollock Dan	P.O. Box 643	Palmer	AK	99645
POLSKY JUSTEN J	7474 MEADOW ST	ANCHORAGE	AK	99507
POLUSHKIN KORNILY	PO BOX 487	WILLOW	AK	99688
POMPA GLORIA E HERNANDEZ	PO BOX 200485	ANCHORAGE	AK	99520
Pond Dale Ann	P.O. Box 940027	Houston	AK	99694
POND DAVID D & DALE ANN	PO BOX 520603	BIG LAKE	AK	99652
POND ROBT J	PO BOX 521309	BIG LAKE	AK	99652
PONSNESS STANLEY E & K D	8900 TEMPEST CIR	ANCHORAGE	AK	99507
POOLE ROBT L & NOMA L	PO BOX 521043	BIG LAKE	AK	99652
POOLER IVAN C & PATRICIA	PO BOX 233092	ANCHORAGE	AK	99523
POON WM F T & GREENWOOD C	1220 E 68TH	ANCHORAGE	AK	99518
POORBOY CARL E	2906 DORIS ST	ANCHORAGE	AK	99517
POPE RICHARD C	3117 S CIRCLE	ANCHORAGE	AK	99507
POPLARCHIK THEO JOHN	4651 KENT ST	ANCHORAGE	AK	99503
PORTER DAVID	PO BOX 521363	BIG LAKE	AK	99652
PORTER KELLY J.	1611 S ABBY BLVD	WASILLA	AK	99654
PORTER N CLAIBORNE JR & M	1834 STANFORD DR	ANCHORAGE	AK	99508
Porter Sandra	PO BOX 13152	TRAPPER CREEK	AK	99683
PORTERFIELD THOS E	3309 WISCONSIN	ANCHORAGE	AK	99517
PORTLOCK LUCILLE D	2970 PRINCETON WAY	ANCHORAGE	AK	99508
Portwood Vicki	8301 Schoon Street, Suite 200	Anchorage	AK	99518
POSPISIL GORDON	3001 MCCOLLIE AVE	ANCHORAGE	AK	99517
POST DEBORAH	13121 SCOTTIE CT	ANCHORAGE	AK	99515

Anchorage Economic Development Corp.

Specialized Woodworks

City of Houston

Alaska State Homebuilders Association

POST HARRY H JR	6487 VILLAGE PKWY	ANCHORAGE	AK	99504
POST STEVEN J & DIANNA L	21308 LOWLAND AVE	EAGLE RIVER	AK	99577
POSTISHEK ANDREW W & NANCY	5823 E MORNING MIST DR	PALMER	AK	99645
POSTON DENNIS	PO BOX 520068	BIG LAKE	AK	99652
POTTER DALE PHILLIP	PO BOX 671623	CHUGIAK	AK	99567
POTTER JEANETTE Y ALLISON	211 E COOK AVE	ANCHORAGE	AK	99501
POTTER JEFFERY C & KARIN	1298 MASTERS DR	CRANDALL	TX	75114
POTTER TAMMY L BAILEY	PO BOX 940243	HOUSTON	AK	99694
POTTER TIMOTHY C & LORI J	1800 STANFORD DR	ANCHORAGE	AK	99508
Potts Jeff	P.O. Box 2587	Palmer	AK	99645
POTTS JOANNE E	HC 89 BOX 530	WILLOW	AK	99688
POUBLON ROBT D & TARA S	PO BOX 142403	ANCHORAGE	AK	99514
Pourchot Pat	441 W. 5th Avenue, Ste 402	Anchorage	AK	99501-23
POWELL CRAIG	4715 EGRET ROCK CIR	ANCHORAGE	AK	99507
POWELL HARRIET	800 DOGWOOD ST	ANCHORAGE	AK	99501
POWELL SCOTT E & CATHALEEN	9571 ARLENE DR	ANCHORAGE	AK	99515
POWERS BILLY RAY	PO BOX 233252	ANCHORAGE	AK	99523
POWERS BURTON J & S W	PO BOX 190563	ANCHORAGE	AK	99519
POWERS CHARLIE & KATHY	5925 FRANKLIN DR	ANCHORAGE	AK	99518
POWERS JOHN & CARRIE C	PO BOX 3347	PALMER	AK	99645
POWERS MARK D	9450 GROVER DR	ANCHORAGE	AK	99516
POYFAIRE MARTIN P	2639 N TAHITI LOOP	ANCHORAGE	AK	99507
PRATT SHAWN	1200 N CLINTON CIR	WASILLA	AK	99654
PRENTKI RICHARD T & K G	15100 E KENT ST	PALMER	AK	99645
PRESLEY JOHN D	430 DONNA DR	ANCHORAGE	AK	99504
PRESTEGARD KENNETH & LYNN	PO BOX 5174	FORT RICHARDS	AK	99505
PRESTON RAYMOND & BLANCHE	8640 GEIRINHAS PL	ANCHORAGE	AK	99507
PRIBENOW LUCY	2815 CAMPBELL AIRSTRIP RD	ANCHORAGE	AK	99504
Price Amanda	5761 RIGI ST	ANCHORAGE	AK	99507
PRICE ANTHONY A	1057 W. Fireweed Lane, Ste 100	Anchorage	AK	99503
PRICE JOHN E JR	540 OCEANVIEW DR	ANCHORAGE	AK	99515
PRICE MYONG	PO BOX 940452	HOUSTON	AK	99694
PRICE PAULA M	2612 COTTONWOOD ST	ANCHORAGE	AK	99508
PRICE WAYMAN R	PO BOX 92075	ANCHORAGE	AK	99509
Price William "Bill"	PO BOX 521625	BIG LAKE	AK	99652
PRIEBE BRIAN L & VICKI M	16515 Centerfield Drive, #201	Eagle River	AK	99577
PRIMERA STEPHEN	PO BOX 589	WILLOW	AK	99688
PRINCE DEVERY L & DIANNE D	PO BOX 672213	CHUGIAK	AK	99567
PRISAZNUK DANIEL R	8747 PLUTO DR	ANCHORAGE	AK	99507
PROBASCO LEONARD R	PO BOX 190806	ANCHORAGE	AK	99519
Probasco Peter	PO BOX 520407	BIG LAKE	AK	99652
Prosser CAROLINE W	P.O. Box 861	Palmer	AK	99645
	1111 GOLDEN DAWN CIR	ANCHORAGE	AK	99515

Alaska Conservation Foundation

American Heart Association

Eklutna, Incorporated

PROUDFOOT ARCHIE	PO BOX 232101	ANCHORAGE	AK	99523
PRUDIC JIRI	PO BOX 520381	BIG LAKE	AK	99652
PRUTT MITCHELL K& SUSAN K	22647 FRIENDSHIP LN	CHUGIAK	AK	99567
PRYOR DONALD V & ELLEN B	2421 BRITTANY CIR	ANCHORAGE	AK	99504
PRYS DAVIEL J & BRANDI L	PO BOX 520307	BIG LAKE	AK	99652
PSENAK JAS	PO BOX 419	PALMER	AK	99645
PUDDICOMBE WM J& RAMONA L	6951 E LOWER MESA DR	PALMER	AK	99645
PUDWILL W R & DIXON ANN R	PO BOX 1009	WILLOW	AK	99688
Puff Jack	P.O. Box 196650	Anchorage	AK	99519
PUHR THERESA L HIEBERT	4050 MARIAH DR	EAGLE RIVER	AK	99577
PULVER JOEL D	1100 N ARNOLD PALMER ST	WASILLA	AK	99654
PURCELL JACQUELINE K	431 E 46TH PL A	ANCHORAGE	AK	99503
Purcell Pat	5280 E. Alder Drive	Wasilla	AK	99654
PURCELL ROGER & BIANCA M	PO BOX 521557	BIG LAKE	AK	99652
PURCELL ROGER, PRESIDENT	PO BOX 940231	HOUSTON	AK	99694
PURCELL ROGER, PRESIDENT	PO BOX 940231	HOUSTON	AK	99694
PURCELLA RICK A & KATHY A	3701 E 121ST AVE	ANCHORAGE	AK	99516
PURKEY SCOTT L	PO BOX 987	WILLOW	AK	99688
PURNELL-JOHNSON JO MASON	7008 WINDSOR PL	ANCHORAGE	AK	99502
PURSELL HARRY JOS	13751 VENUS WAY	ANCHORAGE	AK	99515
PURSER CAROLYN	PO BOX 2445	PALMER	AK	99645
PURSLEY SHANNON K	1626 LOGAN ST	ANCHORAGE	AK	99508
PURVANCE SHAWN & LORI	2910 MONARCH CIR	ANCHORAGE	AK	99516
PYLANT LOUIS R&MARGARET I	PO BOX 771787	EAGLE RIVER	AK	99577
PYSZ KENNETH J	8161 E COTTRELL CAMPUS DR	PALMER	AK	99645
Quaas Marty & Agnes	21550 E. Arnold Circle	Palmer	AK	99645-82
Quaintance Bob	P.O. Box 13249	Trapper Creek	AK	99683
QUAM ERIC J	7447 WATERFALL DR	EAGLE RIVER	AK	99577
QUANTE DONNA J	PO BOX 1085	WILLOW	AK	99688
QUEENER MYRNA T	PO BOX 982	WILLOW	AK	99688
QUELETTE ALFRED & B T	3300 MULDOON RD	ANCHORAGE	AK	99503
Quesnel Chiree	17310 Alice Loop	Eagle River	AK	99577
QUEZADA ALEXANDER &MARY R	1578 NICHOLS ST	ANCHORAGE	AK	99508
Quill Bonnie	HC 01 Box 6166 J21	Palmer	AK	99645
QUIMBY KARL & CORAL	3014 KERRY CIR	ANCHORAGE	AK	99504
QUINCY RONALD E & P P	213 PEPPERTREE LOOP	ANCHORAGE	AK	99504
QUINN LEWIS G & GAYLE A	3529 COTTONWOOD ST	ANCHORAGE	AK	99508
QUINN PHILIP & ELIZABETH	PO BOX 212934	ANCHORAGE	AK	99521
QUINN RICHARD S	PO BOX 770155	CHUGIAK	AK	99567
RABUNG ANDREW J & JULIE A	22322 WAPITI CIR	CHUGIAK	AK	99567
RABUNG DONALD E & DIANNE	PO BOX 220428	ANCHORAGE	AK	99522
RACH TIFFANY	PO BOX 836	WILLOW	AK	99688
RADER DEEANA	8177 LAKE OTIS PKY	ANCHORAGE	AK	99507

Municipality of Anchorage

Mat-Su Borough School Board

HOUSTON CHAMBER OF COMMERCE  
HOUSTON CHAMBER OF COMMERCE

Petersville Community Council

Why Not Travel

Mat-Su Convention & Visitors Bureau

RADER MAXINE	2901 UAA DR	ANCHORAGE	AK	99508
RADLINSKI NICHOLAS J& J K	PO BOX 940155	HOUSTON	AK	99694
RAFTER JAS P & PATTY	1721 EARLY VW	ANCHORAGE	AK	99504
RAGSDALE DOROTHY S	6030 E GERSHMELOOP	PALMER	AK	99645
RAINES RUBY POWELL	4909 E 24TH AVE	ANCHORAGE	AK	99508
RAINEY DEL	3812 ARKANSAS DR	ANCHORAGE	AK	99517
RAISKUP ROBT K	1015 SEQUOIA CT	FOWLER	CA	93625
RAJEK JOHN A & SUSAN M	18000 TEKLANIKA DR	EAGLE RIVER	AK	99577
RALLS RICHARD W & G M	PO BOX 521293	BIG LAKE	AK	99652
Ralph Oakley	P O BOX 125	WILLOW	AK	99688
Ralston Joe	3316 Discovery Bay Drive	Anchorage	AK	99515
Ralston Joe	1200 19th Street NW, Suite 400	Washington	DC	20036
RAMEY CAROL L	PO BOX 520191	BIG LAKE	AK	99652
RAMEY JOHN L & DIANE	6150 ANDOVER CIR	ANCHORAGE	AK	99516
RAMIREZ AARON J& MICHELLE	4105 RASPBERRY RD	ANCHORAGE	AK	99502
RAMIREZ JONATHAN F& EMILY	111 E GLEN CIR	WASILLA	AK	99654
RAMONDETTA GAETANO T& KIM	PO BOX 672334	CHUGIAK	AK	99567
RAMOS RICHARD JR & KACIE	396 N TIFFANY DR	PALMER	AK	99645
Ramras Jay	State Capitol, Room 104	Juneau	AK	99801-11
RAMSAY GENE L & K K	3331 BEAMREACH CT	ANCHORAGE	AK	99516
Ramsey Earl	1730 Rebel Ridge Dr	ANCHORAGE	AK	99504
Ramsey Earl	1730 Rebel Ridge Dr	ANCHORAGE	AK	99504
RAMSEY EARL R JR	PO BOX 190666	ANCHORAGE	AK	99519
RAMSTAD RONALD J	PO BOX 670749	CHUGIAK	AK	99567
RANDAL GERALD V JR & L H	2942 MARSTON DR	ANCHORAGE	AK	99517
RANDALL COREY K	3813 N TEAKWOOD WAY	PALMER	AK	99645
RANDALL CRANDEL D	PO BOX 770229	EAGLE RIVER	AK	99577
RANDALL JAS C & MARIE	PO BOX 201783	ANCHORAGE	AK	99520
RANDOLPH DANE	7405 ROVENA	ANCHORAGE	AK	99518
RANEY CARROLL R& ARLENE V	918 W 14TH AVE	ANCHORAGE	AK	99501
RANNALS LARRY F	1751 WESTVIEW CIR	ANCHORAGE	AK	99504
RANUM JOEL	PO BOX 942	WILLOW	AK	99688
RAPER LYNETTE ANN	PO BOX 136	WILLOW	AK	99688
RAPP D & KIMBERLY L	12220 BROOKWOOD CIR	ANCHORAGE	AK	99516
Rappaport Ann	605 W. 4th Avenue, Room G-62	Anchorage	AK	99503
RASKEY JEROME R & S D	1251 PINECONE CIR	WASILLA	AK	99654
RASMUSSEN JOHN R & MYRTLE	9741 ABBOTT LOOP RD	ANCHORAGE	AK	99507
RAUER RAPHEL J	1030 SENECA AVE	WASILLA	AK	99654
RAUK RANDAL L	PO BOX 521141	BIG LAKE	AK	99652
RAULERSON LYNN	14835 E LAKERIDGE DR	EAGLE RIVER	AK	99577
RAULERSON TERESA M	PO BOX 670317	CHUGIAK	AK	99567
RAWLIUK RONALD A & K A	PO BOX 520520	BIG LAKE	AK	99652
RAY JENNY W	3831 CHINIAC BAY DR	ANCHORAGE	AK	99515

Alaska Railroad Board of Directors  
The Cohen Group

Alaska State Legislature

U.S. Fish & Wildlife Service

RAYBURN EDMOND F	PO BOX 1038	WILLOW	AK	99688
RAYFIELD JOS T JR& AILEEN	PO BOX 491	WILLOW	AK	99688
RAYMOND W	11821 TOY DR	ANCHORAGE	AK	99515
REAGAN WM F & LINDA S	PO BOX 110616	ANCHORAGE	AK	99511
REARICK PATTI J	211 BUNNELL	ANCHORAGE	AK	99508
REAVES SHARON	1160 N ROGERS RD	WASILLA	AK	99654
REBER ROCK A & JOHNNA	9370 BIRCH ROAD	ANCHORAGE	AK	99507
REBISCHKE DAVID & DOBORAH	15395 WINDSONG DR	ANCHORAGE	AK	99516
REBISCHKE OTTO	PO BOX 2558	PALMER	AK	99645
REBISCHKE TIMOTHY J	6902 CARLINE PL	ANCHORAGE	AK	99502
REDDING CHRISTOPHER S	PO BOX 520051	BIG LAKE	AK	99652
REDDING CLETUS LEO JR & J	7341 CANTONMENT CT	ANCHORAGE	AK	99507
REDDING JAS R & THERESA L	738 S KLEVIN ST	ANCHORAGE	AK	99508
REDFORD BARRY D	PO BOX 940344	HOUSTON	AK	99694
REDICK RONDA L	PO BOX 520285	BIG LAKE	AK	99652
REDMOND TOM II	2421 AUTUMN CIR	ANCHORAGE	AK	99516
REECE JULIANNE LAMP	PO BOX 221222	ANCHORAGE	AK	99522
Reed DANNY	1045 BEECH LN	ANCHORAGE	AK	99501
REED GARY L & JANE LOUISE	PO BOX 425	BETHEL	AK	99559
REED JASON P & ELANA B	PO BOX 2146	PALMER	AK	99645
REED KEITH T	PO BOX 244992	ANCHORAGE	AK	99524
REED KENNETH R	HC 89 BOX 127	WILLOW	AK	99688
REED NANETTE J	830 BOUNTY DR	ANCHORAGE	AK	99515
REED RICHARD W	PO BOX 815	PALMER	AK	99645
REED SARA ELIZABETH	PO BOX 2754	BETHEL	AK	99559
REED THOS L	PO BOX 520354	BIG LAKE	AK	99652
REEKIE ROBT J	731 E 8TH AVE	ANCHORAGE	AK	99501
REESE JAS L	PO BOX 769	WILLOW	AK	99688
Reese Jill	918 Colony Way	Palmer	AK	99645
REESE RODNEY A & TONI D	PO BOX 521613	BIG LAKE	AK	99652
REESE STEPHEN D	3129 DOIL DR	ANCHORAGE	AK	99507
REFLECTION RIDGE	7901 SUSAN CIR	ANCHORAGE	AK	99516
REGAN PETER GREGORY	2814 WILL ROGERS PL	ANCHORAGE	AK	99517
REGISTE MARTIN P & DORNIS	PO BOX 2902	PALMER	AK	99645
REHMANN JAS J & ROBYN	4800 HUFFMAN RD	ANCHORAGE	AK	99516
REICH GISELA CLARA	4200 TAHOE DR	ANCHORAGE	AK	99515
Reich Randall	P.O. Box 4125	Palmer	AK	99645
REICHERT FRANK L III	HC 89 BOX 488	WILLOW	AK	99688
REID LEROY C JR & SABRA L	1273 ANNAPOLIS DR	ANCHORAGE	AK	99508
Reid-Bennett Roxanne	16928 Park Place, #3	Eagle River	AK	99577
REILLY KRISTI BETH	PO BOX 520166	BIG LAKE	AK	99652
REIMANN PATRICIA A	PO BOX 521226	BIG LAKE	AK	99652
Reimer Gary	6881 Abbott Loop Road	Anchorage	AK	99507

Gold Miners Hotel

Bureau of Land Management

REINHART JENNIFER	PO BOX 1322	PALMER	AK	99645
REINHART TOMMY A	PO BOX 268	WILLOW	AK	99688
Reinwand Debbie	900 W 5th Ave Ste 100	Anchorage	AK	99501
REISS JAS F & DEBRA M	PO BOX 2856	PALMER	AK	99645
REITER LORETTA G	13220 REEF PL	ANCHORAGE	AK	99515
REITER WILLIAM REX JR MGR	3343 WESLEYAN DR	ANCHORAGE	AK	99508
Reitler Carolyn	P.O. Box 128	Talkeetna	AK	99676
REITZ DAN'L	31100 BEECHEY PT CT	EAGLE RIVER	AK	99577
REITZ LYNN J & JILL	3701 AKULA DR	ANCHORAGE	AK	99516
REMMY SUNNY SHANNON	1304 W 13TH AVE	ANCHORAGE	AK	99501
REMPEL DIETRICH & MARY E	PO BOX 202759	ANCHORAGE	AK	99520
RENDON JORGE	PO BOX 103414	ANCHORAGE	AK	99510
RENFREW WYTHE LEE	4141 B ST	ANCHORAGE	AK	99503
RENFRO CHAS R	PO BOX 237	WILLOW	AK	99688
RENNER JENNIFER C	PO BOX 4808	PALMER	AK	99645
RENNER LUKE & AUKTWEENA	PO BOX 1913	PALMER	AK	99645
RENNER STEVEN J	PO BOX 3310	PALMER	AK	99645
Rensch Elizabeth	4307 Arctic Blvd	Anchorage	AK	99503
RENSCHLER CHAS F & G J	4110 TAHOE DR	ANCHORAGE	AK	99515
RENSHAW ANSON L JR	1850 WICKERSHAM DR	ANCHORAGE	AK	99507
RENSHAW DAN'L E	519 W 8TH AVE #209	ANCHORAGE	AK	99501
REPNOW KELLY	PO BOX 671447	CHUGIAK	AK	99567
REPUYA FLORENCIO	3824 EASTWIND DR	ANCHORAGE	AK	99516
RESMAN BRANKO	PO BOX 549	WILLOW	AK	99688
RESSEL JOSEF	PO BOX 91442	ANCHORAGE	AK	99509
RETZLAFF DENNIS W	3916 CHECKMATE DR	ANCHORAGE	AK	99508
REUTER CHAS E & PAMELA G	3900 MARCY CT	ANCHORAGE	AK	99502
REUTOV EFERY	PO BOX 283	WILLOW	AK	99688
REUTOV ILIA L	PO BOX 823	WILLOW	AK	99688
REVET LEONARD & MARGARET	PO BOX 1683	BETHEL	AK	99559
REXFORD ANDREW ALEK	3821 PATRICIA LN	ANCHORAGE	AK	99504
REYNOLDS DAVID A & PAMELA	PO BOX 940005	HOUSTON	AK	99694
REYNOLDS ELIZABETH	PO BOX 520305	BIG LAKE	AK	99652
REYNOLDS JOSEPH MICHAEL	5821 E 6TH AVE	ANCHORAGE	AK	99504
REYNOLDS JOYCE A	PO BOX 772195	EAGLE RIVER	AK	99577
RHINE DARWIN S&JACQUELINE	PO BOX 521664	BIG LAKE	AK	99652
RHINE STEVEN M	PO BOX 521868	BIG LAKE	AK	99652
RHOADES AARON M & JODI E	2957 YALE DR	ANCHORAGE	AK	99508
RHODE PHILLIP E & MAIRE I	PO BOX 997	WILLOW	AK	99688
RHODES DAVID J & LINDA S	PO BOX 520751	BIG LAKE	AK	99652
RHODES MITCHELL DALE	7540 ALATNA AVE	ANCHORAGE	AK	99507
RHODES R J & CLARA R	5216 SHORECREST DR	ANCHORAGE	AK	99515
RHODUS NONA RAY	2001 CANNONEER CIR	ANCHORAGE	AK	99507

RICE JOHN J	751 HIGH VIEW DR	ANCHORAGE	AK	99515
RICE THOS A & KATHLEEN	3006 LOIS DR	ANCHORAGE	AK	99517
RICE-WILLIAMS TAMMY S	733 W 4TH AVE STE 308	ANCHORAGE	AK	99501
Rich Thistle	15687 W Sun Drive	Wasilla	AK	99654
Richard Hamilton	PO Box 252	Willow	AK	99688
Richard Hamilton	PO Box 252	Willow	AK	99688
RICHARDS DUANE & ARDITH	14630 E GUNNYSACK RD	PALMER	AK	99645
RICHARDS DUANE & MARY	3403 PUSSYWILLOW ST	ANCHORAGE	AK	99504
RICHARDS EDWIN E & LYNDA H	19650 1ST ST	EAGLE RIVER	AK	99577
RICHARDS FREDERICK RAY	2601 KLAMATH DR	ANCHORAGE	AK	99503
RICHARDS KARYL	4846 BARRINGTON LOOP	ANCHORAGE	AK	99503
RICHARDS PETER	7421 AUGUSTINE DR	ANCHORAGE	AK	99504
RICHARDS RANDY	PO BOX 3804	PALMER	AK	99645
Richards Ron	HC 04 Box 9741	Palmer	AK	99645
RICHARDSON CHAS & JOCELYN	PO BOX 876	WILLOW	AK	99688
Richardson Cheryl	1747 Laurence Court	Anchorage	AK	99501
RICHARDSON J SCOTT & C L	PO BOX 2025	PALMER	AK	99645
RICHARDSON RONDA	3110 DONNINGTON DR	ANCHORAGE	AK	99504
RICHEY FOREST M & VIRGINIA	PO BOX 3	WILLOW	AK	99688
RICHEY PERRY & BARBARA	PO BOX 154	WILLOW	AK	99688
RICHEY WM L & STEPHANIE	PO BOX 1052	WILLOW	AK	99688
RICHMOND DEBORAH	16605 THEODORE DR	EAGLE RIVER	AK	99577
RICHMOND MARGARET	2801 MCCOLLIE AVE	ANCHORAGE	AK	99517
RICHTER BERND C & SUSAN E	1260 E WOODCREST DR	WASILLA	AK	99654
RICHTER SCOTT & DEBORAH	PO BOX 521591	BIG LAKE	AK	99652
RICK CLAUDE G	4541 SANDY BEACH DR	ANCHORAGE	AK	99502
RICKARD DAVID	PO BOX 770241	EAGLE RIVER	AK	99577
RICKENBACHER D S & D M	PO BOX 211273	ANCHORAGE	AK	99521
RICKER CARROLL H	2456 COTTONWOOD ST	ANCHORAGE	AK	99508
RIDELL ROGER T	12503 S FARMERS RD	WASILLA	AK	99654
RIDENOUR PHILIP F & LESLIE	PO BOX 1506	PALMER	AK	99645
RIEDEL STEVEN J	760 W 71ST AVE	ANCHORAGE	AK	99518
RIELAND PAUL G & DEBRA A	5903 MILEY DR	ANCHORAGE	AK	99504
RIFE NATASHA	11934 W BALLYSHANNON DR	WASILLA	AK	99654
RIGDEN CHAS H & GAIL E	PO BOX 90453	ANCHORAGE	AK	99509
RIGGS EUGENE D & J'ANN	PO BOX 520205	BIG LAKE	AK	99652
Riggs John	137 E. Arctic Suite 101	Palmer	AK	99645
RIGGS ROBT E & LUCY A	PO BOX 521034	BIG LAKE	AK	99652
RILEY GARY W & JANICE E	PO BOX 521722	BIG LAKE	AK	99652
RILEY JAS W & ROBERTA A	PO BOX 242772	ANCHORAGE	AK	99524
RILEY JOHN	4714 S HIBBARD LN	BIG LAKE	AK	99652
RILEY MARK P & CINDY M	PO BOX 520130	BIG LAKE	AK	99652
RIMA ROBT S JR	PO BOX 142662	ANCHORAGE	AK	99514

Silvertip Design

Alaska Clean Air Coalition

Great Northern Engineering

RINNER ERWIN V	6490 TESHLAR CIR	ANCHORAGE	AK	99507
RIoux DAVID J & TAMMIE R	PO BOX 672327	CHUGIAK	AK	99567
Ripley Elizabeth	P.O. Box 1687	Palmer	AK	99645
RIPLEY J JUSTIN II	PO BOX 221893	ANCHORAGE	AK	99522
RISCH WM F	1341 SHORE DR	ANCHORAGE	AK	99515
RISTIG WAYNE & LAVERNE	1407 MARTEN ST	ANCHORAGE	AK	99504
RITTER DON E & RENE S	PO BOX 520570	BIG LAKE	AK	99652
RITTER DONALD & RENE	PO BOX 520852	BIG LAKE	AK	99652
Rivera Ed	P.O. Box 100035	Anchorage	AK	99510-00
RIVERO JUAN	9704 NEWHAVEN LOOP	ANCHORAGE	AK	99507
RIVEROS GUILLERMO	PO BOX 90256	ANCHORAGE	AK	99509
RIVERS G PATRICIA	PO BOX 202143	ANCHORAGE	AK	99520
RIVES WILFREDO	PO BOX 520637	BIG LAKE	AK	99652
RIZER BRUCE	PO BOX 113003	ANCHORAGE	AK	99511
RIZZO SANDRA A	PO BOX 220913	ANCHORAGE	AK	99522
ROBANCHO DANILO & VIVIAN	1900 POWDERHORN CIR	ANCHORAGE	AK	99507
ROBBINS DARREN	PO BOX 122	TRAPPER CREEK	AK	99683
ROBBINS J MICHAEL	12621 LUPINE RD	ANCHORAGE	AK	99516
ROBBINS SHAWN	205 E DIMOND	ANCHORAGE	AK	99515
Robert B Stiles	711 H Street, Suite 350	ANCHORAGE	AK	99501
Robert B Stiles	711 H Street, Suite 350	ANCHORAGE	AK	99501
Robert Sexton	24845 W. Lucky Shot Tr.	Willow	AK	99688
ROBERTS ANTHONY & WINDY	PO BOX 232065	ANCHORAGE	AK	99523
ROBERTS CECIL L	PO BOX 940033	HOUSTON	AK	99694
ROBERTS CLOVIS C & H	PO BOX 770311	EAGLE RIVER	AK	99577
ROBERTS DAN'L EDW	HC 89 BOX 179	WILLOW	AK	99688
ROBERTS KIT W	PO BOX 1430	PALMER	AK	99645
ROBERTSON ALAN H	12110 BUSINESS BLVD	EAGLE RIVER	AK	99577
ROBERTSON ANDREW L	887 E 75TH CT	ANCHORAGE	AK	99518
ROBERTSON DANIEL B	11301 SNOWLINE DR	ANCHORAGE	AK	99507
ROBERTSON DANIEL B	11301 SNOWLINE DR	ANCHORAGE	AK	99507
ROBERTSON DAVID D & C M	10734 8TH AVE NE	SEATTLE	WA	98125
ROBERTSON JACOB L & JACKIE	1777 HAMILTON DR	ANCHORAGE	AK	99515
ROBERTSON MARTY	PO BOX 220642	ANCHORAGE	AK	99522
ROBERTSON MARY	202 N PARK ST	ANCHORAGE	AK	99508
ROBERTSON PHILIP A & C A	PO BOX 91051	ANCHORAGE	AK	99509
ROBERTSON THAD L & SHANNON	13948 W KLUANE DR	BIG LAKE	AK	99652
ROBERTSON WAYNE	151 A ST LOOP	ANCHORAGE	AK	99515
ROBICHAUX ALLAN J	PO BOX 520287	BIG LAKE	AK	99652
ROBINSON ARTHUR S	820 E 8TH AVE	ANCHORAGE	AK	99501
ROBINSON CLEVE & KATIE	PO BOX 2441	PALMER	AK	99645
ROBINSON HARRISON & LORETTA	600 W 76TH AVE	ANCHORAGE	AK	99518
ROBINSON JULIE	6222 W DIMOND BLVD	ANCHORAGE	AK	99502

Valley Hospital

Alaska Railroad Workers (AFGE), Local #183

STE A06



ROBINSON MICHAEL D&MARY A	1513 W 29TH	ANCHORAGE	AK	99503
ROBINSON ROBERT L & V J	PO BOX 1032	WILLOW	AK	99688
ROBINSON ROBT W II	11039 E HARRIS HAWK TRL	SCOTTSDALE	AZ	85262
ROBINSON-PANASEWICZ S	1537 SUMMIT VIEW	ANCHORAGE	AK	99504
Robison Duane	HC01 Box 6448	Palmer	AK	99645
ROBISON GARY L	PO BOX 210634	ANCHORAGE	AK	99521
ROBRECHT FREDERICK J JR	PO BOX 520524	BIG LAKE	AK	99652
ROBSON THOS J & TERRI L	3121 W 62ND AVE	ANCHORAGE	AK	99502
ROCHELEAU KENNETH R & M J	7081 E TREE CT	ANCHORAGE	AK	99516
ROCK ROYCE R & CONSUELO C	4311 MARS DR	ANCHORAGE	AK	99507
ROCKEL PATRICK J & MARY	PO BOX 201093	ANCHORAGE	AK	99520
ROCQUEMORE CALVIN	PO BOX 743	PALMER	AK	99645
Rodda John	11901 Business Blvd., Suite 209	Eagle River	AK	99577
RODDA JOHN H & RODDA SUSAN	19100 NUNIVAK CIR	EAGLE RIVER	AK	99577
RODERICK KAYLYNNE A	358 DEERFIELD DR	ANCHORAGE	AK	99515
Roderick Paul	P. O. Box 73	Talkeetna	AK	99676
RODGERS JAS BRADLEY	3602 CHECKMATE DR	ANCHORAGE	AK	99508
RODGERS JOE N	PO BOX 201671	ANCHORAGE	AK	99520
RODGERS JOHN E & CAROLINE	1301 ZARVIS ST	ANCHORAGE	AK	99508
RODRIGUEZ DAVID M&BLANCHE	2271 FOXHALL DR	ANCHORAGE	AK	99504
RODRIGUEZ MANUAL JUAN	PO BOX 520605	BIG LAKE	AK	99652
ROE JAS N & ALICE C FAM TR	10715 E PALM RIDGE DR	SCOTTSDALE	AZ	85255
ROE JODY	1450 S BODENBURG LOOP	PALMER	AK	99645
ROEDDING DENNIS D	PO BOX 846	WILLOW	AK	99688
ROELFS VERNON L & INGRID	10319 STEWART DR	EAGLE RIVER	AK	99577
ROGERS BURL A	PO BOX 671487	CHUGIAK	AK	99567
ROGERS JIMMIE G & NANCY K	PO BOX 520477	BIG LAKE	AK	99652
ROGERS JOHN	PO BOX 762	WILLOW	AK	99688
ROGERS LARRY MICHAEL	4808 SUNDI DR	ANCHORAGE	AK	99502
ROGERS MICHAEL S & MARY E	6840 TALL SPRUCE	ANCHORAGE	AK	99502
ROGERS OLGA E	7657 E 20TH AVE	ANCHORAGE	AK	99504
ROGERS PATRICK J	HC 02 BOX 7803	PALMER	AK	99645
Rogers Rick	560 E 34th Ave	Anchorage	AK	99503-41
ROGERS ROBT K & SHARON A	17931 MEADOW CREEK DR	EAGLE RIVER	AK	99577
Rogers Wesley	P. O. Box 102033	Anchorage	AK	99510
ROGNESS DAN'L R & C J	2220 SORBUS WAY	ANCHORAGE	AK	99508
ROGUSKA JOS N	PO BOX 520641	BIG LAKE	AK	99652
ROHLFING ROBT R & MARY	315 E ST	ANCHORAGE	AK	99501
ROHRBAUGH-HAYES HEATHER	PO BOX 4014	PALMER	AK	99645
ROHWER MERRILEE A	7861 COX DR	ANCHORAGE	AK	99516
ROIT BRIAN M	PO BOX 90192	ANCHORAGE	AK	99509
ROLFSON HARLEN L.	2030 E 66TH AVE	ANCHORAGE	AK	99507
ROLLING MARY L	2006 W 47TH AVE	ANCHORAGE	AK	99517

Municipality of Anchorage

Talkeetna Air Taxi

Chugach Alaska Corporation

Local 1626

ROLLINS ANTHONY C & DENISE	PO BOX 211682	ANCHORAGE	AK	99521
ROLLINS ELIZABETH A	PO BOX 521271	BIG LAKE	AK	99652
Rolston Tom & Debbie	P.O. Box 3434	Palmer	AK	99645
ROMACK R GREG	6300 W DIMOND BLVD	ANCHORAGE	AK	99502
ROMAK MICHELLE D	3725 CHALLENGER CIR	ANCHORAGE	AK	99517
ROMAN KAREN D	PO BOX 140514	ANCHORAGE	AK	99514
ROMEO MICHAEL & BARBARA	PO BOX 295	PALMER	AK	99645
Romer Richard	3601 C Street, Ste. 1320	Anchorage	AK	99503
ROMERO JUDIE	PO BOX 243233	ANCHORAGE	AK	99524
ROMIG HOWARD G JR & K L	5531 RICKY RD	ANCHORAGE	AK	99516
ROMINE ANDREW M & NINA B	8411 MILES CT	ANCHORAGE	AK	99504
ROMINES TERESA E	PO BOX 671582	CHUGIAK	AK	99567
Ron & Marilyn Wilson	PO BOX 670	WILLOW	AK	99688
Ron Peck, President & COO	2600 Cordova Street	Anchorage	AK	99503
Ron Travis	14766 Apryl lane	Big Lake	AK	99652
Ron Travis	14766 Apryl lane	Big Lake	AK	99652
ROONEY MICHAEL C	PO BOX 771245	EAGLE RIVER	AK	99577
ROOT RICHARD	PO BOX 210733	ANCHORAGE	AK	99521
ROPER RONALD W & DEBRA D	PO BOX 1011	PALMER	AK	99645
ROSE BEATRICE	2263 KISSEE CT	ANCHORAGE	AK	99517
ROSE DAVID A & JULIE C	1980 N MIDTOWN DR	PALMER	AK	99645
Rose Diane	P.O. Box 1943	Palmer	AK	99645
ROSE JAMIE S	PO BOX 520470	BIG LAKE	AK	99652
ROSE RUSSELL E	3325 DICKSON RD	ANCHORAGE	AK	99504
ROSE STEPHEN EDW	PO BOX 520858	BIG LAKE	AK	99652
ROSEBERRY SETH O & K R	PO BOX 1445	PALMER	AK	99645
ROSENBERG STEVEN M & T L	19721 BIG DIOMEDE CIR	EAGLE RIVER	AK	99577
ROSHEIM DALE R & LAVONNE	8320 E 17TH AVE	ANCHORAGE	AK	99504
ROSS BRUCE A	9700 SLALOM DR	ANCHORAGE	AK	99507
ROSS ELBERT A & ALICE I	PO BOX 940242	HOUSTON	AK	99694
ROSS GREGORY A & KELLY L	7701 OUR OWN LN	ANCHORAGE	AK	99516
Ross Jonathon	8800 Heritage Center Drive	Anchorage	AK	99506
ROSS WM A	2220 SHORE DR	ANCHORAGE	AK	99515
ROTH COLIN E	7551 RANDAMAR PL	ANCHORAGE	AK	99507
ROTH GAYLE C	4321 SUNSTONE CIR	ANCHORAGE	AK	99516
ROTH KAREN L	7701 BERRY CIR	ANCHORAGE	AK	99502
ROTH LORETTA T	4321 TRAPLINE DR	ANCHORAGE	AK	99516
ROTH RAY & SHIRLEY	PO BOX 1134	WILLOW	AK	99688
ROTH TERRY W & MARILYN TRES	8040 KING ST	ANCHORAGE	AK	99518
ROTHFUSS ALBERT F SR	PO BOX 4383	PALMER	AK	99645
ROUNDS MICHAEL E	7101 CROOKED TREE DR	ANCHORAGE	AK	99507
Roundtree Keith	350 E. Dahlia Avenue	Palmer	AK	99645
ROUSE DONALD W	PO BOX 520462	BIG LAKE	AK	99652

ROUSE JACK V JR & BETTY C	PO BOX 551	WILLOW	AK	99688
ROUSEY JAMES SR	830 JAY CIRCLE	ANCHORAGE	AK	99504
ROUSEY KATIE DRYDEN	PO BOX 140	PALMER	AK	99645
ROWE JAS H & LAURIE	PO BOX 520256	BIG LAKE	AK	99652
ROWLAND MARK C.	600 W 76TH AVE	ANCHORAGE	AK	99518
ROY DENNIS & MELISSA	22540 MCMANUS DR	CHUGIAK	AK	99567
Roy Goldman N.	P O BOX 2883	PALMER	AK	99645
ROYAL PAUL WARREN	PO BOX 200925	ANCHORAGE	AK	99520
ROYCE WM G	2065 CLIFFSIDE DR	ANCHORAGE	AK	99501
RUBEO JOHN S	1101 S GREENWOOD	PARK RIDGE	IL	60068
RUDA JAS R & JUNE M	PO BOX 520089	BIG LAKE	AK	99652
RUDD STEVEN C & SHARON L	PO BOX 111734	ANCHORAGE	AK	99511
RUDISILL LAUREL M	PO BOX 521911	BIG LAKE	AK	99652
RUDOLPH DAN'L & DANISA M	PO BOX 141	WILLOW	AK	99688
RUDOLPH DAVID	561 S DENALI ST	PALMER	AK	99645
RUEBLING EDWARD	6985 BIG MOUNTAIN DR	ANCHORAGE	AK	99516
Ruehle Jerry	Alaska Dept. of Transportation and Public Facilities 4111 Aviation Avenue	Anchorage	AK	99519
RUF JONATHAN S	PO BOX 232566	ANCHORAGE	AK	99523
Ruff CHRISTOPHER A	1230 PINE ST	ANCHORAGE	AK	99508
RUFF MICHAEL L JR	PO BOX 482	WILLOW	AK	99688
RUHS TAMI	8207 E 5TH AVE	ANCHORAGE	AK	99504
RUIZ ROBERT	HCR 05 BOX 6873C	PALMER	AK	99645
RUNKEL DAN'L & JACQUELINE	3251 AMBER BAY LOOP	ANCHORAGE	AK	99515
RUNSER MARGIE A	PO BOX 770594	EAGLE RIVER	AK	99577
RUNSTROM ESTHER J	PO BOX 520856	BIG LAKE	AK	99652
RUNSTROM LIEFE	PO BOX 520735	BIG LAKE	AK	99652
RUNYON GLEN I & SHARON P	PO BOX 554	WILLOW	AK	99688
RUNYON RICHARD & LA RAINE	PO BOX 3144	PALMER	AK	99645
RUPE BILL	1115 CHUGACH WAY	ANCHORAGE	AK	99503
RUPE TAMRA J	1041 THOMASSON DR	ANCHORAGE	AK	99515
RUPP JACK J	PO BOX 190122	ANCHORAGE	AK	99519
RUPPERT JEAN G MICHOU	PO BOX 90734	ANCHORAGE	AK	99509
RUSENSTROM ALLAN	9255 KIRKWALL CIR	ANCHORAGE	AK	99502
RUSH ANGELA	PO BOX 520420	BIG LAKE	AK	99652
RUSH CLARK C & MARIANNE B	2131 MCKENZIE DR	ANCHORAGE	AK	99517
RUSH MICHAEL B	PO BOX 111732	ANCHORAGE	AK	99511
RUSH MIKE L & MADELINE C	PO BOX 111601	ANCHORAGE	AK	99511
RUSSELL DAN'L N	PO BOX 577	WILLOW	AK	99688
RUSSELL DAVID P&BARBARA E	6728 WATERFALL DR	EAGLE RIVER	AK	99577
RUSSELL DOROTHY J	PO BOX 521633	BIG LAKE	AK	99652
RUSSELL DWIANE G & CAROL J	7742 WISTERIA ST	ANCHORAGE	AK	99502
RUSSELL JOHN F	2011 S DECAMP CIR	PALMER	AK	99645
Russell Rod	918 South Colony Way	Palmer	AK	99645

RUSSELL-DURANT BRANDON A Rust Suzanne	K2 Aviation	PO BOX 231686 P. O. Box 545	ANCHORAGE Talkeetna	AK AK	99523 99676
RUSTAN HILMER JOHN		3168 E 43RD AVE	ANCHORAGE	AK	99508
RUTH YVONNE V		PO BOX 520714	BIG LAKE	AK	99652
Rutherford Marty	Alaska Dept. of Natural Resources	555 W. 7th Avenue, #1400	Anchorage	AK	99501
Rutherford Marty	Alaska Dept. of Natural Resources	400 Willoughby Avenue, #50	Juneau	AK	99801
RYALL DONALD		PO BOX 521582	BIG LAKE	AK	99652
RYAN JAS M		4400 E 104TH AVE	ANCHORAGE	AK	99516
RYAN MICHAEL & MARILEE		3311 WELLS CIR	ANCHORAGE	AK	99508
RYAN PATRICK P & CAROL J		3610 CLAY PRODUCTS DR	ANCHORAGE	AK	99517
RYAN RICHARD D		PO BOX 773151	EAGLE RIVER	AK	99577
RYJUNG KIM HAE		11541 LOWER SUNNY CIR	EAGLE RIVER	AK	99577
SAFARIK RAYMOND L & DAWN M		PO BOX 520695	BIG LAKE	AK	99652
Sailors Wendy	Alaska Wilderness Recreation & Tourism Associati	2207 Spenard Road, Ste 201	Anchorage	AK	99503
Salisbury Mary & Cliff		P. O. Box 993	Palmer	AK	99645
Salmon Darcie	Knik-Fairview Community Council	P. O. Box 877291	Wasilla	AK	99687-72
Salmon Woodie	Alaska State Legislature	State Capitol, Room 114	Juneau	AK	99801-11
SALTZER JAS M		PO BOX 520351	BIG LAKE	AK	99652
SALTZMAN DOUGLAS J & DAVIA		7030 FERGY CIR	ANCHORAGE	AK	99507
Sampson Roger	Alaska Dept. of Education and Early Development	801 W 10th St Ste 200	Juneau	AK	99801-18
Samuels Ralph	Alaska State Legislature	716 W. 4th Avenue, Ste 630	Anchorage	AK	99501
Samuels Ralph	Alaska State Legislature	State Capitol, Room 126	Juneau	AK	99801-11
SAMUELSON AGNES S		PO BOX 240312	ANCHORAGE	AK	99524
SANDAR PETER A & DEBORAH K		2820 LEXINGTON CIR	ANCHORAGE	AK	99502
SANDERLIN DOUGLAS C & DIANE		PO BOX 4155	PALMER	AK	99645
SANDERS JAS G & CONSTANCE		PO BOX 1747	PALMER	AK	99645
SANDERS JAS K		PO BOX 521741	BIG LAKE	AK	99652
SANDERS JENNIFER		3705 ARCTIC BLVD # 2929	ANCHORAGE	AK	99503
Sanders Keith	Cook Inlet Region, Inc.	PO Box 93330	Anchorage	AK	99509-33
SANDERS SANDRA L		2540 DOUGLAS DR	ANCHORAGE	AK	99517
SANDERSON CHRIS L & DANA G		PO BOX 521238	BIG LAKE	AK	99652
SANDERSON HOLLY		PO BOX 1143	PALMER	AK	99645
SANDOVAL SCOTT P		241 ALASKA PL	ANCHORAGE	AK	99504
SANDSTROM JERRY & MELANIE		1801 BLUE GRASS CIR	ANCHORAGE	AK	99502
SANDVIK ELIZABETH		15220 E LAZY LADY LN	PALMER	AK	99645
Sandvik Helvi	NANA Development Corporation, Inc.	1001 E. Benson Blvd.	Anchorage	AK	99508
SANDYS STEVEN J		18639 SNOWY PLOVER CIR	ANCHORAGE	AK	99516
Sanford Janice	Palmer Public Library	655 South Valley Way	Palmer	AK	99645
SANFORD STEPHEN H		PO BOX 92123	ANCHORAGE	AK	99509
Sanner Carol	Alaska Dept. of Transportation and Public Facilities	4111 Aviation Avenue	Anchorage	AK	99519
SANTILLANA VICENTE		PO BOX 140052	ANCHORAGE	AK	99514
Sargento Elsa	Alaska State Community Service Commission	550 W. 7th Avenue, Ste 1770	Anchorage	AK	99501
Sasseen Laura		319 S. Chugach	Palmer	AK	99645

SASSEEN LAURA A	319 S CHUGACH ST	PALMER	AK	99645
SATO HIROSHI & JOAN	3831 MIDVALE CIR	ANCHORAGE	AK	99507
SATURNINO FRANK	PO BOX 240262	ANCHORAGE	AK	99524
SAUER GILBERT J&DEBORAH J	120 STAN VIEW DR	CHEHALIS	WA	98532
SAUNDERS BARRY D & M C	5411 MOCKINGBIRD DR	ANCHORAGE	AK	99507
Saunders Clark	11823 Old Glenn Hwy., Suite 117	Eagle River	AK	99577
SAUNDERS MICHAEL	PO BOX 521542	BIG LAKE	AK	99652
SAUNDERS PHILLIP C	2506 W 34TH AVE	ANCHORAGE	AK	99517
SAUPE BRIAN & BRENDA	13611 WINDWARD CIR	ANCHORAGE	AK	99516
SAUR DONALD O	1120 KAYLIN CIR	ANCHORAGE	AK	99515
SAVAGE CRAIG AND KYM	3328 Horseshoe Lake Rd.	BIG LAKE	AK	99652
SAVAGE CRAIG AND KYM	3328 Horseshoe Lake Rd.	BIG LAKE	AK	99652
SAVAGE CRAIG L	PO BOX 520403	BIG LAKE	AK	99652
SAVAGE ELLA LOUISE	1051 N RAINBOW DR	WASILLA	AK	99654
SAVAGE JANET S	4500 MONTROSE CIR	ANCHORAGE	AK	99502
SAVAGE LEONARD N	1100 N PINION DR	WASILLA	AK	99654
SAVIDIS JUSTIN G& REBECCA	PO BOX 100206	ANCHORAGE	AK	99510
SAWBY FLORENCE I	16300 E MAUD RD	PALMER	AK	99645
SAWHILL JAS W & SUSAN M	4521 MONTROSE CIR	ANCHORAGE	AK	99502
SAWYER KENNY D	3840 E 65TH AVE	ANCHORAGE	AK	99507
SAWYER RICKY J	PO BOX 1494	PALMER	AK	99645
Saxby Rachel	P.O. Box 770434	Eagle River	AK	99577
SAXTON STEVEN E & MARY J	PO BOX 738	WILLOW	AK	99688
SCALIS MARGO	2237 E 86TH CT	ANCHORAGE	AK	99507
SCHACHLE L A III & S M	PO BOX 684	WILLOW	AK	99688
SCHACHLE LAWRENCE A & S	PO BOX 645	WILLOW	AK	99688
SCHACHLE TED & VALARIE	PO BOX 176	WILLOW	AK	99688
SCHACHLE THEO A	PO BOX 0176	WILLOW	AK	99688
SCHADE GARY C & MARIA O	9141 GRANITE PL	ANCHORAGE	AK	99507
SCHADT GORDON F	2931 ROCKY BAY CIR	ANCHORAGE	AK	99515
SCHAEFFERS DELMAR J&DEANNA	9661 BASHER DR	ANCHORAGE	AK	99507
SCHAEFFER QUINN	5811 RADCLIFT DR	ANCHORAGE	AK	99504
SCHALIG-WINTER LISA A	1350 S PIONEER DR	PALMER	AK	99645
SCHANDELMEIER REBECCA	PO BOX 520677	BIG LAKE	AK	99652
SCHANK ROCKY V& CHRISTINA	4821 CATKIN CIR	ANCHORAGE	AK	99502
SCHAUERMANN & VERRETT T C	431 W 7TH AVE STE 205	ANCHORAGE	AK	99501
SCHEALL JOS A & EDNA E	PO BOX 132224	TRAPPER CREEK	AK	99683
SCHEID WM C JR & MYONG H	8380 PIONEER DR	ANCHORAGE	AK	99504
Schenk Paul	PO Box 3115	Anderson	AK	99744
SCHERP RONALD G & ALICE M	PO BOX 521001	BIG LAKE	AK	99652
SCHIESL RICHARD M	PO BOX 751	TALKEETNA	AK	99676
SCHISLER ERIC K & CYNTHIA	4150 WEST LAKE AVE	ANCHORAGE	AK	99502
SCHIVELY JEFFREY S & S A	5705 LADD LN	ANCHORAGE	AK	99504

First American Title of Alaska

Qwiz, Inc.

Denali Borough

SCHLAFER DAVID J JR	PO BOX 521567	BIG LAKE	AK	99652
SCHLESINGER MAURICE A	PO BOX 213	WILLOW	AK	99688
SCHLEYER RICHARD ALAN	2450 BENZ CIR	ANCHORAGE	AK	99502
Schlitten Norman	2521 Banbury Dr	Anchorage	AK	99504
SCHLITTLER NORMAN L & JANE	2521 BANBURY DR	ANCHORAGE	AK	99504
SCHLOSSER DAVID G	9541 EMERALD LN	ANCHORAGE	AK	99515
SCHMAL MATTHEW J JR&TONYA	11801 FALKLANDS LOOP	EAGLE RIVER	AK	99577
SCHMIDT ANTHONY W	5840 E GERSHMEI LOOP	PALMER	AK	99645
SCHMIDT HAROLD J & R S	18213 MEADOW CREEK DR	EAGLE RIVER	AK	99577
Schmidt Jamie	3301 C Street, Suite 300	Anchorage	AK	99503
Schmidt Joe	431 N Franklin Ste 400	Juneau	AK	99801
SCHMIDT MARK W	1561 EL CADORE	ANCHORAGE	AK	99507
SCHMIDT PATRICK C&CAROL A	PO BOX 74	WILLOW	AK	99688
SCHMIDT TONI F & SUZANNE	PO BOX 520747	BIG LAKE	AK	99652
SCHMIDT TRACY B	PO BOX 627	WILLOW	AK	99688
SCHMIEGE TIMOTHY A & J E	PO BOX 520382	BIG LAKE	AK	99652
SCHMIEGE WYATT A	5748 CRAIG DR	ANCHORAGE	AK	99504
SCHNEIDER ALBERT & GWEN	14561 E GROVER LN	PALMER	AK	99645
SCHNEIDER DEBRA L	PO BOX 671770	CHUGIAK	AK	99567
SCHNELL JAS W & JANIS M	PO BOX 110802	ANCHORAGE	AK	99511
SCHNELL KEVIN E	1520 F ST	ANCHORAGE	AK	99501
SCHNELL ROBT E & JOLENE C	6608 CHEVIGNY ST	ANCHORAGE	AK	99502
SCHNIEDER DAVID M & S G	1120 E HUFFMAN RD STE 24	ANCHORAGE	AK	99515
SCHNOEKER VIRGINIA L M	PO BOX 521057	BIG LAKE	AK	99652
SCHOENLIEN KENNETH E	PO BOX 686	WILLOW	AK	99688
Schorr Betty	555 Cordova Street	Anchorage	AK	99501
Schrader Terri	PO Box 877786	Wasilla	AK	99687
SCHRADER THOS R & KARIN G	9321 BOTHWELL CIR	ANCHORAGE	AK	99502
SCHRAGE TIMOTHY	4900 SPORTSMAN DR	ANCHORAGE	AK	99502
SCHRAM THOS L & BONNIE J	11021 HANE ST	ANCHORAGE	AK	99516
SCHRECK STEVEN ROBT	PO BOX 210525	ANCHORAGE	AK	99521
SCHREINER JAMES U& IRMA H	1604 RICHARDSON DR	ANCHORAGE	AK	99504
SCHROEDER CHAS F & J J	1609 WOLVERINE ST	ANCHORAGE	AK	99504
SCHROEDER CLARENCE	PO BOX 521624	BIG LAKE	AK	99652
SCHUBERT LAURA M	PO BOX 771275	EAGLE RIVER	AK	99577
SCHULTZ WM L	HC 89 BOX 321	WILLOW	AK	99688
SCHULZ JOHN A	2311 W 47TH AVE	ANCHORAGE	AK	99517
SCHULZ WM V	PO BOX 520336	BIG LAKE	AK	99652
SCHUMACHER RODNEY L&CAROL	19814 N MONTAGUE LOOP	EAGLE RIVER	AK	99577
SCHUROSKY KATHERINE M	2351 CANARY CT	ANCHORAGE	AK	99515
SCHWAGER H A	PO BOX 522	WILLOW	AK	99688
SCHWANKL JAS B & VERONICA	PO BOX 709	WILLOW	AK	99688
SCHWANTES JOEL F& CAROL J	2950 S CHARMING VALLEY	PALMER	AK	99645

U.S. Forest Service - Chugach National Forest  
Alaska Dept. of Corrections

PMB 100 # 592

Alaska Dept. of Environmental Conservation

SCHWANTES KARL F & ETHEL	1565 K ST	ANCHORAGE	AK	99501
SCHWARTZ JEFFREY K & K R	12611 E 23RD AVE	SPOKANE VALLE WA		99216
SCHWARZ CLAUDS-PETER	3960 ALITAK BAY CIR	ANCHORAGE	AK	99515
SCHWARZ JAS R	4703 KLONDIKE AVE	ANCHORAGE	AK	99508
SCHWEBEL ROBT L	PO BOX 102500	ANCHORAGE	AK	99510
SCHWICHTENBERG ALAN J & D	6623 E BLUE LUPINE DR	PALMER	AK	99645
SCHWIGER RONALD D	19936 CYRUS ST	CHUGIAK	AK	99567
SCOTT GARY LEE	PO BOX 520613	BIG LAKE	AK	99652
SCOTT MONIKA L	PO BOX 521318	BIG LAKE	AK	99652
SCOTT RAYMOND HENRY	3004 W 30TH AVE	ANCHORAGE	AK	99517
SCOTT TABITHA R	8324 E 130TH AVE	ANCHORAGE	AK	99516
SCOTT THOS R	3120 LOCHWOOD CIR	ANCHORAGE	AK	99504
SCOUAL HESDEN D	PO BOX 520561	BIG LAKE	AK	99652
SCROGGS LISA DEAN	15100 PLATINUM CIR	ANCHORAGE	AK	99516
SCRUGGS SELENA	HC 89 BOX 440S	WILLOW	AK	99688
SCRUGGS VERA	PO BOX 222082	ANCHORAGE	AK	99522
Scudder Mike	17050 Eagle River	Eagle River	AK	99577
Scupholm Bill	P.O. Box 4091	Palmer	AK	99645
Seaberg Stewart	550 W. 7th Avenue, Ste 1420	Anchorage	AK	99501
SEAHOLM ARTHUR L & BONNIE	PO BOX 520491	BIG LAKE	AK	99652
SEALE JOSEPH E	PO BOX 143316	ANCHORAGE	AK	99514
SEAMAN MAX E	PO BOX 857	WILLOW	AK	99688
Seamount Dan Jr.	333 West 7th Avenue, Suite 100	Anchorage	AK	99510
SEARS LEWIS J & BETTY P	13030 ADMIRALTY PL	ANCHORAGE	AK	99515
SEARS LISA R	PO BOX 672	WILLOW	AK	99688
SEARS RAYMOND L	1122 COVINGTON CT	ANCHORAGE	AK	99503
Seaton Paul	State Capitol, Room 102	Juneau	AK	99801-11
SEAWELL SAMMYE L TAPLIN	5000 REBANO DR	ANCHORAGE	AK	99516
SEBASTIAN LEON A	PO BOX 2635	BETHEL	AK	99559
SEDERHOLM TRUK A	2441 W 69TH CT	ANCHORAGE	AK	99502
SEE JOHN W & JEANNE L	3380 WHITENEY CIR	ANCHORAGE	AK	99516
SEELEY JAS R & SALLY	4330 SEELEY CIR	ANCHORAGE	AK	99502
SEELY SCOTT C	1316 E 74TH AVE	ANCHORAGE	AK	99518
SEGERSTROM VALERIE	1205 I ST	ANCHORAGE	AK	99501
Seibert Craig	P.O. Box 13390	TrapperCreek	AK	99683
SEIBERT DEANA	7931 LITTLE MOOSE CIR	ANCHORAGE	AK	99507
SEIBOLD BRYAN S & RONNIE	PO BOX 521037	BIG LAKE	AK	99652
SEIDLER DON H	2228 E 86TH CT	ANCHORAGE	AK	99507
SEIDLER MATTHEW & TAMMY	2330 E 88TH AVE	ANCHORAGE	AK	99507
SEIDLER RICHARD H & M J	8791 CAMERON ST	ANCHORAGE	AK	99507
SEITZ BILL A	PO BOX 520085	BIG LAKE	AK	99652
SEITZ DOROTHY J	1861 E TUDOR RD	ANCHORAGE	AK	99507
SELK GARY L & LINDA R	2811 GILLAM CIR	ANCHORAGE	AK	99517

SELKIRK CAROL A	PO BOX 672002	CHUGIAK	AK	99567
SELKREGG SHEILA	6823 E TUDOR RD	ANCHORAGE	AK	99507
SELLARS ALBERT K	PO BOX 940167	HOUSTON	AK	99694
SELLARS TAMMY R	PO BOX 940223	HOUSTON	AK	99694
Sellick Marilyn	806 South Colony Way	Palmer	AK	99645
SELMAN BRANDON C	10919 NORTHFLEET DR	ANCHORAGE	AK	99515
SELTENREICH LEON & NANCY	PO BOX 647	WILLOW	AK	99688
SEMENDOFF BARRY & CAROL TRE	PO BOX 770743	EAGLE RIVER	AK	99577
Senner Stanley	715 L Street, Suite 200	Anchorage	AK	99501
SENTEP PAPON SAM	22469 DEER PARK DR	CHUGIAK	AK	99567
SERFLING JAS B & PHYLLIS	PO BOX 913	WILLOW	AK	99688
SERFLING PENNIE	PO BOX 520615	BIG LAKE	AK	99652
Serrano, PE Peter	709 W. 9th St., Room 851	Juneau	AK	99802
SETTERS DONALD B	PO BOX 771248	EAGLE RIVER	AK	99577
SEVER SUSAN L	PO BOX 202458	ANCHORAGE	AK	99520
SEVERSON GORDON J & AUD B	3201 WESTMAR CIR	ANCHORAGE	AK	99508
SEVERSON MYRON C & SHARON	4149 CHECKMATE DR	ANCHORAGE	AK	99508
SEVERSON TODD	645 G ST	ANCHORAGE	AK	99501
SEWARD JAS V	5234 LIONHEART DR	ANCHORAGE	AK	99508
SEXTON MICHAEL E & V GRDNS	PO BOX 670008	CHUGIAK	AK	99567
SEXTON ROBT J & SANDRA V	PO BOX 1114	WILLOW	AK	99688
SHACKELFORD DONALD R & P O	2530 BRITTANY DR	ANCHORAGE	AK	99504
Shae Kosmatski	PO Box 931	WILLOW	AK	99688
SHAFFER PHYLLIS J	PO BOX 3475	PALMER	AK	99645
SHAFFER BOYD J JR	PO BOX 92989	ANCHORAGE	AK	99509
SHAFFER BRENT C	PO BOX 670722	CHUGIAK	AK	99567
SHAFFER DAN'L R	PO BOX 525	WILLOW	AK	99688
SHAKE JAS B & BARBARA A	PO BOX 91992	ANCHORAGE	AK	99509
SHAKE JAS BRIAN & CRIS	460 E 56TH AVE	ANCHORAGE	AK	99518
SHAKE PATRICK C	241 E HARVARD AVE	ANCHORAGE	AK	99501
SHAKE ROBT L & PATRICIA K	2300 LORD BARANOF	ANCHORAGE	AK	99507
SHANABERGER TODD	PO BOX 520690	BIG LAKE	AK	99652
SHANNON JOHN C & SHAWNA W	PO BOX 521339	BIG LAKE	AK	99652
SHANTZ ALICE S	PO BOX 1915	BETHEL	AK	99559
Sharon Kosmatski	PO Box 931	Willow	AK	99688
SHARP CREIG M & KATHRYN J	3362 S CHARMING VALLEY LP	PALMER	AK	99645
SHARP LARRY W & PATRICIA A	11151 CANGE ST	ANCHORAGE	AK	99516
SHARRACK PATRICK L & M	8731 SOLAR DR	ANCHORAGE	AK	99507
Sharrow William	510 L Street, Ste 580	Anchorage	AK	99501
SHARY WM R & JANIS L	8991 RENDON DR	ANCHORAGE	AK	99507
Shaub Thyes	217 2nd St Ste 206	Juneau	AK	99801
SHAW BARBARA	11172 E AMFAY DR	PALMER	AK	99645
SHAW DONNA J	1818 PARKSIDE	ANCHORAGE	AK	99501



SHAW KIN Y & SON C	PO BOX 110637	ANCHORAGE	AK	99511
SHAW TIMOTHY & JENNIFER	8131 CLEAR HAVEN CIR	ANCHORAGE	AK	99507
SHAWBACK CHAS W & JOAN K	PO BOX 670810	CHUGIAK	AK	99567
SHEARIN MICHAEL	PO BOX 521491	BIG LAKE	AK	99652
SHEDLACK EDW MARTIN JR	6410 E 9TH AVE	ANCHORAGE	AK	99504
SHEDLACK JANA	7105 SHOORESIN CIR	ANCHORAGE	AK	99504
SHEEHAN MICHAEL D	PO BOX 521765	BIG LAKE	AK	99652
SHEEHAN RONALD L JR & S L	PO BOX 521775	BIG LAKE	AK	99652
Sheffield Bill	2000 Anchorage Port Road	Anchorage	AK	99501
Sheffield, Jr. Kenneth	700 G St Ste 600	Anchorage	AK	99501
Sheldon Holly	P.O. Box 813	Talkeetna	AK	99676-08
SHELDON JESS V& ABIGAIL E	6101 E 162ND	ANCHORAGE	AK	99516
SHELEY ROGER L & ANNE E	8830 GREENBELT DR	ANCHORAGE	AK	99502
SHELL FRANK E	PO BOX 398	WILLOW	AK	99688
SHELL GEO & TERRI ANN	PO BOX 521762	BIG LAKE	AK	99652
SHELL MILES E SR	PO BOX 520847	BIG LAKE	AK	99652
SHELLEY EMORY & DANA	16927 YELLOWSTONE DR	EAGLE RIVER	AK	99577
SHELTON JOHN A	5025 BAINBRIDGE CIR	ANCHORAGE	AK	99516
SHEPARD DOROTHY	4101 UNIVERSITY DR	ANCHORAGE	AK	99508
SHEPARD DOUGLAS A & M A E	1753 HAMILTON DR	ANCHORAGE	AK	99515
SHEPHERD DON & JANICE	PO BOX 972	WILLOW	AK	99688
SHEPHERD JON	PO BOX 4442	PALMER	AK	99645
SHEPPARD BETTY S	8101 PECK AVE	ANCHORAGE	AK	99504
SHERFICK RUSSELLE	PO BOX 75	WILLOW	AK	99688
SHERIDAN S L SATATHITE	PO BOX 240174	ANCHORAGE	AK	99524
SHERIDAN THOMAS M	PO BOX 92313	ANCHORAGE	AK	99509
SHERMAN ALYSON	PO BOX 338	WILLOW	AK	99688
SHERMAN DONALD C & S M	1701 RUSSIAN JACK	ANCHORAGE	AK	99508
SHERMAN HEROLD & MARY TRE	PO BOX 520391	BIG LAKE	AK	99652
SHERRER ROBT A & TAMMI A	PO BOX 521263	BIG LAKE	AK	99652
Sherry Paul	4000 Ambassador Drive, MS C-ADM	Anchorage	AK	99508
SHERWOOD DONALD E & F M	1640 BRINK DR	ANCHORAGE	AK	99504
SHIELDS MARK & LAURI	19114 DANNY DR	EAGLE RIVER	AK	99577
SHIELDS MICHAEL	PO BOX 521282	BIG LAKE	AK	99652
Shields Michael	P O BOX 4787	PALMER	AK	99645
SHIER PATRICIA L	1307 S CENTER POINT DR	WASILLA	AK	99654
SHILANSKI JAMIE C	431 7TH AVE	ANCHORAGE	AK	99501
Shillings William	7926 Old Seward Hwy	Anchorage	AK	99518
SHINE JOHN J & SUSITNA S	17605 KILOANA CT	EAGLE RIVER	AK	99577
SHIPLEY LINDA A	13201 WHALER DR	ANCHORAGE	AK	99516
SHIPMAN JANICE R A	2845 W INT'L AIRPORT RD	ANCHORAGE	AK	99502
SHIRLEY CARL E & BARBARA A	PO BOX 210010	ANCHORAGE	AK	99521
SHIRLEY JOANN	PO BOX 940408	HOUSTON	AK	99694

Port of Anchorage  
Pioneer Natural Resources Alaska, Inc.  
Alaska Freight & Expediting

Alaska Native Tribal Health Consortium

Trident Services

Shively John	Holland America Line/Westours, Inc.	745 W. Fourth Ave., Ste. 304	Anchorage	AK	99501
SHKAVRITKO IHOR		PO BOX 901	WILLOW	AK	99688
SHOGREN SUANNE		106 SWEETGALE CT	ANCHORAGE	AK	99518
SHOLAR AARON N		1135 W BOLD PEAK CIR	WASILLA	AK	99654
SHOLTON ADAM G		1937 LOUSSAC DR	ANCHORAGE	AK	99517
SHOLTON PAUL R & KRISTIN K		1245 G ST	ANCHORAGE	AK	99501
SHORT JANICE S		7301 DEARMOUN RD	ANCHORAGE	AK	99516
SHORT RONNIE L		4604 E 8TH AVE	ANCHORAGE	AK	99508
SHOWERS ALLISON M & Ella		14501 GOLDEN VIEW DR	ANCHORAGE	AK	99516
SHOWERS DONALD R		PO BOX 90442	ANCHORAGE	AK	99509
SHRADER STEPHEN D		1475 ESSEX CIR	ANCHORAGE	AK	99503
SHROFE WILLARD DALE		7316 ZURICH ST	ANCHORAGE	AK	99507
SHUCKEROW NEIL & YOUNG NIM		612 E 15TH AVE	ANCHORAGE	AK	99501
SHULTS MICHAEL C & CYNTHIA		2150 S NISSEN DR	PALMER	AK	99645
Shulz Michael	Knik River Construction	HC 02 7697 A	Palmer	AK	99645
SHUPE KAY		4141 APOLLO DR	ANCHORAGE	AK	99504
SHURTLIFF TERRY D & B J		PO BOX 211	WILLOW	AK	99688
SHUTE LOIS H		200 W 34TH AVE PMB 381	ANCHORAGE	AK	99503
SIEBRECHT MONTE E		2757 S CHARMING LOOP	PALMER	AK	99645
SIECZKOWSKI STANLEY E & B		PO BOX 521021	BIG LAKE	AK	99652
Siegel Dan		P.O. Box 671169	Chugiak	AK	99567
Siegel Steve		P.O. Box 671169	Chugiak	AK	99567
SIEKAWITCH TRAVIS A		6005 SPRUCE MEADOWS LOOP	ANCHORAGE	AK	99507
SIEMENS GORDON L & LAURIE L		6325 RED TREE CIR	ANCHORAGE	AK	99516
SIEWERT WARREN O		920 S CHUGACH ST	PALMER	AK	99645
SIGURDSSON S JOHN & CLAIRE		8601 JUPITER DR	ANCHORAGE	AK	99507
SIKORA GREGORY L & K A		PO BOX 520864	BIG LAKE	AK	99652
Silva Dusty	Palmer Planning & Zoning Commission	970 W. Josh Drive	Palmer	AK	99645
Silva Dusty	Combs Insurance Agency	341 S. Alaska Street	Palmer	AK	99645
SILVA MICHELE R		PO BOX 520669	BIG LAKE	AK	99652
Silver Keith	VECO Alaska	949 E 36th Ave Ste 500	Anchorage	AK	99508
SIMKINS DARRELL M & KRISTIN		PO BOX 521867	BIG LAKE	AK	99652
SIMMELINK JONATHAN J & DAWN		PO BOX 771797	EAGLE RIVER	AK	99577
Simmermeyer Jennifer	Buffalo/Soap Stone Community Council	14350 E. Canyon Drive	Palmer	AK	99645
Simmons CRAIG A & LISA A		12000 TOY DR	ANCHORAGE	AK	99515
SIMMONS GREG & NADINE		PO BOX 132	PALMER	AK	99645
SIMMONS JOHN & RENAY J		1757 DIMOND DR	ANCHORAGE	AK	99507
SIMMONS MIKE		2442 JUNEAU ST	ANCHORAGE	AK	99508
SIMMONS NATHAN		PO BOX 521527	BIG LAKE	AK	99652
SIMON MARLISE D MYERS		6751 AIR GUARD RD	ANCHORAGE	AK	99502
SIMONIAN ARTHUR		PO BOX 101563	ANCHORAGE	AK	99510
SIMONIS REV. VICKI		PO BOX 242113	ANCHORAGE	AK	99524
SIMPSON COLLEEN		HC 04 BOX 7010	PALMER	AK	99645

SIMPSON JAS & JENNIE	2160 STANFORD DR	ANCHORAGE	AK	99508
Simpson Jody	PO BOX 521113	BIG LAKE	AK	99652
Simpson Jody	PO BOX 521113	BIG LAKE	AK	99652
SIMPSON JOHN	18025 VILLAGES SCENIC	ANCHORAGE	AK	99518
SIMPSON MICHAEL T & LOLITA	17209 FOOTHILL AVE	EAGLE RIVER	AK	99577
SIMPSON STEVEN J	PO BOX 521113	BIG LAKE	AK	99652
SIMS MARY L	PO BOX 771721	EAGLE RIVER	AK	99577
SIMS RUBY E	PO BOX 212495	ANCHORAGE	AK	99521
SIMS STEPHEN M	1769 WICKERSHAM DR	ANCHORAGE	AK	99507
SINES PAUL	4801 DE ARMOUN RD	ANCHORAGE	AK	99516
SINGYKE DAVID	6610 LAWLOR CIR	ANCHORAGE	AK	99502
SINKOVICH WM J & TERRY M	1100 VALLEY OAK DR	WINTERS	CA	95694
SIRES & BUSS	PO BOX 203649	ANCHORAGE	AK	99520
SISSOM KENNETH H II&DEBRA	PO BOX 940257	HOUSTON	AK	99694
SISSOM MARY C	PO BOX 940324	HOUSTON	AK	99694
SIX JANICE E	1615 CACHE DR	ANCHORAGE	AK	99507
SIX RAYMOND S	PO BOX 940238	HOUSTON	AK	99694
SJOLSETH DEBRA K	7755 E SAPHIRE RD	PALMER	AK	99645
Skaling Sean	880 H Street, Suite 106	Anchorage	AK	99501
SKALSKY KEITH J & LINDA C	PO BOX 200761	ANCHORAGE	AK	99520
SKINNER LORRAN J&PATRICIA	6620 JOLLIPAN CT	ANCHORAGE	AK	99516
SKOGSTAD JOHN M	133 E HARVARD AVE	ANCHORAGE	AK	99501
SKRABACK EDWIN	PO BOX 72	WILLOW	AK	99688
SKURLA JOHN	3945 BORLAND DR	ANCHORAGE	AK	99517
SLACK KEVIN S & NICOLE R	PO BOX 2495	PALMER	AK	99645
SLADE DENNIS N & SUSAN R	PO BOX 521476	BIG LAKE	AK	99652
SLADEK JOHN J	1508 RICHARDSON DR	ANCHORAGE	AK	99504
SLATEN DARRIN E	2200 WASHINGTON AVE	ANCHORAGE	AK	99515
SLATTER TRIENA	3532 CLAY PRODUCTS	ANCHORAGE	AK	99504
SLEDGE BILL & LINNEA	PO BOX 520764	BIG LAKE	AK	99652
SLEDGE BILL M & CHRISTY M	PO BOX 521696	BIG LAKE	AK	99652
SLIWA GARY & KATHY	5798 E BIDEFORD BLVD	PALMER	AK	99645
SLOAN CHAS E & MARILYN G	4218 APOLLO DR	ANCHORAGE	AK	99504
SLOAN RICHARD F & B SUE	17500 E THREE SISTERS DR	PALMER	AK	99645
SLOANE GORDON R& BILLIE E	7348 TARSUS DR	ANCHORAGE	AK	99502
SLOCUM JOHN L	PO BOX 944	WILLOW	AK	99688
SMALLEY EDW N SR	HC 89 BOX 374	WILLOW	AK	99688
SMALSTIG WM H & ANITA ANN	3910 E 86TH AVE	ANCHORAGE	AK	99507
SMART JOHN S	2928 LILY ST	ANCHORAGE	AK	99508
SMEDLEY DONA M	PO BOX 663	WILLOW	AK	99688
SMID ELSIE M	PO BOX 521187	BIG LAKE	AK	99652
SMILEY JACK & DEE	PO BOX 909	WILLOW	AK	99688
SMITH ANNETTE L	PO BOX 109	WILLOW	AK	99688

MSB Planning Commission  
MSB Planning Commission

Green Star

SMITH BETTY J	1328 NORENE ST	ANCHORAGE	AK	99508
SMITH BILLIE E & ALLIE M	PO BOX 521572	BIG LAKE	AK	99652
SMITH BLAINE CEE	32789 CUMULUS RD	EAGLE RIVER	AK	99577
SMITH CAROLYN L	511 N WILDERNESS DR	PALMER	AK	99645
SMITH CHADWELL B & CARRIE	1340 E DOWN WIND CIR	PALMER	AK	99645
SMITH DALLAS M & CAREN M	1035 W HOLIDAY DR	WASILLA	AK	99654
SMITH DAN'L E & BRENDA M	8945 EMERALD DR	ANCHORAGE	AK	99502
SMITH DARRELL F & KEITH A	PO BOX 521341	BIG LAKE	AK	99652
SMITH DAVID K	3525 GRISSON CIR	ANCHORAGE	AK	99517
SMITH DAVID W	17618 S JUANITA LOOP	EAGLE RIVER	AK	99577
SMITH DEBORAH M	19720 N MITKOF LOOP	EAGLE RIVER	AK	99577
SMITH DELBERT LEE	PO BOX 244813	ANCHORAGE	AK	99524
SMITH DONALD F	8513 TURF CT	ANCHORAGE	AK	99504
SMITH DONALD W	12946 NORAK PL	ANCHORAGE	AK	99516
SMITH DOUGLAS & CAROL L	3641 AMBER BAY LOOP	ANCHORAGE	AK	99515
SMITH ELMER C TR TRE	6469 VILLAGE PKWY	ANCHORAGE	AK	99504
SMITH ERICK E	4925 TIFFIN CIR	ANCHORAGE	AK	99508
SMITH FRANK M & WENDELIN	PO BOX 91803	ANCHORAGE	AK	99509
SMITH FRANK TODD	1055 N ECHOHAWK WAY	EAGLE	ID	83616
SMITH FRED A	1345 W 13TH AVE	ANCHORAGE	AK	99501
SMITH GAIL L CROUSE	PO BOX 670565	CHUGIAK	AK	99567
SMITH GARY	1804 N WILHELM ST	ANCHORAGE	AK	99504
SMITH GARY	1804 TWINING DR	ANCHORAGE	AK	99504
SMITH GEO & BRENDA J	PO BOX 111	WILLOW	AK	99688
Smith Griz	P.O. Box 520584	Big Lake	AK	99652
SMITH GRIZ	PO BOX 520584	BIG LAKE	AK	99652
SMITH JACKLYN BOBBY	2631 W 100TH AVE	ANCHORAGE	AK	99515
SMITH JACOB N & MARY B	7600 LOTUS DR	ANCHORAGE	AK	99502
SMITH JAMES D & MARILYN	PO BOX 770466	EAGLE RIVER	AK	99577
SMITH JEFFERY D	8147 LLOYD DR	ANCHORAGE	AK	99502
SMITH JENNIE F	PO BOX 521481	BIG LAKE	AK	99652
SMITH JOEL B & NOAMI L	PO BOX 34	WILLOW	AK	99688
SMITH KENNETH E JR	300 E COOK AVE	ANCHORAGE	AK	99501
SMITH KENNETH J & MABEL A	2901 W 32ND AVE	ANCHORAGE	AK	99517
SMITH LAWRENCE J & E-A N	10146 COLVILLE ST	EAGLE RIVER	AK	99577
SMITH LEE	2300 BANBURY DR	ANCHORAGE	AK	99504
SMITH LEK G	5102 NOTTINGHAM WAY	ANCHORAGE	AK	99503
SMITH LEO D & HELEN M	PO BOX 520017	BIG LAKE	AK	99652
SMITH LOIS F	4401 E 7TH AVE	ANCHORAGE	AK	99508
SMITH MARIA C	PO BOX 427	WILLOW	AK	99688
SMITH MARIA DARGI V	PO BOX 141453	ANCHORAGE	AK	99514
SMITH MARY LOU	PO BOX 521827	BIG LAKE	AK	99652
SMITH MATTHEW	2921 W 29TH AVE	ANCHORAGE	AK	99517

Fire Art By Griz

SMITH MEDIA LLC	2700 E TUDOR RD	ANCHORAGE	AK	99507
SMITH MICHAEL K & LYNNE A	PO BOX 521035	BIG LAKE	AK	99652
SMITH MICHAEL M & LUCIENNE	6510 LAKEWAY DR	ANCHORAGE	AK	99502
SMITH NANCY J	PO BOX 100968	ANCHORAGE	AK	99510
SMITH PAUL D	1315 E 10TH AVE	ANCHORAGE	AK	99501
SMITH PAUL KEVIN	PO BOX 521262	BIG LAKE	AK	99652
SMITH RODGER W	PO BOX 192	ANIAK	AK	99557
SMITH RON E	PO BOX 3103	PALMER	AK	99645
SMITH RONALD J SR	PO BOX 190689	ANCHORAGE	AK	99519
SMITH SANDRA L	PO BOX 183	PALMER	AK	99645
SMITH SHELLIE	3306 THOMPSON	ANCHORAGE	AK	99508
SMITH STEVEN C & NEVA	4631 SHELIKOF ST	ANCHORAGE	AK	99507
SMITH TADD M	8420 HEATHER CIR	ANCHORAGE	AK	99508
Smith Ted	P O BOX 1026	WILLOW	AK	99688
SMITH TERRENCE P & STACEY	PO BOX 148	WILLOW	AK	99688
SMITH THEO G & JOYCE	PO BOX 1026	WILLOW	AK	99688
SMITH TROY W	1514 W 14TH AVE	ANCHORAGE	AK	99501
SMITH VIRGINIA L	906 CUNNINGHAM ST	ANCHORAGE	AK	99501
SMITH WM L	PO BOX 101437	ANCHORAGE	AK	99510
SMOLE DOUG	2996 S Horseshoe Lake Rd	Big Lake	AK	99652
SMOLE H DOUGLAS & SHARON	PO BOX 520010	BIG LAKE	AK	99652
SMOTHERS DAVID & MICHELLE	PO BOX 691	TALKEETNA	AK	99676
Smothers Missy	P.O. Box 691	Talkeetna	AK	99676
SMOTHERS RANDY	3704 E 16TH AVE	ANCHORAGE	AK	99508
SMULSKI MICHAEL	17436 LAOANA CT	EAGLE RIVER	AK	99577
SMYTH JAMIE C	PO BOX 521362	BIG LAKE	AK	99652
SMYTH RICHARD G & BONITA J	PO BOX 521348	BIG LAKE	AK	99652
SNEDEKER JACOB B	PO BOX 4955	PALMER	AK	99645
SNEDEKER SUSAN E	PO BOX 976	WILLOW	AK	99688
SNODGRASS JOHN JR & ALICE	840 S COLONY WAY	PALMER	AK	99645
SNOW LINCORN L SR & ALICE	1806 DIMOND DR	ANCHORAGE	AK	99507
SNOW PHYLLIS M	7261 E 22ND AVE	ANCHORAGE	AK	99504
SNYDER CYNTHIA L	PO BOX 242022	ANCHORAGE	AK	99524
SNYDER EDW H	HC 89 BOX 330	WILLOW	AK	99688
SNYDER MICHAEL KENT JR	2700 ABBEY LN	ANCHORAGE	AK	99517
Snyder Terry & Houston	PO BOX 521595	BIG LAKE	AK	99652
SNYDER WM H & TERRY L	PO BOX 521595	BIG LAKE	AK	99652
SNYDER WM HOUSTON	PO BOX 521358	BIG LAKE	AK	99652
SO YANG SUN	1527 JUNEAU ST	ANCHORAGE	AK	99501
SOBCZYK STEPHEN M & A Y	PO BOX 520543	BIG LAKE	AK	99652
SOHLMAN LARRY	PO BOX 103321	ANCHORAGE	AK	99510
SOHRWEIDE JOHN D	PO BOX 521864	BIG LAKE	AK	99652
SOLANO JOS	401 CHERRY ST	ANCHORAGE	AK	99504

D&M Concrete

Solberg LesLee & Norm	Denali View Raft Adventures	HC89 Box 8360	Talkeetna	AK	99676
SOLDIN JASON & MARY		1545 NORTHVIEW DR	ANCHORAGE	AK	99504
SOLEE ROSE		PO BOX 520145	BIG LAKE	AK	99652
SOLIN GARY & TONI		3201 DICKSON DR	ANCHORAGE	AK	99504
SOLMONSON MICHAEL E & J		14721 PARK HILLS DR	ANCHORAGE	AK	99516
SOLOMON JESSE		237 DENALI	ANCHORAGE	AK	99501
SOLVANG ARTHUR JR		PO BOX 966	WILLOW	AK	99688
SOLVEIG BILLINGHAM ELSA		PO BOX 231625	ANCHORAGE	AK	99523
SOMMERVILLE WM B JR & T L		PO BOX 521087	BIG LAKE	AK	99652
SONJU RICHARD A SR & A K		PO BOX 787	WILLOW	AK	99688
SONNENTAG TIMOTHY J		10201 E PALMER-WASILLA	PALMER	AK	99645
SOPKO CHRISTOPHER M		PO BOX 1042	WILLOW	AK	99688
SOPKO DOUGLAS W		1701 W 36TH AVE	ANCHORAGE	AK	99517
SOPP KEITH & LINDA		7304 HUNTER CIR	ANCHORAGE	AK	99502
SORENSEN DONALD LEE		2281 LAKE GEORGE	ANCHORAGE	AK	99504
SORENSEN JON E & LOIS M		440 E 11TH AVE	ANCHORAGE	AK	99501
SORENSEN MICHAEL J		110 W 38TH	ANCHORAGE	AK	99503
SOROKA WALTER E JR& MARIE		PO BOX 849	WILLOW	AK	99688
SORONEN ROBT N		1401 WOO BLVD	ANCHORAGE	AK	99515
SORTORE BONNIE E		7151 MIRANDA DR	ANCHORAGE	AK	99507
SOTO JR. ALEXANDER		PO BOX 670428	CHUGIAK	AK	99567
SOULE E R & F E SOULE L&S		12210 RAINBOW AVE	ANCHORAGE	AK	99516
SOULE HAROLD I		PO BOX 521416	BIG LAKE	AK	99652
Sousa A		P.O. Box 169	Talkeetna	AK	99676
Sousa Gerald		P.O. Box 563	Talkeetna	AK	99676-05
SPAETH DARYL R	Talkeetna River Guides	3549 SPINNAKER DR	ANCHORAGE	AK	99516
SPAIN GARY ALLEN		PO BOX 591	WILLOW	AK	99688
SPAIN MICHAEL & MICHELLE		PO BOX 165	WILLOW	AK	99688
SPALDING MARY A		10438 RIDGE PARK DR	ANCHORAGE	AK	99507
SPANGLER BRYAN R & DAWN C		PO BOX 3477	PALMER	AK	99645
SPANGLER EARL T & TINA M		2532 S EVENING FOG CIR	PALMER	AK	99645
SPANGLER RANDY		10797 W WASEY WAY	BIG LAKE	AK	99652
SPANOGLE DALE N		PO BOX 110	WILLOW	AK	99688
SPANOGLE PATRICIA A		PO BOX 789	WILLOW	AK	99688
SPARKS TOMMY A& DOROTHY A		PO BOX 4330	PALMER	AK	99645
SPAULDING JON A		PO BOX 520474	BIG LAKE	AK	99652
SPAULDING LOREN A		HC 89 BOX 465S	WILLOW	AK	99688
SPAULDING RON L		HC 89 BOX 564S	WILLOW	AK	99688
SPEARS BOBBY L		PO BOX 521462	BIG LAKE	AK	99652
SPECKING RAMONA A		2651 MELVIN AVE	ANCHORAGE	AK	99517
Spector Alex	U.S. Dept. of Veterans Affairs	2925 DeBarr Rd	Anchorage	AK	99508
SPENCE OSCAR & WANDA		2520 W 77TH	ANCHORAGE	AK	99502
SPENCER DONN R		2505 MCKENZIE DR	ANCHORAGE	AK	99517

SPERBECK DUANE E&KATHLEEN	PO BOX 31	WILLOW	AK	99688
SPESSARD TERRY	13751 ARNE ERICKSON CIR	ANCHORAGE	AK	99515
SPIKER SARAH LEE	PO BOX 670545	CHUGIAK	AK	99567
SPICKLER JAS E & DIANE E	PO BOX 93049	ANCHORAGE	AK	99509
SPICKLER KEVIN S	2454 CAPTAIN COOK DR	ANCHORAGE	AK	99517
SPIDAL STEVEN J SR & M E	PO BOX 520472	BIG LAKE	AK	99652
SPIETH MARC & EVELYN	8111 MEDELLIN CIR	ANCHORAGE	AK	99507
SPIGELMYER GEOR	5821 E CAMBORNE DR	PALMER	AK	99645
SPIRO PAUL M	PO BOX 101074	ANCHORAGE	AK	99510
SPOHN MARK M & LINDA M	5321 TUDOR TOP CIR	ANCHORAGE	AK	99507
SPONAUGLE DANNY JOS	PO BOX 770844	EAGLE RIVER	AK	99577
SPRAY JANICE F	1835 SUNRISE DR	ANCHORAGE	AK	99508
SPRENGEL JAS	PO BOX 608	WILLOW	AK	99688
SPRINGEN BETTY ANN	931 JOHAM CIR	ANCHORAGE	AK	99515
SPRINGER CARL M	PO BOX 232892	ANCHORAGE	AK	99523
SPRINGER CHASE W & JERI A	PO BOX 638	WILLOW	AK	99688
SPURLOCK LISA X	7773 CHERRYWOOD CIR	ANCHORAGE	AK	99507
SPURLOCK MARGARET A	PO BOX 3661	PALMER	AK	99645
SQUIRE BURT L & MARVELLE Y	PO BOX 520456	BIG LAKE	AK	99652
SROUFE JOHN A	2246 SUSITNA DR	ANCHORAGE	AK	99517
ST ANDREWS PARISH	16601 ARTILLERY RD	EAGLE RIVER	AK	99577
ST JOHN ALAN E & JEANINE M	PO BOX 211043	ANCHORAGE	AK	99521
ST JOHN JOE L	HC 89 BOX 315	WILLOW	AK	99688
St. John Jeanine	6441 S Airpark Pl	Anchorage	AK	99502-18
STAATS WILLARD	9920 CHELATNA CIR	ANCHORAGE	AK	99515
STABENOW MICHAEL	4701 CANTERBURY WAY	ANCHORAGE	AK	99503
STACHELRODT JON L & MARY M	2027 S CHURCH ST	PALMER	AK	99645
STACK ALEX T	6443 LONE TREE CIR	ANCHORAGE	AK	99507
STACKHOUSE WAYNE & ILEEN	4817 KENT ST	ANCHORAGE	AK	99503
STAENGEL JAS O & DANIELLE	18950 HILL RD	ANCHORAGE	AK	99516
STAFFORD TROY D	6212 E VISIONS CREST BLVD	PALMER	AK	99645
STAHL ALAN L & TEAL T	2715 S CHARMING VALLEY LP	PALMER	AK	99645
STAHLMAN TERRY M	9230 BUDDY WERNER DR	ANCHORAGE	AK	99507
STALDER ELAINE	25960 WHITE SPRUCE DR	EAGLE RIVER	AK	99577
STALLMAN JAS F JR	PO BOX 520833	BIG LAKE	AK	99652
STALLONE JOHN & BARBARA A	PO BOX 201172	ANCHORAGE	AK	99520
Stan Hooley, Executive Director	PO Box 870800	Wasilla	AK	99654
STANCEC EMIL J	PO BOX 128	WILLOW	AK	99688
STANDIFORD LEE	332 N PARK ST	ANCHORAGE	AK	99508
STANFORD JEFFREY	2900 BORELAND	ANCHORAGE	AK	99517
STANFORD JEFFREY S & B K	1419 H ST	ANCHORAGE	AK	99501
STANGER JOS R	PO BOX 1022	WILLOW	AK	99688
STANTON LEE V & KIM M	13841 MCDONELL RD	ANCHORAGE	AK	99516

Lynden

Idiatrod Trail Committee, Inc.

STARK DETTA	2561 RIO GRANDE AVE	ANCHORAGE	AK	99507
STARK STEVE C & DONNA L	PO BOX 111901	ANCHORAGE	AK	99511
Starr Red & Dee	P. O. Box 2521	Palmer	AK	99645
Stauber Fred	4315 Beechcraft Drive	Anchorage	AK	99517
STAUBER FREDERICK L & E I	3529 LAKESHORE DR	ANCHORAGE	AK	99517
STAUBER NANCY L	3601 LAKESHORE DR	ANCHORAGE	AK	99517
STAUFFER-TENGE ROSEANNE M	PO BOX 112461	ANCHORAGE	AK	99511
STEARNS WAYNE & JEANNE	3330 W 69TH AVE	ANCHORAGE	AK	99502
STEARNS WM WAYNE	HC 89 BOX 8560	TALKEETNA	AK	99676
STEELE LUTHER CALVIN	19958 BIRCHWOOD LOOP RD	CHUGIAK	AK	99567
STEELE MATTHEW R	1336 INGRA ST	ANCHORAGE	AK	99501
Stefan Alissa	11823 Old Glenn Hwy #107	Eagle River	AK	99577
STEFANSKI JOEL	2251 N HASSEN BEY DR	PALMER	AK	99645
Stefanski Mindy	HC 01 6031 A	Palmer	AK	99645
STEFFENS GARY M & P E	6730 NOTTING HILL DR	ANCHORAGE	AK	99504
STEGICH DAVID	PO BOX 521782	BIG LAKE	AK	99652
STEIGE JERYLL L	2600 BROOKE DR	ANCHORAGE	AK	99517
STEIGE PATRICIA A	PO BOX 928	WILLOW	AK	99688
STEIGLEMAN THOS W & K R	6650 FOOTHILL DR	ANCHORAGE	AK	99504
STEINER BEUFORD S	PO BOX 240614	ANCHORAGE	AK	99524
STEINHILPERT CHAS G SR	PO BOX 4303	PALMER	AK	99645
STEINHOUR RICK	PO BOX 3142	PALMER	AK	99645
STEININGER ALAN	PO BOX 521372	BIG LAKE	AK	99652
STENBERG LAURA B	11150 E PALMER-WASILLA	PALMER	AK	99645
STENEHJEM ENT	200 W 34TH AVE # 96	ANCHORAGE	AK	99503
STENGRIM JON & KIMBERLY	PO BOX 1503	PALMER	AK	99645
STEPANOV GEO A & LORA	4155 KINGSTON DR	ANCHORAGE	AK	99504
STEPHAN GORDON W & KAREN	1301 W VAUNDA AVE	WASILLA	AK	99654
Stephan Lee	16515 Centerfield Drive, #201	Eagle River	AK	99577
STEPHAN MICHAEL W JR	1100 E LEE ST	WASILLA	AK	99654
STEPHAN P GABRIEL	4600 SYDNEY PARK CIR	ANCHORAGE	AK	99516
STEPHAN SHAWN	PO BOX 113313	ANCHORAGE	AK	99511
STEPHENS KRIS W & JILL M	11306 QUEST CIR	ANCHORAGE	AK	99515
STEPHENS RICHARD A & B A	PO BOX 520893	BIG LAKE	AK	99652
STEPHENS-FISHER KACI D	PO BOX 773007	EAGLE RIVER	AK	99577
STEPHENSON LUCAS STEVEN	2441 SCARBOROUGH DR	ANCHORAGE	AK	99504
STEPP ARCHIE & QUERIDA N	2809 IRIS DR	ANCHORAGE	AK	99517
STEPP JOHNATHAN & KATIE	7331 SETTER DR	ANCHORAGE	AK	99502
STERGIOU ELIZABETH M	817 W SIXTH AVE	ANCHORAGE	AK	99501
Sterley Dan	301 West Northern Lights Blvd, Suite 601	Anchorage	AK	99503
STERLING SCOTT A	PO BOX 222383	ANCHORAGE	AK	99522
STERN TED B	1100 S CENTURY DR	WASILLA	AK	99654
Steve & Marian Charles	POBox 76	WILLOW	AK	99688



Steve Ribuffo	PO Box 196650	ANCHORAGE	AK	99519-66
Steve Ribuffo	PO Box 196650	ANCHORAGE	AK	99519-66
Steve Wells	PO Box 998	Willow	AK	99688
Steven Pannone	P.O. Box 102954	ANCHORAGE	AK	99510
Steven Pannone	P.O. Box 102954	ANCHORAGE	AK	99510
STEVENS BEN A&ELIZABETH E	PO BOX 102711	ANCHORAGE	AK	99510
Stevens Gary	State Capitol, Room 103	Juneau	AK	99801-11
STEVENS JAS V	3121 E 144TH AVE	ANCHORAGE	AK	99516
STEVENS LAUREL A.	PO BOX 101461	ANCHORAGE	AK	99510
Stevens Milton	1900 W 46th Ave.	Anchorage	AK	99517
STEVENS ROBT & WENDY	12741 SILVER SPRUCE DR	ANCHORAGE	AK	99516
STEVENS RONALD O	PO BOX 3742	PALMER	AK	99645
Stevens Ted	222 West 7th Avenue, #2	Anchorage	AK	99513-75
Stevens Ted	522 Hart Senate Office Building	Washington	D.C.	20510-02
STEVENSON THEO F& CATHERINE	PO BOX 1000879	ANCHORAGE	AK	99510
STEVENSON ERIC&STEPHANIE	HC 89 BOX 1772	WILLOW	AK	99688
STEVENSON MAUREEN E	PO BOX 2476	PALMER	AK	99645
STEWART DAN'L O& F LOUISE	16350 SANDPIPER DR	ANCHORAGE	AK	99516
STEWART DONALD P	PO BOX 2387	PALMER	AK	99645
STEWART ERNEST J & LINDA	PO BOX 521848	BIG LAKE	AK	99652
STEWART JAQUE	PO BOX 520972	BIG LAKE	AK	99652
STEWART JUNG-SOON	531 MISTY FJORD CIR	ANCHORAGE	AK	99508
STEWART PATRICIA L	PO BOX 521734	BIG LAKE	AK	99652
STEWART SCOTT	PO BOX 111645	ANCHORAGE	AK	99511
STEWART SHAWN J	422 TURPIN ST	ANCHORAGE	AK	99515
STEWART STEVEN L	1800 N ARABIAN LN	PALMER	AK	99645
STEWART TAMI L	PO BOX 4003	PALMER	AK	99645
STEWART W B & B CHARLENE	PO BOX 1007	WILLOW	AK	99688
STEWART WM L & SHELLEY A	PO BOX 669	WILLOW	AK	99688
Stelstra Chuck	523 S. Valley Way	Palmer	AK	99645
Stigar Mark	1150 S. Colony Way, Suite 3	Palmer	AK	99645
STIGAR MARK S & STACIE J	3301 S SKY RANCH LOOP	PALMER	AK	99645
Stiles Robert	711 H St Ste 350	Anchorage	AK	99501
STILL W A & NANCY A	PO BOX 520115	BIG LAKE	AK	99652
STILLER MARLYN C & JANICE	13631 VENUS WAY	ANCHORAGE	AK	99515
STILTNER C BROOKE	16600 CENTERFIELD DR	EAGLE RIVER	AK	99577
STINE STEVE	7791 E REISNER LOOP	PALMER	AK	99645
STINSON BRUCE	2010 N BROADWAY DR	PALMER	AK	99645
STINSON JOHN D & RACHEL M	PO BOX 520665	BIG LAKE	AK	99652
STIRLING JAS A & PHYLLIS	PO BOX 521675	BIG LAKE	AK	99652
STITH MICHAEL R& ANDREA L	16839 CORONADO RD	EAGLE RIVER	AK	99577
STOCK WM R & MARCIA SYREN	3353 WILEY POST LOOP	ANCHORAGE	AK	99517
STOCKER JAS	6088 S ANNETTE CIR	PALMER	AK	99645

Stocker Ron	Stocker Enterprises	HC 01 6169 B	Palmer	AK	99645
STOCKER RONALD W & V A		7205 E DENELLE ST	PALMER	AK	99645
STODDARD MICHAEL A & G W		3143 SEAWIND DR	ANCHORAGE	AK	99516
STOEHR SHON		PO BOX 520718	BIG LAKE	AK	99652
STOKER ROBT L & JANICE L		PO BOX 101	WILLOW	AK	99688
STOKES JEFF W		PO BOX 211345	ANCHORAGE	AK	99521
STOKES LETTA J		PO BOX 191094	ANCHORAGE	AK	99519
STOKESBARY WM K & JANET		1818 BOWDOIN CIR	ANCHORAGE	AK	99508
Stolze Bill	Alaska State Legislature	State Capitol, Room 501	Juneau	AK	99801-11
STONE ANDREW F		PO BOX 110842	ANCHORAGE	AK	99511
STONE BEN & DONNA		21715 LOWER CANYON DR	EAGLE RIVER	AK	99577
STONE KAROLINA A		PO BOX 110882	ANCHORAGE	AK	99511
STONOFF GREGORY LEE		PO BOX 671918	CHUGIAK	AK	99567
STORUD JAS A & BARBARA J		1201 N CLINTON CIR	WASILLA	AK	99654
STORY LENDLE CARL		3045 SEAWIND DR	ANCHORAGE	AK	99516
STOTT WM D & MARIA S		2015 BLUEBERRY ST	ANCHORAGE	AK	99503
STOTTS MATTHEW J & JULIE M		2200 FAIRBANKS ST	ANCHORAGE	AK	99503
STOUT PAUL B & STEPHANIE		PO BOX 520952	BIG LAKE	AK	99652
STOUT RITA A		3419 W 81ST AVE	ANCHORAGE	AK	99502
STOUT SCOTT F & DONNA J		3801 MATTHEWS DR	ANCHORAGE	AK	99516
STOVER RAYMOND J		PO BOX 220162	ANCHORAGE	AK	99522
Strabel Edward		HCR 05 BOX 6873C	PALMER	AK	99645
STRAND ALROY L		401 SCORPIO CIR	ANCHORAGE	AK	99508
STRAND FREDERICK M		9611 ARLENE DR	ANCHORAGE	AK	99515
STRANDBERG JAS S		6456 ST STE 100	ANCHORAGE	AK	99501
STRANIK GERALD MARVIN		2601 BONIFACE PKY	ANCHORAGE	AK	99504
STRASENBURGH JOHN		PO BOX 13145	TRAPPER CREEK	AK	99683
Stratton Jim	National Parks Conservation Association - Alaska	f 750 W. 2nd Avenue, Ste 205	Anchorage	AK	99501
Strausenburgh John		PO BOX 766	TALKEETNA	AK	99676
STRAWN WESLEY O		12421 NE 43RD AVE	VANCOUVER	WA	98686
STREET KEITH & RHONDA L		5660 E 142ND AVE	ANCHORAGE	AK	99516
STREET RAYMOND EDW JR		PO BOX 230386	ANCHORAGE	AK	99523
STRICKLAND JOHN T		PO BOX 521062	BIG LAKE	AK	99652
STRICKLAND RICKY D & J L		1017 ROYAL BIRCH LN	LOS VEGAS	NV	89144
STRICKLAND W&F MARTIN R&D		4858 TAMPA CIR	ANCHORAGE	AK	99508
STRIKE DAVID J & DONITA V		2924 EMORY ST	ANCHORAGE	AK	99508
STRINGER JOHN & CATHERINE		7014 METEOR CT	ANCHORAGE	AK	99504
STRINGER RANDY L & KARIN M		PO BOX 520871	BIG LAKE	AK	99652
STRINGER VICTORIA J		HC 89 BOX 368	WILLOW	AK	99688
STRINGFELLOW TERRY W & S		409 7TH ST	FORT RICHARDS	AK	99505
STROBLE KEN		3620 W 79TH AVE	ANCHORAGE	AK	99502
STRODE RONALD G & LINDA F		1240 S BETTINA WAY	WASILLA	AK	99654
STROH MARY JANE		PO BOX 521444	BIG LAKE	AK	99652

STROHMEYER RICHARD& WENDY	PO BOX 2292	PALMER	AK	99645
STROMSTAD LYLE W & DEBRA	644 E 74TH ST	ANCHORAGE	AK	99518
Strong Greg & Janice	1981 Palmer Wasilla Hwy #210	WASILLA	AK	99654
Strong Greg & Janice	1981 Palmer Wasilla Hwy #210	WASILLA	AK	99654
STRONG PIERRE J & JUDITH	PO BOX 520489	BIG LAKE	AK	99652
Strong Ryan	303 West Evergreen Avenue	Palmer	AK	99645
Strouse Dan	P. O. Box 354	Palmer	AK	99645
STRUEMPF JAS & CONNIE	12305 GREGG LN	ANCHORAGE	AK	99515
STRUEMPLER NANCY L	7818 SANDY PL	ANCHORAGE	AK	99507
STRUMSKY MARK C & SUSAN C	3650 MCMAHON AVE	ANCHORAGE	AK	99516
STRUNK MICHAEL W & MARY P	PO BOX 671650	CHUGIAK	AK	99567
STRUTZ RICHARD & CAREY L	4701 S PARK BLUFF DR	ANCHORAGE	AK	99516
Stu Grenier	8512 Boundary Ave D4	Anchorage	AK	99504
STUART DONALD	8140 KRONOS DR	ANCHORAGE	AK	99502
Stuart George	P O BOX 1804	PALMER	AK	99645
STUART GREGORY JOHN	PO BOX 141855	ANCHORAGE	AK	99514
STUART JOHN J JR & K A	PO BOX 2291	PALMER	AK	99645
Stuart Wally	501 W. Northern Lights Suite 200	Anchorage	AK	99503
STUBER CHAD	PO BOX 670834	CHUGIAK	AK	99567
STUDSTILL BRIAN	3210 S CIRCLE	ANCHORAGE	AK	99507
STUMPF ISABELLE	8005 E 36TH AVE	ANCHORAGE	AK	99504
Sturgeon John	5610 Silverado Way Ste A4	Anchorage	AK	99518
STURGULEWSKI BERNARD R&CM	5120 MANYTELL AVE	ANCHORAGE	AK	99516
STUTZER LAUREL ANN	3400 SOUTHBUFF CIR	ANCHORAGE	AK	99515
STYERS MICHAEL A & TARA L	PO BOX 520993	BIG LAKE	AK	99652
SUCHAN TODD W & CONSTANCE	1501 PATTERSON	ANCHORAGE	AK	99504
SUGDEN SCOTT A & MARY K	501 FREDRICKS DR	ANCHORAGE	AK	99504
Suiter Chet	P.O. Box 701	Palmer	AK	99645
SUKHRAM NOEL & SUSAN	1185 PAPER CREEK DR	LAWRENCEVILLE	GA	30045
SULKOSKY ROBT P & DONNA M	HC 89 BOX 134	WILLOW	AK	99688
SULLIVAN DIANE	PO BOX 670272	CHUGIAK	AK	99567
Sullivan George	1345 W. 12th Avenue	Anchorage	AK	99501
SULLIVAN JIMMY C	PO BOX 770197	EAGLE RIVER	AK	99577
SULLIVAN LAWRENCE & CAROL	244 DAILY AVE	ANCHORAGE	AK	99515
SULLIVAN MARGARET EAGAN	1345 W 12TH AVE	ANCHORAGE	AK	99501
SULLIVAN PAXTON J & NANCY	7124 JILL PLACE	ANCHORAGE	AK	99502
SULLIVAN ROBT M	6635 DESIREE LOOP	ANCHORAGE	AK	99507
SULLIVAN RONALD E	PO BOX 110164	ANCHORAGE	AK	99511
SULLIVAN WM & CHERLON	PO BOX 255	WILLOW	AK	99688
Suit Nancy	Lot 1 Block 1 Iditarod			
Sumida Steve	1569 S Bragaw St, Suite 102	Anchorage	AK	99508
SUMMERS MARTIN F	PO BOX 940144	HOUSTON	AK	99694
SUMMERS VANESSA J	PO BOX 221030	ANCHORAGE	AK	99522

SUMMERSILL MARY			PO BOX 521208	BIG LAKE	AK	99652
SUMNER DAN			PO BOX 521164	BIG LAKE	AK	99652
Sumner Sandi	Sumner, Sandi & James		PO Box 672501	Chugiak	AK	99567
Sumpter Gerri	Congressional Delegation's Office		851 E. West Point Drive, Ste 307	Wasilla	AK	99654
SUN DEAN			2231 YORKSHIRE LN	ANCHORAGE	AK	99504
SUNDBERG ALTA F			PO BOX 520358	BIG LAKE	AK	99652
SUNDBERG ROBT P			1801 STANTON AVE	ANCHORAGE	AK	99508
SUNIGA BILLY A			7521 BLACKBERRY	ANCHORAGE	AK	99502
SUTER ZACHERY S			PO BOX 670144	CHUGIAK	AK	99567
SUTHERLAND JAY & MELISSA			1325 L ST	ANCHORAGE	AK	99501
SUTHERLAND SCOTT A & LISA			1209 N DANVILLE ST	ARLINGTON	VA	22201
SUTHERLIN ROBT RH			2901 WILL ROGERS PL	ANCHORAGE	AK	99517
SUTTER BRADLEY D & P L			PO BOX 521139	BIG LAKE	AK	99652
SWALLING DAVID A			2922 ILIAMNA AVE	ANCHORAGE	AK	99517
SWALLING JOHN C			2650 MARSTON DR	ANCHORAGE	AK	99517
SWALLING JOHN HESS ANDREW			3903 COPE ST	ANCHORAGE	AK	99503
SWAN ANNIE M			9461 N HILLSIDE DR	PALMER	AK	99645
SWANSEN SHAUN			PO BOX 940141	HOUSTON	AK	99694
SWANSON EDW L & DONNA M			PO BOX 521048	BIG LAKE	AK	99652
SWANSON GARY E & BARBARA C			PO BOX 940373	HOUSTON	AK	99694
Swanson Lynn	Prudential Vista Real Estate		16635 Centerfield Drive, #103	Eagle River	AK	99577
SWANSON MICHAEL A			133 W 24TH AVE	ANCHORAGE	AK	99503
Swanson Ron	Matanuska-Susitna Borough		350 E. Dahlia Avenue	Palmer	AK	99645
SWANSON ROY G			5837 E UPDRAFT RD	PALMER	AK	99645
SWANSON TROY D			PO BOX 243231	ANCHORAGE	AK	99524
SWARTZ RICHARD M			3902 IOWA DR	ANCHORAGE	AK	99517
SWARTZ RONALD J			6642 E 99TH AVE	ANCHORAGE	AK	99516
SWEENEY AARON & NICOLE			2530 STERN CIR	ANCHORAGE	AK	99515
Sweeney Calvin	North County Fine		P. O. Box 50	Palmer	AK	99645
SWEETIN JIMMY D			801 W 57TH	ANCHORAGE	AK	99518
SWEETSIR SHEILA			2411 W 70TH	ANCHORAGE	AK	99502
SWEZEY RICHARD C			819 ORCA ST	ANCHORAGE	AK	99501
SYMBOL PATRICIA ANN			PO BOX 520303	BIG LAKE	AK	99652
SYREN ANITA			7027 LAKE OTIS PKY	ANCHORAGE	AK	99507
SYREN TIMOTHY CRAIG			6900 HOWARD AVE	ANCHORAGE	AK	99508
SZIPSZKY LUCAS .			3701 E CAREFREE DR	WASILLA	AK	99654
SZYMANSKI MICHAEL			2281 INNES CIR	ANCHORAGE	AK	99515
TABLER KEVIN A			18722 SNOWY PLOVER CIR	ANCHORAGE	AK	99516
TABRIZI AMIR J AZIMI			3312 MADISON WAY	ANCHORAGE	AK	99508
TACHIBANA JULIANNA M			10833 GLENHAVEN WAY	RANCHO COPDCA	CA	95670
TAIT PHILIP			4004 LOIS DR	ANCHORAGE	AK	99517
TAKASHIMA ALVIN J & M K			121 QUAIL VALLEY ST	LAS VEGAS	NV	89148
TALBOT DAN & THERESA			19110 BUTTE CIR	EAGLE RIVER	AK	99577

TAMANG NARESH	2925 CAMPBELL AIRSTRIP RD	ANCHORAGE	AK	99504
TAMEZ RICHARD E & LAUREL	PO BOX 4344	PALMER	AK	99645
TAMPKE DARLA	PO BOX 520708	BIG LAKE	AK	99652
TAMPLIN GEOFFREY	PO BOX 545	PALMER	AK	99645
TANAKA KATSUTAKA	2121 YORKSHIRE LN	ANCHORAGE	AK	99504
TANGUAY DAVID & BEATRIZ	13031 LAGOON CIR	ANCHORAGE	AK	99515
TANNAHILL ROBT S & E M	3023 EAGLE BAY CIR	ANCHORAGE	AK	99515
TANNER YUKON D & BEVERLY	PO BOX 646	TALKEETNA	AK	99676
TARBERT TONY	PO BOX 191054	ANCHORAGE	AK	99519
TARDY LEO J JR & LORRAINE	7501 E 20TH AVE	ANCHORAGE	AK	99504
TATE CHAS F	3468 S KRYSTAL PL	BIG LAKE	AK	99652
TATLOW CARL & JANICE	PO BOX 1621	PALMER	AK	99645
TAYLOR ANASTASIA J	831 HUNT CIR	ANCHORAGE	AK	99504
TAYLOR BRUCE E	13521 VENUS WAY	ANCHORAGE	AK	99515
TAYLOR GEO M & JOAN F	PO BOX 772704	EAGLE RIVER	AK	99577
TAYLOR HENRY L & ANN M	4420 OMALLEY RD	ANCHORAGE	AK	99516
TAYLOR JAS P & DIANNE K	PO BOX 4160	PALMER	AK	99645
TAYLOR LAWRENCE & ELAINE	BOX 110409	ANCHORAGE	AK	99511
TAYLOR LAWRENCE JR & GAIL	PO BOX 521834	BIG LAKE	AK	99652
TAYLOR ROBT W & SHARON R	PO BOX 693	WILLOW	AK	99688
TAYLOR RONALD W & L M	HC 89 BOX 485	WILLOW	AK	99688
Taylor Wendy & Brian	HCO1 Box 6086	Palmer	AK	99645
TEAGUE BRUCE WILLIAMS	2510 CURLEW CIR	ANCHORAGE	AK	99502
Teal Lans	P.O. Box 587	Palmer	AK	99645
TEBOW STANLEY J	PO BOX 3349	PALMER	AK	99645
TEEKELL KERRY	8801 JEWEL LAKE RD	ANCHORAGE	AK	99502
TELLMAN WALTER R	PO BOX 0088	UNALASKA	AK	99685
TEMPLIN WARREN & DEBORAH	5941 E CAMBORNE DR	PALMER	AK	99645
TENINTY DOUGLAS C & LINDA	3923 DEBORAH LN	ANCHORAGE	AK	99504
TERENCIO DEAN P	7821 COX DR	ANCHORAGE	AK	99516
TERRELL ARTHUR R	PO BOX 201311	ANCHORAGE	AK	99520
TERRY JERRY O & DEBRA K	PO BOX 4281	PALMER	AK	99645
TERRY LISA ANNE	PO BOX 1248	PALMER	AK	99645
TETREault CHAS E	PO BOX 671408	CHUGIAK	AK	99567
THACKER BRUCE P & JOANN J	3206 CAMBELL AIRSTRIP RD	ANCHORAGE	AK	99504
THACKER CHAS A	PO BOX 521167	BIG LAKE	AK	99652
THACKER LAVERNE M JR	4530 MAC ALLISTER	ANCHORAGE	AK	99502
THAMM TIM A & MERIKAYE	PO BOX 520996	BIG LAKE	AK	99652
THEINER DOUGLAS & MARCIA	PO BOX 463	WILLOW	AK	99688
THEIS LYNNE D	600 E NORTHERN LGTS BLVD	ANCHORAGE	AK	99503
THEIS SCOTT & LINDY	22832 WHISPERING BIRCH DR	CHUGIAK	AK	99567
THENO STEVEN M	PO BOX 190422	ANCHORAGE	AK	99519
THEOBALD LARRY G. & SARAH L.	17645 MEADOW CREEK DR	EAGLE RIVER	AK	99577

Musk Ox Farm

THEODORE HERBERT W			HC 89 BOX 520	WILLOW	AK	99688
Theodore Raymond		Knikatu, Inc.	P.O. Box 872130	Wasilla	AK	99687
Theresa Dean			PO Box 85	WILLOW	AK	99688
Therriault Gene		Alaska State Legislature	State Capitol, Room 119	Juneau	AK	99801--11
THERRIAULT JEFFREY S			9599 BRAYTON	ANCHORAGE	AK	99504
THIBERT LEE D			PO BOX 190673	ANCHORAGE	AK	99519
THIEME KARL R & KRISTAN F			PO BOX 940085	HOUSTON	AK	99694
THISTLE JANE M			PO BOX 5	WILLOW	AK	99688
Thistle Patricia & Richard			PO Box 520092	Big Lake	AK	99652
THOERNER FREDERICK D III			5033 W 80TH AVE	ANCHORAGE	AK	99502
Thom Bernier Kristy		Matanuska Telephone Association	1740 S. Chugach Street	Palmer	AK	99645
Thom Robert		Budget Feed & Farm	1100 S Colony Way	Palmer	AK	99645
THOMANN PETER			PO BOX 0171	WILLOW	AK	99688
THOMAS ALAN M & DAWN			10630 TAHNEETA DR	ANCHORAGE	AK	99501
Thomas Burek			25129 W. Malie Lane	Willow	AK	99688
THOMAS DANIL S& LAURA J W			PO BOX 143	WILLOW	AK	99688
THOMAS DAVID K & KELLY K			12940 RIDGEWOOD RD	ANCHORAGE	AK	99516
THOMAS GREGORY H			PO BOX 520002	BIG LAKE	AK	99652
THOMAS HOLLIS O & NORMA J			PO BOX 770051	EAGLE RIVER	AK	99577
THOMAS JAS P			1061 W 70TH AVE	ANCHORAGE	AK	99518
THOMAS MARK L & KRISTI J			PO BOX 520011	BIG LAKE	AK	99652
THOMAS RICHARD L & HILARY			20717 RAVEN DR	EAGLE RIVER	AK	99577
THOMAS WM J & CAROL E			8321 E BARNETT DR	ANCHORAGE	AK	99518
THOMASON ROBT T			18547 WHIRLAWAY RD	EAGLE RIVER	AK	99577
THOMPSEN DAVID A			22620 INLET VISTA DR	CHUGIAK	AK	99567
THOMPSON CHRIST O & P J			2201 DAHL LN	ANCHORAGE	AK	99503
THOMPSON DALTON R			712 W 72ND AVE	ANCHORAGE	AK	99518
THOMPSON DAVID L& RACQUEL			PO BOX 520767	BIG LAKE	AK	99652
THOMPSON DENNIS JR			11001 ELMORE RD	ANCHORAGE	AK	99516
THOMPSON DOROTHY C			3740 MOUNT BLANC CIR	ANCHORAGE	AK	99508
THOMPSON GORDON H			711 M ST	ANCHORAGE	AK	99501
THOMPSON HARRY A&SANDRA S			8700 BLACKBERRY ST	ANCHORAGE	AK	99502
Thompson Herman F.			P O BOX 626	TALKEETNA	AK	99676
THOMPSON MACK E			7064 WHITEHALL ST	ANCHORAGE	AK	99502
THOMPSON MARC R			12531 BRECKINRIDGE DR	EAGLE RIVER	AK	99577
THOMPSON MARVIN E & F M			PO BOX 520038	BIG LAKE	AK	99652
THOMPSON MICHAEL R			PO BOX 520136	BIG LAKE	AK	99652
THOMPSON SHERRY L			3520 VIEW PARK CIR	ANCHORAGE	AK	99502
THOMPSON SHERYL L			PO BOX 773112	EAGLE RIVER	AK	99577
THOMPSON TOMMY G & CHERYL			PO BOX 4408	PALMER	AK	99645
THOMPSON V CRAIG			721 SESAME ST	ANCHORAGE	AK	99503
THOMPSON VARONA KAY			PO BOX 520282	BIG LAKE	AK	99652
THOMPSON VIRGINIA L			7001 WEIMER ST	ANCHORAGE	AK	99502

THOMS JENNIFER			PO BOX 92945	ANCHORAGE	AK	99509
THORALL KENNETH L			1113 N ROGERS RD	WASILLA	AK	99654
Thorson Scott	Network Business Systems		1835 S Bragaw St Ste 425	Anchorage	AK	99512-34
THUNGC STEVEN			840 N KLEVIN ST	ANCHORAGE	AK	99508
TIBBETT KENNETH D & F A			10341 TREE TOP LN	ANCHORAGE	AK	99507
TIEDE ROBT F & KATHLEEN			1250 N DREDGE PL	WASILLA	AK	99654
TIERNEY ROBT L			7980 POTTER HEIGHTS DR	ANCHORAGE	AK	99516
TIFFANY LYNNE M			PO BOX 521604	BIG LAKE	AK	99652
TILGHMAN BETTY L			PO BOX 1122	WILLOW	AK	99688
Timm Charles	U.S. Postal Service		500 South Cobb Street	Palmer	AK	99645
TIMMERMAN CURT E & R A			PO BOX 520153	BIG LAKE	AK	99652
TISCHER D F			1210 CROWBERRY	ANCHORAGE	AK	99515
TISCHER MAE&TISCHER B L/E			HC 89 BOX 500	WILLOW	AK	99688
TIX KIM ELAINE MARKLE			7130 CHESTER CT	ANCHORAGE	AK	99504
TJADEN STEVEN M & NANCY L			401 W 121ST AVE	ANCHORAGE	AK	99515
TOBIN ALBERT R JR			PO BOX 100126	ANCHORAGE	AK	99510
TODD GENELLE			PO BOX 2172	PALMER	AK	99645
TODD JAS P			3121 LATOUCHE	ANCHORAGE	AK	99508
Tokar Jerry			P.O. Box 2650	Palmer	AK	99645
TOLAN JAS C			PO BOX 211024	ANCHORAGE	AK	99521
TOMAZEVIC THOS & MELINDA			9826 DINAAGA	EAGLE RIVER	AK	99577
TOMISSER WM P			4705 E 112TH AVE	ANCHORAGE	AK	99516
TOMLINSON WM R			PO BOX 949	WILLOW	AK	99688
TOMSIC JOHN			PO BOX 102212	ANCHORAGE	AK	99510
Tony & Sandra DeLucia			PO Box 812	Willow	AK	99688
Toohy Cam	Shell Exploration & Production		3601 C St Ste 1334	Anchorage	AK	99503
TOOTHMAN SAMMY L & MARIE A			PO BOX 266	WILLOW	AK	99688
TOPLIFF JILL			10380 BOUNDARY CREEK TER	MAPLE GROVE	MN	55369
TOPLIFF JILL			10380 BOUNDARY CREEK TER	MAPLE GROVE	MN	55369
TOPOLSKI MARY E			9138 ARLON ST STE A3-994	ANCHORAGE	AK	99507
TOPP EUGENE L & DEBORAH I			8431 WILLIWA CIR	ANCHORAGE	AK	99504
Toppenberg John	Alaska Wildlife Alliance		P.O. Box 202022	Anchorage	AK	99520-20
TORKELSON MARK A			PO BOX 521554	BIG LAKE	AK	99652
TORKELSON TYREN J			PO BOX 521910	BIG LAKE	AK	99652
TORKELSON TYREN J & DEONN			16963 W LIGHHOUSE DR	BIG LAKE	AK	99652
TORO MARIO A & JULIE A			3288 VASSER DR	ANCHORAGE	AK	99508
TORRES DOROTHEAL			PO BOX 832	WILLOW	AK	99688
TOSCANO JUAN C & ELSA C			PO BOX 113062	ANCHORAGE	AK	99511
TOTEMOFF CHAS W			3000 C ST	ANCHORAGE	AK	99503
TOTH STEVE & SANDI			PO BOX 112053	ANCHORAGE	AK	99511
TOVSEN MARK A			6943 TERRY ST	ANCHORAGE	AK	99502
TOWERS RICHARD B			4824 MILLS DR	ANCHORAGE	AK	99508
TOWN KEVIN G			PO BOX 211025	ANCHORAGE	AK	99521

Townsend Martin	3604 Bisquier Dr	Anchorage	AK	99508
TOWNSEND MICHAEL RUSSELL	108 S FOREST 3	KENAI	AK	99611
TRACY JAMES D & DEJUAN L	PO BOX 4449	PALMER	AK	99645
TRAEGER ROBT D & ALICE D	4741 MARS DR	ANCHORAGE	AK	99507
TRAILER DAVID EDW & KAY E	3800 ROBIN ST	ANCHORAGE	AK	99504
TRAMPUSH JOHN A & MEREDITH	PO BOX 140312	ANCHORAGE	AK	99514
TRAN MAI	1110 E SENECA AVE	WASILLA	AK	99654
TRASKY LANCE L & WENDY A	3941 TRURO DR	ANCHORAGE	AK	99507
TRAVIS GREGORY A	PO BOX 940263	HOUSTON	AK	99694
TRAVIS RONALD W & LYNDA F	PO BOX 520116	BIG LAKE	AK	99652
TRAVLOS JOHN & JUDITH K	PO BOX 190510	ANCHORAGE	AK	99519
TRAWVER ALAN C & MARY K	7900 UPPER O'MALLEY RD	ANCHORAGE	AK	99507
TRAXINGER JAMES R & K A	8571 BROOKRIDGE DR	ANCHORAGE	AK	99504
TRAYLOR GORDON & CYNTHIA K	11292 TUTTLE RD	HAMMOND	LA	70403
TREAT DAN'L J & CAROL A	5600 115TH AVE	ANCHORAGE	AK	99516
TREMBLAY MITCHELL K & GAIL	PO BOX 1078	WILLOW	AK	99688
TRENT HERMAN	PO BOX 521904	BIG LAKE	AK	99652
TREVITHICK ERIC & BOBBI	7541 SOLDOTNA DR	ANCHORAGE	AK	99507
Trish Rolfe	333 W. 4th Avenue, Ste 307	Anchorage	AK	99501-23
Trish Rolfe	333 W. 4th Avenue, Ste 307	Anchorage	AK	99501-23
Troll Kate	P.O. Box 100660	Anchorage	AK	99510-06
TROLL TIMOTHY E & TROLL M J	3024 PRINCETON WAY	ANCHORAGE	AK	99508
TROST STEVE E	PO BOX 3127	PALMER	AK	99645
TROXEL SARAH CRAIG	3753 W 100 AVE	ANCHORAGE	AK	99515
Truax Bradley	P.O. Box 2952	Palmer	AK	99645
TRUEBLOOD TED B & GLORIA	9860 NEARPOINT DR	ANCHORAGE	AK	99507
TRUETT RICHARD & JOAN I	PO BOX 520056	BIG LAKE	AK	99652
TRUJILLO ERNEST & LOIS	PO BOX 110249	ANCHORAGE	AK	99511
TRYCK MOLLY G	1801 W 13TH AVE	ANCHORAGE	AK	99501
TRYCK WM OSCAR	136 E 8TH AVE	ANCHORAGE	AK	99501
Tubbs Richard	831 S. Chugach Street	Palmer	AK	99645
Tucker Bill	P.O. Box 92225	Anchorage	AK	99509
TUCKER DONALD & DIANE	PO BOX 716	WILLOW	AK	99688
TUCKER GREG H	9715 INDEPENDENCE DR	ANCHORAGE	AK	99507
TUCKER JULIUS C & EMILY F	7260 E DENELLE ST	PALMER	AK	99645
Tucker Micheal	P.O. BOX 871565	WASILLA	AK	99687
Tucker Stan	1001 Delwood Drive	Wasilla	AK	99654
TUCKER SYLVIA	PO BOX 520106	BIG LAKE	AK	99652
TUCKER WARREN A	PO BOX 200988	ANCHORAGE	AK	99520
TUEL CHAS J & MICHELLE A	PO BOX 521691	BIG LAKE	AK	99652
TUFTS JIM	PO BOX 521601	BIG LAKE	AK	99652
TULIN CHAS E & HELEN L	1422 K ST	ANCHORAGE	AK	99501
Tull William	634 S. Bailey, Suite 201	Palmer	AK	99645



TULL WM F			353 S DENALI ST	AK	99645
TUNLEY ARTHUR T & RENEE F			3748 MOUNT BLANC CIR	AK	99508
Tuomi John	Palmer Veterinary Clinic Inc		P.O. Box 3329	AK	99645
TURINSKY KEVIN J			2217 ARCADIA DR	AK	99517
Turner Cheryl	Mat-Su Borough School Board		P.O. Box 1567	AK	99645
Turner Jana			P O BOX 477	AK	99645
Turner Jim	Hatcher Pass Gateway Center		P.O. Box 1567	AK	99645
TURNER LAURENCE R			PO BOX 521381	AK	99652
Turner Rex			P.O. Box 3489	AK	99645
TURNER WALTER E			3811 PATRICIA LN	AK	99504
TUTTLE DALE E			13135 OLD GLENN HWY # 210	AK	99577
TUTTLE TANITH MARIKO			5249 WANDERING DR	AK	99502
TVRDY JIMMIE & ELIZABETH			PO BOX 110633	AK	99511
TWEED STEVEN L			PO BOX 3857	AK	99645
Twigg F.C.			P.O. Box 266	AK	99676
TYLER LORNES L			PO BOX 521077	AK	99652
TYRER DAVID JOHN			PO BOX 15396	AK	99603
Tzou Lih	Peking Garden		775 West Evergreen	AK	99645
UGARTE DAVID			2360 HEATHERBROOK CIR	AK	99504
UHER JOE			PO BOX 770178	AK	99577
ULBRICH CARL L			2730 KEMPTON HILLS DR	AK	99516
ULSHER ANDREW P			6901 DICKERSON DR	AK	99504
UMPHERS DONNY			3303 W 82ND AVE	AK	99502
UNDERWOOD DOROTHY LORINE			6505 CUTTY SARK	AK	99502
UNDERWOOD PERRY L			16600 YELLOWSTONE CIR	AK	99577
UNFREID STEVEN G&KATHERYN			7327 E MATANUSKA SPUR	AK	99645
UNFRIED HILDA C			525 W 3RD AVE	AK	99501
UNGER BRYCE D			PO BOX 233541	AK	99523
UNGER INEZ D			14401 BUFFALO ST	AK	99516
UNGERECHT MARGARET L			1309 HILLCREST DR	AK	99503
UNIN SUE ANNE			PO BOX 2004	AK	99559
UNWIN KAREN C			3310 W 78TH AVE	AK	99502
URDA STEPHANIE B			2401 COPPERWOOD	AK	99516
URIG SUSAN L			2905 MARSTON DR	AK	99517
UTECHT WM E & KATHLEEN M			PO BOX 2415	AK	99645
UTPADEL JON D			9300 NORDIC ST	AK	99516
UTT GARY D			21631 GRAYBILL ST	AK	99567
UTTER RAYMOND F			5361 LITTLE TREE ST	AK	99507
VACENDAK WM A & GOLDIE			14401 TETON PL	AK	99516
VAILLANCOURT JOAN M			PO BOX 521537	AK	99652
VAKALIS GEO J			6311 HABICHT CT	AK	99504
VAKALIS KEVIN			8011 STEWART MOUNTAIN DR	AK	99577
VALENCIA MICHAEL			8223 SEACLIFF ST	AK	99502

VALLADARES WALFREDO O	1221 W RIDGEVIEW DR	WASILLA	AK	99654
VALLELY ROGER	5419 E MORNING MIST DR	PALMER	AK	99645
VALLIANT MARTHA A	5572 KENNYHILL DR	ANCHORAGE	AK	99504
VAN ABEL J M & M K	3430 W 31ST AVE	ANCHORAGE	AK	99517
VAN BAVEL GRANT L & B	7447 E MATANUSKA SPUR RD	PALMER	AK	99645
VAN BUSKIRK LELAND & JANET	PO BOX 521307	BIG LAKE	AK	99652
VAN BUSKIRK ROBT B & AMBER	PO BOX 520131	BIG LAKE	AK	99652
Van Chau Andrew	P.O. Box 196612	Anchorage	AK	99519-66
VAN DAFF PHILIP D	7816 BRENTWOOD DR	ANCHORAGE	AK	99502
VAN DIEST JAY M & SUSAN E	16801 E T & T LN	PALMER	AK	99645
Van Dongen Marc	HC 01 Box 6098-U	Palmer	AK	99645
VAN HEMERT WILLEM & R	1633 W 15TH AVE	ANCHORAGE	AK	99501
VAN HOOMISSEN JIM & DIANE	3260 S SKY RANCH LOOP	PALMER	AK	99645
VAN LANDINGHAM ARYAH J M	HC 89 BOX 460	WILLOW	AK	99688
VAN METER CLAUDIA A	1414 BANNISTER DR	ANCHORAGE	AK	99508
VAN METER KELLY B & GLORIA	940 FAIRBANKS ST	ANCHORAGE	AK	99501
VAN PATTEN LARRY C & F M	1180 PINION DR	ANCHORAGE	AK	99501
VAN TOUR RICHARD C	PO BOX 521042	WASILLA	AK	99654
VANCE DANIELLE E	3725 LUNAR DR	BIG LAKE	AK	99652
VANCE JAMES C & CECELIA J	4403 FOREST RD	ANCHORAGE	AK	99504
VANCE WAYNE L & CAROL L	PO BOX 890	WILLOW	AK	99517
VANDEGRIFF KEVIN	12102 LILAC CIR	ANCHORAGE	AK	99688
VANDEN BERG S K	1556 HIDDEN LN	ANCHORAGE	AK	99516
VANDEN TOORN TIMOTHY J	2231 N GREEN FOREST DR	ANCHORAGE	AK	99501
VANDER BIE MATTHEW & KARLA	11161 RIDGECREST DR	PALMER	AK	99645
VANGORDER JAS D	PO BOX 1824	ANCHORAGE	AK	99516
VANN PERRY & LUELLA P	1141 MOUNTAIN AIRE DR SE	PALMER	AK	99645
VANN SUSAN D WATERMAN	212 ORANGE LEAF CIRCLE	LACEY	WA	98503
Vanover Kathrine	440 N. Bailey	ANCHORAGE	AK	99504
VANSCIVER RODNEY	PO BOX 773464	Palmer	AK	99645
VANWINGERDEN D J & C M	PO BOX 521455	EAGLE RIVER	AK	99577
VARILEK LARRY & YOUNG O	PO BOX 940074	BIG LAKE	AK	99652
VARNELL WM R JR & PATRICIA	4803 SPENARD RD	HOUSTON	AK	99694
VARNEY MATTHEW D & MISTIE	PO BOX 91	ANCHORAGE	AK	99517
VARNEY PATRICK D	PO BOX 187	WILLOW	AK	99688
VATERLAUS QUINN H & M R L	8400 RAINTREE CIR	WILLOW	AK	99688
VAUGHN JIMMY L	PO BOX 521353	ANCHORAGE	AK	99507
VAUGHN MARK E	PO BOX 210801	BIG LAKE	AK	99652
VEENKANT RAYMOND & PATRICIA	PO BOX 232225	ANCHORAGE	AK	99521
Vehrs Betty	P.O. Box 940027	ANCHORAGE	AK	99523
VEIT MELVIN D & MARGIE A	PO BOX 269	Houston	AK	99694
VELADOR-PADILLA ARMANDO	PO BOX 521534	WILLOW	AK	99688
VELASCO OSCAR R	1018 ORCA ST	BIG LAKE	AK	99652
		ANCHORAGE	AK	99501

BP

Matanuska-Susitna Borough

City of Palmer

City of Houston

VELEZ RAPHAEL	4840 CANTERBURY WAY	ANCHORAGE	AK	99503
VELIC MUHAMED & ZARFIJA	6406 E 31ST AVE	ANCHORAGE	AK	99504
VELOCK ROBT J JR& NIKKI H	1150 N ARNOLD PALMERS ST	WASILLA	AK	99654
VELTKAMP CHRISTINA K	9915 AFOGNAK CIR	EAGLE RIVER	AK	99577
VELTKAMP RONALD J & J J	5631 GATE KEEPER AVE	ANCHORAGE	AK	99504
VELTKAMP STEVEN M & C K	6703 FOOTHILL CIR	ANCHORAGE	AK	99504
VENDIOLA DANTE & GRACE T	8650 LITTLE BROOK CIR	ANCHORAGE	AK	99507
VENHAUS ROBT J	600 W 76TH AVE # 409	ANCHORAGE	AK	99518
VENIE EUGENE R. & ANNE M.	18625 MIDWAY RD	DALLAS	TX	75287
VENTGEN CHARISSE S	HC 89 BOX 258	WILLOW	AK	99688
VENTGEN PATRICK D	PO BOX 940352	HOUSTON	AK	99694
VERA IVAN	1211 E 11TH AVE	ANCHORAGE	AK	99501
Vern Halter	PO Box 389	Willow	AK	99688
VERNON KEN	PO BOX 672265	CHUGIAK	AK	99567
VIATOR BRANDON M& MELISSA	1043 W FORAKER DR	WASILLA	AK	99654
VICKERS DARYL& BETTY JEAN	8531 E 11TH CT	ANCHORAGE	AK	99504
VICTORY GEO W & JANET M	11051 KATLIAN DR	EAGLE RIVER	AK	99577
VIGLIONE CORWIN G & TANYA	8225 RACE CIR	ANCHORAGE	AK	99504
VIHTELIC ALICIA C J	12413 W SUNRIDGE	NINE MILE FALLS,WA	AK	99026
VILORIA IMELDA P	6090 SPRUCE MEADOW LOOP	ANCHORAGE	AK	99504
VINCENT RONALD TROY	4432 N HEATON RD	PALMER	AK	99645
VINCENT ZAZHARY G	6022 E 21ST AVE	ANCHORAGE	AK	99504
Virgin Randy	807 G Street, Suite 100	Anchorage	AK	99501
VITT RODNEY W & KIM A	PO BOX 671884	CHUGIAK	AK	99567
VITT THOS J & STEPHANIE S	18060 W BRYANT RD	BIG LAKE	AK	99652
VITTETOE PHILLIP	PO BOX 2583	PALMER	AK	99645
VLAHOVICH VICTORIA L	PO BOX 222154	ANCHORAGE	AK	99522
Vleshouwer Gene	439 West Evergreen	Palmer	AK	99645
VO DEBORAH JOAN ALSTROM	3975 DEFIANCE ST	ANCHORAGE	AK	99504
VOEHL ARLENE	26637 WHITE SPRUCE	EAGLE RIVER	AK	99577
VOGEL J DENNIS & JOY A	PO BOX 202207	ANCHORAGE	AK	99510
VOGEL MATTHEW PAUL & K A	20249 NEW ENGLAND DR	ANCHORAGE	AK	99577
VOLKHEIMER DONALD L&JULIE	PO BOX 778	WILLOW	AK	99688
VOLKMAN RICHARD J & LINDA	4791 N PALMER-FISHHOOK RD	PALMER	AK	99645
VOLLERTSEN MARGARET E	4871 RETRIEVER CIR	ANCHORAGE	AK	99502
VOLPE VINCENT J	17544 SILVERWOOD WAY	EAGLE RIVER	AK	99577
VOLZ JAS R & ANN P	6458 TOLHURST CT	ANCHORAGE	AK	99504
VON SCHEBEN LEO&DEBORAH E	PO BOX 521429	BIG LAKE	AK	99652
VON SCHEELE NEIL & CLYDIA	PO BOX 521781	BIG LAKE	AK	99652
Voncille Gregoire	P.O. Box 349	Talkeetna	AK	99676-03
Vostry Mel	PO BOX 523	PALMER	AK	99645
VOTH STEVEN J	1480 WOO BLVD	ANCHORAGE	AK	99515
VRLSTED DAVID A& BARBARA	14730 PARK HILLS DR	ANCHORAGE	AK	99516

Alaska Center for the Environment

Palmer Chevron

Chase Community Council

VSHIVKOFF SUVELEY	PO BOX 1055	WILLOW	AK	99688
VUE JOUA CHOU	PO BOX 141116	ANCHORAGE	AK	99514
VUKALCIC LISA	1820 CAMFORD DR	ANCHORAGE	AK	99508
VUKICH GEO L JR & JEAN C	8761 E ELDORADO CIR	PALMER	AK	99645
WADDELL WM I & DIXIE L	18915 OLD GLENN HWY	CHUGIAK	AK	99567
WADE BENJAMIN L	10211 LEE ST	EAGLE RIVER	AK	99577
WADE BRUCE & NANCY	PO BOX 856	WILLOW	AK	99688
WADE CHAS S & GEORGIANA	PO BOX 830	WILLOW	AK	99688
WADE LARRAINE	327 W EVERGREEN AVE	PALMER	AK	99645
WADINGTON MARION G SR	PO BOX 091282	ANCHORAGE	AK	99509
WAGAR PAUL A	3953 JAMES DR	ANCHORAGE	AK	99504
WAGNER CHAS M & HEATHER S	3338 S CHARMING VLY LOOP	PALMER	AK	99645
WAGNER DENNIS & KIMBERLY	8661 WILLIWA AVE	ANCHORAGE	AK	99504
WAGNER DONALD P	301 LANGNES CT	ANCHORAGE	AK	99515
Wagner George	P O BOX 964	TALKEETNA	AK	99676
WAGNER LEONARD F & G A	300 W KLATT RD	ANCHORAGE	AK	99515
WAGNER VERNON & VANESSA	5860 S BODENBURG LOOP	PALMER	AK	99645
WAGSTER NANCY G	PO BOX 989	WILLOW	AK	99688
WAITE BRIAN	4761 MARS DR	ANCHORAGE	AK	99507
WAKEMAN NORMAN & SHIRLEY	PO BOX 988	WILLOW	AK	99688
WALATKA FREDERICK W	3107 W 29TH AVE	ANCHORAGE	AK	99517
WALATKA MARK ANDREW	3933 BORLAND DR	ANCHORAGE	AK	99517
WALCUTT STEVEN SCOTT	PO BOX 670215	CHUGIAK	AK	99567
WALKER BRANDON & KJERSTI	5123 E 98TH	ANCHORAGE	AK	99507
WALKER BRETT W & APRYLL	PO BOX 2971	PALMER	AK	99645
WALKER BRITTEN M	618 ACCESS DR	FORT RICHARDS	AK	99505
WALKER CAMMIE L	PO BOX 896	WILLOW	AK	99688
WALKER DAVID B & BEVERLY D	PO BOX 409	WILLOW	AK	99688
WALKER JACK V & EDNA MAE	PO BOX 102256	ANCHORAGE	AK	99510
Walker Kathleen	P.O. Box 1687	Palmer	AK	99645
WALKER RONALD D & MARCELLA	1050 E DONNA CIR	WASILLA	AK	99654
WALKER THOS F & JANE M	5241 WILD MTN DR	EAGLE RIVER	AK	99577
WALKER THOS THEO	PO BOX 1829	BIG LAKE	AK	99652
WALKER VALORIE FAYE	2341 SCARBOROUGH DR	ANCHORAGE	AK	99504
WALKER WM M & DONNA P	2234 KISSEE CT	ANCHORAGE	AK	99517
WALL ALISON M	PO BOX 521576	BIG LAKE	AK	99652
WALL DIANE	1117 ABBINGTON CT	ANCHORAGE	AK	99503
WALL DOUGLAS A	16989 FOOTHILL AVE	EAGLE RIVER	AK	99577
WALL RUBY MAE WOOL	PO BOX 521866	BIG LAKE	AK	99652
WALLACE JAS A & LINDA L	PO BOX 940145	HOUSTON	AK	99694
WALLACE LOUISE D	PO BOX 940163	HOUSTON	AK	99694
WALLACE WESLEY L & N I	HC 89 BOX 130	WILLOW	AK	99688
WALLEN JIMMY R & VIRGINIA R	PO BOX 520122	BIG LAKE	AK	99652

Valley Hospital

WALLIN GEO	8050 MEDELLIN CIR	ANCHORAGE	AK	99507
WALLING REX R	PO BOX 3084	PALMER	AK	99645
WALLIS JACKIE M & LISA M	2028 E NORTHERN LGTS BLVD	ANCHORAGE	AK	99508
Wally Soroka	PO Box 849	WILLOW	AK	99688
WALSH DAN'L A & CECILIA M	PO BOX 520619	BIG LAKE	AK	99652
WALSH PAUL K & STEPHENIE P	PO BOX 670645	CHUGIAK	AK	99567
WALTER CATHY	PO BOX 871253	Wasilla	AK	99687
WALTER CATHY	PO BOX 871253	Wasilla	AK	99687
WALTER MARK F	PO BOX 520095	BIG LAKE	AK	99652
WALTER MARY JOYCE	PO BOX 520294	BIG LAKE	AK	99652
WALTER MICHELLE	PO BOX 1006	WILLOW	AK	99688
WALTERS BRET L	PO BOX 521676	BIG LAKE	AK	99652
WALTHALL ROBT & DOROTHY	9031 EMERALD ST	ANCHORAGE	AK	99502
WALTON C PAIGE	PO BOX 221166	ANCHORAGE	AK	99522
Walton Eddie	1717 Tidewater Road	Anchorage	AK	99501
WALTON KATHY-JO	PO BOX 520026	BIG LAKE	AK	99652
WALTON LOYD C & FAYE M	3809 APOLLO DR	ANCHORAGE	AK	99504
WALZ ROXANNE J	2479 S HORNUNG RD	PALMER	AK	99645
WANAMAKER JAS	5600 E 40TH	ANCHORAGE	AK	99504
WANDLER CLARENCE & KATHERYN	1032 W 11TH AVE	ANCHORAGE	AK	99501
WANSOR-HAWKINS TRACIE J	HC 01 BOX 6488A	PALMER	AK	99645
WARD ALAN D & SALVACION O	3266 ORION CIR	ANCHORAGE	AK	99517
WARD APRIL A	12875 E ALLISON CT	PALMER	AK	99645
WARD DAN'L K	PO BOX 1073	WILLOW	AK	99688
WARD GRADY	927 E 13TH AVE	ANCHORAGE	AK	99501
WARD JEAN M MORGAN	30723 SAGWON AVE	EAGLE RIVER	AK	99577
WARD KENTON L	PO BOX 211272	AUKE BAY	AK	99821
WARDLE GLEN A & JUDITH L	PO BOX 112152	ANCHORAGE	AK	99511
WARENDA LISA A	1701 S PIONEER DR	PALMER	AK	99645
WARFIELD MICHAEL A	2131 E 66TH	ANCHORAGE	AK	99518
Warhus Marni & Donn	HCO3 Box 8100Q	Palmer	AK	99645
WARINER NORMA MAE	3660 AMBER BAY LOOP	ANCHORAGE	AK	99515
Warner Doug	1800 Glenn Hwy, Suite 12	Palmer	AK	99645
WARNER DOUGLAS N	7000 VIBURNUM DR	ANCHORAGE	AK	99507
WARNER MICHAEL G & D C	PO BOX 113029	ANCHORAGE	AK	99511
WARNER WALT	HC 89 BOX 109	WILLOW	AK	99688
WARNER WESLEY VANCE	PO BOX 3925	PALMER	AK	99645
WARREN KALYNN	9020 ARLON	ANCHORAGE	AK	99507
WARREN RYAN	PO BOX 1084	WILLOW	AK	99688
WARREN VELMA	2614 E 17TH AVE	ANCHORAGE	AK	99508
WARREN WENDY A	HC 89 BOX 323	WILLOW	AK	99688
WARTHEN ROBT C & FAITH D	9350 NORDIC DR	ANCHORAGE	AK	99516
WASHBURN CHAS E IV	PO BOX 521784	BIG LAKE	AK	99652

Horizon Lines of Alaska

Alaska Division of Agriculture

WASHINGTON BERNARD W & D R	530 MARY CIR	ANCHORAGE	AK	99515
WASSELLE GERARD W II & S	2020 ABBOTT RD	ANCHORAGE	AK	99507
WASSER MARK L & DINA	PO BOX 141345	ANCHORAGE	AK	99514
WASSERMAN ERIC M & JOAN E	13441 SPENDLOVE DR	ANCHORAGE	AK	99516
WATERS BRENT A	7628 SNOWVIEW DR	ANCHORAGE	AK	99507
WATERS JOHN I SR	PO BOX 1456	PALMER	AK	99645
WATERS PATRICK P	2230 CANDY PL	ANCHORAGE	AK	99508
WATSJOLD JOHN L	1328 BIRCHWOOD	ANCHORAGE	AK	99508
WATSJOLD STANLEY J & SALLY	PO BOX 394	WILLOW	AK	99688
WATSON BRIAN & JENNIFER E	360 E INTL AIRPORT RD	ANCHORAGE	AK	99518
WATSON CLAYTON W	PO BOX 520602	BIG LAKE	AK	99652
WATSON DALE	PO BOX 672225	CHUGIAK	AK	99567
WATTS CAMILLE C	1165 W WINTER AVE	WASILLA	AK	99654
WATTS DAVID D	9926 W SCHULZ DR	BIG LAKE	AK	99652
WATTS RICHARD J JR	10341 TREE TOP LN	ANCHORAGE	AK	99507
WAYCHOFF KIM	HC 05 BOX 6712	PALMER	AK	99645
WAYCHOFF WILMA M	240 BUNN ST	ANCHORAGE	AK	99508
WAYMAN JOHN W SR	PO BOX 221214	ANCHORAGE	AK	99522
Wayne Biessel	HC 32 Box 6706	WASILLA	AK	99654
Wayne Biessel	HC 32 Box 6706	WASILLA	AK	99654
WEBB BRETT O	821 HARBOR CIR	ANCHORAGE	AK	99515
WEBB GARY L & TAMMY L	343 W BENSON BLVD	ANCHORAGE	AK	99503
WEBB MARC	6901 E 12TH AVE	ANCHORAGE	AK	99504
WEBER THOS E II	PO BOX 521536	BIG LAKE	AK	99652
WEBRE WANDA J	3244 STAYSAIL DR	ANCHORAGE	AK	99516
Webster David	HC30 Box 12885	Wasilla	AK	99654
Webster Marvella	PO Box 670233	Chugiak	AK	99567
WEBSTER MARVELLA D	1535 N GOLDEN HILLS DR	PALMER	AK	99645
WEDEKIND KENT	11901 GILLETTE DR	ANCHORAGE	AK	99516
WEDEL ROBE & NICOLE	20252 NEW ENGLAND DR	EAGLE RIVER	AK	99577
WEDEMEYER KATHLEEN	3708 CHECKMATE DR	ANCHORAGE	AK	99508
WEDGE MICHAEL D & JENNIFER	17450 E FAMILY CIR	PALMER	AK	99645
WEDIN BENJAMIN & JESSICA	20840 ICEFALL DR	EAGLE RIVER	AK	99577
WEED LOIS J	636 1ST ST	PALMER	AK	99645
WEEKS JAS D	1629 W 11TH AVE	ANCHORAGE	AK	99501
WEEKS WARREN J & KATHY A	PO BOX 827	WILLOW	AK	99688
WEGNER CAROLE	PO BOX 1065	WILLOW	AK	99688
WEIGMAN DANIEL R & BETTY J	12369 GREGG LN	ANCHORAGE	AK	99515
WEIL BLAKE R	2510 INGRA ST	ANCHORAGE	AK	99508
WEIL HENRY T & BARBARA L	4800 TALUS DR	ANCHORAGE	AK	99516
Weiland Sally	P.O. Box 2995	Palmer	AK	99645
Weiland Terry & Teri	HC04 Box 7463	Palmer	AK	99645
WEIMAN HARLAN R	17251 PALOS VERDIS DR	EAGLE RIVER	AK	99577

Mat-Su Borough Planning Commission  
Chugiak Childrens Services

Pioneer Motel & Apts  
Pioneer Equipment Inc.

WEIMER JAS & ANDREA	PO BOX 520222	BIG LAKE	AK	99652
WEINBERGER RALPH J & G J	PO BOX 520015	BIG LAKE	AK	99652
WEISHEIM LARRY E	PO BOX 110189	ANCHORAGE	AK	99511
WEISMANN THOS M	9811 CARLSON RD	ANCHORAGE	AK	99507
WEISS JOHN D & SHIH YING	6236 PROMIMENCE POINT DR	ANCHORAGE	AK	99516
WEISS TIMOTHY W	7820 MAE RENE CIR	ANCHORAGE	AK	99502
WELBORN ERIC & TERRI	PO BOX 5547	FORT RICHARDS	AK	99505
WELCH JOSEPH C & DONNA C	1300 N IVY CIR	WASILLA	AK	99654
WELCH MATTHEW A & STACEY R	2809 S CHARMING VALLEY	PALMER	AK	99645
WELCH SALLINA N	17349 N JAUNITA LOOP	EAGLE RIVER	AK	99577
WELKER LOUISE E	1501 W 11TH AVE	ANCHORAGE	AK	99501
WELLS CHRISTINE M	2741 W 69TH AVE	ANCHORAGE	AK	99502
WELLS KATHLEEN A	PO BOX 3331	PALMER	AK	99645
Wells Kathy	P.O. Box 116	Palmer	AK	99645
Wells Kathy	P.O. Box 578	Palmer	AK	99645
Wells Robert	HC05, Box 6871	Palmer	AK	99645
WELLS STEPHEN F & SHARON K	1527 G ST	ANCHORAGE	AK	99501
WELLS STEPHEN J & SHYLA M	503 JORDT CIR	ANCHORAGE	AK	99504
WELLS THOS J & PENNY F	6200 DOWNEY FINCH DR	ANCHORAGE	AK	99516
WELLS VYRON C & MARY C	717 BRADLEY CIR	ANCHORAGE	AK	99518
WELP PAUL J	PO BOX 190023	ANCHORAGE	AK	99519
WELSH JARED A	422 S SARAH CIR	PALMER	AK	99645
Welton Sarah	P.O. Box 870725	Wasilla	AK	99687
Welton Sue	303 W. Evergreen Ave	Palmer	AK	99645
WELTON SUSANN J	PO BOX 3846	PALMER	AK	99645
WENDT OTTO WM	4381 WHITE ST	FORT WAINWRIGHT	AK	99703
WENDTE RONALD W	8601 LAVIENTO DR	ANCHORAGE	AK	99515
WENTWORTH JAS	1680 N LOMA PRIETA DR	PALMER	AK	99645
WERDA CARL & JILL S	PO BOX 191003	ANCHORAGE	AK	99519
WERMERS LEONARD CHAS	PO BOX 772873	EAGLE RIVER	AK	99577
WERNER JESS A	2264 S HORNUNG RD	PALMER	AK	99645
WERNING JAS D	8050 PIONEER DR	ANCHORAGE	AK	99504
WERT DUANE EARLE	24920 HOMESTEAD RD	CHUGIAK	AK	99567
WERTS MICHAEL D	8051 WOODGREEN CIR	ANCHORAGE	AK	99518
WESEN CLINTON D & REBECA M	3101 WILLOW ST	ANCHORAGE	AK	99517
WESLEY THOS	2981 CONCORD	ANCHORAGE	AK	99502
WESOLOWSKI CHRIS E & V L	11700 SUNCREST CIR	ANCHORAGE	AK	99515
WESOLOWSKI STACY	26045 LOG CABIN CIR	ANCHORAGE	AK	99577
WESSELS JOHN W & W R	PO BOX 520254	EAGLE RIVER	AK	99577
West Jack	P.O. Box 300	BIG LAKE	AK	99652
WEST KEVIN E	2301 ROOSEVELT CIR	Talkeetna	AK	99676
Westall Tom	290 E. HERNING AVE.	ANCHORAGE	AK	99517
WESTERVELT DAN'L V & S	PO BOX 110394	WASILLA	AK	99654-70
		ANCHORAGE	AK	99511

Westfall Donna	PO Box 520164	Big Lake	AK	99652
WESTFALL KARL W & KURT	2020 MULDOON RD # 120	ANCHORAGE	AK	99504
WESTFALL KEITH M & L K	PO BOX 520164	BIG LAKE	AK	99652
WESTFALL W JOE & SHEILA M	PO BOX 729	WILLOW	AK	99688
WESTIN R H & SANDRA C	5200 SECLUDED CIR	ANCHORAGE	AK	99516
WESTLAND KEN & MARLENE M	PO BOX 520702	BIG LAKE	AK	99652
WESTLAND MARY E KEEL	PO BOX 520911	BIG LAKE	AK	99652
WESTLIEN ERLING & JOYCE	2421 FOXHALL DR	ANCHORAGE	AK	99504
WESTLUND RICHARD D & SUSAN	22454 HILLTOP CIR	CHUGIAK	AK	99567
WETHERHORN IAN D	HC 89 BOX 111	WILLOW	AK	99688
WETZEL GORDON P & LINDA C	6120 W TREE DR	ANCHORAGE	AK	99516
Wetzel Lee Ann	P. O. Box 544	Talkeetna	AK	99676
WAH MARY C	PO BOX 91416	ANCHORAGE	AK	99509
WHALEY WAYNE N & E D	897 W EVERGREEN AVE	PALMER	AK	99645
WHEELER CHARLES E	727 N ST	ANCHORAGE	AK	99501
WHEELER JAS A & LISA D	PO BOX 111064	ANCHORAGE	AK	99517
WHEELER LEONARD E IV & D W	PO BOX 520276	BIG LAKE	AK	99652
WHEELER MICHAEL A	600 A ST	ANCHORAGE	AK	99518
WHEELER SHIRLEY A	PO BOX 520016	BIG LAKE	AK	99652
Wheeler Stephenie	8090 Pinebrook Circle	Anchorage	AK	99507
WHITACRE MARVIN L SR& IVA	PO BOX 521094	BIG LAKE	AK	99652
WHITAKER ROBT K & SHEILA	PO BOX 520756	BIG LAKE	AK	99652
WHITE DAVID W & KELLI K	PO BOX 111427	ANCHORAGE	AK	99511
WHITE DONALD E JR& LISA M	7120 E CHESTER HEIGHTS	ANCHORAGE	AK	99504
WHITE ELSIE M	7924 HIGHLANDER DR	ANCHORAGE	AK	99518
WHITE JAS R & DIANE	8337 SUNDI DR	ANCHORAGE	AK	99502
WHITE JAS T	PO BOX 520094	BIG LAKE	AK	99652
WHITE JOHN M & JUDITH M	PO BOX 670630	CHUGIAK	AK	99567
WHITE JONATHAN T & TYHESIA	327 PAULINE ST	ANCHORAGE	AK	99504
WHITE KEITH CHAS	231 BONNIE JEAN CT	ANCHORAGE	AK	99515
WHITE KENNETH H & LINDA L	PO BOX 520247	BIG LAKE	AK	99652
WHITE LINDA G	8530 GREENHILL WAY	ANCHORAGE	AK	99502
WHITE LOGAN	1650 N LEGACY LANE	PALMER	AK	99645
WHITE THEO F & MELITTA C	PO BOX 975	WILLOW	AK	99688
WHITFORD JOHN H & SHARON	8107 HUCKLEBERRY	ANCHORAGE	AK	99502
WHITNEY RANDY E & KERRY E	1714 PARKWAY DR	ANCHORAGE	AK	99504
WHITT JAS H JR&PATRICIA A	16000 E HUNTLEY RD	PALMER	AK	99645
WHITTED NATHAN A& KRISTAL	PO BOX 521323	BIG LAKE	AK	99652
WHITTERS JAMES P	13945 KNOB HILL	EAGLE RIVER	AK	99577
WICHOREK PAUL & HELEN	2905 JONES AVE	ANCHORAGE	AK	99517
WICHSER RONALD W& KRISTIN	HC 89 BOX 428	WILLOW	AK	99688
Wickes Jim	505 E. Bluff Drive	Anchorage	AK	98660
WICKHAM GREGORY J & P J	5970 E GERSHIMEL LOOP	PALMER	AK	99645

McKinley Air Service

AT&T Alascom



WIDENER MARK A			PO BOX 220793	ANCHORAGE	AK	99522
WIDMAR RAFAEL			5801 AZALEA DR	ANCHORAGE	AK	99516
Wielechowski Bill	Alaska State Legislature		1300 Farrow Circle	Anchorage	AK	99504
WIERTSEMA HARLAN W&SHARON			HC 89 BOX 387	WILLOW	AK	99688
WIESKAMP DAVID A & D K			PO BOX 664	WILLOW	AK	99688
WIFFLER LORETTA MAY			PO BOX 1012	WILLOW	AK	99688
WIGGINS DAN'L L JR			PO BOX 770735	EAGLE RIVER	AK	99577
WIITA JOANNE			PO BOX 111762	ANCHORAGE	AK	99511
WIKHEIM CHAS C			PO BOX 521053	BIG LAKE	AK	99652
WILCHECK MICHAEL W			820 CEDAR ST	ANCHORAGE	AK	99501
WILCOX DONALD G & DOROTHY			6811 HOWARD AVE	ANCHORAGE	AK	99504
WILCOX PAUL S & DAWN B			3116 MADISON WAY	ANCHORAGE	AK	99508
WILCOX STEPHEN A&VIRGINIA			3631 SCAMMON BAY CIR	ANCHORAGE	AK	99515
WILDE LAWRENCE & CLAUDIA			PO BOX 521105	ANCHORAGE	AK	99652
WILFONG JOYCE			3927 BRYANT RIDGE PL	ANCHORAGE	AK	99504
WILHELM REX A & STEPHANIE			6800 CROOKED TREE CIR	ANCHORAGE	AK	99507
WILKEN JAS A			18223 KANTISHNA DR	EAGLE RIVER	AK	99577
WILKEN KURT A			22750 MCMANUS DR	CHUGIAK	AK	99567
WILKEN WAYNE R & SHERRY L	MAT VLY FED CR UNION		10534 SPINDRIFT LOOP	ANCHORAGE	AK	99515
WILKERSON BRIAN R			PO BOX 5521467	BIG LAKE	AK	99652
WILKIE GEO A			PO BOX 1061	WILLOW	AK	99688
WILKINSON GREGORY			21949 LOWER CANYON DR	EAGLE RIVER	AK	99577
WILKINSON LARRY ALAN			414 EKLUTNA ST	ANCHORAGE	AK	99504
Wilkinson Mike	State Farm Insurance		301 w. Northern Lights Blvd. Suite 401	Anchorage	AK	99510-08
WILKINSON WM & KIMALA			16701 RANSOM RIDGE RD	ANCHORAGE	AK	99516
WILKS ROBT J JR&SHANNON K			2941 WESTWIND CT	ANCHORAGE	AK	99516
WILLAMSON MARSHALL B JR			4986 CASTLE CT	ANCHORAGE	AK	99508
WILLDEN BONNIE L H			3300 N DOVE LN	PALMER	AK	99645
WILLE BRIAN R			13161 SHELBURNE RD	ANCHORAGE	AK	99516
Willett Diane	Floral Creations		P.O. Box 3686	Palmer	AK	99645
William and Natalie Luth			PO BOX 428	WILLOW	AK	99688
William C. Jacobson			2221 DeVot Ct.	Anchorage	AK	99502
Williams Byron	Fishhook Community Council		HC05 Box 6835	Palmer	AK	99645
Williams Charles			19551 Highland Ridge Drive	Eagle River	AK	99577
WILLIAMS CHAS L JR			PO BOX 940545	HOUSTON	AK	99694
Williams CHRIS M&VIVIAN L	SLG INV LLC		1060 N LEATHERLEAF	WASILLA	AK	99654
WILLIAMS DAVID M			PO BOX 221513	ANCHORAGE	AK	99522
WILLIAMS DAVID M			PO BOX 521006	BIG LAKE	AK	99652
WILLIAMS DONALD O & M			PO BOX 771972	EAGLE RIVER	AK	99577
Williams George A.			P O BOX 2108	PALMER	AK	99645
WILLIAMS JOE JR			6465 COACHMAN CIR	ANCHORAGE	AK	99507
WILLIAMS JOHN S			PO BOX 521574	BIG LAKE	AK	99652
WILLIAMS JOHN W			PO BOX 520005	BIG LAKE	AK	99652

WILLIAMS JOHNNIE L & D L	PO BOX 140214	ANCHORAGE	AK	99514
WILLIAMS KIMBERLY ANN S	3520 EVERGREEN ST	ANCHORAGE	AK	99504
WILLIAMS LEE A	PO BOX 101561	ANCHORAGE	AK	99510
WILLIAMS MARIE	PO BOX 212914	ANCHORAGE	AK	99521
WILLIAMS MATT	611 CLIPPER SHIP CT	ANCHORAGE	AK	99515
WILLIAMS PAUL J JR & OLA	PO BOX 750	WILLOW	AK	99688
WILLIAMS PHILLIP S	PO BOX 110258	ANCHORAGE	AK	99511
WILLIAMS RAY	840 W 71ST AVE	ANCHORAGE	AK	99518
WILLIAMS ROBT A & P C	1735A S HERITAGE CIR	PALMER	AK	99645
WILLIAMS ROBT W & K L	2102 TASHA DR	ANCHORAGE	AK	99502
WILLIAMS RONALD K & RUTH	PO BOX 2967	PALMER	AK	99645
WILLIAMS ROY W & D H	PO BOX 671011	CHUGIAK	AK	99567
WILLIAMS SUSIE O TRE	11201 AVION ST	ANCHORAGE	AK	99516
WILLIAMS TED L	2709 S BODENBURG LOOP	PALMER	AK	99645
Williams Walter T	HCO1 BOX 6204	PALMER	AK	99645
WILLIAMS WANDA JEAN	510 GLACIER BAY CIR # B	ANCHORAGE	AK	99508
WILLIAMS WARREN L	2464 SPRUCEWOOD ST	ANCHORAGE	AK	99508
WILLIAMS WISTER	821 BIRCH ST	ANCHORAGE	AK	99501
Williamson Donna W.	5729B Valser Avenue	EAFB	AK	99506
WILLINGHAM JAS M & PANDORA	20225 CONSTITUTION DR	EAGLE RIVER	AK	99577
WILLIS ANTHONY LEE	23547 W BELUGA AVE	WILLOW	AK	99688
WILLIS JAS S & SABRINA A	5922 ROMANIA DR	ANCHORAGE	AK	99516
WILLIS JAS SCOTT	8740 CARTER CIR	ANCHORAGE	AK	99507
WILLIS JOEY G	4000 E 142ND AVE	ANCHORAGE	AK	99516
WILLIS PETER C & SHEILA T	12421 BEACHCOMBER DR	ANCHORAGE	AK	99515
WILLIS VICTOR R	444 FREDRICKS DR	ANCHORAGE	AK	99504
WILLOW HIST & WILDLE FND	PO BOX 38	WILLOW	AK	99688
WILMARTH JUDY	PO BOX 200971	ANCHORAGE	AK	99520
WILSON JEAN	11803 PENFORD DR	WHITTIER	CA	90604
WILSON ALLYN G& ROXANNE M	4531 TRAPLINE CIR	ANCHORAGE	AK	99516
WILSON BOBBY G	PO BOX 202866	ANCHORAGE	AK	99520
WILSON CHAS R & CONNIE R	3901 IONA CIR	ANCHORAGE	AK	99507
WILSON CHONG SUN	7505 BOUNDARY AVE	ANCHORAGE	AK	99504
WILSON CHRISTINE M	205 E BENSON	ANCHORAGE	AK	99503
WILSON DAVID & PATTI JO	PO BOX 521652	BIG LAKE	AK	99652
WILSON DAVID J & TRACY S	1470 S LOWER CIR	PALMER	AK	99645
Wilson Deenie	3111 C Street	Anchorage	AK	99503
WILSON ELI	12024 CORAL REEF	ANCHORAGE	AK	99515
WILSON GUNER & HOLLY S	1930 S GLENN HWY	PALMER	AK	99645
WILSON JASON A & ANDREA D	PO BOX 5663	FORT RICHARDS	AK	99505
WILSON JOSEPHINE L	PO BOX 101826	ANCHORAGE	AK	99510
WILSON LEE	14035 VENUS WAY	ANCHORAGE	AK	99515
WILSON LON G	4240 TAHOE DR	ANCHORAGE	AK	99502

WILSON LOU ANN	PO BOX 712	WILLOW	AK	99688
WILSON MARK W & KRISTI B	PO BOX 220467	ANCHORAGE	AK	99522
WILSON TED V & ROSEMARY	PO BOX 521883	BIG LAKE	AK	99652
WILSON WADE J & SALLY A	4317 S JW CT	PALMER	AK	99645
WILSON WM J & ELAINE A	13611 CAPSTAN DR	ANCHORAGE	AK	99516
WILSON YONG SUN	6348 FAIRWEATHER DR	ANCHORAGE	AK	99518
WINCE FRANK W & EVELYN P	2414 DOUGLAS DR	ANCHORAGE	AK	99517
Winder Johnnie	P.O. Box 940027	Houston	AK	99694
WINDER JOHNNIE	PO BOX 920204	HOUSTON	AK	99694
WINDSOR GREG	PO BOX 230307	ANCHORAGE	AK	99523
WINECK WALTER D & CAROLL	1807 MCKINLEY AVE	ANCHORAGE	AK	99517
WINES SHIRLEY J	HC 89 BOX 275	WILLOW	AK	99688
WINEY CAROL J	1583 GARDEN ST	ANCHORAGE	AK	99508
WINGER WM J & MARJORY A	PO BOX 212491	ANCHORAGE	AK	99521
WINGO PENELOPE B	PO BOX 520905	BIG LAKE	AK	99652
WINK JOS M.	1900 CONGRESS CIR	ANCHORAGE	AK	99507
Winkelman Herder	135 N. Alaska Street, Apt. 1	Palmer	AK	99645
WINN WANDAL W & MARTHA J	8721 BELL PL	ANCHORAGE	AK	99507
WINSLOW JEFFERY A & P L	PO BOX 521564	BIG LAKE	AK	99652
WINTER DON E	5422 WINDFLOWER CIR	ANCHORAGE	AK	99507
WINTER SIEGLINDA MARIA	9257 JEWEL LAKE RD	ANCHORAGE	AK	99502
WINTERTON DAN'L D & K A	6750 BAXTER TERRACE CIR	ANCHORAGE	AK	99504
WINZENBURG DANA A	PO BOX 521033	BIG LAKE	AK	99652
WIRUM JOHN MITCHELL	500 L ST	ANCHORAGE	AK	99501
WISDOM DAVID S	20719 WILLIAMSBURG DR	EAGLE RIVER	AK	99577
WISEOWL EULIE & PENNEY	2201 JASPER LN	ANCHORAGE	AK	99504
WISNIEWSKI JAMES S	24808 TEAL LOOP	CHUGIAK	AK	99567
WITHEY CAROL L JAMES	PO BOX 671644	CHUGIAK	AK	99567
WITHEY LINDA J	2155 TASHA DR	ANCHORAGE	AK	99502
WITHEY PHILIP L.	PO BOX 671644	CHUGIAK	AK	99567
WITO VOYTEK L & ULI H	16000 KINGS WAY	ANCHORAGE	AK	99516
WITSOE CLEMENTINE C	PO BOX 520353	BIG LAKE	AK	99652
WITSOE DONALD ALBERT	PO BOX 520576	BIG LAKE	AK	99652
WITT GARY L & CINDY S	1732 KALGIN ST	ANCHORAGE	AK	99504
WITT ROBT A JR & LACINDA	2471 TRADEWIND DR	ANCHORAGE	AK	99516
WITTE JOHN D & MARY S	2650 NATHANIAL CT	ANCHORAGE	AK	99517
WITTHOFT KENNETH & LINDA	6042 E 21ST AVE	ANCHORAGE	AK	99504
WITTMER BONNIE L	PO BOX 985	WILLOW	AK	99688
WITTROCK WAYNE & DEANN	1061 W 70TH AVE	ANCHORAGE	AK	99518
WODKOWSKI DENNIS J&DEBBIE	PO BOX 521702	BIG LAKE	AK	99652
WODKOWSKI EUGENE D	PO BOX 526	WILLOW	AK	99688
WODKOWSKI VIRGINIA N	PO BOX 557	WILLOW	AK	99688
WOELFEL JAS R	8620 SPRUCE BROOK ST	ANCHORAGE	AK	99507

City of Houston

Palmer Planning & Zoning Commission

LARUE TR THE



WOODWARD RICHARD C & M G	ANCH ROOFING & CONTR INC	1308 W 11TH AVE	ANCHORAGE	99501
WOOLARD WM T & CONNIE L		PO BOX 936	WILLOW	99688
WOOLCOCK MICHAEL R		3144 SEA PORT CIR	ANCHORAGE	99515
WOOLEY CHRIS B & BEVERLY K		2073 DIMOND DR	ANCHORAGE	99507
WOOTEN JASON E		PO BOX 241321	ANCHORAGE	99524
WORKMAN SHANNON L		PO BOX 233331	ANCHORAGE	99523
WORLEY MARK A & BARBARA L		3645 W 41ST AVE	ANCHORAGE	99517
Worrell Pattie	Matanuska Electric Association	P.O. Box 2929	Palmer	99645
WORTH HOWARD		6574 DESIREE LP	ANCHORAGE	99507
WORTH ROBIN L		PO BOX 521253	BIG LAKE	99652
WORTHINGTON JAS M & TERESA		13831 KAREN ST	ANCHORAGE	99515
WORTHLEY HERBERT H		3434 CHECKMATE DR	ANCHORAGE	99508
WORTMAN CHARLENE V		PO BOX 2585	PALMER	99645
WREN STAN C & PATRICIA S		2891 W INT'L AIRPORT RD	ANCHORAGE	99502
Wrentmore Robert J	United States Army	Attn Aprv De	Ft Richardson	99505
Wright Bill	Wrightway Auto Carriers	101 Whitney Road	Anchorage	99501
WRIGHT BILL & BETTY L		23024 TUNDRA ROSE AVE	CHUGIAK	99567
WRIGHT CHRISTOPHER A & R L		PO BOX 520724	BIG LAKE	99652
WRIGHT KRYSTAL S		824 W 74TH AVE	ANCHORAGE	99518
WRIGHT LARRY A & JEANETTE		19840 KIRK	EAGLE RIVER	99577
WRIGHT OLIN S		5006 ROGER DR	ANCHORAGE	99507
WRIGHT PATRICK T & JANICE		15819 BRIDGEVIEW DR	ANCHORAGE	99516
WRIGHT STEVEN G & K R		1819 E 24TH AVE	ANCHORAGE	99508
WYATT RICHARD G & DEBRA L		PO BOX 545	WILLOW	99688
WYATT ROBT E & BETTIE A		HC 89 BOX 372	WILLOW	99688
WYRICK BARBARA J		PO BOX 562	WILLOW	99688
WYRICK L E & MARILYN		PO BOX 782	WILLOW	99688
Yachmetz Mark	Federal Railroad Administration	1120 Vermont Avenue NW	Washington	20590
YAN JUSHIN		PO BOX 111836	ANCHORAGE	99511
YANCEY JON P		PO BOX 211266	ANCHORAGE	99521
YANCEY PHILO B II	SR WASILLA LLC	118 SWALLOW CV	HENRICO	27842
YANG ARTHUR C & MAGGIE		8626 VERNON ST	ANCHORAGE	99515
YATES GEO T		PO BOX 91816	ANCHORAGE	99509
YATES GEO T		PO BOX 884	WILLOW	99688
YBARRA JORGE		6514 DESIRE LOOP	ANCHORAGE	99507
YEAGER DAWN		PO BOX 212232	ANCHORAGE	99521
YEAGER MICHAEL D		PO BOX 520476	BIG LAKE	99652
Yeagan Jim	Windhover Construction	11334 Lower Sunny Circle	Eagle River	99577
YELL COLE MICHAEL		4310 E 136TH	ANCHORAGE	99516
YELL DAN'L E & AUDREY L		4310 E 136TH AVE	ANCHORAGE	99516
YERBURY FRED		PO BOX 611	WILLOW	99688
YERGER ROBT P		PO BOX 520249	BIG LAKE	99652
YERGER ROBT P		PO BOX 520304	BIG LAKE	99652

YONCHER SHANON H&AMANDA K	PO BOX 520900	BIG LAKE	AK	99652
YOON YOUNG S & OK S	408 E FIREWEED LN	ANCHORAGE	AK	99503
YORK BRYAN J	5717 SAMOA ST	ANCHORAGE	AK	99507
YORK RICHARD E	1201 CACHE DR	WASILLA	AK	99654
YORKE MICHAEL & MARY	23847 IMMEIMAN CIR	CHUGIAK	AK	99567
YOSHIMURA CONNIE	3801 CENTERPOINT	ANCHORAGE	AK	99503
YOUNG AMY L	3603 EASTWIND DR	ANCHORAGE	AK	99516
YOUNG DAVID H	4201 TUDOR CENTRE DR	ANCHORAGE	AK	99508
Young Don	510 L Street, Ste. 580	Anchorage	AK	99501
Young Don	2111 Rayburn House Office Bldg.	Washington	D.C.	20515-02
YOUNG HAROLD J III& TRACY	5343 E BIDEFORD BLVD	PALMER	AK	99645
YOUNG JACK L & ISABELLA	6801 DEBARR AVE	ANCHORAGE	AK	99504
YOUNG JAS P & MARY T	3310 EASTWIND DR	ANCHORAGE	AK	99516
YOUNG MAYNARD	HC 89 BOX 390	WILLOW	AK	99688
YOUNG RICHARD & CHRISTINA	800 WOODMAR PL	ANCHORAGE	AK	99515
YOUNG VICTOR R & LYNN L	PO BOX 123	WILLOW	AK	99688
YOUNGBLOOD MARK D& MARITT	PO BOX 3453	PALMER	AK	99645
YOUNGMUN GREGORY L&PAMELA	8145 SKYHILLS DR	ANCHORAGE	AK	99502
YU KIRK & MI	16059 HIDDEN CREEK LN	ANCHORAGE	AK	99516
YUN AMY S	PO BOX 230273	ANCHORAGE	AK	99523
Yvonne Lentwytles	PO Box 515	WILLOW	AK	99688
Yvonne Sumner	P.O. Box 872992	Wasilla	AK	99867
Yvonne Sumner	P.O. Box 872992	Wasilla	AK	99867
ZACHARES WM D III&CAMILLE	2370 FOXHILL DR	ANCHORAGE	AK	99504
ZAISER EILEEN M	634 K ST	ANCHORAGE	AK	99501
ZAJAC ANDREW R & SANDRA S	PO BOX 773496	EAGLE RIVER	AK	99577
ZAKURDAEW PETER	321 OCEANVIEW DR	ANCHORAGE	AK	99515
ZAMORA WILLIE & ERLINDA	3201 W 28TH CT	ANCHORAGE	AK	99517
ZANELLA ROBT	PO BOX 2	WILLOW	AK	99688
ZAPOTOSKY DEBORAH LOUISE	9440 VICTOR RD	ANCHORAGE	AK	99515
ZAPPA JOHN A	253 IDAHO ST	ANCHORAGE	AK	99504
ZARR DONALD R	810 ELM ST	ANCHORAGE	AK	99501
ZARTMAN SANDRA JOAN	PO BOX 1423	PALMER	AK	99645
ZEDDIES WM E JR& LOUISE A	PO BOX 520586	BIG LAKE	AK	99652
ZEDLITZ JAY W & BETTY L	PO BOX 401	WILLOW	AK	99688
ZELAYA TR	5239 STRAWBERRY RD	ANCHORAGE	AK	99502
ZENTMIRE ROBT R	PO BOX 721	WILLOW	AK	99688
ZERBEL JACK A	PO BOX 520904	BIG LAKE	AK	99652
ZERNIA RAYMOND J JR & K A	PO BOX 100438	ANCHORAGE	AK	99510
ZIBRAT A DOUGLAS	PO BOX 62	PALMER	AK	99645
ZIETLOW THOS D& THERESA M	113 E COOK AVE	ANCHORAGE	AK	99501
ZIMMER CHAS & LAURIE	4808 BUCKINGHAM WAY	ANCHORAGE	AK	99503
ZIMMERMAN FRIEDRICH M & A	6329 E 35TH AVE	ANCHORAGE	AK	99504



ALASKA STATE OF	1213 R ST	ANCHORAGE	AK	99501
ALASKA STATE OF	12363 DIVISION ST	ANCHORAGE	AK	99515
ALASKA STATE OF	12911 ADMIRALTY PLACE	ANCHORAGE	AK	99515
ALASKA STATE OF	1310 KIRSTEN CIR	ANCHORAGE	AK	99518
ALASKA STATE OF	13350 SEACLOUD CIR	ANCHORAGE	AK	99516
ALASKA STATE OF	15301 LONGBOW DR	ANCHORAGE	AK	99616
ALASKA STATE OF	1556 COLUMBINE ST	ANCHORAGE	AK	99508
ALASKA STATE OF	1717 TALKETNA ST	ANCHORAGE	AK	99508
ALASKA STATE OF	1737 SCENIC WAY	ANCHORAGE	AK	99501
ALASKA STATE OF	1758 W 99TH AVE	ANCHORAGE	AK	99515
ALASKA STATE OF	182 OCEAN PARK DR	ANCHORAGE	AK	99515
ALASKA STATE OF	1850 PARKWAY DR	ANCHORAGE	AK	99504
ALASKA STATE OF	1904 W 46TH AVE	ANCHORAGE	AK	99517
ALASKA STATE OF	216 E MANOR AVE	ANCHORAGE	AK	99501
ALASKA STATE OF	2208 EUREKA ST	ANCHORAGE	AK	99503
ALASKA STATE OF	222 W 7TH AVE # 14	ANCHORAGE	AK	99513
ALASKA STATE OF	2321 MERRILL FIELD DR	ANCHORAGE	AK	99501
ALASKA STATE OF	2431 LEGACY DR	ANCHORAGE	AK	99516
ALASKA STATE OF	2440 E TUDOR RD	ANCHORAGE	AK	99507
ALASKA STATE OF	301 DAILEY AVE # 17	ANCHORAGE	AK	99515
ALASKA STATE OF	3117 PATTERSON ST	ANCHORAGE	AK	99504
ALASKA STATE OF	3307 BONIFACE PKWY	ANCHORAGE	AK	99504
ALASKA STATE OF	3321 W 32ND AVE	ANCHORAGE	AK	99517
ALASKA STATE OF	3331 CAPSTAN CT	ANCHORAGE	AK	99516
ALASKA STATE OF	3411 W 31ST AVE	ANCHORAGE	AK	99517
ALASKA STATE OF	3420 WILEY POST LOOP	ANCHORAGE	AK	99517
ALASKA STATE OF	3513 W 43RD AVE	ANCHORAGE	AK	99517
ALASKA STATE OF	3549 W NORTHERN LGTS BLVD	ANCHORAGE	AK	99517
ALASKA STATE OF	3601 C ST STE 960	ANCHORAGE	AK	99503
ALASKA STATE OF	3628 CARLTON AVE	ANCHORAGE	AK	99517
ALASKA STATE OF	3660 W 78TH AVE	ANCHORAGE	AK	99502
ALASKA STATE OF	440 MELLOW PLACE	ANCHORAGE	AK	99508
ALASKA STATE OF	450 DAILEY AVE	ANCHORAGE	AK	99515
ALASKA STATE OF	4802 MALIBU RD	ANCHORAGE	AK	99517
ALASKA STATE OF	4810 SOUTHPARK BLUFF DR	ANCHORAGE	AK	99516
ALASKA STATE OF	4902 CAMBRIDGE WAY	ANCHORAGE	AK	99503
ALASKA STATE OF	5049 CAPE SEVILLE DR	ANCHORAGE	AK	99516
ALASKA STATE OF	550 W 7TH AVE	ANCHORAGE	AK	99501
ALASKA STATE OF	550 WEST 7TH AVE STE 1380	ANCHORAGE	AK	99501
ALASKA STATE OF	6441 ASKELAND DR	ANCHORAGE	AK	99507
ALASKA STATE OF	6800 E 99TH AVE	ANCHORAGE	AK	99516
ALASKA STATE OF	6811 BAXTER TERRACE CIR	ANCHORAGE	AK	99504
ALASKA STATE OF	6954 FAIRWEATHER DR	ANCHORAGE	AK	99519



ALASKA STATE OF	7030 POTOMAC DR	ANCHORAGE	AK	99504
ALASKA STATE OF	718 L ST STE 202	ANCHORAGE	AK	99501
ALASKA STATE OF	7509 BLACKBERRY ST	ANCHORAGE	AK	99502
ALASKA STATE OF	825 W 53RD AVE	ANCHORAGE	AK	99518
ALASKA STATE OF	8461 LONGHORN ST	ANCHORAGE	AK	99507
ALASKA STATE OF	8500 HEATHER CIR	ANCHORAGE	AK	99502
ALASKA STATE OF	905 MULDOON RD	ANCHORAGE	AK	99504
ALASKA STATE OF	905 RICHARDSON VISTA RD	ANCHORAGE	AK	99501
ALASKA STATE OF	947 W 73RD	ANCHORAGE	AK	99518
ALASKA STATE OF	9801 SLALOM DR	ANCHORAGE	AK	99507
ALASKA STATE OF	PO BOX 110678	ANCHORAGE	AK	99511
ALASKA STATE OF	PO BOX 111362	ANCHORAGE	AK	99511
ALASKA STATE OF	PO BOX 112514	ANCHORAGE	AK	99511
ALASKA STATE OF	PO BOX 190973	ANCHORAGE	AK	99519
ALASKA STATE OF	PO BOX 196900	ANCHORAGE	AK	99519
ALASKA STATE OF	PO BOX 201761	ANCHORAGE	AK	99520
ALASKA STATE OF	PO BOX 211171	ANCHORAGE	AK	99521
ALASKA STATE OF	PO BOX 242221	ANCHORAGE	AK	99524
ALASKA STATE OF	PO BOX 520013	BIG LAKE	AK	99652
ALASKA STATE OF	PO BOX 520107	BIG LAKE	AK	99652
ALASKA STATE OF	PO BOX 520341	BIG LAKE	AK	99652
ALASKA STATE OF	PO BOX 520542	BIG LAKE	AK	99652
ALASKA STATE OF	PO BOX 520604	BIG LAKE	AK	99652
ALASKA STATE OF	PO BOX 521612	BIG LAKE	AK	99652
ALASKA STATE OF	PO BOX 521826	BIG LAKE	AK	99652
ALASKA STATE OF	20040 LORI ST	CHUGIAK	AK	99567
ALASKA STATE OF	20049 TENADA AVE	CHUGIAK	AK	99567
ALASKA STATE OF	25043 THUNDERBIRD DR	CHUGIAK	AK	99567
ALASKA STATE OF	PO BOX 670812	CHUGIAK	AK	99567
ALASKA STATE OF	PO BOX 671804	CHUGIAK	AK	99567
ALASKA STATE OF	12110 BUSINESS BLVD # 6	EAGLE RIVER	AK	99577
ALASKA STATE OF	18536 CULROSS CIR	EAGLE RIVER	AK	99577
ALASKA STATE OF	18636 CITATION RD	EAGLE RIVER	AK	99577
ALASKA STATE OF	19120 TALARIK DR	EAGLE RIVER	AK	99577
ALASKA STATE OF	19121 LAKINA CIR	EAGLE RIVER	AK	99577
ALASKA STATE OF	19274 MIDDLETON LOOP	EAGLE RIVER	AK	99577
ALASKA STATE OF	20215 RAVEN DR	EAGLE RIVER	AK	99577
ALASKA STATE OF	20230 RAVEN DR	EAGLE RIVER	AK	99577
ALASKA STATE OF	25116 EAGLE RIVER RD	EAGLE RIVER	AK	99577
ALASKA STATE OF	PO BOX 770949	EAGLE RIVER	AK	99577
ALASKA STATE OF	PO BOX 13087	TRAPPER CREEK	AK	99683
ALASKA TRUST DEEDS	1504 I ST	ANCHORAGE	AK	99501
ALASKA TRUST DEEDS	1712 SHIP AVE	ANCHORAGE	AK	99501

ALASKA TRUST DEEDS	3830 E 80TH AVE	ANCHORAGE	AK	99507
ALASKA UNIVERSITY OF	3877 UNIVERSITY DR	ANCHORAGE	AK	99508
ALASKA UNIVERSITY OF	3890 UNIVERSITY LAKE DR	ANCHORAGE	AK	99508
ALASKA USA FED CR UNION	PO BOX 196020	ANCHORAGE	AK	99519
ALBRYCE LLC	PO BOX 101466	ANCHORAGE	AK	99510
ALL NATIONS CHR ANC INC	5441 OLD SEWARD HWY	ANCHORAGE	AK	99518
ALMA CORP	12900 HILLSIDE DR	ANCHORAGE	AK	99516
ALYESKAFLATS LLC	6930 BAXTER TERRACE CIR	ANCHORAGE	AK	99504
AMERICAN VETERANS POST #2	3341 FAIRBANKS ST	ANCHORAGE	AK	99503
ANCHORAGE CHRYSLER DODGE	2601 E 5TH AVE	ANCHORAGE	AK	99501
ANCHORAGE NBRHD HSG SVCS	480 W TUDOR RD	ANCHORAGE	AK	99503
ANCHORAGE ROOFING & CONT	PO BOX 110217	ANCHORAGE	AK	99511
Anchorage Snowmobile Club	PO Box 232196	ANCHORAGE	AK	99523
Arctic Power	P.O. Box 100220	Anchorage	AK	99510
Arctic Power	P.O. Box 100220	Anchorage	AK	99510
ARTICORP	701 W 58TH AVE	ANCHORAGE	AK	99518
ARJER PROPERTIES LLC	8752 FLAMINGO DR	ANCHORAGE	AK	99502
ASHWILL REV TR	2212 CLIFF CT	ANCHORAGE	AK	99517
ASSOC OF VLG COUNCIL PRES	PO BOX 219	BETHEL	AK	99559
AURORA INC	2400 DOUGLAS DR	ANCHORAGE	AK	99517
AURORA REAL ESTATE LLC	1900 W NORTHERN LGTS BLVD	ANCHORAGE	AK	99517
B & J INVESTMENTS LLC	PO BOX 110226	ANCHORAGE	AK	99511
BADURA AK COMM PROP TR	5521 WHISPERS SPRUCE DR	ANCHORAGE	AK	99516
BAER ROBT & SONDRALV LG TR	3449 WENTWORTH ST	ANCHORAGE	AK	99508
BAGLEY FAMILY TR	6833 ELMRICH CT	ANCHORAGE	AK	99504
BAILEY NORMA ILEEN TR TRE	PO BOX 3028	ANDERSON	AK	99744
BAILEY-RUIZ P L LVG TR	PO BOX 771304	EAGLE RIVER	AK	99577
BAKIC ROBT S FAM TR TRE	2340 INNES CIR	ANCHORAGE	AK	99515
BALES IRENE F REV TR TRE	4859 WESLEYAN DR	ANCHORAGE	AK	99508
BALFE JANET L LVG TR UTA	3738 N POINT DR	ANCHORAGE	AK	99515
BALL FAMILY LTD PRTRNSHP	3001 DAWSON ST	ANCHORAGE	AK	99503
BAPTIST MID-MISSIONS	PO BOX 520803	BIG LAKE	AK	99652
BARBARICK DEANNA DEC TR	PO BOX 670841	CHUGIAK	AK	99567
BARBER ED & PAT JT REV TR	2060 BELMONT DR	ANCHORAGE	AK	99517
BATEY RONILEE DEC TR TRE	10821 CHAIN OF ROCK	EAGLE RIVER	AK	99577
BCE LLC	4231 MT VIEW DR	ANCHORAGE	AK	99508
BEAL TRUST	4001 LAUREL ST	ANCHORAGE	AK	99508
BEAR ALASKA LLC	1166 S KATIE CIR	ANCHORAGE	AK	99645
BEATTIE GLADYS LVG TR TRE	PO BOX 520225	BIG LAKE	AK	99652
BED LLC	PO BOX 521546	BIG LAKE	AK	99652
BEHR FAMILY 2001 TR	920 E 72ND AVE	ANCHORAGE	AK	99518
BELENSKI HARVEY F & A TRE	3630 BONIFACE PKWY	ANCHORAGE	AK	99504
BELL G W & ELLA S TR TRE	1013 TYONEK DR	ANCHORAGE	AK	99501

BENNETT A HOUTZ T TRE	1319 W 11TH AVE	ANCHORAGE	AK	99501
BERTRAND JEFF& D J LVG TR	11245 OUR RD	ANCHORAGE	AK	99516
BETHEL CHAPEL INC	PO BOX 140255	ANCHORAGE	AK	99514
BEUS BROOKE A SPEC NDS TR	PO BOX 196757	ANCHORAGE	AK	99519
BIBLE BAPTIST CHURCH	PO BOX 940107	HOUSTON	AK	99694
BIELAWSKI MARY K TR TRE	2137 ARCTIC CIR	ANCHORAGE	AK	99517
BIG LAKE CHAMBER OF COMMERCE	PO BOX 520067	BIG LAKE	AK	99653
Big Lake Community Council	PO BOX 520931	BIG LAKE	AK	99652
BIG LAKE LIONS INC	PO BOX 520048	BIG LAKE	AK	99652
BIG LAKE VENTURES LLC	PO BOX 521801	BIG LAKE	AK	99652
BIG LK LIBRARY ADVOCATES	PO BOX 520024	BIG LAKE	AK	99652
BIG RED INVESTMENTS LLC	12369 GREGG LN	ANCHORAGE	AK	99515
BLAHOUS EDW G REV TR	12312 GOLDEN EAGLE DR	EAGLE RIVER	AK	99577
BLANCHARD C A& J C LVG TR	9285 AUTUMN RIDGE CIR	ANCHORAGE	AK	99507
BOESENBERG KARL A TRE	320 OCEANVIEW	ANCHORAGE	AK	99515
BONOMO CARL C& B B TR TRE	PO BOX 582	WILLOW	AK	99688
BORLAND PATRICK& G REV TR	510 BOUNTY DR	ANCHORAGE	AK	99515
BOSELA AK COMM PROP TR	PO BOX 771443	EAGLE RIVER	AK	99577
BOYLE THOS CHAS TR TRE	3340 MT VERNON CT	ANCHORAGE	AK	99503
BRADY & CO	PO BOX 107502	ANCHORAGE	AK	99510
BRAMSTEDT ALVIN O JR TRE	1705 AMHERST CT	ANCHORAGE	AK	99508
BRANDYWINE LTD	PO BOX 1034	WILLOW	AK	99688
BRATTLUND CHESTER H&M EST	12573 W LOOKING GLASS DR	HOUSTON	AK	99694
BREEDEN FARM LLC	5821 E GERSHMELOO LOOP	PALMER	AK	99645
BREEDEN LAVERA F DEC TR	5780 E CAMBORNE DR	PALMER	AK	99645
BRODAHL FAM LTD PRTRNSHP	6510 LIMESTONE CIR	ANCHORAGE	AK	99507
BRONNER AK COMM PROP TR	13336 KONRAD DR	EAGLE RIVER	AK	99577
BRUNS J & H FAMILY TR	3214 PURDUE ST	ANCHORAGE	AK	99508
BRYANT HAROLD O& A TR TRE	7110 E 4TH AVE	ANCHORAGE	AK	99504
BRYANT L&S AK COMM PROPTR	822 OVERLOOK PL	ANCHORAGE	AK	99501
BUCY G E WILLIS FAMILY TR	3523 W 100TH	ANCHORAGE	AK	99515
BUMGARDNER ESTHER FAM TR	5400 E 172ND AVE	ANCHORAGE	AK	99516
BUMGARNER LAURIE L LVG TR	PO BOX 112411	ANCHORAGE	AK	99511
BURGER HOWARD S&JG REV TR	1611 HELEN DR	ANCHORAGE	AK	99515
BURGIN PEGGY A LIVING TR	5124 LAUREL ST	ANCHORAGE	AK	99507
BURNS HOLDINGS LLC	9135 KING ST	ANCHORAGE	AK	99515
BUTT ROBT B& JOANN B TRES	3007 SOUTH CIR	ANCHORAGE	AK	99507
BUTT SISTERS PROPERTIES	877 E DOWLING RD	ANCHORAGE	AK	99518
BUTTO FRANK & BERNICE TRE	3729 W NORTHERN LIGHTS	ANCHORAGE	AK	99517
C & L PROP LLC	7610 EVANDER DR	ANCHORAGE	AK	99518
C-4 PROPERTIES LLC	2820 COMMERCIAL DR	ANCHORAGE	AK	99501
CAMERON PROP GRP LLC	6820 BURLWOOD DR	ANCHORAGE	AK	99507
CAMILLI ROBT H & C A TRES	17418 KAHILTNA DR	EAGLE RIVER	AK	99577

CAPITOL DEVELOPMENT CO	PO BOX 113401	ANCHORAGE	AK	99511
CAREW ELIZABETH P TRE	PO BOX 141883	ANCHORAGE	AK	99514
CAREY HOMES INC	3317 MT VIEW DR	ANCHORAGE	AK	99501
Carter & Burgess, Inc.				
CARTHART-BUSSARD REV TR	1130 W. 6th Ave., Suite 100	ANCHORAGE	AK	99501
CARLESON CUSTOM HOMES	6229 TYRE CIR	ANCHORAGE	AK	99502
CARLSON DARRELL L TRE	508 W 2ND AVE	ANCHORAGE	AK	99501
CARPENTIER DAVID M TR TRE	7311 SETTER DR	ANCHORAGE	AK	99502
CARPENTIER DAVID M TR TRE	PO BOX 102360	ANCHORAGE	AK	99510
CARR FAMILY TR	PO BOX 2360	ANCHORAGE	AK	99510
CARR HEIDI A TRE	4905 DEARMOUN RD	ANCHORAGE	AK	99516
CARTWRIGHT REV TR	6484 VILLAGE PKWY	ANCHORAGE	AK	99504
CARVALHO DAN'L A TRE	11971 TULWAR DR	CHUGIAK	AK	99567
CASA DEL DUENDA INVS LLC	11720 RAINBOW AVE	ANCHORAGE	AK	99516
CASH ALASKA	PO BOX 110575	ANCHORAGE	AK	99511
CASH ALASKA	2911 SPENARD RD	ANCHORAGE	AK	99503
CASTRO 2006 REV TR	2911 SPENARD RD	ANCHORAGE	AK	99503
CASWELL NATIVE ASSN INC	4007 CHANDALAR DR	ANCHORAGE	AK	99504
CBMC LTD	SRA BOX 1411	ANCHORAGE	AK	99515
CHARMLEY MYRA V LVG TRTRE	PO BOX 101333	ANCHORAGE	AK	99510
CHICKALOON-MOOSE CREEK NATIVE ASSOCI/P.O. BOX 1105	6731 DICKERSON DR	ANCHORAGE	AK	99504
CHUGACH ELECTRIC ASSN INC	PO BOX 196300	CHICKALOON	AK	99674
CHURCH OF CHRIST	2700 DEBARR AVE	ANCHORAGE	AK	99519
City of Houston	P O BOX 940027	HOUSTON	AK	99508
City of Houston	P O BOX 940027	HOUSTON	AK	99694
CITY OF WASILLA	PO BOX 672049	CHUGIAK	AK	99567
CITY OFHOUSTON	PO BOX 940027	HOUSTON	AK	99694
CNR HOLDINGS LLC	9651 COPPER DR	ANCHORAGE	AK	99507
COASTAL PEAK CONST INC	PO BOX 201148	ANCHORAGE	AK	99520
COLASKA INC	240 W 68TH AVE	ANCHORAGE	AK	99518
COLBORN BEULAH TRE	PO BOX 520408	ANCHORAGE	AK	99518
COLLINS RICHARD B & A TRE	2553 LOVEJOY DR	BIG LAKE	AK	99652
COLLOPY ROBT & GRACE TRE	HC 89 BOX 414	ANCHORAGE	AK	99508
COLSON MAYANNE L TRE	4018 HAMPTON DR	WILLOW	AK	99688
COLYER ELVA S LVG TR	PO BOX 92611	ANCHORAGE	AK	99504
COMINS H DAVID TRE	2053 CLIFFSIDE DR	ANCHORAGE	AK	99509
COMISKEY ALBERT& JEAN TRE	1649 BIRCHWOOD ST	ANCHORAGE	AK	99501
CONOCO PHILLIPS AK INC	PO BOX 100360	ANCHORAGE	AK	99508
CONWAY JOHN M DEC TR TRE	2335 HIALEAH DR	ANCHORAGE	AK	99510
COOK INLET PROPERTIES LLC	6514 GREENWOOD ST	ANCHORAGE	AK	99517
COOK INLET REGION INC	PO BOX 93330	ANCHORAGE	AK	99518
COOLEY STELLA V TRE	2231 LORD BARANOF DR	ANCHORAGE	AK	99509
		ANCHORAGE	AK	99517

CORTIS DONALD E JR & L C	7901 LADASA PL	ANCHORAGE	AK	99507
CORZAN PROP LTD PRTNRSHP	3933 SPENARD RD	ANCHORAGE	AK	99517
COULTER FAMILY TRUST 2001	7220 E CHESTER HGTS CIR	ANCHORAGE	AK	99504
CRAGEN AK COM PROP TR	4210 WARWICK DR	ANCHORAGE	AK	99508
CRAIG TAYLOR EQUIP CO INC	733 WHITNEY RD	ANCHORAGE	AK	99501
CRAWFORD FAMILY REV TR	8640 KUSHTAKA CIR	ANCHORAGE	AK	99504
CRISWELL SALLY SURV TR	PO BOX 141656	ANCHORAGE	AK	99514
CRL SERVICES LLC	PO BOX 230788	ANCHORAGE	AK	99523
CROWN AFFAIR LLC	PO BOX 93621	ANCHORAGE	AK	99509
CULBERT FAMILY 2004 TR	18926 INSPIRATION CIR	EAGLE RIVER	AK	99577
CUMBERLEDGE RUTHIE LVG TR	PO BOX 100914	ANCHORAGE	AK	99510
CUSACK MICHAEL L TRE	12168 E SCOTT RD	PALMER	AK	99645
D F INV	500 E SHIP CREEK AVE	ANCHORAGE	AK	99501
D H PLUMBING & HEATING INC	7741 KING ST	ANCHORAGE	AK	99518
DALTON CLAIR&ANITA REV TR	8442 JUPITER DR	ANCHORAGE	AK	99507
DAN MAE II REV TRUST	2287 SORBUS WAY	ANCHORAGE	AK	99508
DASH DELORIS V DEC TR	PO BOX 93046	ANCHORAGE	AK	99509
DAVIDSON'S INV	3311 WILEY POST LOOP	ANCHORAGE	AK	99517
DAVIS HANNAH F TRE	4812 WESLEYAN DR	ANCHORAGE	AK	99508
DAVIS ROY & BETTY REV TR	6110 COUNTRY LANE CIR	ANCHORAGE	AK	99504
DEEGAN DONNA M CREDIT TR	1531 W 14TH AVE	ANCHORAGE	AK	99501
DENALI FOODS INC	3301 DENALI ST	ANCHORAGE	AK	99503
DESKA LNDG OUTDOOR ASSN	PO BOX 155	WILLOW	AK	99688
DESPAIN LIVING TRUST	12600 FURROW CREEK RD	ANCHORAGE	AK	99516
DINKINS ELTON P TRE	PO BOX 770576	EAGLE RIVER	AK	99577
Directorate of Public Works	ATTN: APVR-RPW-GIM-RP #6500	FT Richardson	AK	99505-65
DISCOVERY CONST INC	PO BOX 111411	ANCHORAGE	AK	99511
DISOTELL GROUP IMC	PO BOX 770469	EAGLE RIVER	AK	99577
DITTRICH FAM LTD PRTNRSHP	2567 LOUSSAC DR	ANCHORAGE	AK	99517
DOBSON ROBT V CO-TRE	PO BOX 771144	EAGLE RIVER	AK	99577
DOLIN CHARLEENE M MAY EST	3501 E 65TH AVE	ANCHORAGE	AK	99507
DOODAD INN LLC	3301 PRINCETON WAY	ANCHORAGE	AK	99508
DRUM DOUGLAS A DEC TR TRE	HC 52 BOX 8872	INDIAN	AK	99540
DUNBAR BERNARD L TRE	5340 COUNTRY CLUB LN	ANCHORAGE	AK	99516
DWIGGINS D W & J C LVG TR	1401 W 34TH AVE	ANCHORAGE	AK	99503
E & R LTD PRTNRSHP	1500 W 33RD AVE	ANCHORAGE	AK	99503
EDWARDS BERNIE C TRE	PO BOX 670574	CHUGIAK	AK	99567
ELLENBURG THIRD FAM LTD	3001 LAKESIDE DR	ANCHORAGE	AK	99515
EMBLEY FAM LTD PRTNRSHP	2317 RASPBERRY RD	ANCHORAGE	AK	99502
EMMEL JAS N& KAREN REV TR	1300 W 7TH AVE	ANCHORAGE	AK	99501
ENDRES INVESTMENTS LLC	PO BOX 230036	ANCHORAGE	AK	99523
Environmental and Natural Resources Office, Direc	ATTN: APVR-RPW-GE #6500	FT Richardson	AK	99505-65
EPISCOPAL DIOCESE AK INC	1200 I ST	ANCHORAGE	AK	99501

EQUITY TR CO CUST FBO	6241 CORNER TREE DR	ANCHORAGE	AK	99507
EQUIVEST MTG INCOME TR	5313 ARCTIC BLVD	ANCHORAGE	AK	99518
ERIKSON HOMESTEAD OWNERS	PO BOX 1608	PALMER	AK	99645
ERNEST DEBRA S REV LVG TR	3131 KENWOOD CIR	ANCHORAGE	AK	99504
ESS MATTHEW & LINDA LVG TR	5943 GREECE DR	ANCHORAGE	AK	99516
EWERS LINDA D LVG TR	13201 RIDGEWOOD CIR	ANCHORAGE	AK	99516
FABER ALASKA COMM PROP TR	PO BOX 201147	ANCHORAGE	AK	99520
FAIKS ALASKA COMM PROP TR	19559 W BRYANT RD	BIG LAKE	AK	99652
FAIRVIEW PARK INV LTD	PO BOX 92225	ANCHORAGE	AK	99509
Fairview Park Inv Ltd	PO Box 92225	Anchorage	AK	99509
FALCONER & LENTFER PRTNR	3421 KACHEMAK CIR	ANCHORAGE	AK	99515
FALCON'S RIDGE LLC	3940 ARCTIC BLVD	ANCHORAGE	AK	99503
FEJES CHRISTOPHER & M TR	15700 ROBIN HOOD DR	ANCHORAGE	AK	99516
FEJES JOHN A TRE	4000 DE ARMOUN RD	ANCHORAGE	AK	99516
FELTON IVAN W LLC	PO BOX 101559	ANCHORAGE	AK	99510
FENN BETTE J LVG TR	2101 FOREST PARK DR	ANCHORAGE	AK	99517
FERUCCI PATRICIA G TRE	3910 GENEVA PL	ANCHORAGE	AK	99508
FESSLER LOUIS E DEC TR	3100 CHESAPEAKE CIR	ANCHORAGE	AK	99516
FIKES NEIL E EST	5741 DENALI ST	ANCHORAGE	AK	99518
FINANCIAL SERVICES INC	2509 FAIRBANKS ST	ANCHORAGE	AK	99503
FINANCIAL SERVICES INC	650 W INT'L AIRPORT RD	ANCHORAGE	AK	99518
FINELINE BUILDERS INC	4861 S PARK BLUFF DR	ANCHORAGE	AK	99501
FIRST ASSEMBLY OF GOD CH	1540 C ST	ANCHORAGE	AK	99501
FIRST BAPTIST CHR WILLOW	PO BOX 145	WILLOW	AK	99688
FIRST NAT'L BANK AK TRE	PO BOX 100720	ANCHORAGE	AK	99510
FIRST REGIONAL BANK FBO	3201 C ST	ANCHORAGE	AK	99503
FISHER FAMILY LTD PRTRNSP	PO BOX 521470	BIG LAKE	AK	99652
FISHER FAMILY LTD PRTRNSP	PO BOX 521924	BIG LAKE	AK	99652
FISON DAVID K&D ALEEN TRE	6800 OMALLEY RD	ANCHORAGE	AK	99516
FLOWERS VERNON E TRE	3107 DANE CT	ANCHORAGE	AK	99507
FOLAND GARY D TR AGRMT	5041 SETON CIR	ANCHORAGE	AK	99508
FOLSOM REV TR	PO BOX 4861	PALMER	AK	99645
FOSTER MICHAEL L PROP LLC	13135 OLD GLENN HWY	EAGLE RIVER	AK	99577
FOSTERS FINE FINISHES	PO BOX 3511	PALMER	AK	99645
FOUTS T & V FAMILY TR	PO BOX 520142	BIG LAKE	AK	99652
FOWLER & BUETTNER LVG TR	PO BOX 211115	AUKE BAY	AK	99821
FRANK EDW G JR CUST FOR	3901 DOROSHIN AVE	ANCHORAGE	AK	99516
FREMONT INVESTMENT & LOAN	PO BOX 244922	ANCHORAGE	AK	99524
FRIESE LEE A & B LVG TR	3324 ROBIN ST	ANCHORAGE	AK	99504
FULLER FAMILY TR	4314 MACALISTER DR	ANCHORAGE	AK	99515
FULLER QUALITY INV I LLC	3801 CENTERPOINT DR #400	ANCHORAGE	AK	99503
GALLANT RAY & FREDA LVG TR	2915 COLUMBIA ST	ANCHORAGE	AK	99508
GARDINO MICHAEL & P LVG TR	21745 CHANDELLE CIR	CHUGIAK	AK	99567

GARRETT LEE S DEC TR	2400 HASTINGS LN	ANCHORAGE	AK	99504
GASTALDI JEFFERY & L R TRE	4726 W 88TH AVE	ANCHORAGE	AK	99502
GATES HERBERT J REV TR	3051 WENTWORTH ST	ANCHORAGE	AK	99508
Gateway Community Council	PO BOX 586	PALMER	AK	99645
GATOR BROTHERS LLC	6400 GUNNISON DR	ANCHORAGE	AK	99516
GEITZ MICHAEL J & P A TRES	3120 SEAPORT CIR	ANCHORAGE	AK	99515
GENESIS HOMES & DEV LLC	PO BOX 520937	BIG LAKE	AK	99652
GERAGHTY FMLY LTD PRTN SHP	943 W 6TH AVE	ANCHORAGE	AK	99502
GLEASON REV TR	5201 E 100TH AVE	ANCHORAGE	AK	99507
GOCHANOUR & HUNT FAM TR	PO BOX 521842	BIG LAKE	AK	99652
Golden Creek - Susitna Native Association	BOX 847	TALKEETNA	AK	99676
Golden Creek - Susitna Native Association	BOX 847	TALKEETNA	AK	99676
GOOCEY FAMILY 2004 TR	PO BOX 670242	CHUGIAK	AK	99567
GOODMAN N ROY & R A LVG TR	PO BOX 2883	PALMER	AK	99645
GOOSE BAY LTD	603 W TUDOR RD	ANCHORAGE	AK	99503
GOSNELL DWIGHT G TR	7300 E DENELLE ST	PALMER	AK	99645
GOTTSTEIN JAS B REV TR TRE	406 G ST	ANCHORAGE	AK	99501
GRACE COMM CHURCH INC	6689 SEAFOOD DR	ANCHORAGE	AK	99518
GRACE INVESTMENTS LLC	PO BOX 4356	PALMER	AK	99645
GRAHAM REV TR	PO BOX 140130	ANCHORAGE	AK	99514
GRAMDMA'S CABIN LLC	PO BOX 111186	ANCHORAGE	AK	99511
GRAVELLE TAMARA S LVG TR	2610 JUNEAU ST	ANCHORAGE	AK	99508
GREAT LAND INV CO LLC	PO BOX 190028	ANCHORAGE	AK	99519
GREAT PALMER CHAMBER OF COMMERCE	PO BOX 45	PALMER	AK	99645
GREATER WASILLA CHAMBER OF COMMERCE	415 E. RAILROAD AVENUE	WASILLA	AK	99654
GREIFF JOHN W LVG TR	2028 E 36TH AVE	ANCHORAGE	AK	99508
GRENIER D & S FAM TR TRE	PO BOX 110890	ANCHORAGE	AK	99511
GROVER GARTH R TRE	PO BOX 854	PALMER	AK	99645
GRUBBA DOLORES M TR TRE	2201 CORNELL CT	ANCHORAGE	AK	99540
GRUNDMAN AK COMM PROP TR	16910 BETTJEAN ST	ANCHORAGE	AK	99516
GRYTE IDA M 1999 TR TRE	1511 L ST	ANCHORAGE	AK	99501
GSC LLC	PO BOX 1605	PALMER	AK	99645
GSC LLC	PO BOX 3214	PALMER	AK	99645
GUNTER EMERY & L JNT REV TR	6801 LOUISE CT	ANCHORAGE	AK	99507
H & H INVESTMENTS LLC	PO BOX 28	PALMER	AK	99645
H & R INVESTMENTS INC	PO BOX 461	WILLOW	AK	99688
HA YOUNG H LVG TR	8445 SAHALEE DR	ANCHORAGE	AK	99507
HAGEN INVESTMENTS LLC	PO BOX 240186	ANCHORAGE	AK	99524
HALLIWILL J & S S LVG TR	PO BOX 110571	ANCHORAGE	AK	99511
HAMILTON R & M FAMILY TR	5557 YUKON CHARLIE LOOP	ANCHORAGE	AK	99502
HANNAMAN JUDY F TR TRE	PO BOX 528	WILLOW	AK	99688
HANSEN ROGER K REV TR	PO BOX 520343	BIG LAKE	AK	99652
HANSMEIER J & D JT REV TR	3410 PRINCETON WAY	ANCHORAGE	AK	99508

HANSON DIANE MILLER TRE	2023 SHEPHERDIA DR	ANCHORAGE	AK	99508
HANSON HANS S TR TRE	PO BOX 91516	ANCHORAGE	AK	99509
HANSON HEDRIC BAUR REV TR	3260 PROVIDENCE DR	ANCHORAGE	AK	99508
HANSON REV TR	6800 QUEENS VIEW CIR	ANCHORAGE	AK	99504
HARRIS JACK R DEC OF TR	PO BOX 190465	ANCHORAGE	AK	99519
HARRISON HERBERT S CO-TRE	HC 89 BOX 326	WILLOW	AK	99688
HART DAVID A REV TR TRE	9400 MAIN TREE DR	ANCHORAGE	AK	99507
HARTIG INVESTMENTS LLC	9330 VANGUARD DR	ANCHORAGE	AK	99507
HARTLEY LEE W REV TR TRE	PO BOX 800	PALMER	AK	99645
HARTMAN SHIRLEY J TR TRE	4028 CARAVELLE DR	ANCHORAGE	AK	99502
HEARN JEFF REV TR	PO BOX 201969	ANCHORAGE	AK	99520
HEARTLAND HOMES INC	5403 N SANDALWOOD LN	PALMER	AK	99645
HEILALA MATT & SLVG TR	5805 PROMINENCE POINT DR	ANCHORAGE	AK	99516
HELEN NIENHUESER	2561 LOVEJOY DR.	ANCHORAGE	AK	99508
HELLRUNG CUSTOM HOMES INC	1801 REBEL RIDGE DR	ANCHORAGE	AK	99504
HENDRICKS BEVERLY TRE	2700 SCARBOROUGH DR	ANCHORAGE	AK	99504
HENRI JOS & WILES B J TRE	6653 AIR GUARD RD	ANCHORAGE	AK	99502
HIDDEN ASSETS LLC	2708 HAVITUR WAY	ANCHORAGE	AK	99504
HIEBERT WILLARD DEC TR	7101 E LOWER MESA DR	PALMER	AK	99645
HIGGINS FAM LVNG TR	PO BOX 111463	ANCHORAGE	AK	99511
HIGHLANDER INV LLC	2601 BLUEBERRY RD	ANCHORAGE	AK	99503
HILAND RETIREMENT TRUST	3821 BALCHEN DR	ANCHORAGE	AK	99517
HILL FLORESSNA B LVG TR	PO BOX 230415	ANCHORAGE	AK	99523
HIMMELWRIGHT/PERE LVG TR	2439 LA HONDA DR	ANCHORAGE	AK	99517
HMD PROP	PO BOX 2984	PALMER	AK	99645
HODGSON COLLEEN A DEC TR	9806 ST LAWRENCE CIR	EAGLE RIVER	AK	99577
HOLLINGER LVG TR	PO BOX 520495	BIG LAKE	AK	99652
HOLY TRANSFIGURATION CHUR	2800 O'MALLEY RD	ANCHORAGE	AK	99516
HOMESTEADERS COMM CENTER	PO BOX 940277	HOUSTON	AK	99694
HOMESTEADERS COMM CENTER	PO BOX 940323	HOUSTON	AK	99694
HOOPER MAY BELLE A REV TR	609 W 42ND AVE	ANCHORAGE	AK	99503
HOOPYMAN LVG THE TR	2421 BENTZEN CIR	ANCHORAGE	AK	99517
HOPE COMM RESOURCES INC	540 W INTL AIRPORT RD	ANCHORAGE	AK	99518
HOPPER 2006 FAM FAITH TR	7161 TALL SPRUCE DR	ANCHORAGE	AK	99502
Houston Chamber of Commerce	P.O. Box 356	Houston	AK	99694
Houston Chamber of Commerce	P.O. Box 356	Houston	AK	99694
HOUSTON CHAMBER OF COMMERCE	PO BOX 940356	HOUSTON	AK	99694
HOUSTON CHAMBER OF COMMERCE	PO BOX 940356	HOUSTON	AK	99694
HOUSTON CHURCH OF CHRIST	PO BOX 940193	HOUSTON	AK	99694
HOUSTON LODGE INC	PO BOX 940047	HOUSTON	AK	99694
HOWARD JAS & ANNIE LVG TR	3220 AMBER BAY CIR	ANCHORAGE	AK	99515
HRONKIN PROPERTIES LLC	PO BOX 2821	PALMER	AK	99645
HUGHES JOHN C REV TR	PO BOX 230553	ANCHORAGE	AK	99523



HUGHTON STELLA G TR	602 W 10TH AVE	ANCHORAGE	AK	99501
HULCE LYNN A & V G TR TRE	6530 W DIMOND BLVD	ANCHORAGE	AK	99502
HUSTON CLIFF & ALLIE 2003	2071 FOREST PARK DR	ANCHORAGE	AK	99517
HUTTON JAS H TR TRE	PO BOX 231202	ANCHORAGE	AK	99523
Iditarod Trail Committee, Inc.	PO BOX 870800	WASILLA	AK	99687
INDEPENDENT BAPTIST CHR	PO BOX 52803	BIG LAKE	AK	99652
INGRIM INVESTMENTS INC	PO BOX 90355	ANCHORAGE	AK	99509
INV BROKERS LAZY LK LTD	1345 W 9TH AVE	ANCHORAGE	AK	99501
J & B INVESTMENTS LLC	2921 PELICAN DR	ANCHORAGE	AK	99502
J A SPAIN & SONS INC	PO BOX 1069	WILLOW	AK	99688
J.C Hoates/S. Dudek	P.O. Box 632	Talkeetna	AK	99676
J.C Hoates/S. Dudek	P.O. Box 632	Talkeetna	AK	99676
JACQUES STUART M REV TR	311 N SITKA ST	ANCHORAGE	AK	99501
JANIS D T IRREV TR TRE	5655 GATE KEEPER AVE	ANCHORAGE	AK	99504
JANSSEN CONTRACTING INC	1520 POST RD	ANCHORAGE	AK	99501
JAYNES FAMILY LVG TR	2515 ASPEN DR	ANCHORAGE	AK	99517
JEFF & PJ LVG TR	PO BOX 521703	BIG LAKE	AK	99652
JEFFORD M L DEC OF TR TRE	1435 L ST	ANCHORAGE	AK	99501
JEWELL C & B LVG TR	11800 HUMBLE CT	ANCHORAGE	AK	99515
JMD GROUP	7080 CROOKED TREE DR	ANCHORAGE	AK	99516
JOHMOR CO	PO BOX 520639	BIG LAKE	AK	99652
JOHNSON C& H JOINT REV TR	1345 K ST	ANCHORAGE	AK	99501
JOHNSON MARGARET L TR TRE	3729 W 61ST	ANCHORAGE	AK	99502
JOHNSON TIRE SVC INC	3330 DENALI ST	ANCHORAGE	AK	99503
JORDET WILLARD B TR TRE	1605 W 12TH # 40	ANCHORAGE	AK	99501
JSK LLC	PO BOX 520703	BIG LAKE	AK	99652
JULIEN KATIE C DDS MS PSP	16440 SOUTHCLIFF CIR	ANCHORAGE	AK	99516
K AKA INC.	7030 MCLIN CIR	ANCHORAGE	AK	99507
K2 BUILDERS INC	8908 E MINT CIR	PALMER	AK	99645
KALAMARIDES E J DEC TR	711 H ST # 450	ANCHORAGE	AK	99501
KANUSE LLC	PO BOX 220708	ANCHORAGE	AK	99522
KAPPER LVG TR	1103 BROADDUS ST	ANCHORAGE	AK	99515
KASHWITNA COMMUNITY CHRCH	PO BOX 1123	WILLOW	AK	99688
KAYANN WILLOW CO	620 MULDOON RD	ANCHORAGE	AK	99504
KAYANN WILLOW CO	6424 BARCLAY CT	ANCHORAGE	AK	99504
KAYANN WILLOW CO	7828 OLD SEWARD HWY	ANCHORAGE	AK	99518
KAYANN WILLOW CO	PO BOX 101558	ANCHORAGE	AK	99510
KAYANN WILLOW CO	PO BOX 940392	HOUSTON	AK	99694
KEEFE CARYLE E DEC OF TR	PO BOX 940016	HOUSTON	AK	99694
KELLEY SCHIFF LVG TR	12000 TOY DR	ANCHORAGE	AK	99515
KENDALL EDITH H TR	2800 MCCOLLIE AVE	ANCHORAGE	AK	99517
KERN EDW D TRE	PO BOX 416	PALMER	AK	99645
KEY BANK OF ALASKA	PO BOX 100420	ANCHORAGE	AK	99510

KEYSTONE LTD PARTNERSHIP	1921 S SALEM DR	ANCHORAGE	AK	99508
KINN FAMILY PERPETUAL TR	9900 HILLHAVEN CIR	ANCHORAGE	AK	99507
KLH/	4255 S MCKECHNIE LOOP	PALMER	AK	99645
KLONDIKE LTD	3010 DORIS ST	ANCHORAGE	AK	99517
KLOUDA PATRICIA F TR	2061 FOREST PARK DR	ANCHORAGE	AK	99517
KLS FINE HOMES LLC	3000 N SEAGULL DR	PALMER	AK	99645
KNAPP ALICE E TR TRE	2071 CAMPBELL PL	ANCHORAGE	AK	99507
KND INVESTMENTS LTD	20441 PTARMIGAN BLVD	EAGLE RIVER	AK	99577
KNECHT REVOCABLE	3206 LOIS DR	ANCHORAGE	AK	99517
KNIK DEVELOPMENT GRP LLC	PO BOX 110535	ANCHORAGE	AK	99511
KNIK-FAIRVIEW COMM COUNCIL	PO BOX 877291	ANCHORAGE	AK	99687
KOORENNY ELEANOR E TR TRE	7411 PAPA CIR	WASILLA	AK	99518
KOZLER CHAS W TR TRE	1410 INLET PL	ANCHORAGE	AK	99501
KROCHINA PAT & JAN LVG TR	3501 DENALI ST #303	ANCHORAGE	AK	99503
KUSTER LNDG OWNERS ASSOC	PO BOX 521529	ANCHORAGE	AK	99503
KUTIL MILTON E&GLADYS TRE	2912 WENTWORTH ST	BIG LAKE	AK	99652
LACY MARK A CO-TRE	15700 WINDSONG DR	ANCHORAGE	AK	99508
LAKE LUCILLE CONDOMINIUMS	3351 ARCTIC BLVD	ANCHORAGE	AK	99516
LAMBERT VERNELL J REV TR	18540 ROADS END CIR	ANCHORAGE	AK	99503
LAND TRUST 1150	1500 W 33RD STE 100	EAGLE RIVER	AK	99577
LAND TRUST 26A7	6701 PICKWICK PL	ANCHORAGE	AK	99503
LAND TRUST LST 83	PO BOX 1821	PALMER	AK	99504
LANG ELLA M TR TRE	324 N KLEVIN ST	ANCHORAGE	AK	99645
LANGFIELD FAMILY TR	3246 SHERRIE ST	ANCHORAGE	AK	99508
LANIER ANNE P REV TR	2611 SHEPHERDIA DR	ANCHORAGE	AK	99504
LANKFORD ELIZABETH E TR	3365 MONTICELLO CT	ANCHORAGE	AK	99508
LAPPI LINDA O TRE	10616 WASHINGTON CIR	ANCHORAGE	AK	99503
LASHBROOK DEV INC	1871 N DRIFTWOOD CIR	ANCHORAGE	AK	99515
LDB DEC TR	1710 SCENIC WAY	PALMER	AK	99645
LEE HELEN A LVG TR	8340 E 11TH CT	ANCHORAGE	AK	99501
LET LLC	PO BOX 110409	ANCHORAGE	AK	99504
LEWIS SHIRLEY T TRE	2073 ARLINGTON DR	ANCHORAGE	AK	99511
LIBERTY BUILDERS INC	PO BOX 111431	ANCHORAGE	AK	99517
LIETZAU REV TR	20508 MARK CIR	ANCHORAGE	AK	99511
LIGHT MORTON L TRE TR	PO BOX 940382	CHUGIAK	AK	99567
LINCOLN VLG AIRPARK	PO BOX 521131	HOUSTON	AK	99694
LINDSAY N CAROL TRE	2976 MADISON WAY	BIG LAKE	AK	99652
LIVESAY FAMILY REV TR	PO BOX 520725	ANCHORAGE	AK	99508
LLP PRTNRSHIP	PO BOX 932	BIG LAKE	AK	99652
LOCAL SPIRITUAL ASSEMBLY	PO BOX 100004	WILLOW	AK	99688
LOCHMANN WM DEC TR	10900 CORRIE WAY	ANCHORAGE	AK	99510
LOERA KATHY A LVG TR TRE	8090 KRONOS DR	EAGLE RIVER	AK	99577
LONG WM E REV TR	PO BOX 1831	ANCHORAGE	AK	99502
		PALMER	AK	99645

LOON LAKE LLC	PO BOX 241544	ANCHORAGE	AK	99524
LOPETRONE FAMILY TR	7805 UPPER HUFFMAN RD	ANCHORAGE	AK	99516
LORATO JOINT VENTURE	7405 ROVENNA ST	ANCHORAGE	AK	99518
LORD INVESTMENTS LLC	PO BOX 240922	ANCHORAGE	AK	99524
LORENZ JOINT REV TR	3227 DICKSON DR	ANCHORAGE	AK	99504
LOTS LLC	7720 LODGEPOLE CT	ANCHORAGE	AK	99507
LOUISE DIANE DEC TR	PO BOX 190489	ANCHORAGE	AK	99519
LOWE ROBT C THE REV TR	1933 BEAVER PL	ANCHORAGE	AK	99504
LOWE SHIRLEY M DEC TR TRE	PO BOX 671877	CHUGIAK	AK	99567
LRA PROPERTIES LLC	7961 ALATNA AVE	ANCHORAGE	AK	99507
LSO LLC	220 BOTANICAL CIR	ANCHORAGE	AK	99515
LYONS 2001 TR/LYONS J TRE	7613 DOVER AVE	ANCHORAGE	AK	99504
MAAKESTAD JOHN L TRE	4304 NEEDLE CIR	ANCHORAGE	AK	99508
MACY BARBARA K LVG TR	PO BOX 221766	ANCHORAGE	AK	99522
MADSEN A C JR & SL CO-TRES	PO BOX 636	WILLOW	AK	99688
MAHONEY JEAN R TR TRE	4324 MACALISTER DR	ANCHORAGE	AK	99502
MAINES RAYMOND L & E J TRE	1721 DIOMEDE ST	ANCHORAGE	AK	99504
MANS WM & PENELOPE LVG TR	8620 PIONEER DR	ANCHORAGE	AK	99504
MARSH CHAS F REV TR TRE	PO BOX 2750	PALMER	AK	99645
MARTIN ERMA A LVG TR	PO BOX 110334	ANCHORAGE	AK	99511
MARTIN ROBERT J CO-TRE	PO BOX 212872	ANCHORAGE	AK	99521
MATANUSKA ELECTRIC ASSN	PO BOX 2929	PALMER	AK	99645
MATANUSKA TELEPHONE ASSN	PO BOX 3550	PALMER	AK	99645
MATANUSKA-SUSITNA BOROUGH	1326 K ST	ANCHORAGE	AK	99501
MATANUSKA-SUSITNA BOROUGH	2101 STANFORD DR	ANCHORAGE	AK	99508
MATANUSKA-SUSITNA BOROUGH	2312 LINCOLN AVE	ANCHORAGE	AK	99517
MATANUSKA-SUSITNA BOROUGH	2524 W 66TH AVE	ANCHORAGE	AK	99502
MATANUSKA-SUSITNA BOROUGH	2910 RUBY DR	ANCHORAGE	AK	99502
MATANUSKA-SUSITNA BOROUGH	4300 SOUTHPARK BLUFF DR	ANCHORAGE	AK	99516
MATANUSKA-SUSITNA BOROUGH	8834 B JEWEL TERRACE ST	ANCHORAGE	AK	99502
MATANUSKA-SUSITNA BOROUGH	920 W 6TH AVE	ANCHORAGE	AK	99501
MATANUSKA-SUSITNA BOROUGH	PO BOX 112149	ANCHORAGE	AK	99511
MATANUSKA-SUSITNA BOROUGH	PO BOX 233406	ANCHORAGE	AK	99523
MATANUSKA-SUSITNA BOROUGH	PO BOX 90921	ANCHORAGE	AK	99509
MATANUSKA-SUSITNA BOROUGH	PO BOX 520657	BIG LAKE	AK	99652
MATANUSKA-SUSITNA BOROUGH	PO BOX 520684	BIG LAKE	AK	99652
MATANUSKA-SUSITNA BOROUGH	PO BOX 670250	CHUGIAK	AK	99567
MATANUSKA-SUSITNA BOROUGH	PO BOX 670925	CHUGIAK	AK	99567
MATANUSKA-SUSITNA BOROUGH	PO BOX 770149	EAGLE RIVER	AK	99577
MATANUSKA-SUSITNA BOROUGH	PO BOX 940064	HOUSTON	AK	99694
MATANUSKA-SUSITNA BOROUGH	PO BOX 940481	HOUSTON	AK	99694
MATANUSKA-SUSITNA BOROUGH	350 E DAHLIA AVE	PALMER	AK	99645
MATANUSKA-SUSITNA BOROUGH	HC 89 BOX 251	WILLOW	AK	99688

MATANUSKA-SUSITNA BOROUGH	PO BOX 644	WILLOW	AK	99688
MCAFFEE JAS D TR TRE	3750 E HUFFMAN RD	ANCHORAGE	AK	99516
MC FALL LEROY & J TR TRE	6433 IMLACH DR	ANCHORAGE	AK	99502
MC GHAN CONST CO INC	PO BOX 520356	BIG LAKE	AK	99652
MCINTYRE ENT INC	19978 TULWAR DR	CHUGIAK	AK	99567
MCKAY FAMILY LTD PRTRNSHP	3412 STANFORD DR	ANCHORAGE	AK	99508
MCNAMARA MICHAEL&J LVG TR	3930 N POINT DR	ANCHORAGE	AK	99502
MDM GENERAL CONT LLC	7640 KING ST	ANCHORAGE	AK	99518
MEADOW LAKES COMM COUNCIL	P O BOX 878666	WASILLA	AK	99687
MECHLING POLLY J TR TRE	4905 BRYN MAWR CT	ANCHORAGE	AK	99508
MEL-ANNIE-K LLC	PO BOX 520135	BIG LAKE	AK	99652
MELLIN-BRAMSTEDT R LK TR	2900 CHESAPEAKE AVE	ANCHORAGE	AK	99516
MERIT HOMES LLC	PO BOX 111982	ANCHORAGE	AK	99511
MERRICK DEVELOPMENT CORP	18178 E WALLING RD	PALMER	AK	99645
METRO MTG & SEC CO INC	PO BOX 211945	ANCHORAGE	AK	99521
MID-VALLEY SENIORS INC	PO BOX 940168	HOUSTON	AK	99694
MILLER ARCHIE J LVG TR	PO BOX 520437	BIG LAKE	AK	99652
MILLER INV LLC	3920 BRENTWOOD CIR	ANCHORAGE	AK	99502
MILLER J & L COMM PROP TR	PO BOX 520296	BIG LAKE	AK	99652
MILLERS REACH HOMEOWNERS	19332 1ST ST	EAGLE RIVER	AK	99577
MINER JAY R & L CO TRE	3770 GUNWALE CT	ANCHORAGE	AK	99516
MITCHELL MARY C 2000 TR	6033 E 21ST AVE	ANCHORAGE	AK	99504
MLAKAR L & F SUP NEEDS TR	PO BOX 112164	ANCHORAGE	AK	99511
MOE FAMILY TR	11833 E HELEN DR	PALMER	AK	99645
MONAGHAN CONST INC	4521 TRAPLINE CIR	ANCHORAGE	AK	99516
MONAGHAN FAMILY TR 2002	3439 SCARLETT PL	ANCHORAGE	AK	99517
MONSTER PROPERTIES INC	425 G ST # 900	ANCHORAGE	AK	99501
MONTAGE LTD PRTRNSHP	1725 LAURENCE CT	ANCHORAGE	AK	99501
MONTANA CRK NTV ASSN INC	PO BOX 102307	ANCHORAGE	AK	99510
MORGAN ALBERT L&VF LVG TR	PO BOX 2435	PALMER	AK	99645
MORRIS COMM CORP / TOWER	201 ARCTIC SLOPE AVE	ANCHORAGE	AK	99518
MOSESIAN FAMILY LLC	13700 SPECKING AVE	ANCHORAGE	AK	99515
MPM LLC	405 W 27TH AVE	ANCHORAGE	AK	99503
MUNK WERNER R LIV TR TRE	12170 WOODWARD DR	ANCHORAGE	AK	99516
MURFITT ALLAN W LVG TR	13810 VENUS WAY	ANCHORAGE	AK	99515
MURKOWSKI FRANK H & N TRE	716 CALHOUN	JUNEAU	AK	99801
M-W DRILLING INC	PO BOX 110378	ANCHORAGE	AK	99511
N STAR TERM& STEVEDORE CO	790 OCEAN DOCK RD	ANCHORAGE	AK	99501
NATION DEAN J&MARY LVG TR	PO BOX 749	WILLOW	AK	99688
NEESER CONST INC	2501 BLUEBERRY ST	ANCHORAGE	AK	99503
NELSON AK COMM PROP TR	1342 ZARVIS PL	ANCHORAGE	AK	99508
NELSON RALPH E& NANCY TRE	PO BOX 231071	ANCHORAGE	AK	99523
NEW HARVEST/APOSTOLIC	555 N GULKANA ST	PALMER	AK	99645

NIEMANN CHAS W TR TRE	625 S LANE ST	ANCHORAGE	AK	99508
NIMS IONA M REV TR	3400 OREGON DR	ANCHORAGE	AK	99517
NOLAN LORA JEAN 1996 TR	PO BOX 140043	ANCHORAGE	AK	99514
NORTH LAKES COMM COUNCIL	P O BOX 871514	WASILLA	AK	99687
NORTHLAND BAPT MINISTRIES	PO BOX 110828	ANCHORAGE	AK	99511
NORTHLAND MINERALS INC	1817 PARKSIDE DR	ANCHORAGE	AK	99501
NORTHPOINT DEV LLC	2102 CLEVELAND AVE	ANCHORAGE	AK	99517
NOVELLI STEVEN & M B TRES	6431 E 9TH AVE	ANCHORAGE	AK	99504
NUNAT DEVELOPMENT LLC	PO BOX 244083	ANCHORAGE	AK	99524
NYMAN FRANK & M REV TR	3711 W 40TH AVE	ANCHORAGE	AK	99517
Office of the Governor - Mat-Su	877 Commercial Drive	Wasilla	AK	99654
OLSON GUNNAR O TRE	4711 MELVIN AVE	ANCHORAGE	AK	99517
OSGOOD NEAL E& GORDON TRE	PO BOX 112016	ANCHORAGE	AK	99511
OUR LADY OF COMP CARE CTR	4900 EAGLE ST	ANCHORAGE	AK	99503
OUR LADY OF FATIMA SAN	3341 LAKESHORE DR	ANCHORAGE	AK	99517
OURTRUST LLC	5301 BISHOPS CASTLE	ANCHORAGE	AK	99516
OVERLEES MILDRED G TR	2912 BONIFACE PKY	ANCHORAGE	AK	99504
P & P PROPERTIES LLC	4200 SHOSHONI AVE	ANCHORAGE	AK	99516
PALUBA JOS & V REV LVG TR	1701 ALEUTIAN ST	ANCHORAGE	AK	99508
PANNONE MICHAEL L TR TRE	PO BOX 211062	ANCHORAGE	AK	99521
PAPE FAMILY REV TR UTA	206 STEWART ST	ANCHORAGE	AK	99508
PAR 4 DEVELOPMENT CO. LLC	6741 E 10TH AVE	ANCHORAGE	AK	99504
PARAMOUNT INV LLC	774 FISCHER AVE	ANCHORAGE	AK	99518
PARGETER E THOS DEC TRTRE	PO BOX 347	WILLOW	AK	99688
PARKS ELIZABETH A TRE	3750 GUNWALE CT	ANCHORAGE	AK	99516
PARKS GLENN CORP	4159 HOOD CT	ANCHORAGE	AK	99517
PAYNE JOHN B REV TR	6636 FOOTHILL DR	ANCHORAGE	AK	99504
PEARCE FAMILY TR	4962 NOTTINGHAM WAY	ANCHORAGE	AK	99503
PENDING SALE FROM STATE	5561 S BODENBURG LOOP	PALMER	AK	99645
PENDLETON KENNETH E TRE	8631 VIGOR CIR	ANCHORAGE	AK	99504
PEOT SHARON M LIVING TRST	11101 MOUNTAIN LAKE DR	ANCHORAGE	AK	99516
PETERSON L&L JOINT REV TR	850 BREAKWATER	ANCHORAGE	AK	99515
PETERSON MARGARETE S TRE	4817 NEWCASTLE WAY	ANCHORAGE	AK	99503
PG PROPERTIES LLC	PO BOX 90100	ANCHORAGE	AK	99509
PHILLIPS D W SR LVG TR	PO BOX 520587	BIG LAKE	AK	99652
PHOTON INVESTMENT CO	PO BOX 213689	ANCHORAGE	AK	99521
PINK ELEPHANT STORES INC	8215 OLD SEWARD HWY	ANCHORAGE	AK	99518
PLOOY 2002 TR	4920 SPORTSMAN DR	ANCHORAGE	AK	99502
POGANY LOUISE P TRUST	PO BOX 770323	EAGLE RIVER	AK	99577
POINT BLUFF LLC	16208 ESSEX PARK DR	ANCHORAGE	AK	99516
POINT MACKENZIE CC	PO BOX 877393	WASILLA	AK	99687
PORTER MARILYN M 2001 TR	PO BOX 101726	ANCHORAGE	AK	99510
POTTER ELAINE L LVG TR	2450 W 71ST CIR	ANCHORAGE	AK	99502

POTTER PLACE LLC	2810 KEMPTON HILLS DR	ANCHORAGE	AK	99516
POWER SPORTS LLC	3720 WESTMINSTER WAY	ANCHORAGE	AK	99508
PRESTON HILLS LLC	619 E SHIP CREEK AVE	ANCHORAGE	AK	99501
PRINGLE HL & L R LVG TR	6051 SPRUCE MEADOWS LOOP	ANCHORAGE	AK	99507
PROTZMAN JAS C LVG TR	PO BOX 520050	BIG LAKE	AK	99652
PROVIDENCE HEALTH SYS-WA	PO BOX 196501	ANCHORAGE	AK	99519
PTF INVESTMENTS	4101 ARCTIC BLVD	ANCHORAGE	AK	99503
PUHL R & B JOINT REV TR	PO BOX 521189	BIG LAKE	AK	99652
Q-1 CORP	1230 W INTL AIRPORT RD	ANCHORAGE	AK	99518
QUAINTANCE BOB	PO BOX 13249	TRAPPER CREEK	AK	99683
QUAINTANCE BOB	PO BOX 13249	TRAPPER CREEK	AK	99683
QUALITY SAND & GRAVEL LLC	2193 VIKING DR	ANCHORAGE	AK	99501
QUEEN VERONICA B LVG TR	7420 TUTNA CIR	ANCHORAGE	AK	99504
QUEZADA MARY RUTH LVG TR	12841 MARINER DR	ANCHORAGE	AK	99515
R & D DEVELOPMENT LLC	3746 CLAY PRODUCTS DR	ANCHORAGE	AK	99517
R M INV	6812 CUTTY SARK DR	ANCHORAGE	AK	99502
RAPP RICHARD H JR IRREVTR	1341 ST GOTTHARD AVE	ANCHORAGE	AK	99508
RASMUSON EDW B TRE	PO BOX 196127	ANCHORAGE	AK	99519
RATCLIFFE LAYNE R TR TRE	8323 JODPHUR ST	ANCHORAGE	AK	99502
REBCO INV	8604 SWISS PL	ANCHORAGE	AK	99507
RECKNAGLE FAMILY TR	5401 CARIBOU AVE	ANCHORAGE	AK	99508
RECREATION DEV CORP	PO BOX 0106	WILLOW	AK	99688
RED SAND LTD	3920 MT VIEW DR	ANCHORAGE	AK	99508
REDINGTON VIOLET E EST	1230 PINE ST	ANCHORAGE	AK	99508
REESE ROGER K REV TR TRE	6830 TONDI LN	ANCHORAGE	AK	99507
REILLY FAMILY 2003 TR	1402 INLET PL	ANCHORAGE	AK	99501
REINBOLD-DEARBORN AK TR	PO BOX 772033	EAGLE RIVER	AK	99577
REISNER FAM LTD PRTRNSHIP	PO BOX 1385	PALMER	AK	99645
RELYING ON THE WORD	PO BOX 940164	HOUSTON	AK	99694
RENSCH THOS J CO-TRE	12840 ATHERTON RD	ANCHORAGE	AK	99516
REYNOLDS FAMILY TR	PO BOX 110232	ANCHORAGE	AK	99511
RHODES CHAS& LINDA LVG TR	3918 W 37TH CT	ANCHORAGE	AK	99517
RICHARDS SUSAN R LVG TR	10710 E TREE DR	ANCHORAGE	AK	99507
RIDGE DEV CO THE LLC	7420 SOLARSET CIR	ANCHORAGE	AK	99507
RING DAVID B TR AGRMT	5111 W 80TH AVE	ANCHORAGE	AK	99502
RITA LLC	2960 C ST	ANCHORAGE	AK	99503
ROBERTSON JAS M REV TR	24220 HEARTHSTONE DR	CHUGIAK	AK	99567
ROBINSON K D REV LVG TR	7730 CHARLOTTE CIR	ANCHORAGE	AK	99502
ROCKY LK ASSET PROTECT TR	833 E 4TH AVE	ANCHORAGE	AK	99501
ROSS KERSTEN PERPETUAL TR	12540 ATHERTON RD	ANCHORAGE	AK	99516
ROUNDABOUT AUTO LLC	6020 GREENWOOD ST	ANCHORAGE	AK	99518
ROWLAND CARITA A TRE	PO BOX 242894	ANCHORAGE	AK	99524
RUDD CONRAD LVG TR	2940 YALE DR	ANCHORAGE	AK	99508

RUSH EVELYN C ROCKY LK TR	1345 W 11TH AVE	ANCHORAGE	AK	99501
RUSSIAN ORTHODOX CH TRE	PO BOX 210569	ANCHORAGE	AK	99521
S B INC	6741 E 10TH AVE	ANCHORAGE	AK	99504
S DOUBLE L K PARTNERSHIP	6500 GUNNISON DR	ANCHORAGE	AK	99516
S S & T ROBERTSON LLC	PO BOX 520634	BIG LAKE	AK	99652
SACRED HEART PARISH CORP	PO BOX 102239	ANCHORAGE	AK	99510
SAFE-T-WAY ELECTRIC INC	6208 MCKAY ST	ANCHORAGE	AK	99518
SAGE ALASKA PROP LLC	2511 S SKY RANCH LOOP	PALMER	AK	99645
SALVATION ARMY	PO BOX 101459	ANCHORAGE	AK	99510
SAMATT LLC	PO BOX 102621	ANCHORAGE	AK	99510
SAMPLE JOHN & PAM LVG TR	1923 BEAVER PL	ANCHORAGE	AK	99504
SAVIERS CREDIT SHELTER TR	9101 EMERALD DR	ANCHORAGE	AK	99502
SAYERS STANLEY LVG TR	2031 SARATOGA AVE	ANCHORAGE	AK	99517
SCHAEFER GARY ERNEST TR	804 E 15TH AVE	ANCHORAGE	AK	99517
SCHENDERLINE COMM PROP TR	540 W POTTER DR	ANCHORAGE	AK	99518
SCHMELZER FAMILY TR	12030 SHENANDOAH PL	ANCHORAGE	AK	99516
SCHNEIDER MICHAEL IRRV TR	2414 SUSITNA DR	ANCHORAGE	AK	99517
SCHNEIDER MICHAEL IRRV TR	880 N ST #202	ANCHORAGE	AK	99501
SCHOPPERT DOROTHY MAY TR	3234 DELTA DR	ANCHORAGE	AK	99502
SCHWANK A A & D E TRE	3601 LOIS DR	ANCHORAGE	AK	99517
SCHWANTES INC	2209 EUREKA ST	ANCHORAGE	AK	99503
SCSL INC	PO BOX 1028	WILLOW	AK	99688
SCW LLC	3512 WESLEYAN DR	ANCHORAGE	AK	99508
SEALASKA CORP	ONE SEALASKA PLAZA	JUNEAU	AK	99801
SEGLUSN SHRS HMONRS ASSOC	SRA BOX 1624A	ANCHORAGE	AK	99507
SEPIAN PAUL S REV TR TRE	7540 MARGARET CIR	ANCHORAGE	AK	99518
SEPPI RUSSELL W LVG TR	889 LANCASTER DR	ANCHORAGE	AK	99503
SEQUENTIAL RESERVE TR	PO BOX 286	WILLOW	AK	99688
SEVENTH DAY ADVENTISTS	6100 O'MALLEY RD	ANCHORAGE	AK	99516
SHAMBUREK LAW OFFICE LLC	9110 CHIPWOOD CIR	ANCHORAGE	AK	99507
SHILANSKI 2001 LLC	PO BOX 525508	ANCHORAGE	AK	99652
SHINN DEBRA D LVG TR	950 P ST	BIG LAKE	AK	99501
SHUMAKER JOE A&JUANITA TR	1903 DIOMEDE ST	ANCHORAGE	AK	99504
SIEGEL CONST LLC	PO BOX 671169	CHUGIAK	AK	99567
SIMON MARION Z REV TR	3676 N POINT DR	ANCHORAGE	AK	99515
SIMPSON JOHN E LVG TR	19025 VILLAGE SCENIC PKY	ANCHORAGE	AK	99516
SMITH B J & P A LVG TR	PO BOX 520371	BIG LAKE	AK	99652
SMITH BARBARA A TR AGRMT	3670 RICHARD EVELYN BYRD	ANCHORAGE	AK	99517
SMITH HEATHER N LVG TR	2509 W 43RD CT	ANCHORAGE	AK	99517
SMITH RODNEY& LUCY LVG TR	3818 WESLEYAN DR	ANCHORAGE	AK	99508
SNELL AMELIA E TR TRE	12105 NE 181 Place A-302	Bothell	AK	98011
Snowball Express	PO BOX 212314	ANCHORAGE	AK	99521
Snowball Express	PO BOX 212314	ANCHORAGE	AK	99521

SO LO MA PARK INC	300 W 36TH AVE	ANCHORAGE	AK	99503
SOUTH KNIK RIVER COM COUN	P O BOX 289	PALMER	AK	99645
SOUTH LAKES COMM COUNCIL	4701 E BEGICH CIRCLC	WASILLA	AK	99654
SOUTHCENTRAL FOUNDATION	4501 DIPLOMACY DR	ANCHORAGE	AK	99508
SPARROWS SONG CEMETERY	HC 89 BOX 412	WILLOW	AK	99688
SPENARD BUILDERS SUPPLY	840 K ST STE 200	ANCHORAGE	AK	99501
SPERNAK ANITA L TR AGRMT	7310 SETTER DR	ANCHORAGE	AK	99502
SPERNAK ELEANOR P TR TRE	1700 EASTRIDGE DR	ANCHORAGE	AK	99501
SPEZIALY DOMINIC L TR TRE	PO BOX 112852	ANCHORAGE	AK	99511
SPINELL HOMES INC	1900 W NORTHERN LIGHTS	ANCHORAGE	AK	99517
SPINELLI C WASILLA RES TR	2110 SONSTROM DR	ANCHORAGE	AK	99517
ST CLAIRE LVG TR	221 MCCARREY ST	ANCHORAGE	AK	99508
ST MICHAELS CATHOLIC CHR	432 E FIREWEED AVE	PALMER	AK	99645
STAPLETON ROBT W&E L TRES	1217 CRESCENT AVE	ANCHORAGE	AK	99508
STARBOARD COVE HOMEOWNERS	PO BOX 520186	BIG LAKE	AK	99652
STARK GINDRA L LVG TR TRE	PO BOX 770318	EAGLE RIVER	AK	99577
STEPHENS ADA E REV TR TRE	23108 BARBARA ST	CHUGIAK	AK	99567
STERNWHEELERS INV PTRNSHP	PO BOX 101582	ANCHORAGE	AK	99510
STONEBRIDGE HOMES INC	2600 RAILROAD AVE	ANCHORAGE	AK	99501
STONEBRIDGE INV INC	PO BOX 232844	ANCHORAGE	AK	99523
STONY CREEK LLC	12861 JOHNS RD	ANCHORAGE	AK	99515
STRONG FAMILY TR	1403 ERMINE	ANCHORAGE	AK	99504
STURE LINDA L DEC TR TRE	3632 CARLETON AVE	ANCHORAGE	AK	99517
SUBURBAN LAND SALES CORP	PO BOX 521012	BIG LAKE	AK	99652
SUBURBAN PRINTING CO INC	521 W 41ST AVE	ANCHORAGE	AK	99503
SUNNYVALE EST	PO BOX 520654	BIG LAKE	AK	99652
SUNNYVALE EST	PO BOX 521297	BIG LAKE	AK	99652
SUNNYVALE EST	PO BOX 521632	BIG LAKE	AK	99652
SUNNYVALE ESTATES	3216 E 46TH	ANCHORAGE	AK	99507
SUNSHINE CHAMBER OF COMMERCE	PO BOX 810	WILLOW	AK	99688
SUNSHINE VENTURES	4015 SYCAMORE LOOP	ANCHORAGE	AK	99504
SUSITNA ASSOC INC	6100 TRAPPERS TRAIL RD	ANCHORAGE	AK	99516
SUSITNA GIRL SCOUT CONCIL	3911 TURNAGAIN BLVD E	ANCHORAGE	AK	99517
SUSITNA INVESTMENTS LLC	13340 RIDGEWOOD CIR	ANCHORAGE	AK	99516
SUSITNA SLOUGH LLC	2000 ATWOOD DR	ANCHORAGE	AK	99517
SUTTON DAVID O&A R REV TR	5501 RABBIT CRK RD	ANCHORAGE	AK	99516
SYREN ROY& CRYSTAL LVG TR	8507 LAVIENTO DR	ANCHORAGE	AK	99515
TAIT PROPERTIES LLC	2555 S COGGIN DR	PALMER	AK	99645
Talkeetna Chamber of Commerce	P.O. Box 334	Talkeetna	AK	99676
TALKEETNA COMMUNITY COUNCIL	PO BOX 608	TALKEETNA	AK	99676
Talkeetna Historical Society	P.O. Box 76	Talkeetna	AK	99676
TAPSCOTT JAS A TRE	1913 PARKVIEW CT	ANCHORAGE	AK	99501
TAYLOR M S IRREV ASSET TR	900 W 5TH AVE	ANCHORAGE	AK	99501



TEMPLETON S & R LVG TR	18935 MONASTERY DR	EAGLE RIVER	AK	99577
TEW'S ENT LLC	PO BOX 521314	BIG LAKE	AK	99652
TGI FUNDING CO LLC	PO BOX 52777	BIG LAKE	AK	99652
The Birchwood Corporation	PO BOX 101700	ANCHORAGE	AK	99510
THOMAS CLEVEN & BETTY K TRE	1120 KAYLIN CIR	ANCHORAGE	AK	99515
THOMAS COMPANY INC THE	PO BOX 2128	PALMER	AK	99645
THOMAS ELAINE L TRE	19546 CITATION	EAGLE RIVER	AK	99577
THOMSON JAS D&RUBY TR TRE	26735 PARADISE LN	CHUGIAK	AK	99567
THREE A INV	3340 ARCTIC BLVD	ANCHORAGE	AK	99503
THROCKMORTON S R FAM TR	1510 SHORE DR	ANCHORAGE	AK	99515
TILBY LTD PARTNERSHIP	PO BOX 91872	ANCHORAGE	AK	99509
TIMBER BASIN CONST LLC	PO BOX 772461	EAGLE RIVER	AK	99577
TINDALL DENISE A TRE	PO BOX 521233	BIG LAKE	AK	99652
TORY PROPERTIES LLC	PO BOX 232069	ANCHORAGE	AK	99523
TRAGER REINHOLD&SHARON TR	1611 EARLY VIEW DR	ANCHORAGE	AK	99504
TRAPPER CREEK COMMUNITY COUNCIL	P O BOX 13021	TRAPPER CREEK	AK	99683
TRAPPER CREEK COMMUNITY COUNCIL	P O BOX 13021	TRAPPER CREEK	AK	99683
TRUMP LLC	PO BOX 240961	ANCHORAGE	AK	99524
TUNISTA PROP INC	301 CALISTA CT	ANCHORAGE	AK	99518
TUNISTA PROP INC	9210 STRATHMORE DR	ANCHORAGE	AK	99502
TURINSKY ANTHONY C TR TRE	PO BOX 241754	ANCHORAGE	AK	99524
Ultimate Alaska Experiences	P.O. Box 83	Talkeetna	AK	99676
Ultimate Alaska Experiences	P.O. Box 83	Talkeetna	AK	99676
UPPER SUSITNA SHOOTERS	PO BOX 393	TALKEETNA	AK	99676
VADEN HENRIETTA R FAM TR	1903 W 36TH AVE	ANCHORAGE	AK	99517
VADLA FRANCES REV TR	PO BOX 520933	BIG LAKE	AK	99652
VALLEY OPEN BIBLE FLWSHP	PO BOX 520749	BIG LAKE	AK	99652
VALLEY TRADES CENTER LLC	2515 A ST	ANCHORAGE	AK	99503
VALLEY VISIONS LLC	3940 ARCTIC BLVD STE 101	ANCHORAGE	AK	99503
VERRETT TIMOTHY PERS REP	431 W 7TH AVE	ANCHORAGE	AK	99501
VREELAND RICHARD SR GDN	740 W 86TH AVE	ANCHORAGE	AK	99515
WAGNER JAS & JOANN LIV TR	3752 LINCOLN ELLSWORTH CT	ANCHORAGE	AK	99517
WAGNER MITCHELL THOS TRE	3315 LAKE PARK CIR	ANCHORAGE	AK	99517
WALCH KENNETH B REV TR	7902 ARLENE ST	ANCHORAGE	AK	99502
WALDRONS POND COMMUNITY	PO BOX 182	WILLOW	AK	99688
WALKER GARRETT JAS TR TRE	PO BOX 222372	ANCHORAGE	AK	99522
WALKER RENADA B REV TR	7027 APOLLO CT	ANCHORAGE	AK	99504
WARD JACK W&CHERYL A TRES	PO BOX 13	WILLOW	AK	99688
WASILLA CITY OF	1600 N BLOCK CIR	PALMER	AK	99645
WASILLA CITY OF	523 SOUTH VALLEY WAY	PALMER	AK	99645
WASILLA CITY OF	PO BOX 489	WILLOW	AK	99688
WASILLA FOUR PROP LLC	2501 BLUEBERRY RD	ANCHORAGE	AK	99503
Wasilla Historical Society	300 Boundary Street	Wasilla	AK	99654

WATERS LAURIE A DEC TR	7815 KIANA CIR	ANCHORAGE	AK	99507
WD CORPORATION	341 W TUDOR RD STE 103	ANCHORAGE	AK	99503
WEBB GARY M & CORA L TRES	16041 ST JAMES ST	ANCHORAGE	AK	99516
WEBB GARY M TRE	16041 SAINT JAMES ST	ANCHORAGE	AK	99516
WEBB GARY TRE	PO BOX 404	WILLOW	AK	99688
WEBB RON PAVING & SNOW	PO BOX 230228	ANCHORAGE	AK	99523
WEDEL MILDRED J REV TRTRE	PO BOX 111489	ANCHORAGE	AK	99511
WEILAND AK COMM PROP TR	3912 DANDELION WINE	ANCHORAGE	AK	99507
WEILAND LARRY LVG TR	5400 S BODENBURG LOOP	PALMER	AK	99645
WELCH LVG TR	PO BOX 521487	BIG LAKE	AK	99652
WELLS FARGO BANK CUST	1210 CROWBERRY	ANCHORAGE	AK	99515
WELLS FARGO BANK OF AK	11940 NORTHERN RAVEN DR	ANCHORAGE	AK	99516
WEST TAYLOR L TR TRE	3100 SEAPORT CIR	ANCHORAGE	AK	99515
WHITE RAVEN DEV INC	351 E 104TH AVE	ANCHORAGE	AK	99515
WIEDERHOLT DELORES I TRE	2620 FOREST PARK DR	ANCHORAGE	AK	99517
WILLIAMS CX4 LVG TR	2950 DRAKE DR	ANCHORAGE	AK	99508
WILLOW AREA COMMUNITY ORG	P O BOX 1027	WILLOW	AK	99688
WILLOW AREA SENIORS INC	HC 89 BOX 57	WILLOW	AK	99688
WILLOW CHAMBER OF COMMERCE	PO BOX 183	WILLOW	AK	99688
WILLOW TRADING POST INC	8140 NORTHVIEW DR	ANCHORAGE	AK	99504
WIN LLC	PO BOX 240971	ANCHORAGE	AK	99524
WINN CECIL L & M J FAM TR	1417 KARLUK ST	ANCHORAGE	AK	99501
WOOD FAMILY INVESTORS LLC	PO BOX 520206	BIG LAKE	AK	99652
WOODS LILLIAN C TRE	3314 NEWCOMB DR	ANCHORAGE	AK	99508
WOOLIVER STEPHEN P REV TR	13201 CARITA LN	ANCHORAGE	AK	99516
WUERTH JUERGEN W TRE	931 MAHO CIR	ANCHORAGE	AK	99515
Y COMMUNITY COUNCIL	HC 89 BOX 8575	TALKEETNA	AK	99676
Y COMMUNITY COUNCIL	HC 89 BOX 8575	TALKEETNA	AK	99676
YAP DENIS C F LVG TR	5527 S GARRETT DR	BIG LAKE	AK	99652
YATES FAMILY TR	4240 MAC INNES ST	ANCHORAGE	AK	99508
ZERKEL JOYCE E TRE	12541 TANADA LOOP	ANCHORAGE	AK	99515
ZOETIE LLC	3130 E 46TH AVE	ANCHORAGE	AK	99507
ZORICK ROBT J&JOYCE M TRE	3706 E 67TH AVE	ANCHORAGE	AK	99507
ZWEIFEL PROPERTIES INC	PO BOX 521810	BIG LAKE	AK	99652
Anchorage Snowmobile Club	PO Box 232196	ANCHORAGE	AK	99523
Willow Trail Committee	PO Box 845	WILLOW	AK	99688

**Appendix A:  
Materials Presented**

# COMMENT FORMS

*(This page intentionally left blank.)*



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name

Janice Strong

Phone

3573841

Address

P.O. Box 875169 WASILLA 99687

Email

strong.jp@mtaonline.net

Your Comments:

Please be aware of watershed issues. The drainage from Beaver Lake(s), Horseshoe Lake + West Lake would be restricted by the barrier created for the rail line. Please do NOT obstruct drainage to the Tuttle Susitna River.

*Janice M. Strong*

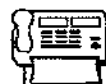
The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Gregory Strong

Phone

892 6525

Address

3559 S. Horseshoe Lake Rd

Email

strong@mtaonline.net

Your Comments:

Last fall we experienced a very wet fall. The lake elevations rose exactly 18.75 inches. The creation of an earthen dam to support the railroad proper will create a GIANT DAM. Please refer yourselves to the watershed studies for this area to confirm this observation. The far westerly route is on the "other side" of the Little Susitna River and if chosen would avoid this inevitable flood hazard.

*Greg Strong*

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portnacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

COMMENT FORM

Name

George B. Woodbury III

Phone

892-6504

Address

PO box 520917

Email

woodbury@GCI.NET

Your Comments:

Based on history and projected growth models the Willow Route would be the best route

- 1) Less impact for growth in Big Lake Area around Peters Lakes
- 2) Access for remote property sales for Borough in Fish Creek Area for growth between both Willow & Big Lake communities Develop Ag Land lots & Recreation - Access to Fish Creek Area
- 3) Build on moraine from the west end just before the wet lands @ 50 flats (between Little Su - to Big Su) Best for construction and would not impend sub-terrain drainage effect - wont "dam" and will be sound for construction and it edged from fault area relatively stable for earthquake surge
- 4) Right of way would be minimal mostly state and Borough property
- 5) Private property east impacted

This route should benefit both Willow and Big Lake from growth projection and maximize available property for development & transportation.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
 Port MacKenzie Rail Extension Comment  
 836 S. Colony Way, Suite 2  
 Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022





# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

Name

ORIE ORIEN

Phone

907 852-0965

Address

P.O. BOX 520716  
BIG LAKE, AK 99652

Email

rorien@AOL.COM

Your Comments:

### HOUSTON ROUTE:

- 1) Houston route cuts through the heart of the Big Lake / Little Susitna trail system. There are dedicated trails crossing the route at possibly 5 intersections and would disrupt the entire system.
- 2) The area between Pappose Twin Lakes and Crooked Lake is an extremely narrow corridor (1/2 mile or less) and is filled with wetlands (small ponds and marsh).
- 3) The outlet stream from Crooked Lake flows to the Little Susitna River and is an anadromous stream.
- 4) There may also be significant private property issues throughout this corridor.

COMMENT FORM

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



IIDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Jim Fairles

Phone

907-892-8482

Address

Box 521152 Big Lake

Email

Your Comments:

99652

I think the ~~road~~ <sup>Attending</sup> ~~road~~ <sup>Attending</sup> makes the most sense. It has little negative impact on existing population + land ownership

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**




Name <i>Deanna Swaney</i>	Phone <i>907-892-3303</i>
Address <i>2049 S Horseshoe Lake Rd, Big Lake</i>	Email <i>dsswaney@aol.com</i>

Your Comments:

*The Willow to Port MacKenzie route seems the most logical one to take. Lowest property values, least social impact, least impact on future development, fewest level crossings, best terrain and base material.*

*Houston + Big Lake options would disrupt far too many families and create too much noise in relatively heavily populated areas.*

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:

	HDR Alaska Inc. Port MacKenzie Rail Extension Comment 836 S. Colony Way, Suite 2 Palmer, AK 99645		<a href="mailto:info@portmacrail.com">info@portmacrail.com</a>
			Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name

BOB JONES

Phone

892 7369

Address

BX 521016 Big Lake 99652

Email

bob n pal@sci.net

Your Comments:

I firmly support the willow route because of what appears to be the least socio/economic impact to this area. My only concern is that the major trail crossings are respected / allowed for and that adequate provisions be made to prevent backing up water from the drainages to the sea river. The wet lands should neither have an increase or decrease in water level. This route appears to have the least effect on short term expansion of what will become a very populated area and also crosses some of the least desirable ground to be developed.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name  
*Rich Thioffe*

Phone  
*892-8626*


Address  
*15687 W SUNDR*

Email


Your Comments: *wasilla AK  
99654*

*Please keep the Railroad as far to the west as possible to keep impact low. Come thru Willow to meet existing railroad.*


The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



[info@portmacrail.com](mailto:info@portmacrail.com)



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name

Noel W. Woods

Phone

(907) 745-3027

Address

P.O. Box 827 Palmer, AK 99645

Email

Your Comments:

*I strongly agree that this rail to Port MacKenzie is very much needed.*

*Please go full speed ahead*

*Noel*

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Lance Barve

Phone

907-495-1131

Address

Hc 89 Box 248 Willow AK

Email

Lanceb@mtgonline.net

Your Comments:

I am very pleased to see the state & the borough going forward with much needed infrastructure improvements to bring Willow & the port into the 21st century

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



IIDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022

Rec. 11/20/07



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Carl Moore

Phone

892 4663

Address

PO Box 521107  
Big Lake, AK 99652

Email

cast@vntaonline.net

Your Comments:

My preference for the rail extension would be Mac West - Conn 1 - Houston - Houston South, This route would minimize the number of public roads crossing the tracks. It keeps the train and rail construction out of the Susitna SRA and avoids the Little Su Rec River and Nancy Lake SRA.

Make safety your number 1 goal and avoid public roads and avoid residential areas.

Carl Moore

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portnacrail.com



Fax (907) 644-2022





# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name  
*Kim Woodbury*

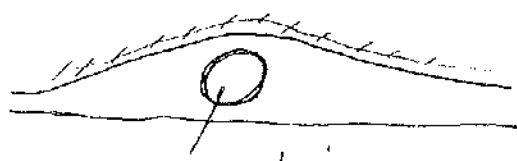
Phone  
*892 6504*

Address  
*POB 520957 Big Lake*

Email  
*woodbury@gci.net*

Your Comments:

RE: Trails.  
The proposed "trail crossings" design of a "culvert" or grade separated crossing needs addressed. Many of our trails are in wetlands, how will you place these crossings below grade or will the railroad use 1000's of yards of material to build up the area to cross above ground.



Culvert/crossing

We are very concerned about our trails and not losing access to our riding, hunting, fishing grounds beyond Corridor 3.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



Rec  
11/01/07



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Sarah M. Richardson

Phone

(907) 495-8994

Address

PO Box 865

Email

noahsart@mtaonline.net

Your Comments:

Dear Ms. Sullivan,  
I am writing this letter to let you know of my disliking at the proposed rail road corridor through Willow. I moved out here 3 years ago from Anchorage to live in & enjoy this natural recreational area & to be near Willow Creek for prime fishing & camping.

The idea of this proposed corridor would most certainly disrupt this ecosystem & would be another display of poor environmental decisions & lack of stewardship.

Sincerely,

Sarah Richardson

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



Rec 11/01/07



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name  
*Greg R Binder*

Phone  
*495-1977*

Address  
*P.O.B. 476 Willow Lake, Alaska*

Email

Your Comments:

- 1) Please update all repairs on existing railroads
- 2) Put down on maps the exact land tracts that are to be effected in new railroad plans be up front who is to be effected & why
- 3) Those who do not wish to be a part of the project - should be allowed to say NO
- 4) Be prepared to pay the price the owners will accept. offer all these may be someone's family home etc.

(A) Do not over tax & force people out  
(B) Do not condemn land so as to acquire  
(C) Do not lose public domain as an excuse to force people off their land  
This is Alaska & should be allowed to best continue to be the last frontier. Let the Railroad be an asset not a hindrance to any community - be fair & honest in whatever you do. The public should be aware of all transactions & business ventures that effect everyone

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



Rec 11/01/07



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name  
*Sandra De Lucia*

Phone  
*495-3424*

Address  
*PO Box 812 Willow AK 99688*

Email

Your Comments:

*Thank you for having people like Dana Hills available for information to the general public outside formal town meetings.*

*I favor the route that bypasses Willow due to noise impact that could occur. It makes sense to have a commercial route for trains & it would reduce rail traffic on main line and accident potential.*

*Looking forward to Oct 24<sup>th</sup> Q1A meeting in Willow.*

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



Rec  
11/01/07



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Nina Davidson

Phone

907-495-1112

Address

Po Box 954 Willow Ak 99688

Email

ninadean@mtaonline.net

Your Comments:

My emphasis is to underscore the fact that I AM one of those professionals who retired to Willow to enjoy the outdoor Alaskan experience. We purchased over 10 acres on Crystal Lake to access Willow Swamp, the Big Swamp, West gateway trail system & the Susitna. We spend many many hours on that trail system every winter along with our family, friends & visitors from Anchorage & outside. Please do not sacrifice this near one-of-a-kind recreation area to a rail corridor. Once lost it can never be regained.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



Willow  
AK



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name  
PATRICIA ECKHOFF

Phone  
907-495-8994

Address  
PO Box 865 Willow AK.

Email

Your Comments:

DEAR MS. SULLIVAN,

I would like to voice my opinion  
 EXPRESSING my disagreement and  
 CONCERN REGARDING the PORT MACKENZIE  
 RAIL EXTENSION PROJECT.

I CHOSE to live in Willow to be in  
 A NATURAL RECREATION AREA, to be  
 NEAR WILLOW CREEK for prime fishing  
 & camping.

Anything to disrupt this ecosystem  
 would be OUTRAGEOUS & ANOTHER display  
 of POOR ENVIRONMENTAL DECISIONS &  
 LACK OF STEWARDSHIP.

Sincerely,  
 Pat Eckhoff

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name

Bruce H. Mattson

Phone

907 495 2031

Address

POB 233  
Willow, AK 99688

Email

Your Comments:

The RR routing west of Red Shirt Lake makes the most sense to me. Engineering logic dictates selection of the most direct, efficient, and structurally suitable soils for construction.

The ROW proposed avoids nearly all privately owned property, which is preferred. The relocation of the RR traffic away from Nancy Lake and the increasingly dense housing areas would be a noise improvement as well.

A small vocal minority (read mushers & friends) are opposed to every development which could potentially interfere with their perceived "right" to use all unoccupied lands for their benefit.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



PLEASE CONSIDER THIS AS A SPOILING TACTIC.



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

COMMENT FORM



Name  
Pralle, Delores

Phone  
495-5230

Address  
HC89 Box 56

Email

Your Comments:

Willow is growing and will continue to grow. If we reject growth and war against it and the progress it brings we only hurt ourselves and our future. Embrace growth and make it work for you and your community --

I really feel that the rails and Highway from Port MacKenzie can be of a big help to the Willow Community. Preserve our Country style of living and our trails but let us grow and mature with Alaska.

Make the best decisions but keep in mind we want a Country way of living to remain.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022





# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

BOBBIE LEWIS

Phone

445-1200

Address

PO BOX 661 - WILLOW AK 99665

Email

Your Comments:

I AM FOR GROWTH! THE SPUR THAT COMES OUT ABOVE (NORTH) OF WILLOW SEEMS TO BE THE BEST FOR THE RAILROAD, MAT-SU BOR. & WILLOW IF THE ENVIRONMENTAL IMPACT STUDIES PROVE TO WARRANT THIS ROUTE. I THINK WE SHOULD DO WHAT IS BEST FOR ALL CONCERNED NOT JUST A FEW! THIS NORTHERN ROUTE WOULD STOP ALL THE TRAFFIC ACROSS HATFIELD PASS RD & OTHERS THAT ARE SO DANGEROUS.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name *Jane Norcross*

Phone

[Empty box for name]

*495-6650*

Address

Email

*PO Box 242  
Willow, AK 99688*

[Empty box for email]

Your Comments:

*I am in favor of the railroad corridor from Point MacKenzie to north of Willow.*

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



[info@portmacrail.com](mailto:info@portmacrail.com)



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

Name

Tony DeLuca

Phone

495-3424

Address

PO Box 812 Willow 98688

Email

delucia@com.net

Your Comments:

I prefer the Willow route to take the heavy railroad traffic out of the middle of Willow. Restart access to Willow creek.

COMMENT FORM

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

COMMENT FORM

Name

MIKE Whedbee

Phone

907-892-5556

Address


450 Brazil Cirite Big Lake

Email


MGWhedbee@AOL.com

Your Comments: AFTER ATTENDING 3 meetings - IT APPEARS THE WILLOW ROUTE WILL BE THE LEAST DISRUPTIVE TO RECREATION, PRIVATE LAND OWNERS, AND THE DOMESTIC ENVIRONMENTAL IMPACT. THE WILLOW ROUTE WILL RESULT IN LESS POLITICAL NEGATIVE OR LEGAL LITIGATION POSSIBLE ON THE OTHER ALTERNATIVES. THE WILLOW ROUTE MAY CROSS MORE RIVERS & STREAMS, BUT IN THE LONG TERM THE COST OF THE BRIDGES WILL BE ABSORBED QUICKLY FIGURING THE 300 PERCENT SAVINGS. THE PRIVATE PROPERTY IMPACT WILL BE MINIMAL ON THE HOUSTON & BIG LAKE ROUTES. THE WILLOW ROUTE WITH PROPER CROSSINGS FOR TRAILS & HABITAT MAKES THE MOST SINCE GOING OVER STATE LAND AND FARM LAND WILL HAVE THE LEAST IMPACT. THE NOISE & VIBRATION IMPACT WILL BE MUCH LESS USING THE WILLOW ROUTE. THE WILLOW ROUTE WILL MINIMIZE EXPOSURE TO THE LAKES AND STREAMS AND FISHING IMPACT IN THE LAKES IN THE MATSUE BIG LAKE AREA & NADY LAKE AREA. THE FARWEST WILLOW ROUTE WILL HAVE THE LEAST IMPACT ON WILD LIFE HABITAT. IN CONCLUSION I FEEL THIS IS THE MOST BENEFICIAL FOR ALL TO BE ABLE TO GET THE RAILROAD COMPLETED WITHOUT MAJOR OPPOSITION.


The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

Name	Phone
Address	Email

Your Comments:

The 'red' Willow Proposal eliminates the need for an additional Road Crossing and seems to have lower impact on housing.

In addition the ground is marginally better for building a stable sub-base

This route may be shorter reducing the cost of base, ballast, ties and rails.

**COMMENT FORM**

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



IHDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name

Jim Huston Willow Chamber of Commerce

Phone

495-9000

Address

P.O. Box 38 Willow AK.

Email

AKH0ST@MTA.net or Live

Your Comments:

Many if not most of Willow residents are in favor of the Willow Corridor. We would like to move noise from increased traffic further west away from our homes.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name  
*ANNA L KEVINSON*

Phone  
*495-5494*

Address  
*PO Box 689 Willow 99688*

Email

Your Comments:

*It would seem efficient to combine the rail & a road corridor from Willow to Pt MacKenzie & then to bridge (for road) to Anchorage.*

*I'm definitely in favor of Willow corridor.*

*A. L. L.*

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



[info@portmacrail.com](mailto:info@portmacrail.com)



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name

Phone

Address

Email

Your Comments:

Do the  
Western Most Route

*MN*

Willow Western Route is my  
preferred route

*an*

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022





# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name

Judy Tymick

Phone

907 . 892 . 7846

Address

HC 31 Box 2596  
Wasilla, AK 99654

Email

tymick@yahoo.com

Your Comments:

- ① Very concerned about preserving local multi-use trails snowmachines + dog mushing. what kind of crossing will be provided.
- ② Big lake has suffered economically & has worked hard to rebuild after the lake wildfire. Railroad will decrease property values in this residential community.
- ③ ~~Had~~ ~~more~~ more info should have been provided about road plans & how they impact the community
- ④ I strongly prefer the willow route. I feel it is best long-term solution. Railroad will primarily move cargo & some tourists. Community needs & community access should cater to commuters transportation, not long-distance commercial.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

NOREEN AUSTERMULL

Phone

907 892 8256

Address

PO Box 521281 Big Lake 99652

Email

clonnor@gci.net

Your Comments:

THE WILLOW SITE HAS BETTER SOILS - will disrupt less families - open up land to the west. Thank you for your presentation. Everyone was polite informative. It was easy to follow AND maps were excellent.

Thank you.  
Noreen

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

V. K. [Handwritten Name]

Phone

(907) 727-5555

Address

517 W. 12<sup>th</sup> Ave




Email

vik@gol.net

Your Comments:

*I think the Willow corridor would have the least impact on full time residents*

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:

	HDR Alaska Inc. Port MacKenzie Rail Extension Comment 836 S. Colony Way, Suite 2 Palmer, AK 99645		<a href="mailto:info@portmacrail.com">info@portmacrail.com</a>
			Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



COMMENT FORM

Name

Jamie West

Phone

(907) 495-8544

Address

PO Box 534, Willow

Email

jamiw@po box, alaska.net

Your Comments:

I am not in favor of a rail spur into the Willow area. Willow is presently known for it's recreational value. The Willow spur seems more costly as well as being in conflict with the wishes of many of it's residents. The reason that my husband and I live here is that it is a recreational area. A rail corridor will interfere with that. For our family it is important that the ~~current~~ trail systems ~~are~~ <sup>be</sup> maintained in their current form.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Brenda Jager

Phone

688-8969

Address

Chugach & Willow

Email

bjager@mtnonline.net

Your Comments:

I just wandered the display table & noticed that at no point do they consider the "snowline" that divides Willow from the rest of South Central Alaska. The Willow area has a consistent snow base - unlike Houston & Big Lake. That is why snow machines, dog sleds, cross country skiers & snow shoes flock to the community.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Don Keller

Phone

907  
688-8969

Address

3112 S Bawney Lane, Chugiak, AK

Email

donk@1999@yahoo.com

Your Comments:

If willow area considered, please don't use below grade culverts esp. in Big Swamp area, need above ground / ~~grade~~ grade access across rail / tracks.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Kelley Griffin

Phone

373-1126

Address

HC35 Box 53357

Email

Your Comments:

99654

There are many trails crossing each of the proposed routes - (trails typically running east/west) my concern is that the railroad does not "consolidate" the many trails into just a few crossings.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

CLIFFORD DARNELL

Phone

892-7031

Address

P.O. Box 520955


Email

cdarnell@mtasoualaska.net


Your Comments:

I think the Big Lake Route would be the best for long term goals; as it would give other businesses along the route to bring in large loads of equipment, pipes, etc. Therefore the railroad would have hauling north as well as south (with over-etc)


The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022





# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Theresa Dean

Phone

495-6343

Address

PO Box 85 Willow

Email

theresacdean@yahoo.com

Your Comments:

Willow Creek Resort  
Please use the Big Lake Corridor.  
Willow doesn't need another railroad  
disabling it. We have a business,  
Camping + raft rental business on Willow  
Creek and this will be a very negative  
impact to our business. Use the existing  
railroad and ~~make~~ make it wider for  
2 tracks or <sup>use</sup> bypass tracks. What about  
Moose Kill. More railways means more  
~~more~~ moose killed. What about this &  
our loss of business, not fair! It will

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



IHDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portnacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

LANOE WILSON

Phone

Address

PO Box 940065

Email

lwilson@mtaonline.net

Your Comments:

Houston, AK 99694

I like the Houston South route. If the rail extension to Port Mac is going to be built, and it should be, I'd like to see the city of Houston get some benefit from the project. If Houston is able to ~~promote~~<sup>develop</sup> an industrial park, rail access would be critical to make it successful.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name  
*Diane Dusek*

Phone

Address  
*POB 521902-BL #99652*

Email

Your Comments:

*Do not approve of the Houston route (purple on map) - goes through too much development.*

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



[info@portmacrail.com](mailto:info@portmacrail.com)



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name

Christian Hartley

Phone

907-354-6984

Address

PO Box 8316, Willow AK 99716-8316

Email

hartley@myopinionmatters.org

Your Comments:

Why is the Houston North alternative given that name, when no ~~mentioned~~ section of that alternative touches Houston City limits, and ALL of it lies in the Willow boundaries? Should be Willow South.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Norman Wakeman

Phone

495 5267

Address

PO Box 928, Willow

Email

mwakeman@mtc-online.net

Your Comments:

How convenient that the borough did not give the planners the Willow Trails Plan which has been submitted for addition to the borough trails plan. This done in June 2006.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

L. JACOBSON

Phone

495-3356

Address

P.O. Box 343 Willow

Email

Your Comments:

What ever decision is rendered - The  
LEAST IMPACT ON THE MOST PEOPLE  
SHOULD BE THE SOLE CONSIDERATION  
OTHER THAN COST AND PRACTICALITY!

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

COMMENT FORM

Name

Kathleen Himmelright

Phone

907-349-6294

Address

7510 Solarset Cir Anch. Ak. 99507

Email

himmels@alaska.net

Your Comments:

My husband and I purchased 10 acres of land on west lake in the hopes of building our retirement home and someday having a peaceful retirement in a quiet area in the state we have grown up in and lived. we are almost at completion of our home.

we now understand that this rail proposal in the big lake area will come right down our property and road to this home we have worked so hard to build and looked forward to enjoying.

During the years we have ~~to~~ investigated our land and the surrounding area we have seen numerous wild life, muskrats, we have used the numerous snowmachine trails and enjoy the returning swans and loons on the lake each year.

This will all be impacted by this as will the value of our land.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Sven Haltmann

Phone

495 22 05

Address

Po Box 882

Email

sledolog@mtaonline.net

Your Comments:

Mr and my wife live at Mile 72.8 where one of the proposed corridors crosses the Parks Hwy and with that our property. By moving the line appr. 600-700 feet south, our property would be left intact and with that our lifestyle we choose to live. (Dog kennel, tour business)

I would appreciate your consideration and like to thank you for your time

*Sven and Audrey Haltmann*

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022





# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name

Address

Phone

Email

Your Comments:

*As a 55 Year Resident  
I believe that the least environmental  
Damage, would be to follow the  
KIMIK Goose Bay Road where there  
is already a transportation foot print  
This route would be the most  
economical also*

*Pierre Strong*

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Mary Suresnes

Phone

Address

Willow

Email

Your Comments:

Access to the Trails Map for the Willow area can be accessed at

www.waco-ak.org  
- Trails

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

William C Jacobson

Phone

907-301-7682

Address

2221 Duvoy CT ANC AK 99502

Email

Homesteadake@Auc.com

Your Comments:

I believe Any or North of Big Lake Routes, would cause MAJOR Problems with the snow machine activities. I believe there will be Lost of Life.

I Am sure There will be ADDITIONAL ROAD Just to move the VAST AMOUNT OF Fill that will be needed. This will ALSO create LARGE PITS.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name

Mary Shreves, P.E.

Phone

Address

Willow

Email

mary@matadonline.net

Your Comments:

Attached is letter from  
Mat-Su Stodd Park  
Citizens Advisory Board

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name

Mary Shreve

Phone

Address

Email

Your Comments:

Willow community council (WAC) letter to MSB re: rail extension through Willow

Filed under letters received.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022

February 13, 2007

John Duffy, Manager  
Matanuska Susitna Borough  
350 E. Dahlia Ave.  
Palmer, AK 99645

RE: Public Notice MSB004842; the Willow to Pt. McKenzie Railroad Corridor

Dear Mr. Duffy,

The Mat-Su State Park Citizen's Advisory Board wishes to respond to MSB Public Notice MSB004842 regarding the Willow to Pt. McKenzie Railroad Corridor Easement. The Board has a special interest in this corridor because of the direct impact to the Willow Creek and Nancy Lake State Recreation Areas and the West Gateway Trail System, a popular winter recreation area.

Because the board felt this railroad location is incompatible with the mission of State Parks and Outdoor Recreation and when there are viable alternatives, the Mat-Su State Parks Citizens Advisory Board voted unanimously to oppose the acquisition of easements for corridor three.

There are few places in Alaska or the United States where there is such a wealth of recreational opportunities so close to an urban area as there are in Willow. It is uniquely Alaskan recreation: snowmachining, dog mushing, skiing and ski-joring, hunting, fishing, hiking and ATV riding over miles of open areas, maintained trails and parks. A 2004 survey by Willow2020 and the Mat-Su Borough found that over 90% of respondents chose Willow as the place to live and recreate because of the open country, beauty and quiet. (The survey can be seen at [www.waco-ak.org](http://www.waco-ak.org)).

The 2003 Rail Corridor Study by Tryck Nyman Hayes, Inc. vastly understated the importance of recreation in the study area, barely mentions two very popular state recreation areas and does not mention any mitigation measures for the damage or impacts from the rail line. For example it states on p. 49, "*The project [Corridor 3] would be expected to have some direct impacts on recreation, especially trail use and limiting access to recreation sites particularly if mitigation measures such as below and above ground crossings over trails for example are not utilized*".

A flagrant omission by the borough in their public notice requirements is its notification and consultation with State Parks. There was no consultation prior to the corridor three recommendation and, as with the Willow community, no mailings or local meetings after corridor three was an option. Since public comment was the basis for its recommendation and almost all comment came from a different community, the conclusions are biased and unrepresentative of public opinion.

The three popular recreation areas severely impacted by the corridor are the following:

### **Willow Creek State Recreation Area**

Home to international renowned salmon and trout fishing, corridor three will be the second railroad line to cross Willow Creek and bisect the recreation area's 8 square miles. The disruption of a rail line would dramatically effect natural and esthetic resources people expect from a state recreation area. There are no possible mitigation measures for "surround sound" trains. The railroad line will cross Willow Creek, a recognized RS2477 trail, the Lucky Shot Trail and the main road to the SRA, all within a half mile. This scar will open up and expose the heart of the SRA.

Safety issues will heighten as the public will have easy access to the tracks. This area already has problems with visitors walking on and fishing from the existing railroad trestle. Recently, a child was killed by a train after fishing from the tracks.

Again, the fact that there was no consultation with State Parks in regards to corridor three bisecting the Willow Creek SRA is unfathomable.

### **Nancy Lake State Recreation Area**

This popular recreation will be impacted by the rail line running adjacent to its western boundary and will have similar recreational compromises as those in the Willow Creek SRA. The 2003 study states the rail line will follow the lateral moraine adjacent to Red Shirt Lake which will bring it very close to recreational cabins. In contrast, the borough public notice places the rail line in the Big Swamp wetlands. Either option is bad news for resource and recreational protection.

### **West Gateway Trail System**

This important winter recreational area supports one hundred miles of groomed and community maintained trails. All the trails have easements, are included in the Mat Su Trails Plan and are considered "regionally significant." Any given winter weekend will see hundreds of trail users exploring this semi wilderness area. A railroad bisecting the Willow and Big Swamps will compromise this Alaskan experience.

The West Gateway Trails support many races including the Iditarod Restart, Klondike 300, Don Bowers 300 and the Junior Iditarod sled dog races and the Klondike 400 and Aurora 200 snowmachine races. Since much of the area is wetlands, trail/rail crossings will be few and expensive, thus these races will find other venues. These same trails support a vital training for such Iditarod teams as from Martin Buser, Dee Dee Jonrowe and many others.

There are other viable alternatives for a Pt. MacKenzie rail corridor that need to be researched in more detail that was done in the 2003 study. Corridors four and five will have less impact on important recreational areas. Before easements are acquired more research needs to be done on costs and social/economic impacts rather than assumptions and unrepresentative public comment.

Tourism and recreation in our state are dependent on the Alaska mystic of wilderness and adventure. The proposed Willow to Pt. McKenzie Railroad Corridor will drive a stake through those visions and dreams. The Mat-Su State Parks Citizen's Advisory Board recommends for the borough research other options before acquiring easements on corridor three.

Sincerely,

Mary Anderson, chair  
Mat-Su State Parks Citizens Advisory Board



*(This page intentionally left blank.)*



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



COMMENT FORM

Name

Joan Bryner

Phone

907 495 6253

Address

P.O. Box 297 Willow AK

Email

jbryner@mtaonline.net

Your Comments:

The whole idea of a spur is a bad idea, it seems the card is being put before the horse. Before even considering such a project the environmental studies ~~are~~ should first be made and the money necessary should be budget and to a great part required. It sounds like a spur really isn't necessary because the port of Bethel already exists. Mat Su Borough is wanting to transport goods that aren't even necessary at this time. For example coal for agriculture plant in Kenai which just shut down. Or wood chips from the Willow area <sup>the project is</sup> which is currently going through legal proceedings. As for a spur to Willow, ~~that~~ it would ~~definitely~~ be very detrimental to the area. This is a community well known for being a recreational area. A spur to Willow would

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
 Port MacKenzie Rail Extension Comment  
 836 S. Colony Way, Suite 2  
 Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022

Cut through an entire system of  
Trails including the area where Tree Island  
Restart has taken place for the past 5 years.  
The whole area is wetlands. We do not  
want more wetlands destroyed.

Bad Bad idea - ~~any~~ <sup>spur</sup> Willow  
does not want it



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Jeff Fuller

Phone

495-8771

Address

P.O. Box 197, Willow

Email

Your Comments:

I am opposed to the willow corridor railroad proposal largely because this corridor will affect our way of life. The corridor goes through our prime recreational areas (Willow Creek Park). It exposes our community to more hazardous materials. It will have an environmental as well as a negative economic impact on our community.

If this corridor is built where will people from the valley as well

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022

as Anchorage come to recreate and  
reconnect with the natural  
environment. The cost to Willow  
is too great, the cost too great!!  
I vote no!!!



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Ellen Wolf

Phone

Address

PO Box 371 Talkeetna, AK 99676

Email

ellenmwolf@yahoo.com

Your Comments:

~~Of course~~ I do not support any rail extension project from Port MacKenzie because I ~~do~~ have not supported the "build it and they will come" project from its inception.

The Willow and Houston North routes ~~are~~ should be eliminated for many reasons including the fact that they cross the Little Su and Nancy Lake State Recreation Area. All three routes look like

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022

they will COST too much  
to warrant ~~the~~ building them.

mark me in the  
"No Action Alternative"  
box even before any  
EIS occurs.



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

COMMENT FORM

Name

THOMAS BUREK

Phone

495-5310

Address

25129 W. Make Lane

Email

trailside@akcentr.org

Your Comments:

willow

Hello,

My name is Thomas BUREK and my family and I live at mile 73 of The Parks Hwy. I want to submit for record that I disagree with the willow route as it sits in my backyard. The impact to my family and other residents affected by this proposal is immense. By choosing this route the ARB spur will interfere with valued aspects of rural living, for example recreational values like trails, quiet + solitude, many homes will be displaced and dreams destroyed, and property value will be destroyed. In closing I once again oppose the rail spur through willow and its impact on residents and recreation users. The excessive impacts to the environment, primarily wetlands and rivers, I am also concerned that the cost to the government and (over)

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
 Port MacKenzie Rail Extension Comment  
 836 S. Colony Way, Suite 2  
 Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



taxpayers will exceed economic return.

Finally along with the majority of willow residents, I oppose the acquisition of the Willow Rail spur.

Sincerely



THOMAS BUREK

Willow

495-5310

PO Box 1156

Willow, AK 99688



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name

Shirley Wakeman

Phone

795-5267

Address

P.O. Box 988 Willow AK 99688

Email

shirleywakeman@mtaonline.net

Your Comments:

What about looking @ this route?

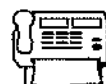
The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



IIDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022

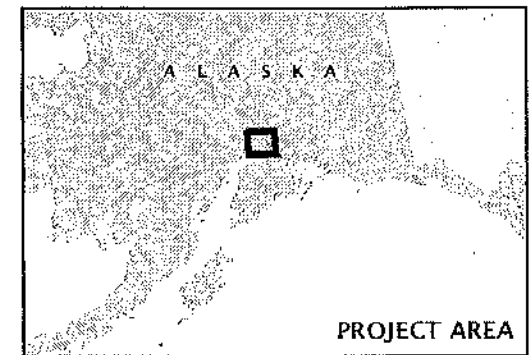
*(This page intentionally left blank.)*

# TRAILS

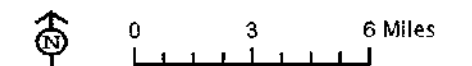


## LEGEND

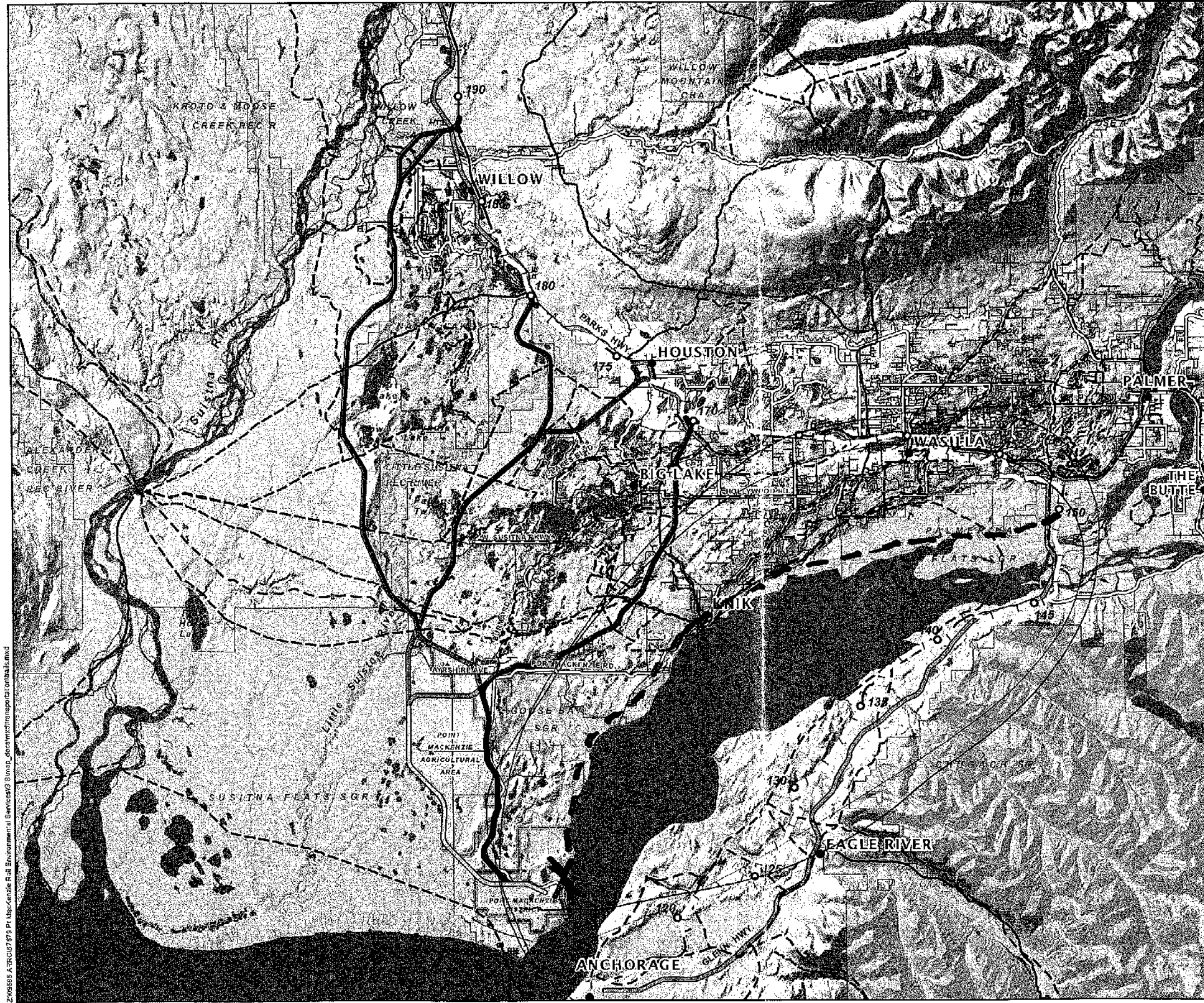
- |                                  |  |
|----------------------------------|--|
| <b>Preliminary Alternatives*</b> | ○ ARRC Milepost                        |
| — Mac East                       | —+— ARRC Track                         |
| — Mac West                       | — RS2477 Historic Transportation Route |
| — Conn 1                         | — MSB Trails                           |
| — Conn 2                         | — Iditarod Trail                       |
| — Conn 3                         | — Highway                              |
| — Houston                        | — Medium Rd.                           |
| — Houston North                  | — Minor Rd.                            |
| — Houston South                  | — City Boundary                        |
| — Willow                         | — Park or Refuge                       |
| — Big Lake                       |  |
- \*These lines generally represent corridors which are subject to future refinement.



This map represents a conceptual level of utility, detail, and accuracy. The information displayed here is for planning purposes only. Base information shown constitutes data from various federal, state, public, and private sources. These maps are for review purposes only and are not intended for use in securing permits, design or for construction purposes.



Date: September 30 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ARRC, HDR Alaska, Inc., MSB GIS, INH- Hanson, USGS.



*(This page intentionally left blank.)*



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

DEAN W. DAVIDSON

Phone

495-1112

Address

P.O. Box 954

Email

NINADEAN@MTAONLINE.NET

Your Comments:

WILLOW IS BECOMING MORE AND MORE A RECREATION CENTER FOR FOLKS FROM ANCHORAGE AND THE VALLEY. WILLOW'S 3-TRAIL SYSTEMS ARE THE FINEST TO BE FOUND IN S.C. ALASKA, AND ARE HEAVILY USED BY SNOWMACHINERS, SKIERS, AND JOE MUSHERS. I FEEL THAT A RAIL CORRIDOR THROUGH THE WILLOW WEST GATEWAY TRAIL SYSTEM IS NOT COMPATIBLE WITH THAT RECREATIONAL PUBLIC USE, AND FOR THAT REASON, I OPPOSE THE WILLOW CORRIDOR OPTION.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Heidi Jenkins

Phone

907-495-6562

Address

P.O. Box 171, Willow, AK 99688

Email

hladv@mtaonline.net

Your Comments:

*It is not feasible to build such an expensive project through wetlands. This Railroad Corridor would have a very negative impact for the wetlands, recreational Parks, Fish-Habitat, Wildlife and on all the very important recreational Trails. These are used by Dogmashers, Snowmobilers and Residents who live out there, and these trails need to be protected.*

*Most residents of Willow have chosen to live here because of the quality of life, together away from noise and environmentally unsafe conditions.*

*Quite a number of businesses in Willow depend on making their money with Tourism, like guided Tours.*

*Also I do not think that Willow residents would ~~have~~<sup>get</sup> any advantage out of this project.*

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
 Port MacKenzie Rail Extension Comment  
 836 S. Colony Way, Suite 2  
 Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name

MARIE SOROKA

Phone

4952627

Address

PO Box 849 WILLOW

Email

SOROKA@MTAONLINE.NET

Your Comments:

I OPPOSE THE WILLOW ROUTE FOR THE PROPOSED RAIL EXTENSION THE WILLOW ROUTE IS SWAMP AND A RECREATIONAL AREA. THE BIG LAKE + HOUSTON ROUTES LOOK MORE DIRECT.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portnacrail.com



Fax (907) 644-2022





# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Yvonne Leuthwiler

Phone

Address

POB 515 Willow AK 99688

Email

Your Comments:

When planning the best route, pls. see the elephant in the room: the "willow corridor" not only passes through large areas of recreational activities + plant- + wildlife habitat, it is also the longest route going through the wettest areas. How can it be feasible to lay a track along such a route?

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Dana Hills

Phone

495-8771

Address

Box 1062 Willow, AK 99688

Email

dhills@alaska.pacific.edu

Your Comments:

I do not want the rail line going through Willow. I feel that if it were to be put in Willow that there would be a great impact on the environment as well as ecotourism and recreational impacts. As Wasilla grows more + more residents of Wasilla as well as Anchorage rely on the Willow area for recreation.

My preference is to run the new line next to the existing rail line, less impact though I am sure more costly. I personally do not want Alaska to be like the lower 48, let's make informed, responsible decisions concerning growth + development.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name

Eric Miknich

Phone

(907) 344-8587

Address

12000 Gillelke Dr. Anch, AK 99516

Email

EJmik@acsalaska.net

Your Comments: (property owner of 5 acre parcel on Hook Lake).

I oppose the willow corridor due to concerns of impact on wildlife habitat and recreational opportunities. This corridor will criss-cross many regularly used trails and create numerous safety issues.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portnacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Brenda Jaeger

Phone

688-8969

Address

21128 Bowery, Chugiak

Email

bjaeger@MTAonline.net

Your Comments:

4 Shoen Drive, Willow

We need to have our community values / lifestyle given value on a par with land value, economic benefit, etc. The Willow community is focused on active, outdoor lifes. We are not in Willow to make a fortune. We are here to live a rich, authentically Alaskan life.

Brenda

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name

Wally Soroka

Phone

495 2627

Address

Box 849 Willow Av-

Email

SOROKA@MACKENZIE.NET

Your Comments:

I'm opposed to the route through Willow (w of Redshirt Ik)

Route should remain on existing RR tracks/right of way, and create absolute minimum amount of new track/right of way.

Willow route will be detrimental to willow's recreational areas and to the people who live in Willow now and in the future.

Absolutely no benefit to us in Willow.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
 Port MacKenzie Rail Extension Comment  
 836 S. Colony Way, Suite 2  
 Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Donna Quante

Phone

495-3251

Address

17438 N. Heights Dr Willow

Email

Your Comments:

NO - not thro Willow  
I don't see why you're spending the money for an extension anyway. These are empty promises for future wealth.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022

Rec. 10/19/07



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

BRIAN J TWOTHY

Phone

907 373 3522

Address

26990 W HOLSTEIN  
WASILLA 99654

Email

Your Comments:

I WISH TO BE ON RECORD AS PREFERING THE 'MAC EAST' ROUTE IF THIS PROJECT COMES TO FRUITATION.

THE EASTERN ROUTING WOULD CONFINE THE INDUSTRIAL TRAFFIC, NOISE + POLLUTION TO A DEFINED, EASILY ACCESSABLE LOCATION.

I WILL VEHEMENTLY PROTEST ANY ACTION TO ACQUIRE RIGHT OF WAY WITHIN THE A9 TRACTS BY THE USE OF EMINENT DOMAIN POWERS, AND CAN BE COUNTED ON ASSIST FINANCIALLY AND THROUGH THE COURTS TO STALL THIS PROJECT IF IT ENTAILS THE TAKING OF PRIVATE PROPERTY.

THANK YOU

*[Signature]*

BRIAN J TWOTHY  
10/14/07

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



# COMMENT FORM

Name

E. H. PETRAEUS

Phone

907.223.2555

Address

P. O. BOX 241843

Email

N/A

Your Comments:

ANCHORAGE, AK 99524

① ATTENDING THE MEETING WAS INFORMATIVE

② LOOKING TOWARDS DEVELOPING MY LAND/WOULD BE TO CLOSE TO THE PROPOSED BIG LAKE EXTENSION. THE LAND IS GOING TO BE IMPACTED BY SHAKING AND NOISE (CLOSE TO)

NO TO THE EXTENSION BEING PROPOSED THERE.

③ NORTH TO WILLOW MORE LOGICAL ROUTE

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska, Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 200  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022

THANKS / PETRAEUS





Rec 10/24/07



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

**COMMENT FORM**

Name

Karen Mattson

Phone

495-6636

Address

PO Box 233; Willow, AK

Email

kamakass@hotmail.com

Your Comments:

I am in favor of the westerly route of the rail extension, because

1. All heavy rail traffic would not be going through Wasilla, Meadow Lakes and Willow.
2. Potentially even reduce heavy truck traffic on the Parks Hwy, therefore reducing road maintenance.
3. Reduce noise in populated areas.

Note ... when speaking to residents in Willow, friends and neighbors only have one concern and it's the "me" factor. How will it affect their lifestyle. They are missing the overall picture that without a railroad, Willow may not exist as we know it today. Progress is GOOD!

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
 Port MacKenzie Rail Extension Comment  
 836 S. Colony Way, Suite 2  
 Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.

COMMENT FORM

Name

B & B FARMS (BASKIN)

Phone

373-6167  
376-7104

Address

GUERNSEY RD Pt. MacKenzie

Email

lyw-decam15@hotmail

Your Comments:

The proposed route West of the Agriculture Lands and east of the game refuge is our concern:

- ① it runs N-S the ENTIRE length of our farm
- ② it runs through our irrigation system (1/4 mile <sup>center</sup> pivot)
- ③ runs through established and productive hay fields
- ④ B & B FARMS has been established since 1982
- ⑤ The N-S Run separates our Home from the main farm operations
- ⑥ B & B FARM HAS CONTRIBUTED millions of dollars to the economics of the borough and STATE over the last 2.5 years
- ⑦ the route following Port MacKenzie Rd is our preferred route, even though we have a farm which would be impacted with that route, \*However, a less impacted farm in comparison.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022



# Port MacKenzie Rail Extension Project

Your input is important to the Matanuska-Susitna Borough and the Alaska Railroad as we pursue the Port MacKenzie Rail Extension project jointly.

Please write legibly (printing is appreciated). Attach additional sheets if necessary.



**COMMENT FORM**

Name

LoRei & Margaret Heaven

Phone

907-376-5679

Address

Hc 31, Box 5113  
WASILLA, 99654

Email

lomarhea@hotmail.com

Your Comments:

1. Thank you for doing the presentation for us.
2. We think the RR should use the East Mackenzie choice - of course we haven't heard the presentation yet and we don't live near PT Mackenzie, so we don't have a lot of say in any of it.
3. We think the Big Lake area is too congested already. It looks like coming out at or near Houston would be a good option (at <sup>Houston, No</sup> MP 180) especially if you expect to develop any commuter rail service.
4. We like the idea of underpasses for the trails.
5. Next meeting you have in a gym, have them turn the fan off when you start talking.

The Mat-Su Borough and Alaska Railroad welcome your input. Please send comments to:



HDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645



info@portmacrail.com



Fax (907) 644-2022

# LETTERS RECEIVED

*(This page intentionally left blank.)*

**Alcantra, Rosetta M.**

**From:** Stephenie Wheeler [wheelers@akrr.com]  
**Sent:** Wednesday, December 26, 2007 2:41 PM  
**To:** Pippel, Tony  
**Cc:** Alcantra, Rosetta M.; Brad.Sworts@matsugov.us; Lindamood, Brian; Sullivan, Patty  
**Subject:** Re: Port McKenzie Rail Extension Project  
**Follow Up Flag:** Follow up  
**Flag Status:** Red

Anthony -

Thank you for your comment. We will include your comment in our package of public input, which will be shared with the Surface Transportation Board once the application is made and the STB takes over the environmental phase of the project. The STB will organize its own public involvement effort in conjunction with the NEPA-required environmental work.

If you would like to be added to the mailing list to receive project updates, including STB public involvement events, please go to the project web site and sign up - go to [www.portmacrail.com](http://www.portmacrail.com) then click on "Get Involved" and then "Join the Mailing List".

We appreciate your interest and local perspective on this important project.

Regards,

Stephenie Wheeler  
Public Involvement Officer  
Alaska Railroad  
265-2671

>>> "Tony Pippel" <sayhey@mtaonline.net> 12/26/07 11:19 AM >>>  
Ms. Wheeler

My name is Tony Pippel. I am a lifelong resident of Palmer, and am active and aware of local happenings and politics. I have served on the Palmer City Council for ten years.

DISCLAIMER: My family has owned recreational property on Horseshoe Lake for fifty years. We have an old, modest cabin on 4.5 acres that we use 5-6 times a year.

My greatest fear on this project is that we will, once again, reject the best LONG\_TERM alternative, Willow, and select the cheapest to build, Houston South. Short-term solutions create long-term problems, but are less expensive to construct. We little value long-term consequences once we get into the project frenzy.

I have two basic objections to the Houston South route, both of which are addressed by the Willow route. First, why would we build a major rail corridor through a rapidly developing RESIDENTIAL area, when the Willow route runs through much less occupied territory? Horseshoe lake/West Lake is quickly morphing into Hillside Anchorage on the lake. This area has always been noted for its relative quiet, peaceful nature. To run a major transportation corridor so near to all these people doesn't seem right, and can only create more future problems.

Second, the construction method proposed would significantly, and negatively impact local water flows. Although I am no expert in the field, the significant blockage and rerouting of water flows doesn't strike me as a good thing. Most of these concerns are better addressed in the Willow route.

Whoops, there are three concerns. The third concern is the use of the corridor, and its usefulness for opening new land. Much of this land is Borough land, so they should have an interest, most things being equal, in selecting the Willow route, as it would provide new access to a whole new area. The Houston South route would open up relatively little new land, as it runs through areas that already enjoy access.

I strongly urge all the decision makers on this project to think long-term, to consider lifetime project costs, not only in dollars, but in less easily quantifiable values of peace, quiet, economic development potential, damage to watersheds, etc. I urge them to resist the lure of the cheap and easy, and do it right the first time. Lastly, I would like for them to consider this: If it was your personal project, would you half-ass it, or build it right?

I appreciate the opportunity to comment.

Daniel B. Robertson  
11301 Snowline Drive  
Anchorage Ak, 99507

December 26, 2007

Re: Point McKenzie Railroad Extension

I am writing in opposition to the railroad segment denoted as Conn 3 on figures 1 and 2 below.

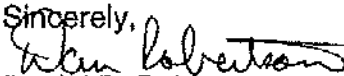
I am an owner of one the agricultural parcels that will be adversely impacted by this route, specifically the half section of land northwest of the Ayrshire and Farmers road intersection (see figure 2). I was not notified by the Matanuska Susitna Borough or the Alaska Railroad of the planning process or public meetings regarding the rail extension, but eventually found out via word of mouth from other disgruntled landowners in this area.

The Carpenter Lake area indicated by the red oval on figure 2 has a number of year round and recreational homes and cabins, small farms, and at least one small business requiring large undeveloped acreage. It is a relatively quiet semi rural area. Carpenter Lake public access provides a nice picnic and fishing area.

A railroad routed through this area is inconsistent with the current area land usage. The noise, emissions, dust, possibly herbicides, etc, associated with a railroad would destroy the aesthetic rural nature of the area. Land values seem likely to decline far below whatever right of way reimbursement might occur. Small businesses in the area would likely be devastated by the Conn 3 route, forcing these people out of business. The maps available via the portmacrall website do not even seem to indicate that there are agricultural parcels impacted by the Conn 3 route.

There seem to be several other viable alternatives. I would strongly prefer that some route other than Conn 3 be selected.

Please forward this letter to the Surface Transportation Board for consideration.

Sincerely,  
  
Daniel B. Robertson  
907-346-3714

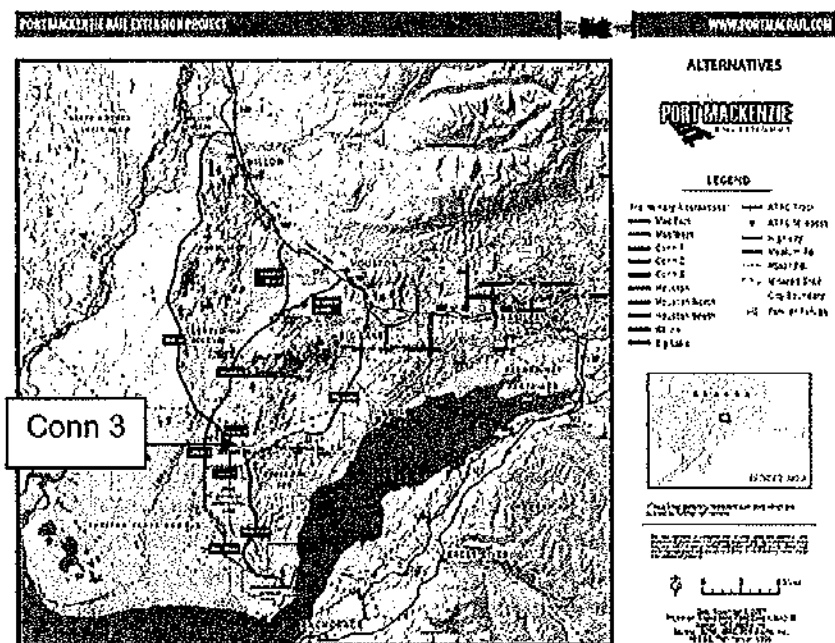


Figure 1. Point McKenzie railroad extension alternatives

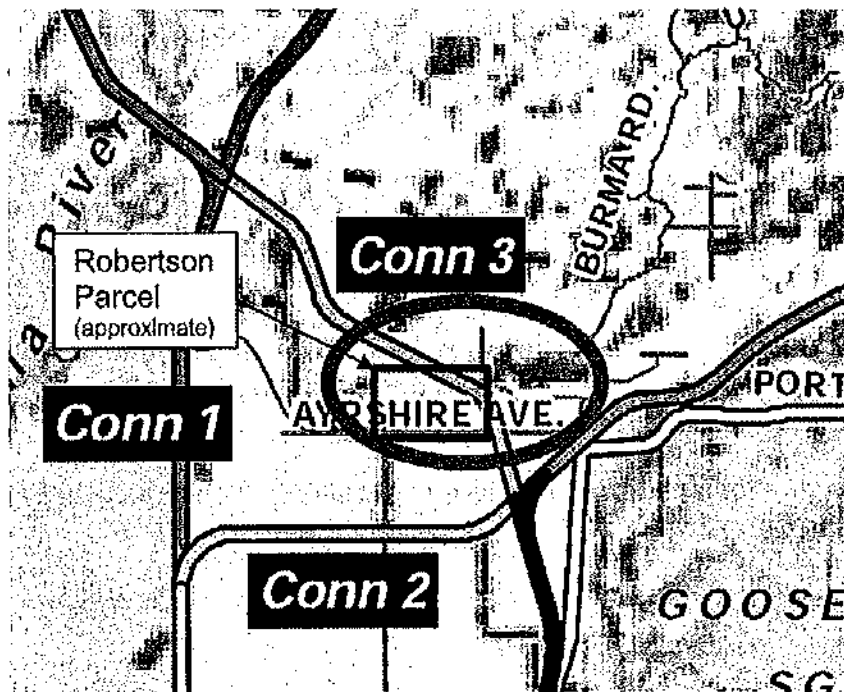


Figure 2. Conn 3 route noise, dust, emissions will adversely impact many private homes, cabins, small farms, businesses in the area indicated by the red oval.



*(This page intentionally left blank.)*

**KNIKATNU, INC.**  
P.O. Box 872130  
WASILLA, ALASKA 99687-2130  
PHONE: (907) 376-2845 FAX: (907) 376-2847  
knikcorp@gci.net

December 20, 2007

Brian Lindamood, P.E.  
Alaska Railroad Corporation  
PO Box 107500  
Anchorage, AK 99510-7500

Re: Port Mac Rail Extension Route

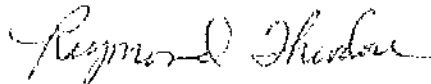
Dear Mr. Lindamood:

This letter is in regards to the Port Mac Rail Extension Presentation you gave at our November 3, 2007 Board of Director's Meeting. There are a few proposed sites running from Houston to Point MacKenzie that are on Knikatnu, Inc. property and you had inquired about what route the corporation would favor for the extension.

At our Board of Director's Meeting held on December 13<sup>th</sup> the Board decided to support the Houston South Route. This route was chosen because we think it would have less of an impact on our archeological and historical sites, as indicated by a map provided to us by Fran Seeger-Boss of the Mat-Su Borough.

Please contact the office at the above number if you have any questions or need further information. Thank you for giving us the opportunity to respond to this issue.

Sincerely,



Raymond Theodore  
President

**Alcantra, Rosetta M.**

---

**From:** Alcantra, Rosetta M.  
**Sent:** Wednesday, December 26, 2007 11:18 AM  
**To:** Patty Sullivan; Alcantra, Rosetta M.; Stephenie Wheeler  
**Subject:** 12-22-07 Steve Campbell

-----Original Message-----

**From:** [mailto:srctac7@hotmail.com]  
**Sent:** Saturday, December 22, 2007 7:33 AM  
**To:** Public\_Comment@akrr.com; info@portmacrail.com; Alcantra, Rosetta M.; Longtin, Wendy; info@homestead-graphics.com  
**Subject:** Port MacKenzie Comments

Below is the result of your feedback form. It was submitted by  
(srctac7@hotmail.com) on Saturday, December 22, 2007 at 07:32:32

---

name: Steve Campbell

address: Delroy Road

city: Houston

state: AK

zip: 99687

phone: 892-8206

emailconfirm: srctac7@hotmail.com

emailconfirm2: It makes sense to open more areas with an eye toward mitigation of any negative effects, since AK has a somewhat limited transportation infrastructure. The project should be inclusive and show foresight that integrates anticipated development along the lines. There should be twin tracks laid for unobstructed 2 way bidirectional, travel, all crossings should be isolated from other transportation systems via bridge or tunnel, infrastructure for running utilities that can be used for electric needs of future rail and small communities should be integrated into the project. This should be a piece of a complete plan, and governed by needs/efficiency, that is not controlled by one private entity, but rather a part of a holistic and team approach to area development with an eye to the future.

button: Submit

---

HTTP USER AGENT: Mozilla/4.0 (compatible; MSIE 7.0; Windows NT 5.1; YComp 5.0.0.0; MSDigitalLocker; .NET CLR 1.0.3705; .NET CLR 1.1.4322; .NET CLR 2.0.50727)

## Memo For Record: Project Comment

FROM: Marc Lind, resident of Chugiak with property owned in Willow

TAKEN BY: Stephenie Wheeler, Public Involvement Officer, 265-2761

DATE: December 21, 2007

RE: Comment on the Port MacKenzie Rail Extension Project

Mr. Lind expressed the following comments verbally:

I got a newsletter in the mail, and I want to comment on the Port MacKenzie Rail Extension project. I don't do computers, and so I want to provide my comments over the phone. I don't want this rail project to come up to the Willow area. I hear that the people in Houston do want it. There would be lower impact on the environment going into Houston. You wouldn't go across as many swamps. The Willow area route comes through more swamp land that is also a moose wintering ground. If the train came through here, there would be a higher number of moose kills in the winter time by a train. The Willow route would cost a lot more, too, because it is all swamp past Red Shirt Lake. I have property in Willow and will be building a house on it in the near future. I don't want to hear the train on either side of me all night long.

Marc Lind, Phone: (907) 688-4515

I want to be on the project mailing list.

P.O. Box 670182  
Chugiak, AK 99567

### NOTE:

Wheeler assured Mr. Lind that she would pass along his comments to the project team. She also encouraged him to participate when the STB started its public involvement process. Wheeler noted that the STB will likely prefer written comments, as opposed to taking comments over the phone, and that Mr. Lind may consider writing a letter to convey his opinions to the STB. In the meantime, the ARRC-Borough project team will forward to the STB any public comment received, as well as our mailing list of folks interested in getting project updates and announcements.

## **EMAILED RESPONSE BACK FROM MR. SHARROCK 12/21/2007**

Thank you very much Stephenie. From my review of the STB web site it appears that I need more numbers added to the number you gave me. The docket numbers that I have viewed show a 2 digit front number and a single digit last number, even if it's a zero. However, I'll try searching the number you gave me. I'll let you know if I'm successful.

Regards and Merry Christmas

Pat

## **EMAILED RESPONSE, FOLLOWING COORDINATION WITH BRIAN LINDAMOOD 12/21/2007:**

----- Original Message -----

**From:** [Stephenie Wheeler](mailto:Stephenie.Wheeler)

**To:** [patsharrock@alaska.net](mailto:patsharrock@alaska.net)

**Cc:** [Lindskoog, Wendy](mailto:Lindskoog,Wendy) ; [Thompson, Tim](mailto:Thompson,Tim)

**Sent:** Friday, December 21, 2007 5:07 PM

**Subject:** STB Docket Number for Port Mac Rail

Pat -

This is what I have found out. The Alaska Railroad DID file for a waiver of the 6-month notice stipulation with the STB. Due to this filing, the STB has assigned the project a number - 35095.

Just to clarify, we have not filed anything other than the waiver request.

In spring, we intend to file an application for a license to construct and operate a rail extension from Port MacKenzie. Once this application is received, STB will in turn file an Intent to perform the necessary environmental study (EIS or EA). I just wanted to keep the various "filings" straight... mostly in my own mind!

Hope this is what you were looking for.

Have a great Christmas holiday!

Regards,

Stephenie Wheeler  
Public Involvement Officer  
Alaska Railroad  
265-2671

## **ORIGINAL INQUIRY, 12/21/2007:**

Stephenie Wheeler received an inquiry from Patrick Sharrock on December 21, 2007, via phone, 345-5353, requesting the STB Docket Number for the Port MacKenzie Rail Realignment.

**Alcantra, Rosetta M.**

---

**From:** Patty Sullivan [Patty.Sullivan@matsugov.us]  
**Sent:** Wednesday, December 19, 2007 4:46 PM  
**To:** Alcantra, Rosetta M.  
**Subject:** Fwd: Railway Extension to Willo.

Patty Sullivan  
Public Affairs Director  
Mat-Su Borough  
[psullivan@matsugov.us](mailto:psullivan@matsugov.us)  
(907)745-9577

Begin forwarded message:

**From:** "Demboski" <[demboski@mtaonline.net](mailto:demboski@mtaonline.net)>  
**Date:** December 18, 2007 6:08:25 PM AKST  
**To:** <[Patty.Sullivan@matsugov.us](mailto:Patty.Sullivan@matsugov.us)>  
**Subject:** **Railway Extension to Willo.**

Mrs Sullivan,  
I am writing you to voice my opinion on the Railway Ext to willow. As a long time land owner in the area that this is not the best plan. According to the proposed route the railway would run through a very highly used area for recreation and Hunting. The swamp area that has been proposed has a great Moose population and is a source for me and my family of our Moose for the year. We have been hunting that area for 20 years. This would cut through the middle and ruin the hunting area. The winter Moose kill along the tracks would be of concern as well. There is already tracks on the Hatcher pass side of the Highway and I see no reason to run the tracks through Willow. The Houston or big lake route would make much more sense. If you have any concerns or questions I can be reached at the Numbers listed below.

Respectfully,  
Ben Demboski  
907-688-2671  
907-301-9177

Noise At Crossings Presented by Larry Johnson 12/11/07 M.



# Train Whistle Noise Reduction Systems

# PROJECT FACTS

## Project Scope

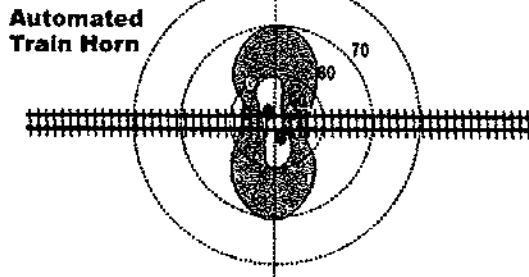
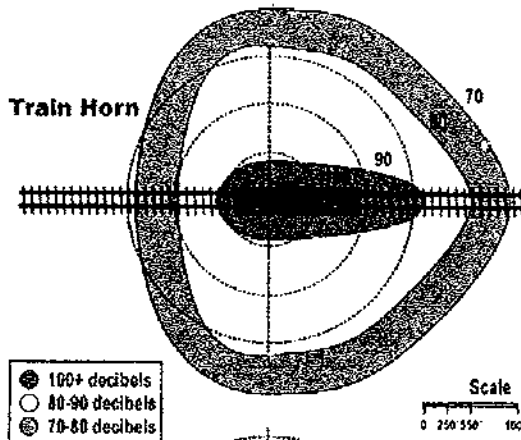
Federal regulations and Alaska Railroad rules require trains to sound their horns when approaching a public crossing and to continue sounding the horn until the crossing is reached. In June 2005, the Federal Railroad Administration (FRA) began enforcing a new rule requiring train engineers to sound the horn at least 15-20 seconds before the crossing and to continue sounding the horn until the crossing is reached. Given significant consequences for not complying (monetary fines to the railroad and suspension of train crews), train engineers have no choice but to blow the horn as required — day and night.

Anticipating potentially greater noise disruption to railbelt communities, ARRC began testing whistle noise reduction systems at two crossings in Anchorage in 2004. This technology, which is in use in other locations in the U.S., is designed to reduce noise from train whistling. The test determined that at least two whistle reduction systems can operate reliably in the Alaska environment.

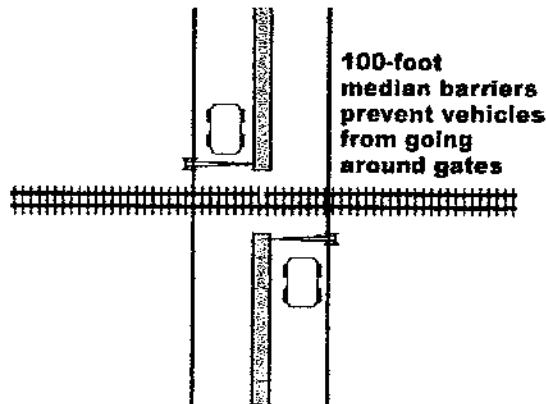
At one test site a stationary automated horn system was installed. When this system senses an approaching train, it sounds a whistle-like warning that is aimed perpendicular to the track, down the road, toward oncoming highway traffic. The system uses two stationary horns mounted at the crossing, instead of on the train. By directing the noise into the street, instead of the surrounding area, noise is more focused and less disruptive to the surrounding community.

At the other test site median barriers were erected down the middle of the street for about 100 feet on either side of the track. Crossing gates close against the median, thereby preventing vehicles from getting around the gates and onto the track. This system eliminates the need for trains to sound their horns for the crossing, making it a good choice for residential areas.

With either of these systems in place, train engineers do not need to blow the train whistle

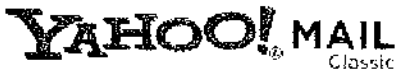


Federal regulations and ARRC rules require horns to generate at least 96 decibels at 100 feet. A study conducted in Mundelein, Illinois, by the Northwestern University Center for Public Safety, showed the automated horn reduced noise by 85% compared to train-mounted horns.



1/08/2007

Trains in motion AT OR Below 150 mph CAN NOT EXCEED 90 DB - Diesel Horns Have A Public Involvement Activities Summary January 2008 100 FT



Print - Close Window

**Date:** Tue, 11 Dec 2007 10:31:10 -0800 (PST)  
**From:** "Larry Jacobson" <atcjake@yahoo.com>  
**Subject:** noise2  
**To:** "Jake Jacobson" <[redacted]@yahoo.com>, [redacted]@mtaonline.net>

All Locomotives Manufactured After 31, December 1979  
Paragraph and section Noisc source Noise standard--A  
weighted sound level in dB Noisc measure \1\  
Measurement location  
201.11(b) Stationary, Idle Throttle Setting. 70  
Lmax (slow) Do.  
201.11(b) Stationary, All Other Throttle Settings.  
87 .....do..... Do.  
201.12(b) Moving 90 Lmax (fast) Do.  
201.11(c) and 201.12(c) Additional Requirement for  
Switcher Locomotives Manufactured on or Before 31  
December 1979 Operating in Yards Where Stationary  
Switcher and other Locomotive Noise Exceeds the  
Receiving Property Limit of. 65 L90 (fast)\2\  
Receiving property  
201.11(c) Stationary, Idle Throttle Setting. 70  
Lmax (slow) 30 m (100 ft)  
201.11(c) Stationary, All Other Throttle Settings.  
87 .....do..... Do.  
201.12(c) Moving Rail Cars 90 Lmax (fast) Do.  
201.13(1) Moving at Speeds of 45 mph or Less. 88  
.....do..... Do.  
201.13(2) Moving at Speeds Greater than 45 mph. 93  
.....do..... Do



**YAHOO! MAIL**  
 Classic

Print - Close Window

**Date:** Tue, 11 Dec 2007 10:22:20 -0800 (PST)  
**From:** "L. Jacobson" <[REDACTED]@yahoo.com>  
**Subject:** railroad noise  
**To:** "Jake Jacobson" <[REDACTED]@yahoo.com>, "[REDACTED]@yahoo.com">

Interstate Railroads. The Noise Control Act also required EPA to establish noise control standards for trains and railway stations engaged in interstate commerce, and it authorized the Federal Railroad Administration to enforce them.<sup>10</sup> The standards do not apply to sound levels generated by horns, whistles, or bells, when operated as warning devices for safety purposes. There are separate standards for locomotives, railway cars, and railway station activities such as car coupling.<sup>11</sup> For locomotives built before 1980, the level of noise is limited to 73 dbA in stationary operation and at idle speeds, and is limited to 96 dbA at cruising speeds. The standards for locomotives built after 1979 are more stringent and limit noise in stationary operation and at idle speeds to 70 dbA and at cruising speeds to 90 dbA. Noise from railway cars must not exceed 88 dbA at speeds of 45 miles per hour (mph) or less and must not surpass 93 dbA at speeds greater than 45 mph. Noise from car coupling activities at railway stations is limited to 92 dbA.

**Alcantra, Rosetta M.**

**From:** Patty Sullivan [patty.sullivan@matsugov.us]  
**Sent:** Wednesday, November 21, 2007 11:35 AM  
**To:** Alcantra, Rosetta M.; Brian Lindamood; Brad Sworts  
**Subject:** Fwd: Comments regarding PortMac Rail Extension  
**Importance:** High

Patty Sullivan  
 Public Affairs Director  
 Mat-Su Borough  
 psullivan@matsugov.us  
 (907)745-9577

Begin forwarded message:

**From:** "Keith Dobson" <krdobson@hotmail.com>  
**Date:** November 21, 2007 11:01:53 AM AKST  
**To:** "John Duffy" <jduffy@matsugov.us>, "Curt Menard" <cmenard@matsugov.us>, <cindybettine@mtaonline.net>, "Patty Sullivan" <patty.sullivan@matsugov.us>  
**Cc:** "Patrick Sharrock" <patsharrock@gci.net>, <tsdobson@gmail.com>, "Marcie Sharrock" <marciesharrock@gci.net>, <gstrong@mtaonline.net>, <krdobson@hotmail.com>  
**Subject: Comments regarding PortMac Rail Extension**

To all concerned, I attended the assembly meeting last evening and would like to make just a few comments regarding the Port MacKenzie Rail Extension Project.

First off, my family has owned property on West/Little Horseshoe lake for over 30 years. I have snow-machined, hiked and canoed much of the area west of the lake around Mule Lake along the proposed Houston rail route. This area is primarily one big marsh with a very sensitive eco-system. Any disruption in this eco-system will most certainly disturb the local loon and crane bird population which are highly sensitive to any eco-system changes. A proposed rail track in this area would require massive amounts of material to be added to establish a firm base for trains, producing in affect an extremely large damn cutting through the heart of this sensitive eco-system. We can use the best available science to estimate the impact of this damn, but I am sure that no-one really knows what the long term impact will be on the bird and wildlife population and the personal property owners who may and most likely will see a change in their soil conditions (which will affect well and septic systems, foundations could be flooding as a result of the railroad damn...). In reviewing the evaluation matrix last evening, I suspect that the following criteria was not included in the cost estimates for this project.

1. Additional costs associated with unexpected water drainage issues resulting from rail extension damn.
2. Additional costs associated with unexpected soil conditions.
3. Cost over-runs as a result of the above delays which require additional engineering.
4. Litigation costs due to personal property damage.
5. Cost of delays associated with personal property litigation issues.

Now for the social and economic consideration for this project. I am in full support of economic development for Alaska as long as we consider both the short term and long term costs and benefits associated with any

project like this. I would submit to you that over the next 50 years the Matsu valley area around Big Lake and Nancy Lakes will be highly desirable locations for Alaskans to both work, recreate and live. With proper management and infrastructure (roads, business development, schools..) this area will likely grow significantly and become a wonderful place for people to live. With that said, I would like you to consider the following questions in light of a 20-50 year plan for this area.

1. What would be the social impact of a rail project slicing through the heart of the Matsu Valley as proposed in the Houston route.
2. What will be the social impact of rail accidents and coal dust and other airborne debris for any proposed route.
3. Should a future car carrying ferry or bridge connecting Anchorage to Port MacKenzie be built, have you considered obtaining a large enough right-of-way to accommodate a future highway along the rail extension line for car commuters to the valley.
4. If the above is desirable, which of the proposed routes is best suited to accommodate a future road project along the rail extension line.

Thank you for your consideration.

Keith Dobson  
11071 Mtn. Lake Drive.  
Anchorage, AK, 99516

---

You keep typing, we keep giving. Download Messenger and join the i'm Initiative now. [Join in!](#)

Patty Sullivan  
Public Affairs Director,  
Mat Su Borough

November 9, 2007

I'm writing this letter because I was never informed that the borough is considering building a railroad diagonally across my farm. AG parcel 2, N1/2 Sec. 31, T16N, R4W. Turns out my neighbor to the south, Dan Robinson, was never informed you intend to build rails across his farm either. In fact, our farms and sections weren't even identified on your map. (How much are you paying these guys any way?) There is a rail right of way that runs along the east border of my property; there is no right of way that runs diagonally NW to SE through my 303 acres. I heard there were public meetings (after the fact) and that public comments were accepted. I'd like to give you a few comments now.

**I strongly oppose your comm 3 route.**

A lot of money was spent about 3 years ago on studies and routes for this railroad idea; I think all those meetings and money was wasted.

No doubt this will be another \$10 million wasted.

I know you guys are in trouble with this whole port thing. Nothing is happening out there; they're pulling the wood chip outfit and I know you're trying hard to bail out a bad idea. — But Anchorage has the port, has the infrastructure and they even have rails. I'd like to see the figures on the amount of freight that goes out of Anchorage by rail, and how much of that freight do you think will move over to your port?

How many years has it been that the surveyors come in and re-stake the port gravel road and it never does get paved? A ferry system? I'm not sure who is going to use it, but I think they'll want a paved road.

Or maybe... you could put in a marina there. That way if the cause-way ever does get built (and let's think; what is it? 45 years now we've been spending money on studies for building a cause-way?) all the Anchorage people would have a dock for their pleasure craft.

Did you bother to run this idea by the Anchorage airport? It was just a few years ago they came forward with their plan to build an airstrip over all these farms.

I see where the borough states "*The rail could also provide long term benefits by lowering property taxes.*" — —I wonder if you people really believe your own bullshit?

\$300,000,000??? Do you think we're all going to rush out to vote and approve a bond for that?

I talked to some people who did attend your public meetings. They said you were quick to respond with the phrase "*eminent domain*". I think you need to review Supreme Court rulings. That doesn't always work anymore.

You guys in Juneau... you need to take your \$10,000,000 dollars back.

Thank you for giving me the opportunity to express my comments.

Sincerely, Gary Thompson

cc:

Anch. Daily News	Lyda Green	Bert Stedman	Gary Wilken	Bill Wieiechowski
The Frontiersman	Mark Neuman	Albert Kookesh	Gene Therriault	Bettye Davis
Charlie Huggins	Stephanie Wheeler	Joe Thomas	Fred Dyson	JohnnyEllis

Continued:

Hollis French  
Lesil McGuire  
John Cowdery  
Con Bunde  
Thomas Wagoner  
Gary Stevens  
Lyman Hoffman  
Donny Olson  
Kyle Johansen  
Peggy Wilson  
Beth Kerttula  
Andrea Doll  
Bill Thomas Jr  
Woodie Salmon  
Mike Kelly  
David Guttenberg  
Scott Kawasaki  
Jay Ramras  
John Coghill  
John Harris  
Carl Gatto  
Wes Keller  
Bill Stoltze  
Anna Fairclough  
Nancy Dahlstrom  
Bob Roses  
Max Gruenberg  
Harry Crawford  
Sharon Cissna  
Les Gara  
Berta Gardner  
Mike Doogan  
Lindsey Holmes  
Bob Buch  
Craig Johnson  
Ralph Samuels  
Kevin Meyer  
Bob Lynn  
Mike Hawker  
Kurt Olson  
Mike Chenault  
Paul Seaton  
Gabrielle LeDoux  
Bryce Edgmon  
Mary Nelson  
Richard Foster  
Reggie Joule

**Roger Purcell**  
**President: Houston Chamber of Commerce**  
**P.O. Box 940231 Houston, AK 99694**  
**907.373.1378 Work 907.373.1390 Fax**  
**Roger\_Purcell@hotmail.com**

November 2, 2007

To: Patty Sullivan  
Public Affairs Director

Re: Railroad Extension Houston South

Dear Patty Sullivan,

We, at the Houston Chamber of Commerce, are in support of the Railroad Extension Houston South.

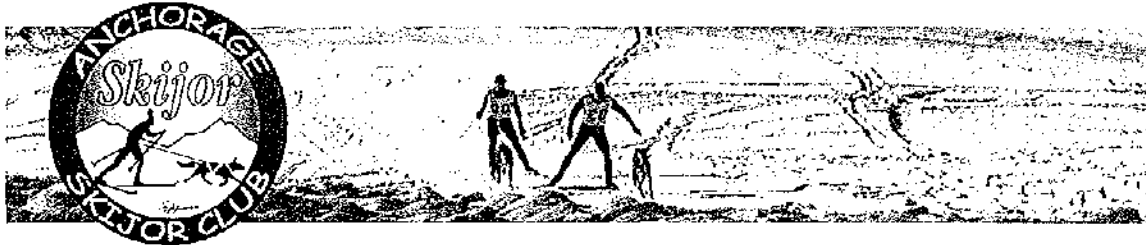
This railroad extension will bring much needed economic growth to the community. With this railroad extension Houston will be able to attract new businesses to our community.

We also believe that this route will have the least Environmental Impact.

Thank you in advance for all of your hard work. We look forward to seeing the railroad come through Houston.

Sincerely,

Roger Purcell  
President  
Houston Chamber of Commerce



## The Anchorage Skijor Club

P.O. Box 240573  
Anchorage, Alaska

November 2, 2007

Patty Sullivan  
Director of Public Affairs  
Matanuska Susitna Borough  
350 Dahlia Ave  
Palmer, AK 99645

RE: Comment to Port MacKenzie Rail Extension Project

Dear Ms. Sullivan,

I am representing the Anchorage Skijor Club and would like to comment on the proposed Port MacKenzie Rail Project. The Nancy Lake State Recreation Area and the West Gateway Trail System are frequently used by the members of the Anchorage Skijor Club. We feel that the Willow Corridor as described in the 2003 Rail Corridor Study will interfere significantly with our ability to use this area for our recreational activities. We hold day tours and weekend camping trips in the Nancy Lakes Recreation area. We stage a yearly race on the West Gateway Trail System. This race is one of our most popular races and has been used as a qualifying race for the Sled Dog Sports World Championship Races. Willow often has earlier snow than Anchorage and is heavily used in the early winter for training by Anchorage Dog Musers, skijorers, and skiers. Later, when Anchorage has snow, the area is used heavily as a wilderness get-away not far from Anchorage.

Railroad tracks through and around this area would destroy the wilderness experience as it is now known. Grade separated crossings are often used as a way to get around conflicting user interests. This is extremely expensive and never satisfactory. We have found from past experience that these tunnels, bridges, overpasses are often promised at the beginning of a project, but when construction begins, somehow get left out because of the expense.

We would like to strongly encourage your consideration of a different area for this Railway Extension project to an area that would benefit from the project. The Willow corridor would be seriously compromised.

Sincerely,

Lulie Williams, President

Patty Sullivan  
Director of Public Affairs  
Matanuska Susitna Borough  
350 Dahlia Ave  
Palmer, AK 99645



RE: Port MacKenzie Rail Extension Project

Dear Ms. Patty Sullivan,

Speaking on behalf of the Iron Dog I would like to express our concerns for the Port MacKenzie Rail Project of Corridor 2 and 3. Of the three proposed routes Corridor 2 and 3 cross the Iron Dog trail easement. It is our understanding that the project proposes a bridge or tunnel would accommodate the easement of this trail.

We feel that Corridor 2 and 3 would impose the greatest safety risk as well as the greatest negative impact on recreation and wildlife in this area. Iron Dog, Inc. is involved with trail development and has worked with local groups such as; Willow Trail Committee, Lower Susitna Drainage Association and the Big Lake Chamber to help provide recreational access to these areas.

Corridor 2 and 3 will cross one of the most widely used recreational areas not only by local residents but thousands that live in other urban areas like Eagle River and Anchorage that come here to recreate. Bottle-necking all the user groups; hunters, skiers, snowmobile users, hikers, ATV riders, bicyclist, dog mushers, other outdoor enthusiast and wildlife into crossing under the railroad in a few tunnels will create a huge safety risk.

It is commonly known that the existing railroad kills hundreds of moose each year. Not only does these proposed corridors cross through a populated moose area but it also affects bears and other small animals that depend on the local streams that provide the fish for their survival.

Please consider other options that present a less risky investment not only to human welfare but also to the abundant wildlife. Thank you for your consideration.

Sincerely,

Laura Bedard  
Executive Director  
Tesoro Iron Dog



Stephanie Wheeler  
Public Involvement Officer  
Alaska Railroad Corporation  
327 W. Ship Creek Ave  
Anchorage, AK 99501



RE: Port MacKenzie Rail Extension Project

Dear Ms. Stephanie Wheeler,

Speaking on behalf of the Iron Dog I would like to express our concerns for the Port MacKenzie Rail Project of Corridor 2 and 3. Of the three proposed routes Corridor 2 and 3 cross the Iron Dog trail easement. It is our understanding that the project proposes a bridge or tunnel would accommodate the easement of this trail.

We feel that Corridor 2 and 3 would impose the greatest safety risk as well as the greatest negative impact on recreation and wildlife in this area. Iron Dog, Inc. is involved with trail development and has worked with local groups such as; Willow Trail Committee, Lower Susitna Drainage Association and the Big Lake Chamber to help provide recreational access to these areas.

Corridor 2 and 3 will cross one of the most widely used recreational areas not only by local residents but thousands that live in other urban areas like Eagle River and Anchorage that come here to recreate. Bottle-necking all the user groups; hunters, skiers, snowmobile users, hikers, ATV riders, bicyclist, dog mushers, other outdoor enthusiast and wildlife into crossing under the railroad in a few tunnels will create a huge safety risk.

It is commonly known that the existing railroad kills hundreds of moose each year. Not only does these proposed corridors cross through a populated moose area but it also affects bears and other small animals that depend on the local streams that provide the fish for their survival.

Please consider other options that present a less risky investment not only to human welfare but also to the abundant wildlife. Thank you for your consideration.

Sincerely,

*Laura Bedard*  
Laura Bedard  
Executive Director  
Tesoro Iron Dog



**Laura Bedard**  
EXECUTIVE DIRECTOR

ph 907.563.4414  
fx 907.563.4080  
irondog@ptialaska.net

**www.IronDog.org**

7100 Old Seward Hwy.  
Anchorage, Alaska 99518

PO Box 766  
Talkeetna, AK 99676  
November 1, 2007

Patty Sullivan, Public Affairs Director  
Matanuska-Susitna Borough

Stephanie Wheeler, Public Involvement Officer  
Alaska Railroad

Via e-mail: [patty.sullivan@matsugov.us](mailto:patty.sullivan@matsugov.us)  
[wheelers@akrr.com](mailto:wheelers@akrr.com)

Dear Ms. Sullivan and Ms. Wheeler:

These are my comments on the Port MacKenzie Rail Extension Project

I oppose corridor 3, which ends at Willow, for the following reasons.

I am very concerned with the adverse impacts that the Willow corridor would have on recreation. I sometimes run my dogs in Willow, taking off from Vera Lake and going across the swamps to Rolly Creek or Corral Hill and onto the Susitna River. Sometimes I go through the Willow Creek State Recreation Area and enter the Susitna River near the mouth of Willow Creek. Sometimes I tour the "Big Swamp."

The Willow trails people have developed a first rate, well maintained, and very popular trail system. It is a fun trail system to use. It is part of the West Gateway trail system.

I've done railroad crossings before on a dog team, and it is a real dicey situation. The sled has to hit the tracks absolutely square to avoid upset and losing the team. You have to go very slowly to avoid hurting the dogs or breaking one's sled. Unfortunately, given that dogs are dogs, going slowly is at times not an option. It is a very difficult situation. Given the number of mushers, from Iditarod to mid-distance to recreational, in the Willow area, I don't see how it would be feasible to reroute the myriad of trails (formal and informal) or accommodate all the crossing needs with underpass culverts. The situation would be greatly exacerbated if roads, utilities, etc. are ever added in the future.

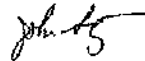
Another concern is has to do with impacts to State Park units, especially the Willow Creek State Recreation Area (WCSRA). This corridor bisects the WCSRA, which is an important public recreation resource, and the adverse impacts would be significant.

The Willow people don't want this, and I don't blame them. It would severely and adversely impact their recreation and their community. The Houston people, from a recent Anchorage Daily News article, want the rail spur, and it is my understanding that they prefer the route that ends just south of the Little Susitna River.

I urge selection of the south Houston alternative.

Thank you for this opportunity to comment.

Sincerely,

A handwritten signature in black ink, appearing to read "John Strassenburgh".

John Strassenburgh

# WACO

*Willow Area Community Organization*  
Linda Oxley *Chair*  
chair@waco-ak.org

P.O. Box 1027 • Willow • AK 99688  
(907) 495-6633  
www.waco-ak.org

Patty Sullivan, Public Affairs Director  
Matanuska-Susitna Borough  
Office of Public Affairs  
350 East Dahlia Avenue  
Palmer, Alaska 99645

Stephenie Wheeler  
Public Involvement Officer  
Alaska Railroad Corporation  
327 W. Ship Creek Ave  
Anchorage, Alaska 99501

November 1, 2007

*Re: Comment Packet/Port MacKenzie Rail Extension Project*

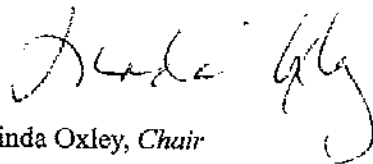
Dear Ms. Sullivan and Ms. Wheeler:

Enclosed is a packet of letters and resolutions that oppose Corridor 3—the Willow route. All of these have been individually filed with you but the Community of Willow wants to make sure this packet of letters are included in any submissions or application to the Surface Transportation Board (STB).

This packet contains important user groups and stakeholders that will be adversely impacted by a Corridor 3–Willow route. Each individually sets forth specific concerns and all agree that Corridor 3 would harm the Willow area and believe mitigation would do little to offset this harm.

Thank you for filing this packet with the STB.

Sincerely,



Linda Oxley, *Chair*

cc: Distribution

Patty Sullivan, Public Affairs Director  
November 1, 2007  
Page 2

Distribution-

Sarah Palin, Governor  
Tom Irwin, Commissioner-DNR  
Jim King, Director-State Parks  
Lisa Murkowski, Senator  
Ted Stevens, Senator  
Don Young, Representative  
Curt Menard, Mayor-Mat Su Borough  
Tom Kluberton, Mat Su Borough Assembly  
Charlie Huggins, Senator  
Mark Neuman, Representative  
John Binkley, Chairman-ARR Board  
Mark Begich, Mayor-City of Anchorage  
Steve Frost, Mayor-City of Houston

Attachments-

Willow Area Community Organization (WACO) letter  
Willow Dog Musers Association (WDMA) Resolution  
Iditarod Trail Committee, Inc. Letter  
Mat-Su Parks Citizens Advisory Board Letter  
Tesoro Iron Dog Letter  
Willow Trail Committee Letter/Willow Area Comprehensive Trail Plan  
Mat-Su Convention and Visitors Bureau (Mat-Su CVB) Letter  
City of Houston, Alaska/Newspaper Articles/Requests

# Houston on board early with rail plan

Borough, railroad  
in early stages of  
proposed spur line

BY JOHN R. MOSES

*Frontiersman*

HOUSTON — A new rail line to Port MacKenzie is welcome in Houston, as long as it stops there and helps build industry. That's the message the Mat-Su Borough and railroad are receiving from some on Houston City Council. Others say trains should roll through even without a depot.

Borough and railroad officials said it's natural for a town or city to want new services. The problem with the Port MacKenzie spur line is it's so early in the planning process that no meaningful negotiations can happen. No one yet knows where the federally approved rail route will begin. The tracks will run a course between 25 and 40 miles long, depending on where the line starts, which could be in Big Lake, Willow or Houston.

"First, they've got to decide the route," Borough Economic Development Director Dave Hanson said.

It isn't unreasonable for a city like Houston to want transportation options like a railroad depot. Wherever the tracks

## RAIL

*Continued from Page A1*

wind up going, project planners will work with the affected communities, Hanson said. There was no strong public opposition to the rail line at a recent Houston City Council meeting, but some want more than just new tracks and a fast-moving train.

Councilwoman Sandy McDonald said the council will likely revisit the issue when it meets again Nov. 15.

No council vote has been taken, but members have their own opinions. McDonald said she agrees with newly elected Councilman Roger Purcell that there should be a siding and/or a depot in Houston.

"If it comes through here we would like it to benefit the city," she said.

One obstacle to any deal-making is uncertainty facing even railroad planners. The final route plans will be decided in December and then submitted to federal authorities.

"At this point the railroad's not ready to bargain," McDon-

aid said.

Once a route is decided locally it still must be approved by the federal Surface Transportation Board, which is likely to OK the chosen route, she said. That federal board also investigates the proposed route through an environmental impact process that is open to the public and — if the route is approved — gives the railroad and Borough a list of contractors to choose from for construction.

Houston Councilwoman Carla Hendrix said she's in favor of having the new line go through the city, even if there is no guarantee of a depot or siding. "My initial reaction is, hey, bring it on."

Without a route, Hendrix understands why there can't be guarantees, and she thinks if Houston is chosen the city can work with the railroad.

And if the railroad refuses a depot or siding as part of the deal?

"There's always the future," Hendrix said.

The \$300 million rail project is scheduled for completion in 2012.

See RAIL, Page A14

# OPINION

MANAGING EDITOR: GREG JOHNSON PHONE: 352-2268 FAX: 352-2276 E-MAIL: [REDACTED]

## FRONTIERSMAN EDITORIAL

# *Houston needs follow-through with rail desires*

Plans to build a railroad spur to Point MacKenzie are chugging along, and it's time for interested communities to get aboard.

The rail route will be anywhere from 25 to 40 miles long, depending on whether it starts in Houston, Willow or Big Lake. While Willow representatives have spurned the spur with a lukewarm reception, some Houston city leaders are waving flags in the hope the train will stop in their community, complete with a depot.

While it is too early to put details to the new rail line plans, it isn't too early for an interested community like Houston to conceptualize how the railroad could benefit its community. City council members are already lobbying for the Borough and Alaska Railroad to seat the spur there and are eager for the advantages a rail line originating in Houston could bring.

Some businesses could benefit greatly from having rail access. More importantly, city officials believe rail could be just the ticket Houston needs to stoke its boilers as a viable place to locate new business and industry. They may be on the right track. We are certain the wheels are spinning among some leaders in Houston as they consider the potential.

What Houston needs now is to be more proactive in luring the rail spur. Don't stop at simply saying, "Hey, over here!" Develop a rail plan. If economic development is the key to Houston's success, invest in an economic development coordinator to help draft a plan that specifically spells out how Houston and the Mat-Su Borough could use rail access and a depot as a vehicle to chug more industry and wealth into Houston and the Valley.

Houston may want to be Alaska's version of "The Little Engine That Could," but will need more than an "I think I can" approach to make believers out of the Borough and railroad.

The council expects to examine the railroad issue again at its Nov. 15 meeting. Perhaps by that time, it will be ready to assign to committee or other body some of the preliminary work of pushing Houston's agenda in regard to the railroad. While final plans aren't expected until December, Houston can influence the process by putting initiative to its interest.

The same is true for residents of Big Lake or Willow (although meetings in Willow indicate that community would prefer the rail locate elsewhere). Perhaps by starting in Houston and running a route favorable to Big Lake, the railroad spur could aid both communities and still reach its destination in Point MacKenzie.

# Houston wants rail guarantees

**SPUR:** Line to Port MacKenzie could bring industry with it.

By RINDI WHITE  
rwhite@adn.com

**HOUSTON** — Houston residents would eagerly accept a branch of the Alaska Railroad through their city, but only if it brings jobs and industry to the struggling local economy, some said at a recent meeting.

Houston, with more than 1,500 residents, is growing in size. But city government struggles to pay for road maintenance and other services with the property tax and 2 percent sales tax it collects. The town needs industry, residents say.

Now, the Matanuska-Susitna Borough hopes by 2012 to extend a branch of the Alaska Railroad from the main line, which already divides Houston, to Port MacKenzie.

A branch line could attract industry to Houston, but only if trains stop long enough for goods or passengers to be loaded and unloaded. Some Houston residents want that guarantee before they give the project any support.

However, community support is but one factor in the decision. The federal Surface Transportation Board, and not the railroad nor the borough nor the city, would decide where a line should be built, if at all, said Alaska Railroad special projects manager Brian Lindamood. He said the railroad this week will present the concept for the first time to the Surface Transportation Board. The railroad expects a decision by mid-2009, Lindamood said.

Houston has its roots in rail. In 1917, it was a railroad siding named after congressman William C. Houston of Tennessee. Coal mines in the area fueled Anchorage stoves. But trains no longer stop in Houston and most people who live there commute to jobs elsewhere. Commerce in Houston amounts to some tourism, retail and services but little manufacturing.

"Houston grew up as a railroad town. I don't think we should give up our history just to let a railroad go through. But we need some guarantees to make it beneficial to all of us," said newly elected City

## online

Comments about the plan to build a rail line to Port MacKenzie will be accepted until Nov. 2. Learn more about project at

[www.portmacrail.com](http://www.portmacrail.com)

Councilman Roger Purcell.

The City Council has taken no stance on the project. But Purcell said he wants promises of a railroad siding and loading dock in place before Houston agrees to the line.

Purcell isn't alone. Rick Dilley, owner of Alaska Cozy Coal and Furnace, said he'd like to get coal shipments by rail instead of trucking the coal from Healy himself.

Dilley is a Usibelli coal distributor for the Valley and Anchorage.

A former North Slope worker, Dilley three years ago started his coal-furnace and coal-supply business because he got sick of paying high fuel oil bills.

He sold 12 tons of coal the first year. This year, he sold 72 tons.

"I don't want to see the railroad come through Houston and just cycle right through. I want to see industrial development in Houston. I want Houston to be the next star. It's our turn," Dilley said.

Vaughn Nadeau, manager of Spenard Builders Supply's Big Lake truss manufacturing plant, said SBS might benefit from a local railroad siding as well.

The Big Lake plant, which is within Houston city limits, shipped about \$1.5 million worth of trusses to Fairbanks this year, Nadeau said, and more were shipped out of the Anchorage port to Bush communities.

Lindamood said it's far too early in the planning process to make promises to Houston, or to any other community.

"We're trying to figure out if the route goes to Willow, to Houston or to Big Lake," Lindamood said.

The Alaska Railroad and the borough are wrapping up an analysis of several routes from the main line to the borough-owned Port MacKenzie. A rail line is key to making the port profitable.

Find Daily News reporter Rindi White online at [www.adn.com/contact/rwhite](http://www.adn.com/contact/rwhite) or call 1-907-352-8709.





# Iditarod Trail Committee, Inc.

Post Office Box 870800 • Wasilla, Alaska 99687-0800  
907.376.5155 (voice) • 907.373.6998 (facsimile)  
www.iditarod.com

October 31, 2007

**Board of Directors**

President  
Richard Burmeister

Vice-President  
Lee Larsen

Secretary  
Jim Palin

Treasurer  
Rick Swenson

Director  
John Handeland

Director  
Sam Maxwell

Director  
Mark Moderow

Director  
Mike Owens

Director  
Dan Seavey

Director Emeritus  
Joe Redington, Sr.

Ms. Stephanie Wheeler  
Public Involvement Officer  
Alaska Railroad Corporation  
327 West Ship Creek Avenue  
Anchorage, AK 99502

Dear Ms. Wheeler:

This letter, written on behalf of the Iditarod Trail Committee board of directors, is in regards to the proposed rail corridor alternatives connecting Port MacKenzie and the Alaska Railroad.

We understand the need for further economic development within the State of Alaska. We also understand the potential for a rail corridor to serve as an economic stimulus. That being said, we strongly urge the Alaska Railroad and the Matanuska Susitna Borough to focus its energy on a corridor which least impacts the traditional recreational habits and opportunities for residents and visitors to our state.

In general terms, the Matanuska Susitna Valley is the playground for the majority of our state's population base. In terms more specific to the Iditarod itself, trail systems in the valley are of utmost importance to dog mushers. While the Iditarod is visible to millions of race fans around the globe each March, the daily training runs over hundreds of miles of trails by teams preparing for the race take place in relative obscurity. Yet for the race to be a success, or at a more basic level to exist at all, mushers must have ready access to trails. Perhaps nowhere is this more important than the Willow area where many mushers live and train.

As planning continues on this project, it is imperative that the integrity of trail systems are maintained, with more than adequate access and well designed overpasses. The cost of the chosen corridor cannot be the driving issue in this matter.

For those reasons outlined herein we believe that the Big Lake and Houston South alternatives (in that order) should be considered, with adequate access as an integral part of the planning and implementation processes.

Thank you in advance for your consideration.

Sincerely yours for,

**THE IDITAROD TRAIL COMMITTEE**

Stan Hooley  
Executive Director

Executive Director  
Stan Hooley  
Post Office Box  
870800  
Wasilla, AK 99687  
907.232.8730 voice  
907.373.6998 fax  
shooley@iditarod.com

October 29, 2007

Patty Sullivan  
 Port MacKenzie Rail Extension  
 Public Affairs Director  
 Matanuska-Susitna Borough  
 350 E. Dahlia Ave.  
 Palmer, Alaska 99645

Dear Patty,

This submission intends to express fervent opposition to one of the proposed alternate railroad corridors to Port MacKenzie. That corridor is known as the Houston/Houston North-South Route. Comments are unscientific but hopefully reflect common sense issues involved.

Our family has owned property on the west side of West Lake (sometimes called Little Horseshoe Lake) for 38 years. Our property lies within approximately 2,500 feet from the proposed Houston corridor that would run just east of Muleshoe Lake and between an adjacent unnamed lake.

The proposed alternative route area involved is bounded on the north by the Parks Highway, on the east by the proposed Big Lake Route and on the west by the proposed Willow Route. It includes the Nancy Lake Recreational Area and the unnamed Big Lake recreational area. Each area is punctuated by numerous lakes and ponds and encompasses a large recreational area that appears to be bisected by the proposed Houston Route. Additionally, much of the area is semi-settled with permanent residences and recreational cabins. The area is depicted by red hash marks on the attached map.

I have snow-machined and hiked the area west from our property many times (summer and winter) to and just beyond the Little Susitna River. The landscape is flat with swamps and numerous stands of black spruce, but contains several snow machine, dog team, skiing and hiking trails. I have also canoed on Muleshoe Lake and the adjacent unnamed lake and did not observe any discernable inlet or outlet streams on either lake, so these bodies of water must originate and be sustained by subsurface sources.

Also, during times of heavy rain, I have observed, on several occasions, heavy surface run-off into our lake (West Lake) from the area directly behind our property. Additionally, our lake feeds Horseshoe Lake. However, I have no idea how or where the distribution of water is determined in the area.

Having briefly described the "route" area, I submit the following points in opposition to the Houston Route:

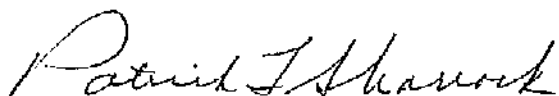
1. It clearly bisects a large recreational area.
2. It clearly is disruptive to people's residences and recreation properties – i.e. noise, potential derailment, damage to surface water and drainage and unknown amounts of coal dust.
3. It clearly invites dangerous crossing of the rail line by recreational users.
4. It clearly disrupts habitat for muskrat, loon, beaver, and other small animals and birds.

5. It clearly, and most importantly, is a potential divider that alters, restricts or eliminates the free movement of water that sustains the watershed drainage in the eastern area of the route. In essence it could be a dam.

In conclusion, I understand the necessity of providing a rail connection to Port MacKenzie. However, it should be developed with the least disruption of developed property and public use areas. In my opinion, common sense dictates that the Willow Route is the most reasonable route.

Thank you for your courtesy and consideration.

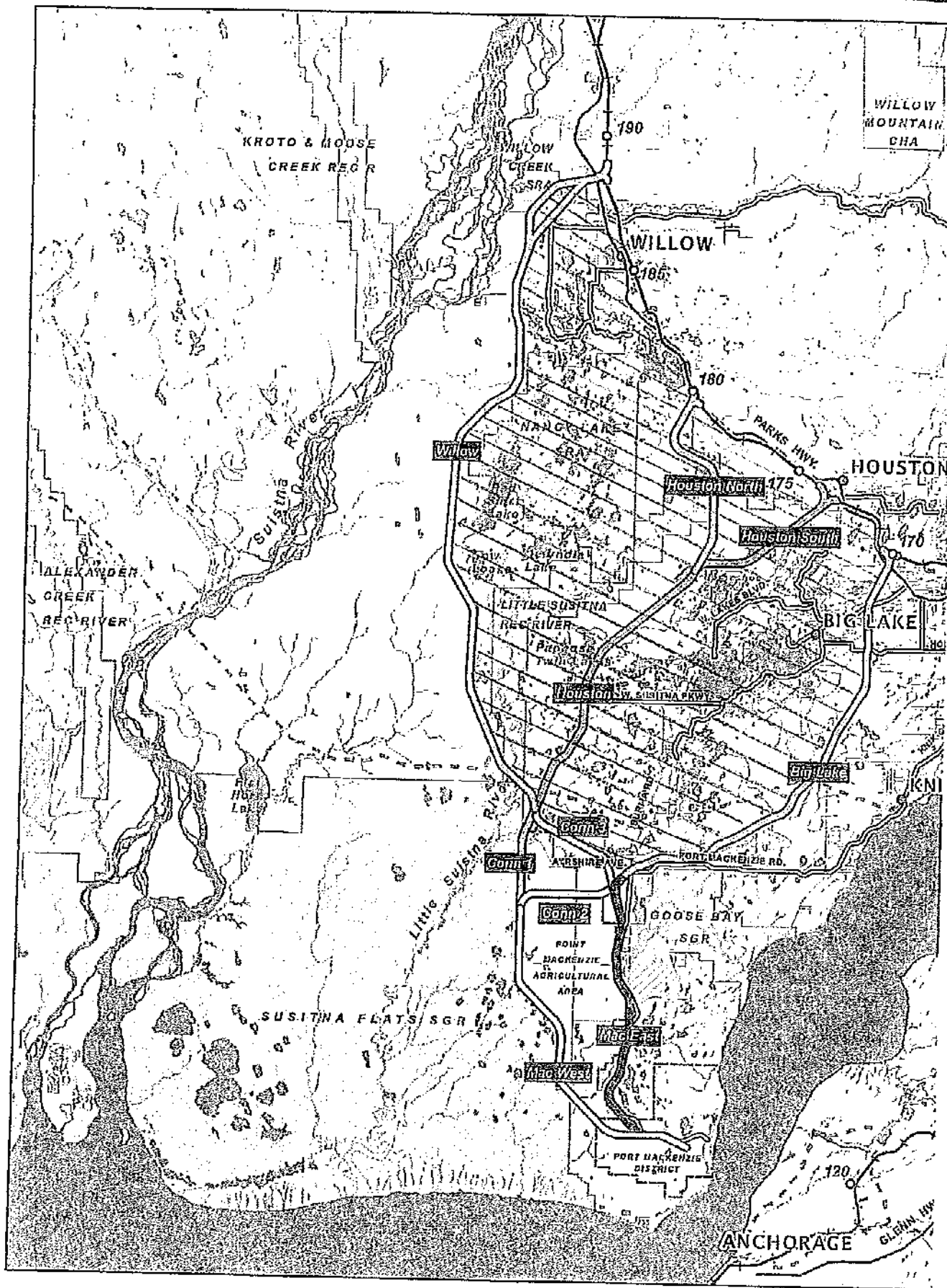
Respectfully,



Patrick L. Sharrock  
8731 Solar Drive  
Anchorage, Alaska 99507  
(907)345-5353

cc: Cindy L. Bettine  
Matanuska-Susitna Borough  
PO Box 870008  
Wasilla, Alaska 99687

Cathi Kramer  
PO Box 521783  
Big Lake, Alaska 99652



*(This page intentionally left blank.)*

NOV 28 2007

Administration/  
Human Resources


---

**GREGORY E. STRONG**


---

Mayor Curtis Menard  
350 E. Dahlia Ave.  
Palmer, AK 99645

November 28<sup>th</sup>, 2007

RE: The Castle Mountain Earthquake Fault in Houston, Alaska

Dear Mayor <sup>Curt</sup> Menard,

At last week's Assembly Meeting you were provided with an "Evaluation Matrix" for the proposed routes for the Port MacKenzie Rail Extension Project (Exhibit A). The Matrix reviewed ten (10) categories for eight (8) proposed routes. A map reflecting each of the proposed routes is included (Exhibit B).

What is of concern, and the reason for this letter is that the status of the sub surface geologic estate is NOT part of the Evaluation Matrix. The proposed Houston South route runs perfectly parallel for its entire length of travel with the Castle Mountain Earthquake Fault (Exhibit C). Let me be clear, we're not talking about the proposed rail line merely crossing the fault, but rather the proposed route runs directly on top of or directly along side of the fault from the Susitna River to the Parks Highway. Dr. Peter Haeussler of the USGS states that this fault could fail at anytime with an expected 7.2 magnitude earthquake.(1) This fault line has failed every 650-700 years for the last 2500 years. The last time this fault line failed was 650 years ago.

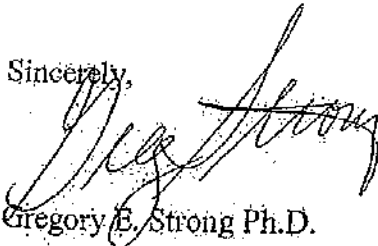
The construction of a portion of a quarter of a billion dollar rail project paid for with taxpayer dollars on top of a known, well documented and well studied earthquake fault is something prudent officials, such as your self, should avoid.

Even minor quakes could create frequent rail alignment failures resulting in numerous, expensive derailments. Before this process gets passed along to persons in Washington D.C. with the Surface Transportation Board, let me urge you to remove from consideration the Houston South route as it is certainly not a safe, viable, commercial transportation route.

P.O. Box 875169, WASHILA, ALASKA 99687

PHONE: (907) 745-9096 • FAX: (907) 746-6440

Sincerely,



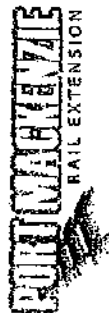
Gregory E. Strong Ph.D.

(1) Haeussler, Peter J., Seismic Disturbances of Upper Quaternary Deposits along the  
Castle Mountain Fault near Houston, Alaska: US Geological Survey Open File  
Report 1998  
Attachments (3)

Cc:

John Binkley, The Alaska Railroad Corporation  
Sen. L. Green  
Sen. C. Huggins  
Rep. M. Neuman  
Mayor C. Menard  
Surface Transportation Board

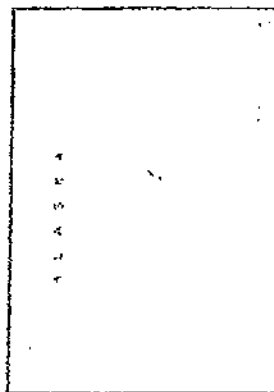
ALTERNATIVES



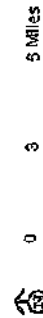
LEGEND

- Preliminary Alternatives\*
- Mac East
  - Mac West
  - Conn 1
  - Conn 2
  - Conn 3
  - Houston
  - Houston North
  - Houston South
  - Willow
  - Big Lake
- ARRC Milepost
- ARRC Track
  - Highway
  - Medium Rd.
  - Minor Rd.
  - Iditarod Trail
  - City Boundary
  - Proposed Prison
  - Point MacKenzie Correctional Farm
  - Park or Refuge

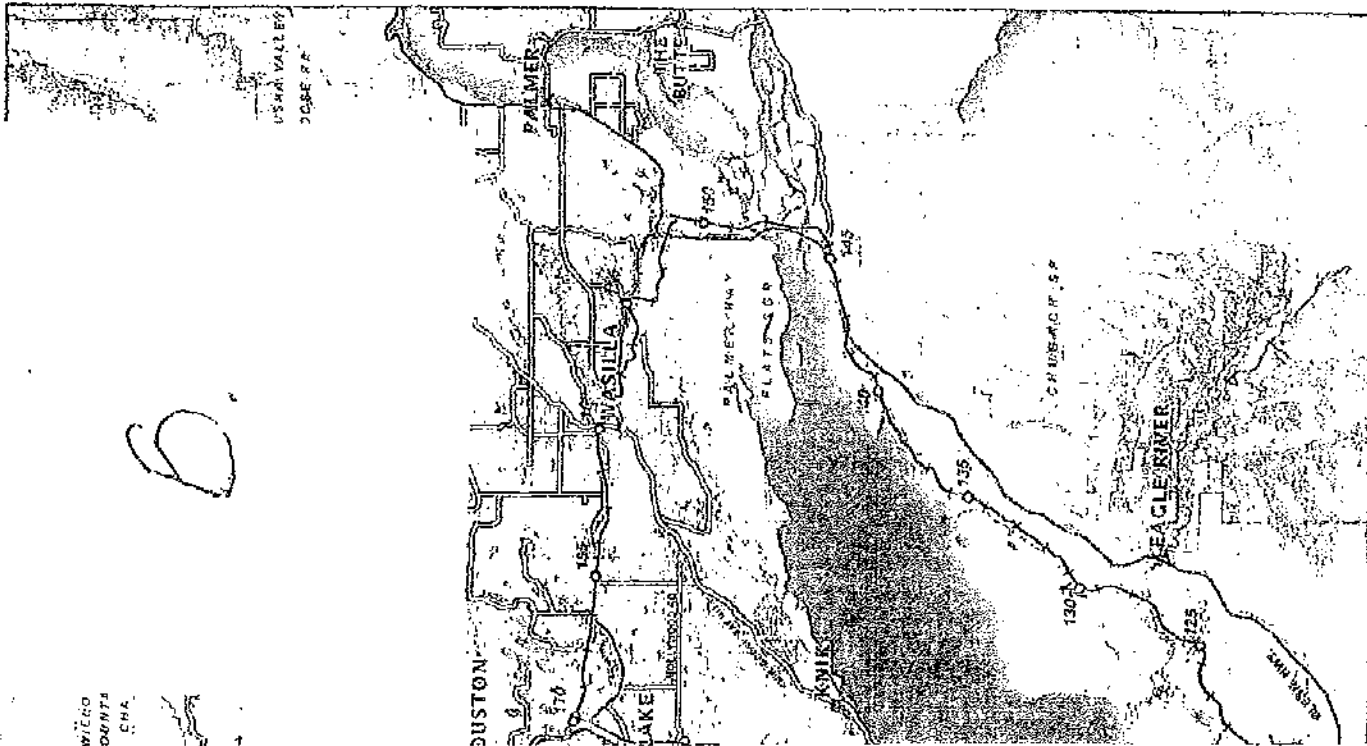
\*These lines generally represent corridors which are subject to further refinement.



This map represents a conceptual level of study, design, and construction. The information contained herein is for informational purposes only. Some information shown on this map may be preliminary and subject to change. The information shown on this map is not intended to be used for any other purpose.



Date: September 20, 2007  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: ADNR, ASGC, HDR Alaska, Inc., USB GIS, TNH-Hanson, USGS.





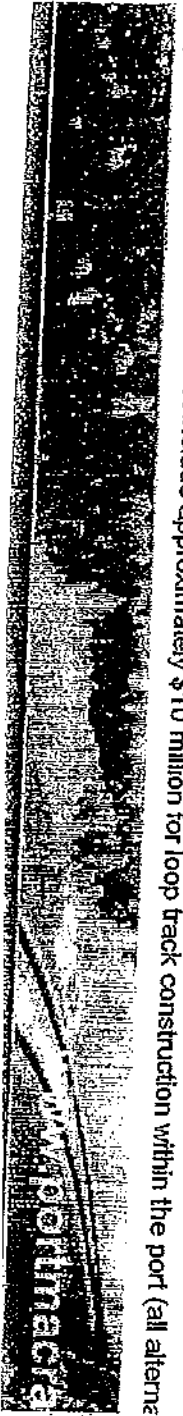
# PORT MACKENZIE RAIL EXTENSION

## Evaluation Matrix

PROPOSED ROUTES	CRITERION										Preliminary Cost Estimate (millions)	
	1	2	3	4	5	6	7	8	9	10		
Mac West - Willow	0	+	0	0	-	0	0	0	-	-	-	\$320
Mac West - Houston North	-	+	0	+	-	+	-	0	+	-	-	\$250
Mac West - Houston South	0	+	0	+	-	0	-	0	+	-	-	\$250
Mac West - Big Lake	+	-	-	-	-	-	0	-	-	-	0	\$220
Mac East - Willow	0	0	0	+	+	0	+	+	-	-	0	\$290
Mac East - Houston North	-	0	0	+	+	+	0	0	+	-	-	\$330
Mac East - Houston South	0	0	+	+	+	0	+	+	+	-	-	\$260
Mac East - Big Lake	+	-	0	-	+	-	+	-	-	+	+	\$230
												\$285

1. (+) Positive; (0) Neutral; (-) Negative
2. Criteria not weighted / Routes are unranked
3. Large parcels of undeveloped land owned by State of Alaska MSB, University of Alaska, Mental Health Trust and Alaska State
4. Lands that are designated for parks, refuges, residential, or agricultural uses
5. Costs do not include approximately \$10 million for loop track construction within the port (all alternate

(+) Positive; (0) Neutral; (-) Negative  
 Criteria not weighted / Routes are unranked  
 Large parcels of undeveloped land owned by State of Alaska MSB, University of Alaska, Mental Health Trust and Alaska State  
 Lands that are designated for parks, refuges, residential, or agricultural uses  
 Costs do not include approximately \$10 million for loop track construction within the port (all alternate



A

DEC 04 2007

Administration/  
Human Resources


---

**GREGORY E. STRONG**


---

Mayor Curtis Menard  
Mat Su Borough  
350 Dahlia Ave.  
Palmer, AK 99645

December 4, 2007

RE: Horseshoe Lake/Little Susitna Watershed

Dear Mayor ~~Menard~~,  
*Wat*

August 2006 was a typical August for Horseshoe Lake residents as it began to rain, and rain it did for thirty days straight. On September 2, 2006 the water level of the lake had risen by 18.75 inches. This extreme increase in water level caused docks to become totally submerged and shoreline erosion appeared imminent. This was a year of continuous heavy rainfall, but it should not be confused with what is commonly referred to as a "100 year flood."

This flooding occurred with the absence of any man made obstruction between Horseshoe Lake and the Little Susitna River, which serves as its natural drainage corridor. The establishment of a rail bed to support a rail line would be the equivalent to the construction of an earthen dam prohibiting the flow of both surface and sub surface water to the Little Susitna River. The Little Susitna River removes water from the numerous lakes in the immediate area.

The attached Exhibit (A) illustrates the following:

- A. The Horseshoe Lake area drains into the Little Susitna River (Drainage), which is outlined in RED
- B. The LIGHT BLUE area around Horseshoe Lake(s) indicates that the lakes in and of themselves serve as an immediate, yet transitional watershed for the area
- C. The BLACK line reflects the proposed Houston South railroad corridor

The proposed Houston South route bisects the natural drainage of water into Horseshoe Lake and the immediate lakes that surround it, as well as their subsequent discharge into the Little Susitna River. It takes little imagination to consider what an earthen rail bed would do to watershed drainage in the Horseshoe Lake area. Depending on the time of year the water would rise you could potentially witness:

- A. The destruction of waterfowl nests
- B. The destruction of salmon and trout spawning beds

P.O. Box 875169, WASILLA, ALASKA 99687

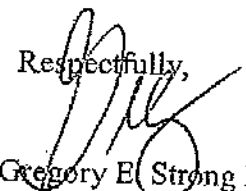
C. Significant shore erosion effecting the food chain and reproductive waterfowl and fish habitat

There will be those that may suggest that such ecological disruptions could be avoided by placing the rail line on an elevated trestle. The difficulty with that suggestion is that the length of construction of that line would need to be at least 5 miles long in order to not confuse the natural "lakes" drainage into themselves as shown in LIGHT BLUE on the exhibit. That elevated trestle would be constructed along the well studied Castle Mountain Fault line. Such a solution seems to be a clear "recipe for disaster".

Yet others may suggest a series of culverts to allow for the continuous drainage of water to the West. Granted, on a "good day" these culverts may work. On a "bad day" they may become dammed with ice, or the numerous dead spruce trees lying on the ground that are the aftermath of the Big Lake (Miller's Reach) Fire. Some of these millions of dead trees could choke off the flow of water potentially resulting in the ecological destruction suggested earlier.

The Houston South route contains poor soils, extensive wetlands, is located directly on top of an active earthquake fault line, and bisects the area's 10,000 year old watershed. These facts are undisputed, hence, let me suggest to you and the numerous additional parties involved with this discussion that the Houston South should NOT be worthy of further consideration.

Respectfully,

  
Gregory E. Strong Ph.D.

Attachment(1)

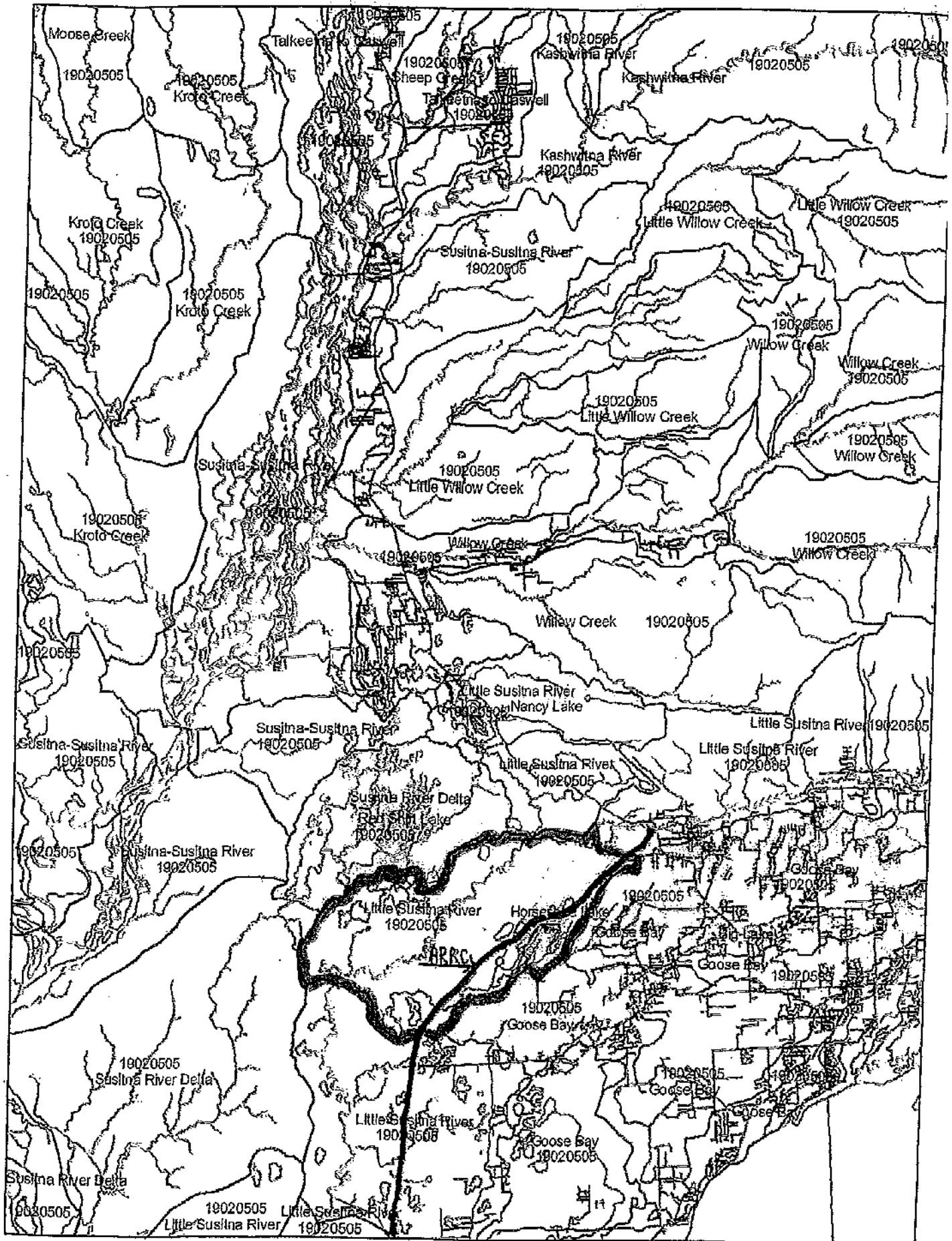
Cc:

John Binkley, Alaska Rail Road  
Sen. L. Green

Sen. C. Huggins  
Rep. M. Neuman  
Mayor C. Menard  
Mat Su Borough Assembly  
Michael Szerlog EPA  
Matt LaCroix EPA  
Ann Rappaport USFWS  
Phil Brna USFWS  
Irvin Joy US Army Corps of Engineers  
Surface Transportation Board  
Brian Lindamood ARRC

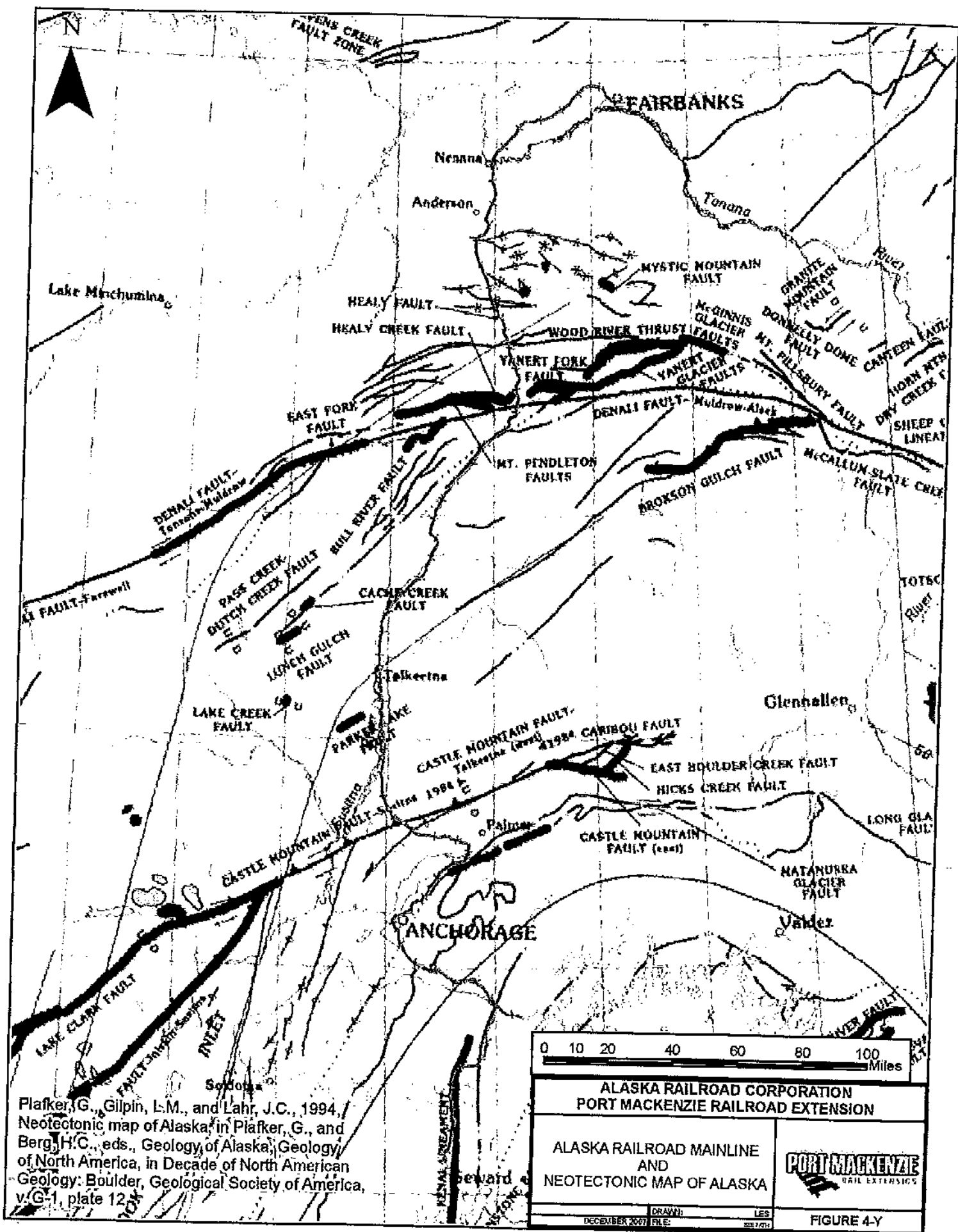
*(This page intentionally left blank.)*





0 5,000 10,000 20,000 Meters

Exhibit A





*(This page intentionally left blank.)*



**Willow Dog Musher's Association**

P.O. Box 858  
Willow, Alaska 99688

October 24, 2007

Ms. Stephanie Wheeler  
Public Involvement Officer  
Alaska Railroad Corporation  
327 W. Ship Creek Ave  
Anchorage, AK 99501

Re: Port Mackenzie Rail Extension Project

Dear Ms. Wheeler:

Attached is Resolution Number 07-10-01 from the Willow Dog Musher's Association in opposition to the Port Mackenzie Railroad Project Extension – Willow/Corridor 3.

We had representatives from our organization attend all the Rail Corridor Open Houses the first week of October and we hosted the recent Public Forum. So, we have reviewed this project very carefully and found it not to be compatible with our mission statement of protecting and preserving the musher's way of life for today, tomorrow and future generations.

Please enter this Resolution No. 07-10-01 as our official comment.

Sincerely,

Erin McLarnon  
President

**WILLOW DOG MUSHERS ASSOCIATION**  
Willow, Alaska

**RESOLUTION NO. 07-10-01**

**A RESOLUTION OPPOSING THE PORT MACKENZIE RAILROAD EXTENSION PROJECT-  
WILLOW/CORRIDOR 3**

WHEREAS, the Willow Dog Mushers Association is a recognized State of Alaska non-profit organization formed to protect and preserve the dog musher's way of life for today, tomorrow and future generations as well as to protect and preserve our historical trails and Willow's vital mushing habitat, and

WHEREAS, the health of Willow dog mushing is significantly tied to a rural, and open recreational environment within our three historical trail systems (West Gateway, Haessler-Norris and Emil-Stancec), our parks, rivers, and our wetlands, and

WHEREAS, Willow is home to the West Gateway Trail system, a world class trail system, which utilized by mushers and other types of winter recreationalists such as but not limited to snowmachiners, skiers, and hikers, and

WHEREAS, Willow's economy is based on these many recreational opportunities, and

WHEREAS, the Iditarod Trail Sled Dog Race is Alaska's most important winter event which supports dozens of local mushers and attracts thousands of fans from around the world, which has started six (6) of their past eight (8) races in Willow utilizing the West Gateway Trail System, and

WHEREAS, the Port Mackenzie Rail Extension Project-Willow/Corridor 3 crosses the West Gateway Trail System in over ten (10) locations that have easement protection, which divides the existing historical trail system, as well as crosses through miles of wetlands commonly used by mushers and other winter recreationalists, crosses through Willow Creek State Park and Recreation Area, and crosses Willow Creek as well as several other small anadromous fish streams, and

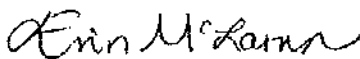
WHEREAS, the alternative corridors south of Willow are shorter, cross fewer trails, harm less wetlands and meet the needs of the Rail Extension project, and

WHEREAS, the Willow Dog Mushers Association had representatives attend the Port Mackenzie Rail Extension Project Open Houses held the first week of October in Wasilla, Big Lake, Willow, Knik and Houston, and hosted a Port Mackenzie Rail Extension Public Forum in Willow, Alaska on October 24<sup>th</sup>, 2007 that was attended by representatives of the Mat-Su Borough, the Alaska Railroad Corporation, our members, and the Willow community.

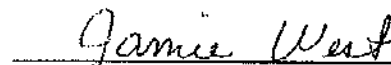
NOW THEREFORE BE IT RESOLVED that the Membership of the Willow Dog Mushers Association opposes Willow/Corridor 3 as an acceptable route for the Port Mackenzie Rail Extension.

**CERTIFICATION:**

The foregoing resolution was passed and approved by a duly convened meeting of the Willow Dog Mushers Association this 24<sup>th</sup> Day of October, 2007, by a unanimous vote.



Erin McLarnon  
President



Jamie West  
Secretary

Willow Trail Committee  
P.O.Box 845  
Willow, AK 99688  
ph. 495-6368

October 20, 2007

Patty Sullivan  
Public Affairs Director  
Matanuska Susitna Borough  
350 E. Dahlia Ave  
Palmer, AK 99645

RE: Official filing of the Willow Area Trail Plan for the Port MacKenzie Rail Extension Project.

Dear Ms.Sullivan,

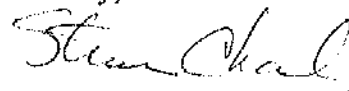
Enclose is the Willow Area Trail Plan that has been in the works for years. Please enter this document and letter as official comments for this project. The West Gateway Trail System is most valued from the standpoint of use, history, and economic impact to the Matanuska Susitna Valley. The Willow Trail Committee, since it's inception, has been working closely with the Matanuska Susitna Borough and the State of Alaska in acquiring easements and maintaining the West Gateway Trail System as well as other trail systems in the community.

The Willow Trail Committee was formed in 1996 as a special standing committee within the Willow Area Community Organization, it's mission is to provide safe, enjoyable and legal recreational trails for Willow residents and visitors. Recreational trails are strongly supported in the community by evidence of the 94-0 vote passage of the Willow Area Trail Plan.

On active wcecknds, the West Gateway Trail System supports thousands of trail users including snowmachiners, dog mushers, skiers and skijjorers. It is also the venue for many local, state, national and international races, including the Iditarod, Junior Iditarod, Klondike and Don Bowers dog sled races and the Iron Dog, Klondike 400, Aurora 200 snowmachine races, plus many Willow Winter Carnival events.

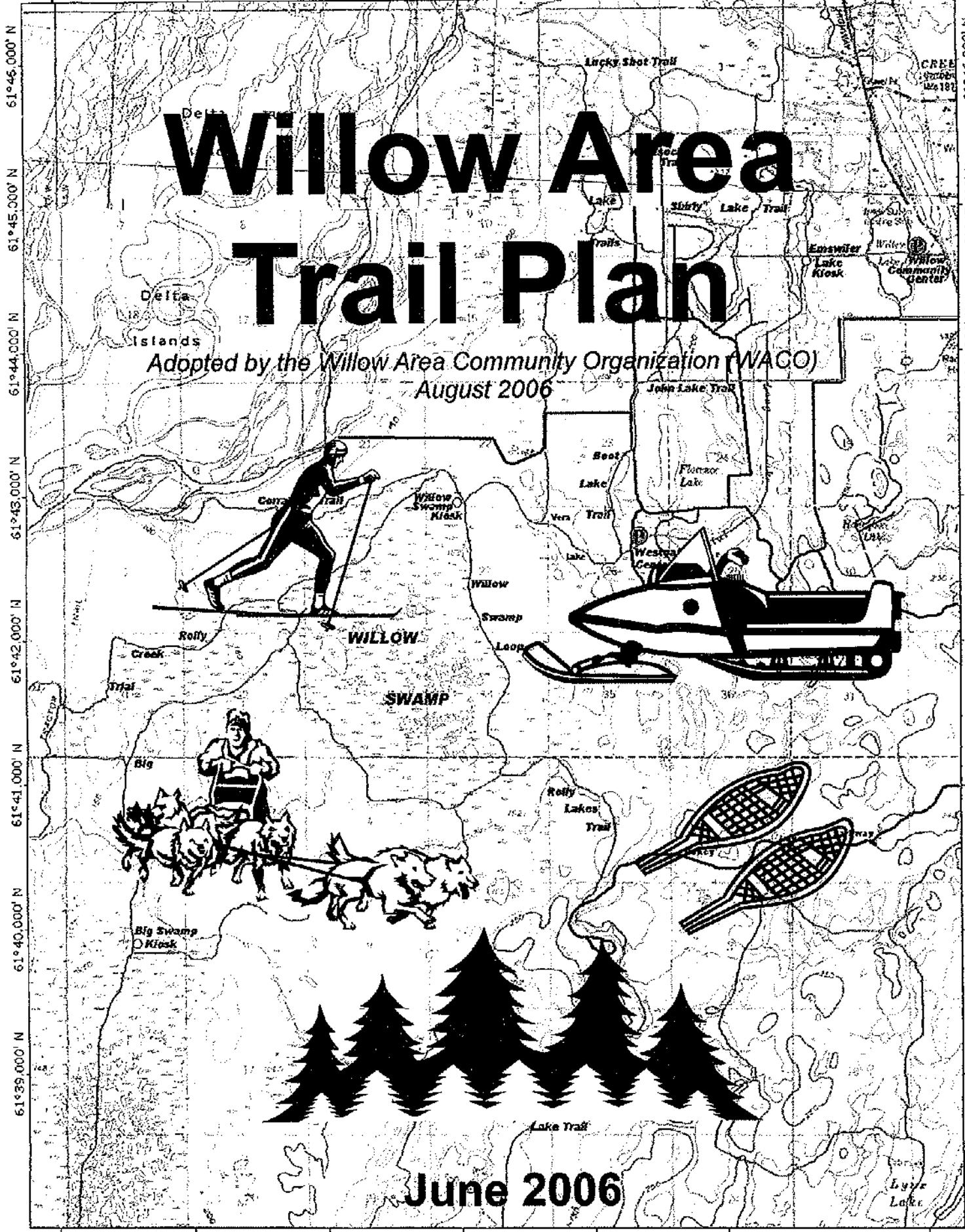
As the selection process continues for the Port MacKenzie Rail Extension, it is important for the decision makers to gather and analyze as much information as possible. The Willow Trail Committee strongly encourages the Willow Area Trail Plan be an important part of that decision making process.

Sincerely,



Steve Charles, chair  
Willow Trail Committee

*(This page intentionally left blank.)*



# Willow Area Trail Plan

Adopted by the Willow Area Community Organization (WACO)  
August 2006

WILLOW SWAMP

June 2006

*(This page intentionally left blank.)*

**TABLE OF CONTENTS**

**PREFACE** ..... 3

**WILLOW**

**A Gateway Community for Outdoor Activities.** 4

**TRAILS**

**West Gateway Trail System** ..... 6

**West Gateway Map.** 7

**Haessler-Norris Trail System** ..... 8

**Haessler-Norris Map** ..... 9

**Emil Stancec Trail System** ..... 10

**Emil Stancec Map** ..... 11

**Trails Preservation** ..... 12

**MUSHING HABITAT**

**The Need** ..... 13

**Mushing Habitat Preservation** ..... 14



*(This page intentionally left blank.)*

## **PREFACE**

The purpose of the Willow Area Trail Plan is to promote the preservation and development of recreational trails within the Willow area.

As Willow grows, the importance of recreation and trails to our community needs to be conveyed. However, rapid population growth threatens existing trails for all users, and for the sports snow machining and dog mushing in particular.

This document is meant to guide:

Public agencies in making policy decisions,

Private companies in their investment decisions, and

Individuals in making their personal decisions.

Planning is a continuous process and this plan was written with information and opinions of Willow residents in 2006. As future developments and community needs change over time, this document should be revised to reflect those changes.

*(This page intentionally left blank.)*

## **WILLOW- A Gateway Community for Outdoor Activities**

The community of Willow is situated on the edge of the most developed area in Alaska and this population is steadily moving into the Willow area. A residential and recreational community of several thousand people, it is unincorporated and has a voice through its community organization, the Willow Area Community Organization (WACO). Growth in population and recreational activity during the last decade has been among the fastest in the state and will certainly continue. Quality of life is one of the main reasons families relocate to Willow where they can enjoy a rural lifestyle with its wildlife lakes, streams, rivers and mountains. Winter is a popular time of year with abundant snowfall providing opportunity to enjoy the outdoors. A winter weekend on the Parks Highway will see numerous vehicles loaded with snow machines, dog teams and skis heading for recreation in the Willow area.

Our trail systems not only enhance this quality of life, but they also support businesses and provide employment opportunities and economic benefits to the area, particularly important in this customary sluggish part of the year for merchants. Further, it has been well documented in the Anchorage area that trails, in and of themselves, actually increase the value of the properties having access to the trails as well as the overall community. Willow has well established trails that already are in use and it is the goal of this Willow Area Trail Plan that they be protected and enhanced.

Tourism, having become the second largest industry in Alaska, has brought an increased number of visitors to the community as well. Here, too, Willow's trail systems play a significant role.

The Willow Trail Committee (WTC) is a special standing committee of WACO. As a Mat-Su Borough Trail Crew Member, it recently received a *2005 Miles by 2005* award by the Mat-Su Borough. The Trail Committee has identified and mapped three trail systems in the area:

**West Gateway Trail System**  
**Haessler-Norris Trail System**  
**Emil Stancec Trail System**

Maps and trail routes of these systems are attached. The routes of all three systems are based on consistent historical use for mining, hunting, trapping, dog mushing, supply freighting, and recreational snowmachining. These systems have been submitted to the Mat-Su Borough Lands Division for inclusion as multi-use trail systems requiring cooperation among trail users. All are extensively used today.

Our goals are to:

- Identify historic and currently used trails.
- Obtain public easements for recreational trails and trailheads.
- Maintain trails for safe and enjoyable use.
- Provide information to the public on trail safety and trail etiquette.
- Provide a forum for trail conflicts.
- Recognize trails as an important natural resource that contributes to the economic well-being and the quality of life in Willow.

## TRAILS

### West Gateway Trail System

Located directly west of Willow between the Parks Highway and the Susitna River, the West Gateway Trail System is a recreational jewel. These trails connect Willow to Big Lake and Nancy Lake Recreation Area Trails to the south and access to the Big Su, and Deshka Rivers to the west. Willow's location makes it the most popular jumping off point for people traveling west to back country cabins and lodges.

The earliest trails within the West Gateway system, the Lucky Shot and Corral Hill Trails, were blazed nearly 100 years ago for freighting to the Hatcher Pass mines from the Susitna River. Homesteaders, hunters, trappers and dog mushers through the years have extended the trails.

At present, the Willow Trail Committee, in conjunction with the Mat-Su Borough has acquired legal easements on all the West Gateway trails. They are also included in the Mat-Su Trails Plan as "regionally significant." As Mat-Su Borough Trail Crew Members, the WTC has been assigned maintenance of these trails. WTC volunteers brush and sign the trails year around and groom each week during the winter months. In addition, kiosks with maps, safety and educational information are maintained at all trailheads and strategic locations.

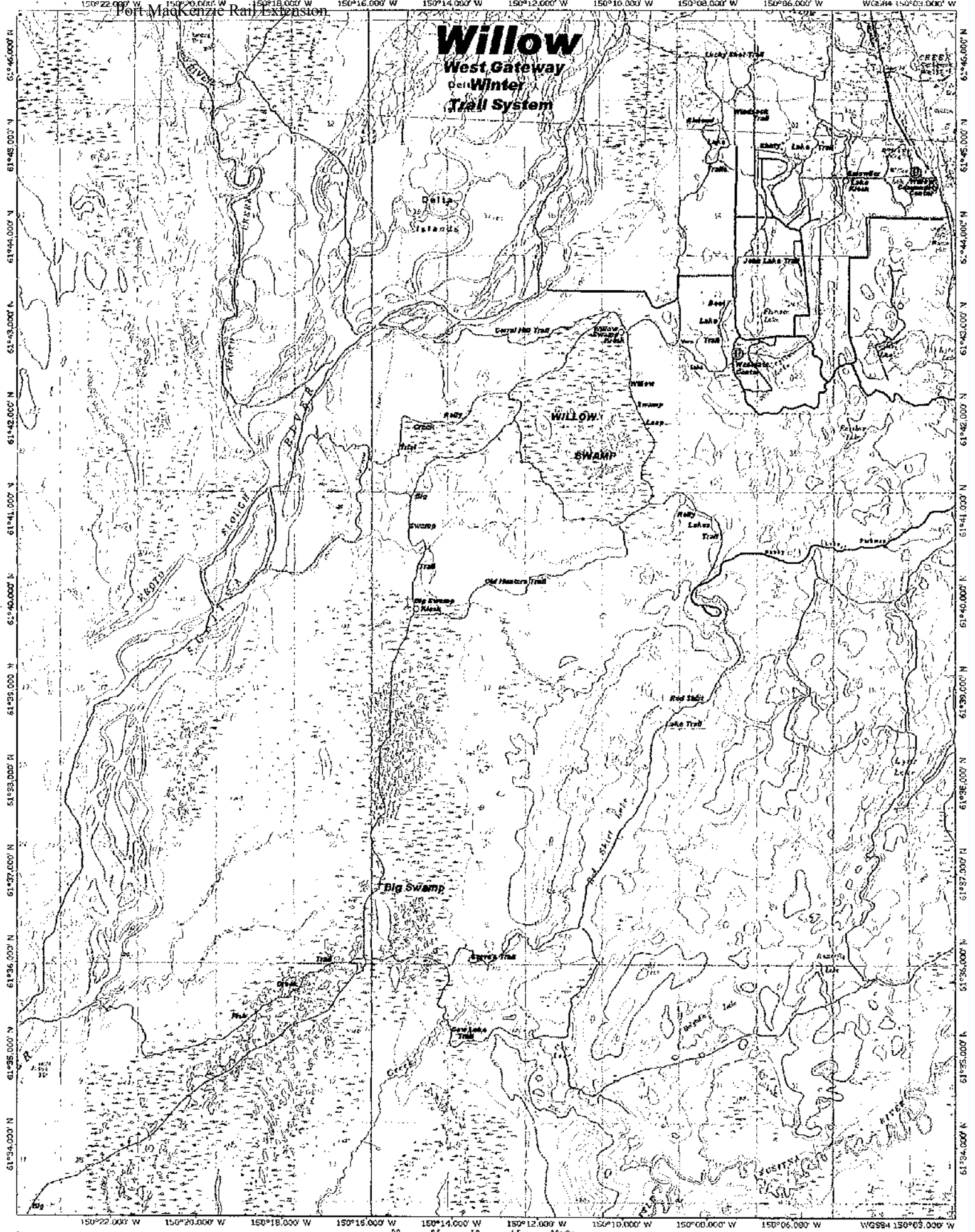
Because of its proximity to residential areas and trailheads, the West Gateway trails are very popular with recreational trail users of all varieties including snowmachiners, dog mushers and skiers. Willow is host to the Alaska State Winter Carnival each year and is dependent on these trails for the many snowmachine, dog mushing, skiing and skijoring events.

In addition, other events utilize these trails including the Klondike 400 and Aurora 200 snowmachine races, the Junior Iditarod, the Don Bowers 200/300 sled dog race, Klondike 300 sled dog race and the Earl Norris Open Sled Dog Race. The Iditarod Trail Sled Dog Race often uses the West Gateway Trail System for its restart.

#### Future Plans and Goals for the West Gateway Trail System:

- Establish legal protection for the trails
- Improve safety for road crossings/plan for tunnels under roads
- Continue maintenance projects through grants and funding
- Replace signs to conform with State standards
- Plan and improve trail access
- Expand trail events and race capabilities
- Develop detailed maps of Trail Systems
- Incorporate the Willow Community Center as the hub

# Willow West Gateway Delta/Winter Trail System



## Haessler-Norris Trail System

The Haessler-Norris Trail System has its primary roots in freighting, trapping, dog mushing and mining. It lies east of the Parks Highway; its boundaries are the Little Su River to the south, Hatcher Pass Road to the north and Hatcher Pass Recreation Area to the east. All three trail systems in the area are connected to one another historically.

The southern portion of the Haessler-Norris System includes the Willow Creek Sled Trail, otherwise known as the Herning Trail. This trail was an original route from Knik to the gold fields in Hatcher Pass. Hatcher Pass Road to the north was formerly a trail also leading to the gold mines. Trapline trails following the Nancy Creek drainage past Windy Lake to Deception Creek were the origin of many other trails in the system.

The Haessler-Norris Trail System was named for dog mushing pioneers in the area. Lloyd Haessler was a local trapper and musher who established "Lloyd's Trapline", one of the several trails documented in the Deception Creek Management Plan. Earl and Natalie Norris, sprint dog mushers who homesteaded in Anchorage in the 1940's, were instrumental in establishing sled dog racing in Anchorage. Earl was co-founder of the Fur Rondy Sled Dog Race in 1946. Due to development in Anchorage, they moved to Willow where they helped expand and improve the existing trails.

Primarily used by mushers today, this system provides an important training circuit for Iditarod, mid-distance, distance, sprint and recreational teams as well as excellent trails for the growing tourism business in the area.

However, the trail system is in danger of being compromised. Planned transfer of state land in the middle of this system to the University, the sale of public lands to the private sector, and the placement of subdivisions without trail consideration are causes of concern.

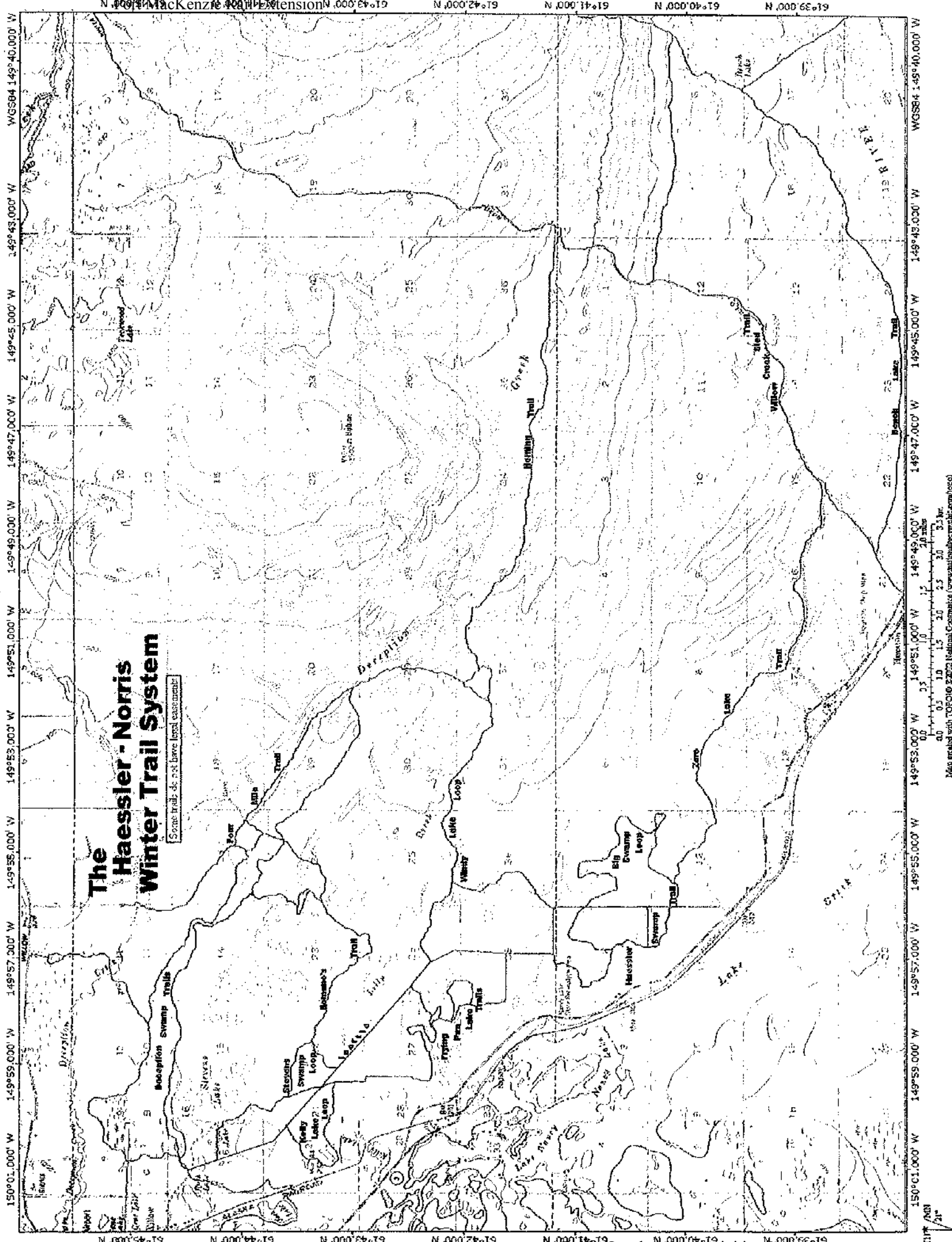
### Future Plans and Goals for the Haessler-Norris Trail System:

- Establish legal protection for all trails in system
- GPS/Survey/Map trails
- Reconnect this system to the Emil Stancec System
- Expand system connecting trails to the north and east
- Install uniform signage
- Develop trail events and racing capabilities for system
- Protect, maintain and expand trail access



# The Haessler - Norris Winter Trail System

Some trails do not have local easements.



## **Emil Stancec Trail System**

Most of this system is located to the east of the Parks Highway. It runs from Hatcher Pass Road on the south to the Kashwitna River on the north and to the foothills of Hatcher Pass bearing east. A small part of this system is also located on the west side of the Parks Highway where it connects to the West Gateway Trail System.

This system presents a variety terrain. From the low-lying swampland to the higher elevations of the Talkeetna foothills, one can experience panoramic views of three mountain ranges.

Emil Stancec worked for the Alaska Railroad and trapped for a living. His traplines provided many corridors that are now a part of the trail system; earlier development of these trails was done by the U.S. military conducting maneuvers.

The main trail in this system is the "Tank Trail", also known as the Talkeetna Mail Trail. Connecting from the north end of what is now 4-Mile Road, in the Haessler-Norris system, the Tank Trail continues north, originally crossed the Kashwitna River and proceeded to Montana Creek and Talkeetna. Although documented in the Kashwitna Management Plan and evidenced in the U.S.G.S. maps of the region, the continuity of the Tank Trail is now blocked by subdivision development. Proposed logging (wood-chips) in the Willer-Kash area further endangers the existence of the trails within this system.

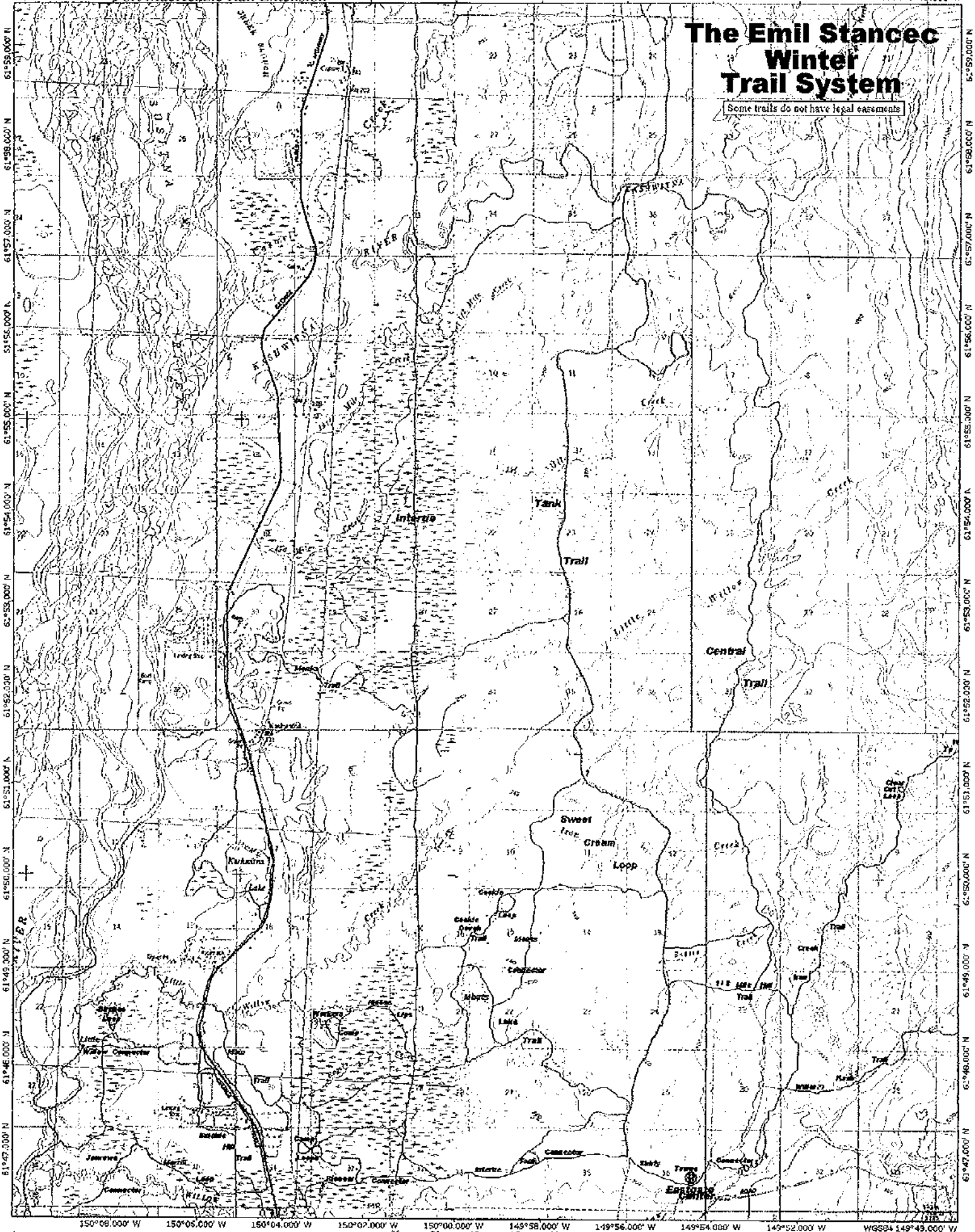
Dog mushers, snowmachiners and hunters use this system extensively.

### Future Plans and Goals for the Emil Stancec Trail System:

- Establish legal status of trails within system
- GPS/Survey/Map trails
- Improve Central Trail and connect it to Tank Trail
- Connect to trails east of the system
- Reconnect this system to the Haessler-Norris Trail System
- Develop trail events and racing capabilities within system
- Install uniform signage
- Maintain and improve trail access
- Incorporate the Willow Community Center as the hub

# The Emil Stancec Winter Trail System

Some trails do not have legal easements



## Trails Preservation

Aside from the Mat-Su Borough being the fastest developing area in the state, recent events (listed below) have highlighted the need for assistance from the Borough and State in preserving these trails.

- The transfer of state lands to the University of Alaska, an area in the heart of the Haessler-Norris Trail System, was legislated over the objections of the Community of Willow.
- The rapid transfer of Borough and State land to private ownership.
- Scheduled timber sales (wood-chips) in the Kashwitna Management Area will have a tremendous impact on the Emil Stancec Trail System.
- New subdivision developments that block trails and/or access to the above trails.

The Willow Area Trail Committee requests recognition and preservation of the trail systems documented in this Plan to facilitate protection and development of these trail systems,

*We request* that Willow's three trail systems be legally dedicated upon all public lands owned by the State of Alaska or the Mat-Su Borough,

*We request* that any future land transfers by the State of Alaska, the Mat-Su Borough or quasi-public entities such as the University of Alaska, or Mental Health Trust Lands be subject to these trail systems, and that title to transferred lands reserve out these trails for public use, as well as access to them,

*We request* recognition and preservation of Willow's trail systems during any platting process by the Mat-Su Borough. This applies particularly to the Haessler-Norris and Emil Stancec areas, since the legalization process for trail protection often takes years to accomplish, and

*We request* funding for surveying and documentation of trails within the Haessler-Norris and Emil Stancec areas to accelerate legal protection and inclusion in governmental trail plans, and

*We request* funding and assistance in relocating trails off of private property if necessary, since previous land transfers by the State and the Borough did not protect these trails.

## VITAL MUSHING HABITAT

The Willow Trail Committee maintains that the wisest and best use of public lands within the *Emil Stancec and Haessler-Norris Trail Systems* is for recreation and dog mushing related activities. These two trail systems cover an area of 300 square miles.

The preservation of trails for dog mushing also translates into preservation of trails for other outdoor sports enthusiasts. It is by no means the intention of this document to suggest that this area be reserved exclusively for mushers or other winter sports advocates.

The Willow Trail Committee would like to point out however; the urgent need to set aside a geographically suitable area where sled dog kennels can legally operate. For this reason, the Willow Area Trail Plan wishes to address the establishment of what we have called an area of *Mushing Habitat*.

### The Need

Dog mushing is the official sport of State of Alaska; Willow is considered, by many, the mushing capital of Alaska. The community is home to many professional kennels that support teams for the Iditarod Sled Dog Race, the Fur Rondy Sprint Race, the Open North American Sprint Race, the Yukon Quest International Race, Willow Carnival/Norris Cup Sprint Race and the Norman Vaughan Serum Run. In addition to these professional kennels, recreational sled dog teams and tourist oriented kennels make kennel owners an important contributor to the local economy.

Certain crucial things are necessary in order for a sled dog kennel to operate and properly train canine athletes:

- Looped trail systems that comprise enough miles for teams to train for the Iditarod and other distance races.
- Continued access to these trail systems from adjoining sled dog kennels.
- Trails that will not be compromised by future subdivisions.
- Legal protection from noise ordinances that would limit times of day and length of noise emanating from a kennel.

With abundant snowfall and large open spaces, Willow is the perfect "mushing habitat." The Haessler-Norris and the Emil Stancec trail systems, in particular, provide the trails necessary for professional as well as recreational sled dog kennels to operate. This document has already addressed the importance of protecting the trail systems. 13

The protection of these trail systems, however, is not the only requisite for a sled dog kennel to exist. The concern of noise must also be addressed. It is no secret that dogs make noise; a kennel of sled dogs can bark and howl during any part of the day. Dogs get excited before eating and training; they bark when a moose or other visitor wanders into their area. A kennel of sled dogs cannot exist if a noise ordinance allows the animals to vocalize for only a certain length of time during a certain part of the day. That is why a solution to this concern is absolutely necessary for mushers and teams to flourish.

Failure to protect training trails and failure to amend noise ordinances, both Borough and State, will ultimately lead to the decline and elimination of sled dog kennels. Evidence of this is the loss of kennels and training trails in the Palmer and Meadow Lakes areas, and the rapid decline around Knik. One important factor in the Iditarod Restart being held in Willow in recent years is not only because of the lack of snowfall in the Wasilla area, but also because of the difficulty in providing trail access from Wasilla to Knik due to over 30 road crossings.

*We do not want Willow to be the next community to lose its mushing heritage.*

*Mushing habitat preservation; once lost, it can never be regained*

With these concerns in mind the Willow Trail Committee would like to make some proposals which we deem necessary if the sport of dog mushing is to continue to thrive in the area.

*We call for* public lands incorporating the Haessler-Norris and Emil Stancec Trail Systems be recognized as areas of *Vital Mushing Habitat* and be incorporated as such in the Deception Creek Management Plan, the Willow Sub-Basin Area Plan, the Kashwitna Management Plan and/or any other land use plan affecting these areas. These plans should be amended to recognize and protect these areas for public recreation, tourism and dog mushing.

*We call for* the State of Alaska, the MSB and the MSB Animal Care Advisory Board, with approval of the community in question, to modify or amend the noise nuisance or annoyance ordinances and the statutes to allow and encourage kennels to operate and train within the Haessler-Norris and Emil Stancec Trail areas.

*We call for* the State of Alaska to amend AS 09:45 to include "dog kennels, whether operated for profit or recreation" within the protection of AS 09:45:235.

The importance of dog mushing to the heritage of our great state is undeniable. The importance of dog mushing to tourism and the economy is also significant. But, as the state grows and Alaskan lifestyles change to resembling those outside of Alaska, we need to preserve our unique Alaskan qualities.

Establishing *Mushing Habitat* in the lands encompassing the Emil Stancec and Haessler-Norris Trail Systems will not only allow dog mushers and dog kennels to continue to live, operate and train on these trails, but it will also protect a true Alaskan heritage.

# WACO

Willow Area Community Organization  
Linda Oxley Chair  
chair@waco-ak.org

P.O. Box 1027 • Willow • AK 99688  
(907) 495-6633  
www.waco-ak.org

September 30, 2006

To who it may concern:

At a regular meeting on August 7th, 2006, the Willow Area Community Organization passed a motion by the Willow Trail Committee to adopt the Willow Area Trail Plan. The plan was adopted by a unanimous vote, 94-0.

Discussion preceding the vote and the subsequent vote clearly shows the level of support in the community for trails and recreation. This document should be strongly considered when government agencies make policy decisions and businesses and individuals make investment choices.

Sincerely,

Linda Oxley / PM

Linda Oxley, Chair

WACO is a 501(c)4 organization



**Willow Dog Musher Association**  
Willow, Alaska

**RESOLUTION NO. 07-06-01**

**A RESOLUTION SUPPORTING THE *WILLOW AREA TRAIL PLAN* AS PRESENTED BY THE *WILLOW AREA COMMUNITY ORGANIZATION (WACO) – WILLOW TRAILS COMMITTEE.***

**WHEREAS:** The Willow Dog Musher Association is a State of Alaska recognized non-profit whose main mission is to protect and preserve the dog mushers' historical way of life for today, tomorrow and future generations, and

**WHEREAS:** The sport of dog mushing is the State of Alaska's Official Sport, and

**WHEREAS:** We, the members of the community of Willow and the Willow Dog Musher Association utilize the West Gateway, Haessler-Norris, and Emil Stancec Trail System in order to enjoy and train our sled dogs, and

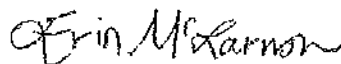
**WHEREAS:** The Willow Dog Musher Association see the above mentioned trails as vital mushing habitat to our livelihood and will seek ways of protecting these trails and the rights of dog mushers.

**NOW THEREFORE BE IT RESOLVED THAT** the Willow Dog Musher Association supports the *Willow Area Trail Plan*, and authorizes the Willow Area Community Organization (WACO) – Willow Trails Committee to present it with our support in order to protect our way of life and to secure our uses of these areas for today, tomorrow and future generations.

**BE IT FURTHER RESOLVED THAT** the Willow Dog Musher Association will address any necessary issues to bring this plan to fruition.

**CERTIFICATION:**

The foregoing resolution was passed and approved by a duly convened meeting of the Willow Dog Musher Association, this 26<sup>th</sup> Day of July, 2006, by a unanimous vote of its voting membership.



Erin McLarnon  
President



Justin Savidis  
Secretary



**MATANUSKA-SUSITNA CONVENTION & VISITORS BUREAU**

October 16, 2007

Stephanie Wheeler  
Public Involvement Officer  
Alaska Railroad Corporation  
327 W. Ship Creek Ave.  
Anchorage, AK 99502

Dear Ms Wheeler:

This letter on behalf of the Mat-Su Convention & Visitors Bureau board of directors is intended to be included in the public comments regarding the proposed rail corridor connecting Port MacKenzie to the Alaska Railroad.

Tourism is the second leading industry in the state and the area being considered is a highly used recreational area. Fishing, rafting, snow machining, dog mushing, and wildlife viewing are common activities in the area. Two well-known and popular parks are also on the map and they include Nancy Lakes State Park and Willow Creek State Park.

The Mat-Su CVB board of directors opposes the rail corridor crossing any park land. We ask that you consider an alternative that least affects the recreation opportunities for residents and visitors.

Sincerely,

Bonnie Quill  
Executive Director, Mat-Su CVB

*(This page intentionally left blank.)*

Received  
10/16/07 CW

October 18, 2007

Patty Sullivan  
Public Affairs Director,  
Matanuska-Susitna Borough  
350 E. Dahlia Ave.  
Palmer, Alaska 99645

Dear Ms. Sullivan,

My name is Bob Chlupach.

I live in Willow, Alaska.

Due to a long ago pre-arranged trip to visit my 80s plus Mother, who is still in good health, I will not be able to attend the forum on Oct. 24 in Willow regarding the Port Mackenzie Rail Extension. I am therefore submitting my concern, comments, questions and desired considerations to you at this time. To do this requires a framework of time line history to preface where my concerns lie and how they were arrived at. Presently, a concern I have is that this is not viewed as just another harangue by Joe Public and I will keep the tone of this message to avoid this demeanor. As well, I apologize for its length but too, there is a lot of information put together like the pieces of a jigsaw puzzle. I am also sending this letter to Stephenie Wheeler, Public Involvement Officer for the Alaska Railroad Corporation, Governor Sarah Palin, Willow Area Community Organization chair person Linda Oxley and Willow Dog Musers Association chairman Erin McLarnon.

When I was younger, my wife and I, settled near Mirror Lake before the 4-lane was constructed, working first in Anchorage and then Palmer. We had sled dogs and before sled dog people are lumped into a category and this letter filed, continue to bear with me, as the contents to follow are not just about sled dogs but as well family and the future of such. Ultimately, we chose a move to Willow and by the time the 4-lane was completed we had. Nothing against the 4-lane but it did force us to evaluate where we could best raise our daughter and still experience the quality of life we sought for the whole family, that being a more rural lifestyle still allowing us to continue as professional people.

Our first location in Willow was between Mile 65 and 66 along the Parks Highway. There, we were able to continue work as professional employees, our daughter got a quality public school education later becoming a Dietetics Program Manager at the Alaska Native Hospital in Anchorage and we were able to enter and complete the running of 10 Iditarod Sled Dog Races over a period of 4 decades. Moving to Willow at that time, the community was quite small. Taking advantage of no longer used trap line corridors and pure and simple lay of the land, for sled dog trails, I flagged and cut many, many miles of trail in addition to what was already existent which was not a lot. Today, the core of that trail network is the Haessler-Norris Trail System and is now extensively used

by Iditarod entrants, snow-mobile enthusiasts, cross country skiers, and many other types of recreation, to list just a few.

Some years later, as our daughter was growing and she at a point where she would choose a vocation we began looking for "Parks Highway frontage". There were two primary reasons; however, as you will see a third emerged. First, our daughter was thinking of becoming a veterinarian and with that we intended to build a corresponding facility for a practice and highway frontage would facilitate its success. Second, no matter what happened, highway frontage would serve as a future investment of which ultimately the value would be realized by our daughter in time as it was very apparent then as it is now, highway frontage is a valued entity of limited quantity. Third, with professional career still intact the ability to commute and be able to train sled dogs was also desired. Parks Highway frontage became a priority. We perused countless soil maps along the Parks Highway and discovered several parcels fitting the bill but none with soils that had an inherent drainage structure, that being sand base below topsoil, such as what we found in Mile 72 to 74. Good drainage soil is of concern to any livestock owner and the fact that we had sled dogs made the decision to pursue necessary. ALSO, we wanted to be sure and locate such that the inherent noise of a kennel of sled dogs at feeding time and while hooking them up for training would not be a nuisance to any neighbors. Much to our liking in this vein of privacy thinking, we sought parcels more isolated by geographical or physical location. Our investigations again yielded parcels in the area of Mile 72-74. This was not by any means a magical area but too provided close enough proximity for tolerable commuting distance. This area was further enhanced by adjacent large parcels that were not subdivided and had isolation to the east by the existence of the Alaska Railroad corridor. So, it was basically a no neighbor situation, what better place to have a kennel of sled dogs and still maintain an investment for the future. So it was, negotiated, and ultimately purchased property in the Mile 73 area.

With continued interest of running the Iditarod Sled Dog Race, I needed to establish another trail network. Another musher lived across the highway and at the time the trail system was relatively limited and confined. By using USGS maps and copious amounts of physical exploration, several potential trail corridors were located; some utilizing old trap lines, some utilizing military trails left over from World War II, and the majority derived from simple lay of the land. It was an effort that took several years and now the original trail network that I and others helped brush out or created is now the core of the extensive Emil Stancec Trail network.

Since the major effort to establish two trail systems, it is now evident just how important these systems are to all recreational users but perhaps most important to those folks with designs of either running sled dogs geared for the Iditarod, Quest or several of many local shorter events. How evident has this impact been? Many, many people have relocated their homes and dog teams to these respective trail systems. From the Willow area, drivers enter Iditarod and Quest each year and frankly the vast majority of them are extremely competitive. Names such as; Dee Jonrowe, Rayme Smyth, Vern Halter, Linnwood Fiedler to mention a few while on the speed angle; Egil Ellis a multiple time champion, Bill Kommuller a regularly top 10 speed placer, JP Norris and myself

entering the World Championship Fur Rendezvous race in Anchorage, just to name a few in their respective events. This letter however is not about a whose who but at this point a piece of the puzzle which determines inherent value to a specific populous and the annual economic spin-off that has been going on for quite a few years now surrounding these two trail systems.

Not immune to the calamities of life I ventured briefly "outside" of Alaska a few years back. I explored several "very well known" dog mushing areas in Idaho, Montana and Wyoming looking for something to simulate the conditions of the Alaska home only with cheaper living expenses. What I found floored me. Food prices were cheaper but not so as expected, land prices in snow country were no different than Alaska and substantially more in the Rockies, same for acceptable rural homes, but "absolutely nowhere" could I duplicate or come remotely close to having the quality of trail system as the Emil Stancec Trail System. Returning to Alaska, I dissected the economic advantages and disadvantages of Tok, Fairbanks, Glennallen, Kenai Peninsula and still none of these were as competitive pricing wise in all areas and again, "nowhere" could I see a trail system that could compete with the Emil Stancec Trail System for everything that makes up quality of life.

So, I ended up reinvesting on some of my previously owned property, subsequently building a house knowing now the quality of life perceived greener elsewhere was pure mental flatulence.

When one tries to put a value on items that make up the quality of life how does one do this and define the very things that make up an inherent quality? The conundrum becomes even more so the older one gets, particularly if they are very active in the sled dog venue. One just doesn't relocate and go cutting trails again when they get aged and to find such potential and still have realized investment while one is still alive, such as owning property along the Parks Highway, is likely non-existent.

I've brought you this far and you may ask what the point of all this is? Why does it seem so personal? Simple, the Willow Corridor for the Port Mackenzie rail extension goes directly through the middle of our house and completely eliminates whatever land integrity and value we currently have, not to mention being displaced off a trail system I had been a primary conceiver and builder of and not to mention the investment in Parks Highway frontage property from a personal development standpoint and not to mention, this is "OUR HOME, OUR LIFESTYLE, OUR LIVELIHOOD".

The inherent value of property depends on your desires for particular aspects in regards to the quality of life. For instance, a dog musher's property where one can train from the doorstep is no different than a skier owning a condo at a ski resort, or a golfer living adjacent to a golf course, a lake dweller who has a penchant for water skiing, a snowmobile enthusiast who lives rural to avoid machine transport and the list can go on and on. Historically dog mushers are not viewed in this same light.

So, two questions come to mind. On a personal level, how does what you are promoting begin to compensate for what I've written about above and do you really think we would have reinvested in this property had we known there were plans for a railroad to run through our house and dissect this property?

One might say, "Bob aren't you putting the cart before the horse? A decision hasn't yet been made as to the corridor route".

That now leads us to a brief review of history of the Matanuska-Susitna Borough public process input. At no time was there ever a meeting in Willow to discuss a rail corridor PRIOR to the Borough being caught for improper disclosure to the public process. A meeting some years back was held at Houston and the Willow corridor decided thereafter. There were "NO" public notices in Willow of this meeting. A statement was made, there aren't enough people in Willow that it would affect, or something very similar to that effect. In the middle of last winter, over the holiday season, a public notice appeared in the Willow Post Office for public comment to a rail corridor originating in Willow, Railroad Corridor 3, to Point Mackenzie. The Borough however was brought to task for trying to get through the public process with minimal process under the people's radar screen. They then extended the comment period another 30 days and it became apparent something was amiss to them and now would have to tow the line via appropriate channels. The Borough in all likelihood would deny any of this and it comes as no surprise to anyone. As it is common knowledge, the bottom line is the Borough is seeking ways to justify Port Mackenzie. Of course we could talk till we are blue in the face over the various facets of the tax base and its burdens on property owners or for that matter the creation of an industrial complex such as Port Mackenzie.

In addition, the Borough has violated its own disclosure codes after the Houston meeting a few years back. Since then, I know of three pieces of property which have been sold and purchased in or near the Willow rail alignment corridor. Not one of these purchasers was ever informed prior to closure of sale that this process, a railroad corridor, was underway, nor were the sellers of said properties. This also is in violation of the Boroughs own codes. Do you think any one of these parties would have bought property had they known?

Since the location of any of the corridors serves to facilitate usage of Port Mackenzie, may we back up some and clarify where the money came from to build the Port? Clearly the cart has been before the horse for quite some time and questions such as this should be answered on the record by the Borough. It all has to do with trust, mutual public trust, and trust in our local government as taxpayers. As taxpayers, we might have bought something we never knew was happening and certainly with a multi-million dollar plan as we are now witnessing, all cards need to be on the table.

Attending a Borough Assembly meeting after my presentation of issues concerning the public process of the railroad corridor last spring, I was told the corridor was "just penciled in", yet it was the only corridor presented on any of the maps and the only one "discussed" at the time. Now of course, the whole process has changed with the Borough

saying we will revisit all other options to which we now witness a glossy polished presentation from the Borough and Alaska Railroad addressing the advantages and need for such a project. This while trying to garner support of the need for a rail connection to Pt. Mackenzie all the while deflecting attention away from the issue of the Willow corridor. The public can comment but obviously cannot produce a shined up version, even if it chose to do so as our comments and presentation cannot be done with taxpayer dollars. The op-out for the Borough then becomes the NEPA process; however, challenging the Borough's mass media builder approach is at best a real David versus Goliath.

I am normally not such a cynic but am forced to be when an entity is not forthright in conduct.

As a retired State of Alaska fishery biologist I personally object to the construction of this corridor. I know well what wetlands mean to surrounding salmon and trout streams. Too, as a sport fisher I know that Willow Creek supports a viable sport fishery on resident species of rainbow trout and Arctic Grayling. People from around the world fish this drainage, more times than not by guided rafting. In my current life, I am a sport fishing guide. In questioning other local guides, they found clients do not want to come to Willow Creek to see a railroad corridor while fishing. This is a major roadside salmon fishery BUT upstream from the confluence with the Susitna River this drainage at specific times can be a world class fishery for trout and grayling. To risk impairing the quality of this fishery and the annual revenue it creates is unconscionable.

I was the lead Fishery Biologist in setting forth a program from inception to fruition that supplied supplemental king salmon released as smolt to return the Willow Creek drainage as harvestable adults in an effort restore a depressed king salmon run at the time. The program coincided with the development of the Willow Creek State Recreational Site by State Parks Division. Currently, this supplemental king salmon release supports one of the most, if not the most popular east side Susitna River roadside fishery in upper Cook Inlet. Hundreds of people fish this annually. Farther upstream, two lodge facilities are nearly always jam packed with RVers, diners, guided fishing trips, and etc. during the summer salmon fishing season. "That" is their livelihood. With almost certainty, the projected number of coal trains would greatly impact the livelihood of these two private enterprises since the rail corridor would be just downstream. In addition, Willow Creek Recreation Area would be dissected by the corridor and as witnessed in other areas, chain link fences installed to prevent people from accessing areas on the other side of the tracks. There is no mitigation possible to come close to compensation to all the users of this area and if a train calamity were to occur in this area, what then? Therefore, I am not in favor of the corridor in this particular location in regards to impact on fishery resources.

Railroad corridors and moose do not get along and it is an undeniable fact during the winter moose gravitate towards railroad and highway corridors. Both the Willow Creek area and the ridge line along Red Shirt are significant wintering grounds for moose. A "new" corridor in core calving and wintering grounds would mean disaster for moose



numbers in the area. What's the railroad going to do for mitigation, say there will be an allowable mortality or fence the whole corridor to prevent movement of wildlife from one side of the tracks to the other? There is a similar circumstance along the highway between Anchorage and Eagle River in the Ft. Richardson area. Therefore, I am not in favor of the corridor in this particular location in regards to impact on wildlife resources, especially in a more remote section of south central compared to the highway along Ft. Richardson.

Farther down, where the corridor travels just west of Red Shirt Lake, undoubtedly this would affect one of the "crown jewels" of State Parks. Steeped in history and usage as a day and multi-day canoe trail system traversing numerous lakes, South Rolly Lake campground would be located right on the periphery of the railroad right-of-way. South Rolly Lake campground is always full of family campers during the summer months and the trail from there along the ridge line to Red Shirt Lake is one of the most intensively used hiking trails in south central Alaska.

Anthropologically, Denaina Athabasca peoples historically used this trail and along the ridge overlooking Willow Creek roughly where the rail corridor would be there is evidence a Denaina village site was present, obviously taking advantage of summer salmon returns.

The number of people utilizing the Nancy Lake Recreation Area and the Willow Creek Recreation Area means a great deal to the local economy. A disruption such as the endeavor by the Borough and Alaska Railroad would most assuredly impact numerous small businesses in the Willow Area.

Traversing wetlands always brings up pros and cons where ever one travels. Wetlands serve to purify water by the time it soaks into the water table. Wetlands are a dynamic living entity, not a stagnant piece of marshy land having no value. All one has to do is look at the Palmer Hay flats and it becomes apparent that the east side of the highway is far more moist than the west. The highway has essentially prevented broad spectrum water perking and dispersal along its corridor. This is merely an observation, not a statement in disfavor of the highway. However, an ecological lesson should be drawn in consideration of what this corridor will do to all the adjacent wetlands on the way to Pt. Mackenzie. A potential mitigation provision of culverts and other such passage devices is not mitigation to the functional soundness of the surrounding lands. That serves more to appeal to a human mental justification designed to feel gooder. There is really no provision that can be made to provide for unfettered water movement along a marshy surface while perking into wetland soils. The net result of an all natural free flowing system is a recharged purified water table. I am therefore against the existence of a railroad corridor in this area.

I am very much aware of the requirement of the Surface Transportation Board (STB) that construction of a new railroad line requires the Board's approval under 49 U.S.C. 10901. Also, I am aware of the requirement of an environmental review in compliance with the National Environmental Policy Act (NEPA), 42 U.S.C. 4321-4335

and the Board's Section of Environmental Analysis (SEA) to ensure that the STB complies with NEPA over related environmental statutes. Please do not say this is where all my concerns will be answered. I've presented enough questions outside the NEPA process for you to address.

In summary, my questions from the above concerns are as follows:

In relation to the alignment of the Willow corridor, due to the uniqueness of some property owners whose lives, home and livelihood are dependent on location, how will an independent evaluator appreciate these facets in determining value if they are not parlay to the situation themselves?

In determining value, in some cases, comparative land and home prices cannot be accurately accomplished by previous sales in a general location, then how will value be determined?

With the railroad corridor now on the table, no matter what the current status is, this process has effectively eliminated anyone of those directly affected families from selling their property. How do you propose to address this in instances such as; spouse has a change in job venue, a family emergency, or simply wanting to relocate?

Will property owners have a say in the determination of the value of their property?

Who determines what mitigation results are acceptable?

What happens if a rail corridor is determined and mitigation results are not acceptable?

Who determines where the rail corridor will go?

Who determines if there will be a rail corridor?

Will a specific corridor be selected for STB to consider or will a "shotgun" of potential corridors be presented to the STB to consider?

This project is designed in essence to support Port Mackenzie. I fully understand the overall "broader" goals but an unanswered question is, who funded and where did the funds come from for the initial construction of the Port?

And so ends, at this time, just one of the various pieces of an enormous puzzle.

Gratefully awaiting answers to my questions.

Please send "hard copy" answers to me at my mailing address which is:

Bob Chlupach  
P.O. Box 931  
Willow, Alaska 99688

Most Sincerely,



Robert S. Chlupach  
Willow, Alaska

No portion of this document may be used or altered without the permission of the author.

## Mat-Su State Parks Citizens Advisory Board

HC 32 Box 6706, Wasilla, Alaska 99654  
(907) 745-3975 Fax (907) 745-0938

October 26, 2007

Patty Sullivan  
Director of Public Affairs  
Matanuska Susitna Borough  
350 Dahlia Ave  
Palmer, AK 99645

RE: Comment to Port MacKenzie Rail Extension Project

Dear Ms. Sullivan,

The Mat-Su State Park Citizen's Advisory Board wishes to respond regarding the proposed Port MacKenzie Rail Project. The Board has a special interest in this corridor because it is within and adjacent to the Willow Creek and Nancy Lake State Recreation Areas (SRA) and the West Gateway Trail System, a popular winter recreation area.

The Mat-Su State Park Citizen's Advisory Board feels the Willow Corridor (Corridor 3), as described in the 2003 Rail Corridor Study and in the more recent project map is incompatible with the mission of State Parks and Outdoor Recreation, especially when there are viable alternatives. Our Board has voted unanimously to oppose such an action. Corridor 3 would disrupt existing trail systems, surround Nancy Lakes SRA with railroad activity on its east and west boundaries, divide Willow Creek SRA, and needlessly destroy the peaceful character of a prime natural recreation area bounded by the Parks highway and the Susitna River.

There are few places in Alaska or the United States like Willow where such a wealth of recreational opportunities exist so close to an urban area. This recreation is uniquely Alaskan: snow-machining, dog mushing, skiing, skijoring, hunting, fishing, canoeing, hiking and ATV riding over miles of open area. Additionally, there are maintained trails and parks. A 2004 survey by Willow 2020 and the Mat-Su Borough found that over 90% of respondents chose Willow as the place to live and recreate because of the open country, beauty and quiet ([www.waco-ak.org](http://www.waco-ak.org)). While the full time resident population of Willow is small, on weekends the actual population is multiplied many times by south central Alaska residents enjoying second homes and cabins - drawn by the recreational activities.

### Board Members

Mary Anderson, Chair  
Roy Wahl  
William Royce  
Glenn Goodman, Vice-Chair  
William Fitzgerald

Ralph Baldwin  
Garret (Gary) Brown  
Shelis Jorgensen  
John Strassenburgh, Vice-Chair  
Bruce Brockett (1st Alternate)

Steve Charles  
Charles Leet  
Darin Markwardt  
Howard Carbone

The 2003 Rail Corridor Study by Tryck Nyman Hayes, Inc., vastly understates the importance of recreation in the study area, barely mentions two very popular State Recreation Areas and does not address any mitigation measures for the impacts from the rail line. For example, it states on p. 49, "The project [Corridor 3] would be expected to have some direct impacts on recreation, especially trail use and limiting access to recreation sites particularly if mitigation measures such as below and above ground crossings over trails for example are not utilized".

Premier recreational areas affected by corridor include:

### **Willow Creek State Recreation Area**

Home to internationally renowned salmon and trout fishing, Corridor 3 would be the second railroad line to cross Willow Creek within a few river miles and would bisect the recreation area. There are no possible mitigation measures for "surround sound" trains. The proposed railroad line would cross Willow Creek, a recognized RS2477 trail, Lucky Shot Trail and the main road to the SRA, all within a half mile. This scar would open up and expose the heart of the State Recreation Area.

Corridor 3 would also bring safety issues such as visitors walking on and fishing from the railroad trestle. Recently, a child was killed by a train after fishing from the tracks.

### **Nancy Lake State Recreation Area**

This popular recreation area would be impacted by the rail line running adjacent to its western boundary and would have similar recreational compromises as those in the Willow Creek SRA. Alaska residents and out of state visitors come to Nancy Lakes SRA in the summer and fall to enjoy peaceful canoe trips through dozens of lakes. If Corridor 3 is utilized, this chain of 130 lakes and ponds would be bound by railroads immediately to the east and to the west of the park. The 2003 study states the rail line would follow the lateral moraine adjacent to Red Shirt Lake which would bring it very close to recreational cabins. In contrast, the borough public notice places the rail line in the Big Swamp wetlands. Either option is bad news for resource and recreational protection. It is poor public policy to establish a railroad corridor west of the park. It is particularly troubling when a shorter route exists which would join the existing line south of Houston.

### **West Gateway Trail System**

Working together over many years, the Willow community, including snowmachiners, mushers, and skiers, has created a premier winter recreational area with one hundred miles of groomed and community maintained trails. All the trails have easements, are included in the Mat Su Trails Plan and are considered "regionally significant." Any given winter weekend will see hundreds of trail users exploring this semi-wilderness area that boasts open views of Denali, the Talkeetnas, and the Chugach Range. A railroad bisecting the Willow and Big Swamps will ruin this Alaskan experience.

The West Gateway Trails support many international and regional races including the Iditarod Restart, Klondike 300, Don Bowers 300 and Junior Iditarod sled dog races, as well as the Klondike 400 and Aurora 200 snowmachine races. Since much of the area is wetlands, trail crossings over the proposed railway would be many and expensive. Thus, these races would have to find other venues. These same trails support a vital training for such Iditarod teams as those of Martin Buser, Dee Dee Jonrowe and many others.

Tourism and recreation in our state are dependent on maintaining accessible natural country. The strip of land north of the Little Su and west of the Parks Highway is an example of accessible and quality wilderness adventure done right. The proposed Willow to Port MacKenzie Railroad Corridor would destroy the visions and dreams and hard work by many to develop a prime recreation area now enjoyed on any given weekend by hundreds of Alaskans and visitors from all over the world. There are other viable alternatives for a Port MacKenzie rail extension such as the Houston North and South and Big Lake routes that would have less impact on important recreational areas. The Mat-Su State Parks Citizen's Advisory Board recommends that the Borough eliminate Willow Corridor #3 from consideration for the Port Mackenzie Rail Extension Project and, instead, consider a route in an area that would benefit from, rather than, be hurt by railroad development.

Sincerely,

*Mary Anderson*

Mary Anderson, chair  
Mat-Su State Parks Citizens Advisory Board

*(This page intentionally left blank.)*

Rec. 10/12/07

10510 Lone Tree Drive  
Anchorage, AK 99507  
October 9, 2007

HDR Alaska Inc.  
Port Mackenzie Rail Extension Comment  
836 S Colony Way, Suite 2  
Palmer, Alaska 99645

To Whom It May Concern:

As a homeowner on Red Shirt Lake , I am submitting the following comments to object to the rail corridor being placed west of the ridge that parallels Red Shirt Lake.

I am a member of an association that has fought to keep Red Shirt Lake a quiet and tranquil part of the Nancy Lake Park System. We have worked with the State Department of Parks and Recreation to:

- Block development of a resort and road to Red Shirt
- Improve access by trail
- Develop a park host cabin at Red Shirt Lake to help maintain the civil behavior in the campground and to help maintain the three public use cabins on the lake
- Ban personal watercraft use on Red Shirt Lake to minimize noise and hydrocarbon pollution
- Establish restrictions on boating times and aircraft landings on the lake
- Create a partnership to protect the wild nature of Nancy Lake State Recreation area that is unprecedented in south central Alaska.

There are several reasons for my objection for this proposed rail route:


- Rail traffic noise in an a State Recreation area that has been set aside as a tranquil, quiet place for canoeing and recreational activity in a pristine wilderness area.
- The large concentration of moose gather on the back side of Red Shirt Ridge seeking shelter from the hunting pressure in the Susitna River Area. Opening up the rail corridor will simply put more pressure on them by providing access from ATVs and the train winter kill will be significant.
- The coal dust pollution is objectionable adjacent to a large, unspoiled outdoor recreational area.
- The Willow corridor is the only one that requires crossing the Little Susitna River and bridging Willow Creek, among many other small streams that will require special environmental handling.
- The area that is pristine and used by mushers, skiers, canoers, hikers, campers, and fishermen should remain pristine.
- Red Shirt Lake has an archeological history as one the most prolific subsistence lakes in the Susitna Valley with Native villages located on the lake during the 1800's. The history of the lake is recorded in the book "Shem Pete's Alaska".
- The route to Point McKenzie is meant to provide revenues to the population of the Mat-Su Borough. They are the benefactor of railroad. The Houston corridor is already impacted by human use; the rail should be located in an area already scarred by development.
- This corridor is the most expensive and difficult to be constructed. It is estimated to cost an additional \$35 million dollars to construct.



I have spent years on Red Shirt Lake, enjoying the call of the loon, the moan of the moose, the wail of the spruce hen, the slap of the beaver tail, the splash of the canoe paddle, and the laugh of youth in my scout troop while enjoying this state recreation area gem. It would be a shame to drowned out those natural sounds with the clacking cars on the track, roar of a diesel engine, belching black clouds of smoke and spreading black coal dust into the air to tarnish the beautiful sunset.

After considering these objections, I feel that the Houston Corridor would be more acceptable to me and to all users of Nancy Lake State Recreation Area's Red Shirt Lake.

Yours truly,

A handwritten signature in cursive script that reads "Ken Hilfiker".

Ken Hilfiker  
346-1755  
Hilfikerkg@chugach.net

**Alcantra, Rosetta M.**

---

**From:** Richard Gaffey [partychief360@gmail.com]  
**Sent:** Thursday, October 11, 2007 7:08 AM  
**To:** Robertson, Donna  
**Subject:** Impacted trails part 1of2  
**Attachments:** GPS Snowmachine Waypoints by RLG.txt; Snowmachine Trails.kmz; Snowmachine Trails BigLake-Houston-Willow by RLG 1of4.JPG; Snowmachine Trails BigLake-Houston-Willow by RLG 2of4.JPG

MS Robertson...I sent this last night to you and the folks indicated...yours came back...It's here now hopefully again as part 1 of 2 with the second installment to follow

Richard Gaffey

----- Forwarded message -----

**From:** Richard Gaffey <partychief360@gmail.com>  
**Date:** Oct 10, 2007 10:05 PM  
**Subject:** Impacted Trails  
**To:** [bpaulsen@matsgov.us](mailto:bpaulsen@matsgov.us), [psullivan@matsgov.us](mailto:psullivan@matsgov.us), [donna.robertson@hdrinc.com](mailto:donna.robertson@hdrinc.com), [wheelers@akrr.com](mailto:wheelers@akrr.com), [lindamoodb@akrr.com](mailto:lindamoodb@akrr.com), [mikep@tnh-hanson.com](mailto:mikep@tnh-hanson.com)

Hello,

I attended two of the live 'Open House' meetings regarding the Port Mackenzie Rail Extension last week, Big Lake Elementary and Houston Middle School. It was enlightening to learn of the extensive effort required to create a rail corridor that leaves all people and government entities happy. A daunting task.

I am not a wacko environmentalist. I understand the need for the rail extension and besides, trains are cool.

I am, however, very concerned about impact upon trails. To Friday's meeting, at Houston Middle School, I brought along maps to help me illustrate my concerns. Maps that I made and use when I, my family and friends, venture out on snowmachines crisscrossing the very same areas that are slated for the rail extension study. I, and many like me, are very concerned that we will loose something we hold most dear...unfettered access to the trails, swamps and marshes of the Big Lake-Houston-Willow area. Many of us choose to live here for just that reason. I myself could not afford a trailer for my two snowmachines, so I bought a home in Big Lake.

You will note that from my back yard I have access to the trail system that spreads over 275,000 acres. That's just the length and breath of the area I mapped...with enough gas I can ride to Nome.

Several of you expressed interest in my data. Well here it is.

**GPS Snowmachine waypoints by RLG.txt** is a CSV file type that can be massaged into working with any GIS software. The waypoints are the result of field observation with a Garmin GPSmap76, map-spotting utilizing either GoogleEarth or National Geographic TOPO and then verified by field proofing.

**Snowmachine Trails.kmz** is a GoogleEarth executable file

The four *JPG images* are the NW, NE, SE, SW Big Lake-Houston-Willow area...sorry my computer could not handle the write from TOPO to JPG. Break out the Scotch tape.

*WOTRR.tpo* is opened by National Geographic TOPO state series software.

These trails are nothing new to the thousands of folks that use the trails...be it mushing, XC skiing, snowmachine, ATV or bicycle in the Big Lake-Houston-Willow area...These are the trails ridden by myself, Rita Loomis and friends during the winters of '05-'06 & '06-'07 via snowmachine. I did not make these trails. They are just plotted as found. The old timers here about could, no doubt, add greatly to this data. I hear that many of these trails have names.

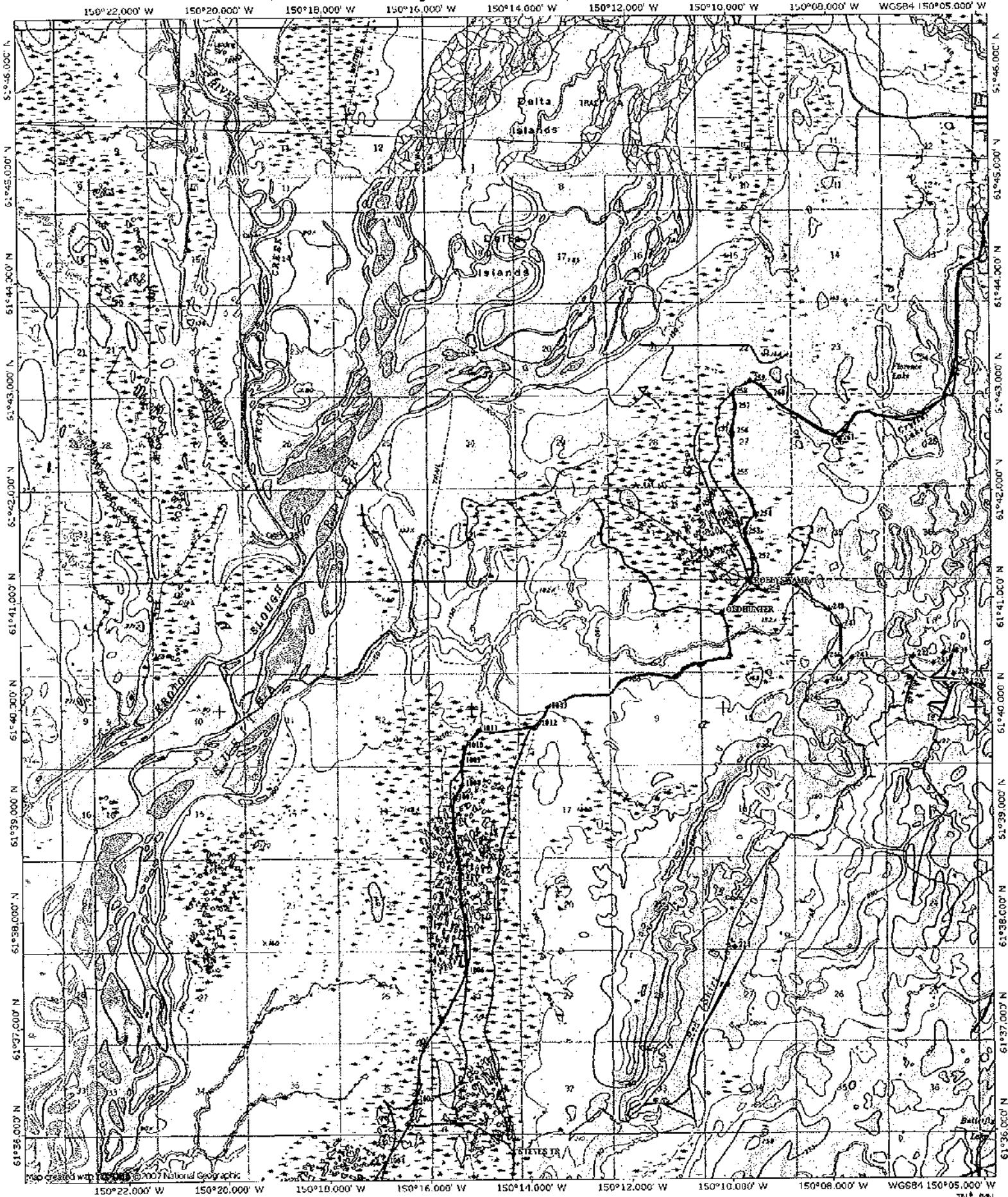
Your friends and neighbors use these trails, be they seismics, legally described section line ROW's or just critter trails that have been used by dog musher's for fifty years . Great care must be taken in placement of crossings to ensure continued and safe access to both sides of the track for all users. Should the crossings be inadequate in number or not in very close proximity to historical trails and limited only to those trails with a legal description, folks will cross and do so at risk. No one desires this issue to be tied up in the courts where only the blood-sucking lawyers win. Make no mistake about it, we are your neighbors.

Thank you for the opportunity to present this wealth of free data. I am sure you will appreciate the time I invested to collect this data and the effort entailed to beat into submission the software to present it. Therefore I pray you will honor my request that my maps, plots, GPS point data and images not be used for any other purpose than the planning of the best possible rail corridor for your fellow Alaskans.

Waiting for the train,  
Richard Gaffey, LSIT  
907-892-5280 h  
907-357-3142 c

Oh yeah...If I can be of any assistance with regard to my data presented here for your review, do not hesitate to call. It would be my pleasure to help.

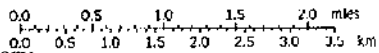
GPS Snowmachine Trails Big Lake-Houston-Willow by Richard Gaffey WGS84 1of4



© 2007 National Geographic

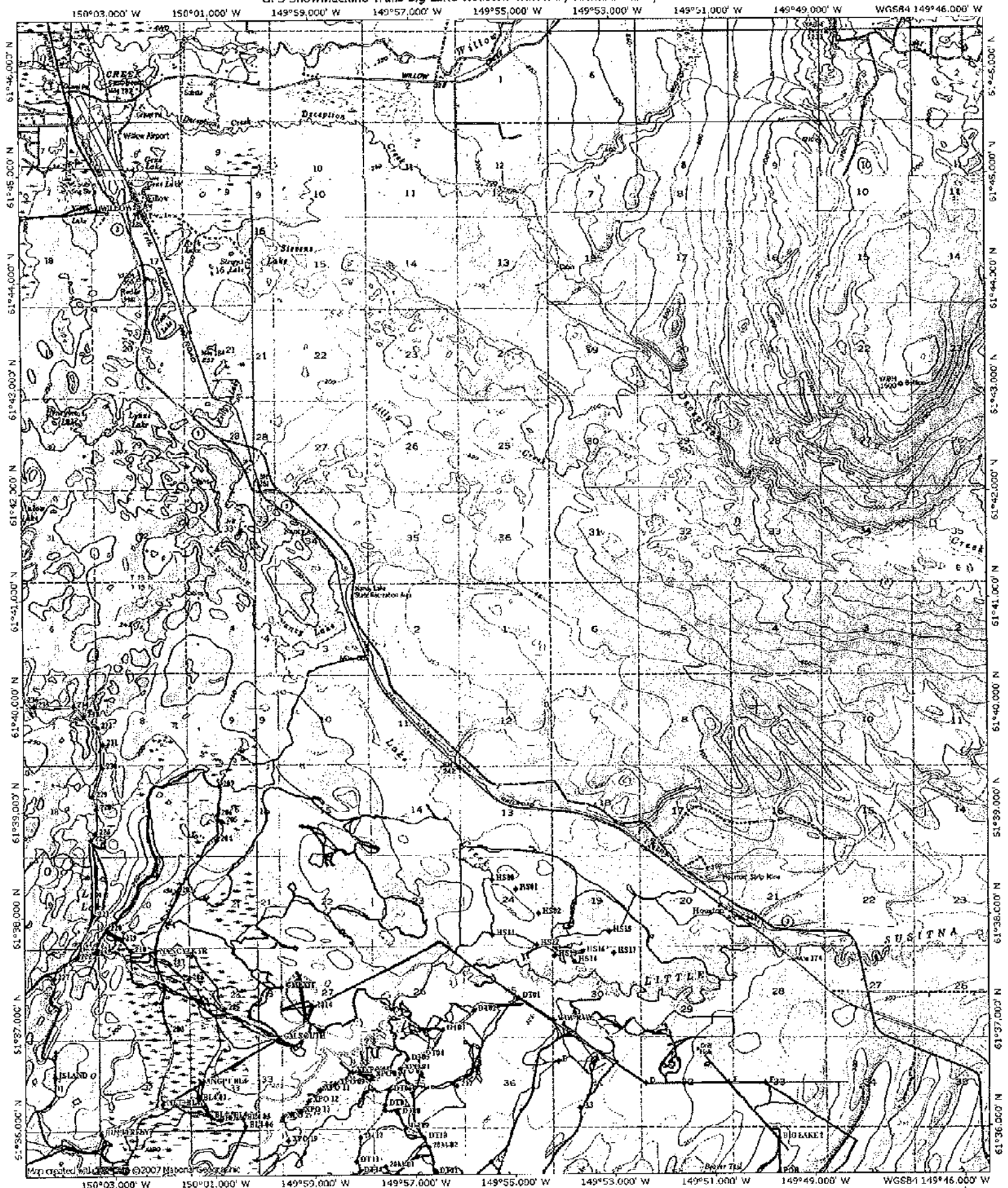
NATIONAL GEOGRAPHIC

Public Involvement Activities Summary  
January 2008

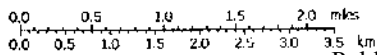


10/09/07

GPS Snowmachine Trails Big Lake-Houston-Willow by Richard Gaffey WGS84 2of4



NATIONAL GEOGRAPHIC



**WACO**

*Willow Area Community Organization*  
 Linda Oxley Chair  
 chair@waco-ak.org

P.O. Box 1027 • Willow • AK 99688  
 (907) 495-6633  
 www.waco-ak.org

Patty Sullivan, Public Affairs Director  
 Matanuska-Susitna Borough  
 Office of Public Affairs  
 350 East Dahlia Avenue  
 Palmer, Alaska 99645

October 3, 2007

Dear Ms. Sullivan,

The Willow Area Community Organization (WACO) approved a motion in the February general meeting to oppose a rail corridor through Willow as described in the 2003 Rail Corridor Study.

Two railroad corridors within our small community, the existing line and the proposed line, will have significant negative impacts on residents and businesses with no clear advantages. Further, the proposed corridor was selected based on an unrepresentative public involvement process.

Having two railroad lines running through one small community will have undue hardship on residents. Many homes will be displaced and dreams destroyed. According to a 2004 survey sponsored by Willow2020 and the borough, 90% of respondents chose Willow as a place to live and recreate because of quiet and rural. Due to the close proximity of the tracks, many residents will be disrupted from two different directions. This surround sound for their homes is not what was intended when they moved here and will diminish property values and quality of life.

Safety is also a concern by Willow residents. The risk factor of two railroad lines in a residential area is not acceptable as children would have easy access to tracks and bridges. Recently, there was a fatal accident of a child sleeping on the tracks near Willow Creek.

Willow is widely known as a year round recreational community. A large portion of our local economy relies on fishing, hunting and boating in the summer to snow machining and dog mushing in the winter. Visitors from around the world come to experience natural resources that are increasing in demand; wilderness, wildlife, beauty, and quiet. Two rail lines, one hauling coal at 50 mph does not fit into the Alaska mystique that visitors have come to see.

The route as proposed in the 2003 Rail Study would be built through the heart of the Willow Creek State Recreation Area, home to an internationally renowned fishing stream. The route continues through the middle of the West Gateway Trail System, the venue for the Iditarod, Junior Iditarod, the Iron Dog, and many other community, state, national and international events that depend on open spaces and a contiguous trail system. Further along, the rail line would run adjacent to the Nancy Lake State Recreation Area, continuing to compromise the natural and aesthetic qualities that the image of Alaska

WACO is a 501(c)4 organization

*Patty Sullivan, Public Affairs Director*

*October 3, 2007*

*Page 2*

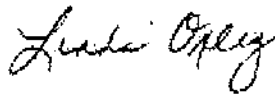
projects and Willow and the tourist industry is trying to sell. Local businesses would suffer, quality of life will suffer.

As stated by the Matanuska Susitna Borough, the intent of the rail extension is to haul coal and other commodities from the interior to Pt. MacKenzie for shipping. As a selling point, the borough touts the line for passenger service as well. In fact, the Alaska Railroad has no plans for a terminal or station for commuters or tourists. If there were, a station could be proposed or built on the existing line. Even so, passenger service to where? The Knik Arm Bridge is speculative and, certainly, the existing line can handle any proposed plans for tourism or commuter traffic.

The public involvement process upon which the Willow corridor was selected was held in the town of Houston. Three public meetings were held at Houston High School during 2002-2003, during which time, several corridors closest to Big Lake and Houston were eliminated and the selection of a corridor through Willow was chosen as a preferred route. Houston High School is approximately 20 miles from the Willow Community Center and as far as 50 miles from people who live in Willow. For a decision that made in large part by public comments, this was a poor example of a fair public process.

The intent of WACO's action to oppose an additional rail corridor through Willow does not mean the community opposes the extension in itself. There are many other viable alternatives where the line would be consistent with another community's vision. It is not consistent with our vision. Therefore, the Willow Area Community Organization opposes the Pt. MacKenzie rail extension through Willow.

Sincerely,



Linda Oxley, Chair

**Longtin, Wendy**

---

**From:** Wurts, David W [david.w.wurts@lmco.com]  
**Sent:** Wednesday, October 03, 2007 9:49 AM  
**To:** info@portmacrail.com; Alcantra, Rosetta M.; Longtin, Wendy  
**Cc:** tc-yahoo (tc-yahoo); erin-pcorp@alaska.com  
**Subject:** Port MacKenzie Rail comment for public meeting Oct 3

My wife and I would like to formally object to the Willow Alternative of the Port MacKenzie Rail Extension.

We own 30 acres of land at milepost 73 in Willow and are building our dream home on the property. We definitely do not want a rail line just south of the property as shown in your proposed layouts. We bought the Willow property because it is a quiet residential area with access to lots of mushing trails.

I think that the rail extension through this area would dramatically alter the community feel of the area and ruin the reasons why we bought land where we did. I understand that the rail corridor may be an unavoidable reality in the future, but I do NOT feel that connecting to the existing line at the Willow junction is wanted by any resident in the area, ourselves included.

Thank you for the opportunity to share our opinion in this matter.

Respectfully submitted.

David Wurts and TC Wait  
6404 Ross Rd  
Morrison, CO 80465

Homeowners at  
35617 Parks Highway  
Willow, AK  
(milepost 72.9)

(Erin - please make sure that our comments are heard at the meeting in case the portmac group forgets to bring a copy. Thanks much! Dave)



*community planning*



*community building*

**Friends of Mat-Su**

IHDR Alaska Inc.  
Port MacKenzie Rail Extension Comment  
836 S. Colony Way, Suite 2  
Palmer, AK 99645

Matanuska-Susitna Borough  
Land Management Division  
350 E. Dahlia Ave.  
Palmer, AK 99645

Re: Port MacKenzie Rail Extension Project

To whom it may concern:

Thank you for the opportunity to comment on the easement request for a rail corridor between Pt. MacKenzie and Willow. Friends of Mat-Su has the following comments for your consideration:

- One of our primary concerns with issues like the rail corridor easement is that the public process be fair and well advertised so that everyone who may be impacted by the development will have a chance to participate. Thank you for providing the opportunity for public participation with the well advertised open house events this fall.
- It is important that the ARRC and the MSB not only choose a route that is best logistically and economically, but also a route that is best for communities who live near the rail belt and a route that doesn't damage significant wetlands, wildlife habitat, agricultural lands.
- The ARRC and MSB also need to consider how the route will impact important recreational lands, especially the local trail system. It may be worth planning for new trail development in conjunction with building the railroad to offer new opportunities.

Thank you for your efforts and we look forward to participating in the next phase of the public process.

Respectfully,  
Mimi Peabody  
*Mimi Peabody*

Projects Coordinator  
Friends of Mat-Su



Patty Sullivan, Public Affairs Director  
Mat-Su Borough  
350 E. Dahlia Ave  
Palmer, AK 99645

Stephanie Wheeler, Public Involvement Officer  
Alaska Railroad Corporation  
327 W. Ship Creek Ave  
Anchorage, Ak 99502

Re: Port Mac Kenzie Rail Extension Project

The Alaska Travel Industry Association (ATIA) represents over 1,100 member businesses from the tourism industry. Many of those members are in the greater Mat-Su Borough and they provide unparalleled outdoor experiences. Maintaining those great experiences is essential to efforts to bring tourists to Alaska and the Mat-Su area. Those visitors help build a strong economic engine for the Valley and all of Alaska.

Currently, comments are being received on the Port Mac Kenzie Rail Extension Project. The development of this rail extension north from Point Mac Kenzie into the Susitna valley includes several optional corridors. ATIA's concern is the integrity of state parks and access to recreational opportunities in the proposed corridors.

ATIA believes that care should be taken in the route selection to minimize conflicts with recreational use and to eliminate encroachment into public parklands.

Sincerely

Ron Peck, President & COO  
Alaska Travel Industry Association  
2600 Cordova Street, Suite 201  
Anchorage, AK 99503

*(This page intentionally left blank.)*

Dear PortMac Rail,

Earlier in the month we had a meeting here at Big Lake discussing the proposed rail line connecting the existing rail to Fairbanks with the development on Point Mackenzie. During that meeting, you asked for input from any who would be impacted with the proposed development. I live south of Big Lake in one of the proposed rail corridors. This would greatly affect me and my family if the route chosen came through this area. There are several impacts that this would have on my family, and I will try to outline them here. It is difficult to list them in any kind of priority that would make sense to another with a different perspective.

First, I would like to point out the general impact to the community of Big Lake. As you are well aware, rail lines increase risk to the communities that they transect. The increased risk of road and trail crossings put the general population at a higher incidence of injury or death from crossing conflicts. As a former ambulance medic serving in this area, I can personally attest to this fact-- rail crossings are dangerous places. The developing community of Big Lake is only going to increase in population and in road congestion. To place a rail line in this area as well does not make sense.

Secondly, the addition of a rail line through our family homestead would be a tragedy for our family. We homesteaded out here in 1959 to remove ourselves from the urbanization that Anchorage was going through. A group of like-minded individuals came with us and established a community south of Big Lake. We have established a private communal road system that has been maintained as a private status since the early sixties. We enjoy the rural situation and the relative quiet that our community provides for us. Our gated community encompasses nearly 2,000 acres and has 15 residences connected with approximately seven miles of private road. Your train would go through the middle of it all.

Thirdly, your rail line would put an end to my livelihood. I am a commercial beekeeper specializing in the production of the highest quality honey available in the state. I am the largest beekeeping

operation in South Central Alaska and likely in the entire state. Alaskan honey is known world-wide as some of the purest and free from contamination as can be had. It commands a premium price similar to our wild salmon industry. In order to produce such a pure product it is necessary to locate apiaries (collections of hives) in areas that are undisturbed from the influence of potential contaminants.

Bees forage primarily within a mile and a half from the hive and anything that is placed in that area has the potential to be absorbed through the plants, concentrated in the nectar and further concentrated in the honey. In times of nectar dearth, bees will forage even farther from the hive so it is necessary to have at least another half mile as a buffer zone.

Given the propensity of the railroad to request herbicides to control vegetation growth, oiling of the rail beds, and the potential for spills, it would be impossible for me to stay in business. Your Big Lake route goes directly over my two most productive bee yards (number 3 and 4) and is too close to all other yards. You can see where our yards are by going to our aerial view page on our website at [http://alaskawildflowerhoney.com/index\\_files/Aerial\\_View.htm](http://alaskawildflowerhoney.com/index_files/Aerial_View.htm)

In closing, I would like to request that the rail line be moved as far west as possible to avoid established communities, and from putting me out of business.

Sincerely,



Stephen Victors  
Alaska Wildflower Honey  
Big Lake Alaska  
892-6175



**RESOLUTION 07-010**

**A RESOLUTION OF THE COUNCIL OF THE CITY OF HOUSTON, ALASKA  
IN SUPPORT OF THE HOUSTON RAIL EXTENSION SOUTH.**

---

**WHEREAS**, the Interior of Alaska has vast deposits of a multitude of minerals; and

**WHEREAS**, these minerals have been unprofitable to market due to the high cost of exportation; and

**WHEREAS**, a rail spur through Houston to Port McKenzie would make the exportation of minerals from the Interior of Alaska to the rest of the state and world feasible; and

**WHEREAS**, the rail spur may bring much needed economic, as well as residential development into Houston, along with more available jobs for Houston's residents; and

**WHEREAS**, the Houston Rail Extension South is part of Houston's Comprehensive Plan; and

**WHEREAS**, this route crosses fewer wetlands, has less developed land nearby, crosses fewer salmon-spawning streams and split fewer designated refuges and recreational areas that the other routes studied; and

**NOW THEREFORE BE IT RESOLVED THAT**, the Council of the City of Houston hereby supports the Houston Rail Extension South.

**PASSED, APPROVED AND ADOPTED** by a duly constituted quorum of the Houston City Council this 13<sup>th</sup> day of December, 2007.

Steve Frost, Mayor

ATTEST:

Dalcann Pond, City Clerk



# BIG LAKE

---

## CHAMBER OF COMMERCE

*"Alaska's Year - Round Playground!"*



December 11, 2007

Matanuska-Susitna Borough  
350 E. Dahlia Ave.  
Palmer, AK 99645

Re: Point MacKenzie Rail Spur

To All Concerned

This is a joint resolution by the Willow Chamber of Commerce and Big Lake Chamber of Commerce.

The Big Lake Chamber of Commerce has considered the various proposed rail spur routes to connect Port MacKenzie to the existing railroad. We would like to express our opinion as to which route we feel would be the best for our area.

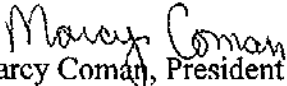
Big Lake is a recreational area and the businesses in the area depend heavily, if not exclusively, on the recreational activity that the area attracts. For this reason we are greatly interested in anything that may change or disrupt the areas surrounding our business district.

Of the three routes, the western (or Willow) route appears to have the least impact on the area as a whole. The fact that the route is mostly on borough, state or federal lands that would minimize the disruption of private property along the right of way is a very great plus for this route. The route is mostly on a natural moraine which would minimize wetland issues. Although this route would cross a number of trails, both snow machine and dog mushing, with a little incorporation into the design of the route, any conflicts could be worked out. So this is the route that we prefer.

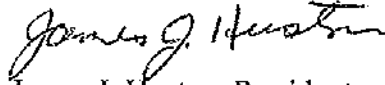
We fully support the development of Port MacKenzie and the rail spur to service it.

Thank you for your attention to our opinion in this matter.

Sincerely,

  
Marcy Coman, President  
Big Lake Chamber of Commerce

Sincerely,

  
James J. Huston, President  
Willow Chamber of Commerce

P.O. Box 520067, Big Lake, AK 99652 ~ Phone (907) 892-6109 ~ Fax (907) 892-6189

Email: [biglake@mtaonline.net](mailto:biglake@mtaonline.net)

[www.biglake-ak.com](http://www.biglake-ak.com)

Public Involvement Activities Summary  
January 2008



# Willow Chamber of Commerce

PO Box 183  
Willow, AK 99688  
907-495-6800

On behalf of the Willow Chamber of Commerce and the silent Majority of Willow residents. I would like it known to all that we are strongly in favor of the Willow Railroad Corridor.

The Willow Corridor is an opportunity to plan and build for the future. Presently, Willow is right on "Track" to become another Wasilla. Both communities settled around the railroad for convenience sake only to find that thousands of homes, cars, and people later that though the love affair isn't over but it's better to be a little further apart.

With the probability of many new freight trains per day, rumbling by our homes and across our access roads, we have definite quality of life and safety concerns. The other corridor alignments do nothing to lessen our conflicts.

We sincerely hope that the Port McKenzie to Willow railroad route is the start of responsible development, that teams with a new highway, that will lessen the growing pressures on our present transportation systems.

Recreational trails are important to our locals, so much so that some Willowites have supported the Houston Rail Alignment. The business leaders of Willow, however, have confidence that the underpasses will be built along the best alignment possible-The Port McKenzie to Willow Route.

Willow never became the capital of Alaska, which may or may not be a good thing. If we were the capital though, I am sure our transportation systems and our access roads would have received more attention. We view the possibility of new rail and new roads through Willow as a favorable addition to our community. Let's plan them and build them.

Thank you,

Jim Huston, President  
Willow Chamber of Commerce





# Willow Chamber of Commerce

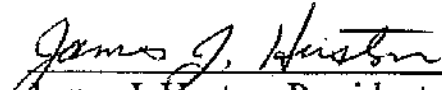
PO Box 183  
Willow, AK 99688  
907-495-6800

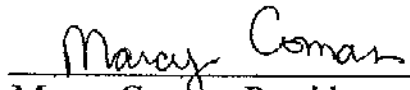
## Joint Resolution

Be it known that the Willow Chamber of Commerce and the Big Lake Chamber of Commerce do hereby enter this documents as a joint resolution.

We the undersigned do hereby support the choice of the Port McKenzie to Willow Railway Corridor. We feel that this alignment will have the least number of negative impacts on local residents.

The joint chambers agree that none of the communities involved would welcome the increased freight traffic and that the best place for the tracks would be out west of Willow where there are not many residents.

  
\_\_\_\_\_  
James J. Huston, President  
Willow Chamber of Commerce

  
\_\_\_\_\_  
Marcy Coman, President  
Big Lake Chamber of Commerce



## MATANUSKA-SUSITNA BOROUGH

**Mayor Curtis D. Menard**

350 East Dahlia Avenue • Palmer, AK 99645

Phone (907) 745-9688 • Fax (907) 745-9669

[www.matsugov.us](http://www.matsugov.us)

December 7, 2007

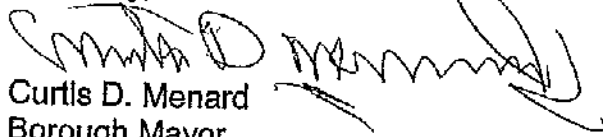
Gregory E. Strong  
P.O. Box 875169  
Wasilla, AK 99687

Dear Mr. Strong,

I am in receipt of both of your letters dated November 28, 2007 and December 4, 2007, and would like to thank you for taking the time to write about the concerns you have regarding a proposed Houston South rail extension.

I am including both letters, including the attachments, in our packet of information that will be sent to the Surface Transportation Board.

Sincerely,

  
Curtis D. Menard  
Borough Mayor

*(This page intentionally left blank.)*



November 26, 2007

File: 07-031

Ms. Eileen Probasco  
 Matanuska-Susitna Borough  
 Department of Planning and Land Use  
 350 E. Dahlia Avenue  
 Palmer, Alaska 99645

MSB Planning Division

NOV 27 2007

RECEIVED

Subject: Willow Comprehensive Plan – Transportation

Dear Ms. Probasco:

Hattensburg Dilley & Linnell, LLC (HDL) attended two meetings to gather preliminary community input for the Willow Comprehensive Plan - Transportation chapter.

On September 27, we met with the Willow Road Service Area Board. They reviewed their Road Maintenance and Repair Capital Projects list. It includes mostly upgrades of existing roads, drainage improvements, dust control, and some paving projects. When asked for their input on future roads needed in Willow, these two suggestions were made: 1) a connection between Nancy Lake Parkway and Lynx Lake Road around the south side of Nancy Lake, and 2) a connection between Crystal Lake Road and Nancy Lake Parkway near the western end.

On October 11, 2007, we attended the Willow Comprehensive Planning Team meeting. Areas of transportation that need to be included in the Comprehensive Plan include roads, trail, aviation, rail, and river. The following is a list of existing plans that need to be reviewed and considered in developing the transportation chapter of the comprehensive plan:

- ADOT&PF Parks Highway Visioning Document
- MSB Long Range Transportation Plan (2007)
- MSB Capital Improvement Program (2007)
- Adjacent Comprehensive Plans (Houston, Big Lake, Y Community)
- Willow Master Plan by M.P. Friedburg
- Fish Creek Management Plan (1984)
- MSB Recreational Trails Plan
- Willow Area Trails Plan
- Lower Susitna River Drainage Trails
- Willow Dog Musers Disaster Plan
- MSB Regional Aviation Systems Plan (2007 draft)
- Willow Airport Master Plan
- MSB Primary Health Care Plan, 2005-2015
- Willow Community Emergency Response Plan
- Susitna Area Plan (1984)
- Willow Sub-Basin Area Plan (1981, update underway)
- State Recreational River Plan
- Susitna River Plan

Scott Hattensburg, PE

Lorie Dilley, PE/CPG

Dennis Linnell, PE

David Lundin, PE

3335 Arctic Boulevard Suite 100 • Anchorage Alaska 99503 • Phone: 907.564.2120 • Fax: 907.564.2122  
 808 South Bailey Street Suite 102 • Palmer Alaska 99645 • Phone: 907.746.5230 • Fax: 907.746.5231

Ms. Eileen Probasco  
November 26, 2007

- Willow Creek State Recreation Area Plan
- Nancy Lake Special Use District
- Lake Management Plans
- Deception Creek Management Plan
- Hatcher Pass Management Plan
- Kashwitna Management Plan

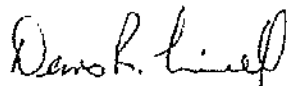
In addition, the following list of transportation issues facing the community were identified by the planning team members that may need to be addressed during the planning process.

Illegal Trails	Winter Trails
River Planning	Summer Trails
Emergency Access	Float Plan Landing
Forestry Management	Eminent Domain
Civil Defense	Coordinating Plans existing and future
Road Maintenance	Grandfather Rights for Existing Uses
ARRC Crossings	Extensions and Upgrades
State and Borough Land Sales	Section Line Easements at ARRC Crossings
ATV Safety	ATV Paths
Bike Path	Pedestrian Safety
Utilities	Whistle Stops
Seniors/Youth	Access to Road-less Areas
Dog Team / Snow Machines	Underserved Areas
Funding	Golf Carts
Roadway Amenities	Communication
Core Area Boundaries	Economic Plan
Population Growth	Pollution
Flood Plains	Iditarod re-Start
Creek Crossings	Fire
Transit – MASCOT, WHO	Mass Transit
Mineral Subsurface Rights	Natural Hazard Mitigation

As studies of future railroad corridors in this area are developed, they should consider these existing plans and coordinate with the Willow Comprehensive Planning team for conformance to the goals of the Willow Comprehensive Plan.

Please contact me at 564-2120 if you have questions in regard to the above meetings.

Sincerely,  
HATTENBURG DILLEY & LINNELL



Dennis R. Linnell, P.E.  
Project Manager



# BIG LAKE

---

## CHAMBER OF COMMERCE



*"Alaska's Year - Round Playground!"*

October 19, 2007

Surface Transportation Board  
395 E Street S.W.  
Washington, D.C. 20423-0001

Re: Point MacKenzie Rail Spur

Dear Board Members:

The Big Lake Chamber of Commerce has considered the various proposed rail spur routes to connect Port MacKenzie to the existing railroad. We would like to express our opinion as to which route we feel would be the best for our area.

Big Lake is a recreational area and the businesses in the area depend heavily, if not exclusively, on the recreational activity that the area attracts. For this reason we are greatly interested in anything that may change or disrupt the areas surrounding our business district.

Of the three routes, the western (or Willow) route appears to have the least impact on the area as a whole. The fact that the route is mostly on borough, state or federal lands that would minimize the disruption of private property along the right of way is a very great plus for this route. The route is mostly on a natural moraine which would minimize wetland issues. Although this route would cross a number of trails, both snow machine and dog mushing, with a little incorporation into the design of the route, any conflicts could be worked out. So this is the route that we prefer.

We fully support the development of Port MacKenzie and the rail spur to service it.

Thank you for your attention to our opinion in this matter.

Sincerely,

Marcy Coman, President  
Big Lake Chamber of Commerce

P.O. Box 520067, Big Lake, AK 99652 ~ Phone (907) 892-6109 ~ Fax (907) 892-6189

Email: [biglake@mtaonline.net](mailto:biglake@mtaonline.net)

[www.biglake-ak.com](http://www.biglake-ak.com)

16 October, 2007

Surface Transportation Board  
395 E Street S.W.  
Washington, DC 20423-0001

Port MacKenzie Rail Spur

Big Lake Community Council  
P.O. Box 520931  
Big Lake, Alaska 99652

Dear Board Members,

The Big Lake Community Council carefully considered the proposed rail spur routes being studied to connect Port MacKenzie to the existing railroad. We would like to share with you our concerns and suggestions for the route we support.

The west route is the one favored by our council as it has the least impact on our area. This route is mostly on borough, state or federal land and has minimal private property along the right of way. It would cross a number of our recreational trails but with crossings incorporated into the design of the route we feel it is a workable route. The route is also mostly on a natural moraine and would minimize wetland crossing. From the borough's perspective, this route would open up a large area of inaccessible land for sale and development, increasing the tax base. Finally, noise pollution caused by rail traffic would be generally far away from existing dwellings.

The central route is the least desirable route of the options presented. This route would impact a tremendous amount of private property and proximity to existing dwellings would create a noise pollution nightmare. Much of the route is wetlands and is in the Big Lake watershed area. Construction of a raised railbed crossing to the west and north of Big Lake would create essentially an earthen dam across a large part of this watershed. This would create havoc with the existing drainage pattern and would have unknown

consequences. This route would cross virtually every trail in and around Big Lake, resulting in many crossings to accommodate the trail users. We are also concerned about the negative impact on the borough from devaluing of property as a result of the rail line, potentially reducing tax revenues.

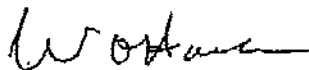
The eastern route is also not recommended due to the route crossing many private lands. Noise pollution would be another issue due to proximity to dwellings. This route would also require a road crossing at Hollywood and at Big Lake Road. It also crosses many wetlands and the Iditarod Trail. The main rail line is across the Parks north of the hiway, requiring an over or under pass to reach the main line.

The council is not opposed to the development of the port and supports a rail spur to service it. We are concerned about the character of the lake and its surrounding areas. This is one of the most prized recreational areas in this state and supports robust summer and winter recreational activities. It is also becoming increasingly popular with year round residents.

The recreational trail system in this area is extensive with thousands of miles in and around Big Lake. It is often called the gateway to the western Susitna Valley area and is extensively used by snowmobilers and dog mushers in the winter. I have enclosed a map of the major Big Lake trails with an approximation of the routes of the rail spur indicated. You will note that the central route crosses and recrosses many of the main trails in this area and many more that are not indicated. The western route has the least impact on the trail system.

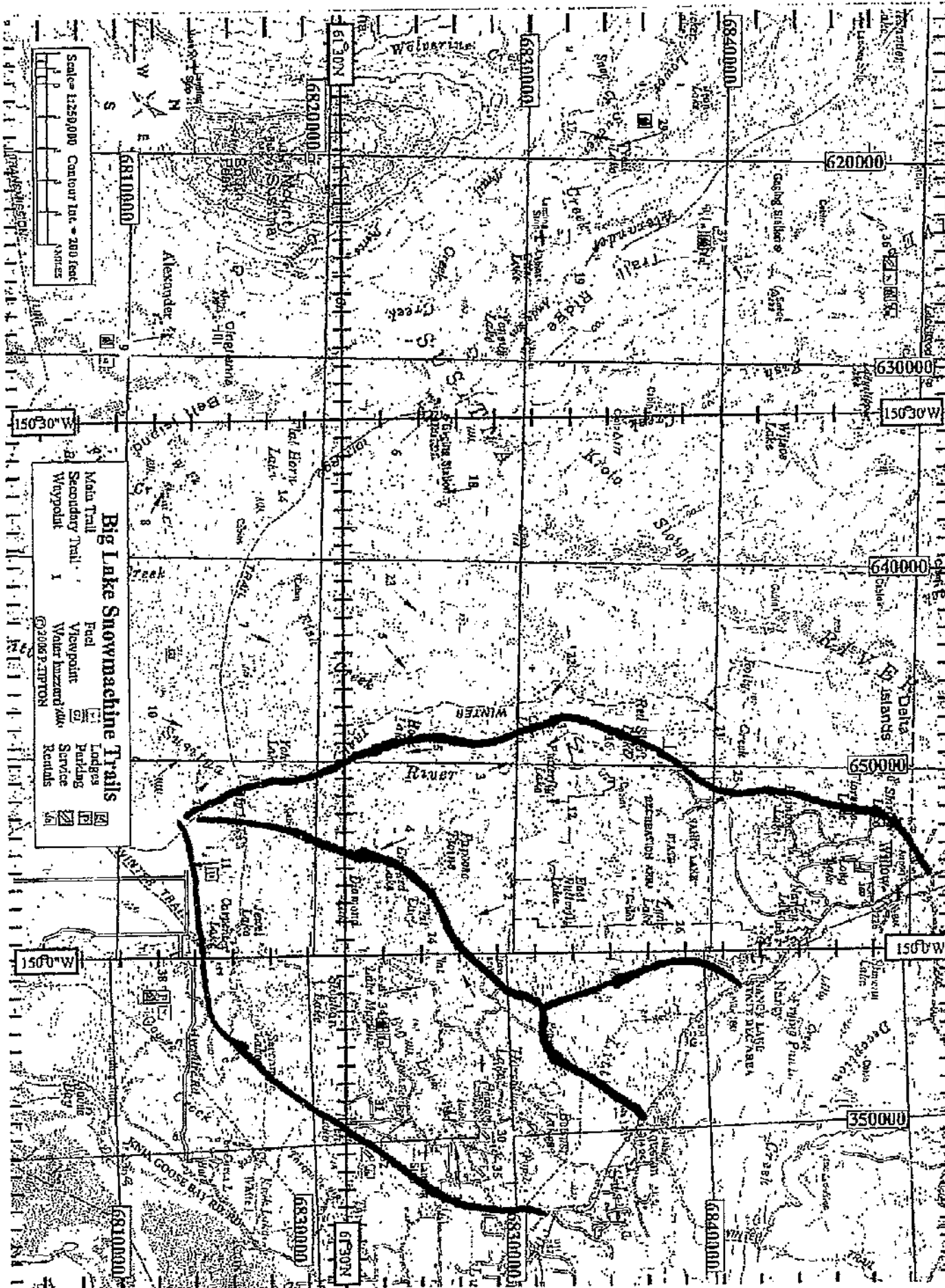
Please consider our input during your decision making process.  
Thank you for your attention to this matter.

Sincerely,



William O'Hara, President  
Big Lake Community Council





# ELECTRONIC COMMENTS

*(This page intentionally left blank.)*

# Comment Report

Commenter Name	Comment	Comment Date
Vern Halter	<p>Hello Patty, Neither Erin Mc Lamon who is president WDMA or Steve Charles who is President of Willow Trail committee know anything about your e-mail. WDMA and Trails work closely on issues such as this. Erin McLarnon would have responded to you right away and the same can be said for Steve. WACO chaired by Linda Oxley would set meetings and agendas too. When the original 2003 study was done no one bothered to set a meeting up in Willow. The State Parks was not involved and this route goes right through Willow Creek State Park and Willow Creek yet Willow's name is used for a route that has huge negative consequences to Willow. When you read that study it was fundamentally flawed in almost all respects. This was commissioned by the Mat Su Borough, yet you knew and know we were left out. Now all of a sudden you say you e-mailed "them" the mushers for a meeting. I didn't receive anything and I am on the Borough Planning commission, Erin didn't receive anything and she is the President of the Mushers. Steve didn't receive anything and he is Pres of the Trail committee. I haven't talked to Linda yet but she is very responsive to those kind of things. Please make sure this is added as public comment to the Rail hearings and we respectfully request that Willow's good name not be associated with a route we had no opportunity to provide input to in 2003. Regards, Vern Halter                      Dream a Dream Dog Farm                      P.O. Box 389                      Willow, AK 99688                      1-866-4AK-MUSH (425-6874)                      www.vernhalter.com</p>	9/18/2007
Gary Stromberg	INTERESTED IN ROUTES AND IMPACT ON CURRENT AND FUTURE PROPERTY OWNERS.	9/18/2007
Karen Nugen-Logan	<p>Per today's edition of the Frontiersman, Sept 18, we can view plans/routes, etc for the proposed macrail at <a href="http://www.port-macrail.com">www.port-macrail.com</a>.                      Can you advise when they'll be available to view. We're very interested since our property is in the immediate area. Thank you.</p>	9/18/2007

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
Paul Gardner	A rail route to Port MacKenzie is well over do. This project need to be moved forward for all the right reasons. Jobs, natural resource development, future commercial development and housing that will follow.  Thank-you for your time	9/19/2007
Debra Huling-Pizak	I am extremely concerned about the recreational impact on this prime outdoor recreational area. As a Willow cabin owner, I already have noise and trail blockage by the current rail line. I am strongly opposed to this project. Private property owners will lose on this deal and only big business will gain.	9/21/2007
Mike Huske	Complete waste of state resources. A rail spur to a port with no business makes about as much since as building a ferry or bridge. There has been one ship there this year to pick up wood chips that sat there since last summer and that's it. Build it and they will come. I don't think so. With the declining funds available to the state they should be used for viable projects not pipe dreams.....	9/21/2007
Vern Halter	I am one hundred per cent opposed to Corridor 3. This route literally destroys the community of Willow. Willow is a rural recreational community trying to maintain this identity. Our economic base and future depend on this. Tourism and recreation and trails define this community. Crossing through Willow Creek State Park, then our rich salmon stream-Willow Creek is a travesty. Corridor 3 is the longest, most costly and most environmentally unsound of all choices. The Community of Willow was never even allowed to be involved in the 2003 selection process. No meetings were held here. The State Parks division was not such a location. Willow is now turned into a rail town with two rail corridors two or three miles apart. You cannot mitigate the long term damage to the future of this community if Corridor 3 is developed like this. The Iditarod Race which produces more jobs, more money and supports mushers wants to relocate to Willow. The Iditarod National Historic Trail which can easily be accessed right out of Willow has as a preamble a roadless, wilderness trail routing. To have a commercial rail line go through Willow when it is not truly necessary is a travesty. There are many other shorter and better alternatives. The Railroad is supposedly about tourism, yet it would directly negatively impact local tourism and local family owned small operators here in Willow. The Railroad aligns itself with the large cruise ship operators and over crowds Talkeetna and Denali Park at the same time it shortchanges Willow. John Binkley, the CEO of the Railroad Board would no doubt be seriously mad if a Rail Bridge over the Chena River was planned where his tour boats operate. Yet the Railroad has the audacity to call Route 3 preferred and it goes through our State Park-Willow Creek State Park and our great fishing, rafting and tourist river-Willow Creek. To pinch in a commercial rail line between the current rail line and the Susitna River to the west bisects Willow and no doubt will reduce property values and reduce quality of life which is why people are here.	9/22/2007

Thursday, December 20, 2007

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
John Himmelrig	The Houston south route will cut off my driveway. I and other neighbors can't get out. What will the rail do if it cuts me off? North side of Little Horseshoe Lake. Only way out is across the lake. (Patty submitted this)	9/25/2007
Jack Hammon	I own 5 Acres @ Settlers Bay and I vote for the Big Lake Route.	9/25/2007
William Luth	This rail link is a waste of money! It will mess a great recreational area! All summer and winter trails must have underpasses and crossings. And if you do waste the money the best and the only area wanting it is the Houston South Route. Please don't ruin Willows wonderful Parks and our large area of TRAILS!!!!	9/25/2007
Bill Fikes	Why not create a coalition with the port mac to anch bridge project, share the project costs and have a direct access from pt mac to the anch yards for the rail? I'm thinking a rail line running under a 4 lane auto.	9/26/2007
Paul R. DuClos Jr.	I support the Rail extension. I prefer the Willow route in hope that the road will follow as soon as the Bridge is constructed. I also think that this route will have less impact on the existing population and has good gravel sources.	9/26/2007
Peg Watkins	The Houston extension, running along the N side of Horseshoe lake is unacceptable; there are 85 homeowners on the lake, more than 1/2 are year-round residences, not rec cabins. The noise, the dust, the construction would upset the ecosystem as it exists right now. The Borough road service is already pathetic there, and you want to add something else? Ridiculous.	9/27/2007

Thursday, December 20, 2007

Page 3 of 23

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
Sharon Kosmalski	<p>1. In previous meetings it was stated the public would have opportunities to comment during the current assessments which are currently contracted. No information has been brought forward as to HOW or WHEN this input could be given or HOW it will be incorporated into these preliminary studies.</p> <p>2. The long term effects on Willow as a current and future playground for Anchoragites, Valleyites and tourists, and the negating impact the rail will have if it splits the Houston/Willow area, need to be considered and studied in-depth. If, in 4 years the "studies" could go from a 20-year forecast showing NO fiscal benefit, to the current analysis which DOES show a benefit, the long-term impacts on recreation - what this state is about, also need to be included and forecasted out to incorporate how the growth will seek to recreate in our area.</p> <p>3. There are places to put this rail that will negate any impacts to previously unsuspecting (read; nondisclosure) landowners.</p> <p>4. There are places where the ROW is already largely existing. The borough and the railroad should seek to utilize these area.</p> <p>5. Impacts to anadromous waters should be avoided.</p> <p>6. Impacts to wetlands should also be avoided. The shortest route will accomplish this.</p>	9/30/2007
John Fray	<p>I recently heard about the proposed Point Mackenzie rail extension project. I am personally opposed to it, and have very much to loose! It appears that one proposal has the rail coming through an area of large homestead parcels, between Three Mile lake and Big Lake. We have a very unique community, secluded and quiet, and would like to keep it that way. Even if the rail did not come on my property, it would destroy the environment with noise, and affect huge areas of wetlands.</p> <p>Myself and all my neighbors will be interested in the proceedings, and would like to be heard in this process.</p>	10/2/2007

Thursday, December 20, 2007

Page 4 of 23

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
Ray Debenham	<p>I attended your Big Lake presentation last night and heard some things which disturbed me.</p> <p>First, after seven previous studies, costing millions of dollars, you are now proposing yet another study.</p> <p>Second, in the 1990's, after spending many months evaluating possibilities, the Borough and the council I served on agreed that the corridor shown on the map last night as the "Houston Corridor" was NOT to be recommended. We all felt that this interrupted too many recreational sites and it would be best to find a corridor which would not interrupt the Big Lake recreation area in its entirety. I think the Borough needs to consider the economic impact which recreation brings to their area. I know that the "Houston Corridor" and most probably the "Big Lake Corridor" would greatly reduce the amount of recreational opportunities in the area. I am equally positive that many people would not enjoy their cabins, fishing trips, snow machine trips, and boating with a coal train coming by every 5 to 10 minutes (based on information from previous study).</p> <p>Third, I am really disappointed that the Borough will not make a specific recommendation as to which route is preferred. We are aware the Federal Government will ultimately decide which route will be used; however, with the Mat-Su Borough stating "they do not care", that is construed to mean that they are not taking care of their citizens. Perhaps these elected officials do not truly have a vested interest in their borough or the citizens they represent.</p> <p>Fourth, I believe a presentation in Anchorage should be scheduled. I learned, during the last study, that a majority of the property which will be affected by this decision is owned by residents of Anchorage. You are obligated to get a true feeling on what Borough tax payers want to see happen on or near their property, therefore, one such presentation needs to be made in Anchorage to facilitate that process.</p> <p>If the Borough officials are unwilling to work with the Anchorage-based taxpayers, then, perhaps, the Anchorage-based land owners need to have a meeting and invite the Federal Officials to attend. At the very least, we would be put in a position of having to obtain a few thousand petition signatures stating what we think about the corridors shown on the map.</p> <p>Fifth, I have spoken with a representative for the Alaska Railroad and was told that they would not put in any new rail spurs unless they could find some way of generating enough freight to pay for the extension. The Alaska Railroad could not find that much freight going to Point MacKenzie. To me, this means the Alaska Railroad did not think an extension would be economically feasible. I do not care where the money is coming from, but, in the end, the taxpayers will be paying for the extension in some way unless some private group steps in to pay for it. I doubt that any private group would find the extension feasible.</p> <p>My recommendations:</p> <ul style="list-style-type: none"> <li>First: If you are going to build it anyway, then put it in a place where the extension interferes the least with local business and recreation opportunities. This could only be the proposed "Willow" extension.</li> <li>Second: The Mat-Su Borough should narrow its scope and recommend one route only as the preferred route. Without this recommendation, the Federal Government will pick the cheapest route, and</li> </ul>	10/3/2007



<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
	<p>from the studies I have seen, this would be right through the largest recreational area in the State of Alaska.</p> <p>In a quick count, there are a minimum of 143 recreation trails which cross the proposed "Houston Corridor." These trails include but obviously are not limited to: six large Big Lake Trails; the Crooked Lake Trail; the Iron Dog snow machine trail; and the Iditarod Dog Sled trail.</p> <p>Let's all work on this together and try to secure a location for the corridor in the least disruptive area possible. This will not happen unless the Borough participates and actively pursues the participation of all affected taxpayers.</p>	10/3/2007
Ron Travis	<p>I attended the meeting last night in Big Lake and listened to the proposals for use of the Port. They mentioned coal, and fertilizer plant using natural gas. I doubt that Seward is going to be quiet on the coal issue and are going to be trying to keep the coal in Seward. The fertilizer plant? Am't they closing the one in Kenia, Do we have a better deal or know something they don't know? Hauling limestone from Iivengood to the port. Can you imagine the storm that would create.</p> <p>It would seem to make more sense to get letters of intent from some of these people before moving ahead. Do you remember the grain silos??Also the dairy farms.</p>	10/3/2007
Marjorie Bellinger	<p>The proposed Willow route is to close to the Nancy Lake State Park. It should either be removed from consideration entirely or moved further to the west at least 5 miles.</p>	10/3/2007
David Wurts	<p>My wife and I would like to formally object to the Willow Alternative of the Port MacKenzie Rail Extension.</p> <p>We own 30 acres of land at milepost 73 in Willow and are building our dream home on the property. We definitely do not want a rail line just south of the property as shown in your proposed layouts. We bought the Willow property because it is a quiet residential area with access to lots of mushing trails.</p> <p>I think that the rail extension through this area would dramatically alter the community feel of the area and ruin the reasons why we bought land where we did. I understand that the rail corridor may be an unavoidable reality in the future, but I do NOT feel that connecting to the existing line at the Willow junction is wanted by any resident in the area, ourselves included.</p> <p>Thank you for the opportunity to share our opinion in this matter.</p>	10/3/2007

Thursday, December 20, 2007

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
Erik Johnson	<p>I think the Willow route would be the least intrusive. It would not effect as many property owners.</p> <p>I have hiked to the Little-Su several times from the end of S. Big Lake road. Where exactly do you plan on crossing the Little-Su?</p>	10/3/2007
George Howard Jr.	<p>I am NOT in favor of the rail extension connection at Willow!! We, and the majority of those that also live in Willow, are living here because there are NO jobs, commerce, and development here. We spent our career's in that proximity, and now desire to live in an area as Willow is now!</p> <p>Houston appears to desire all that a connection at that point would bring, soooooo put it there. It would seem that the geology of that area would also be more beneficial????, and the wetlands impact would also be less.</p> <p>NOT IN WILLOW!!!</p>	10/5/2007
John Scudder	<p>A couple comments</p> <ol style="list-style-type: none"> <li>1. This area is South Central Alaska's playground. People hunt, trap, ride atv and snowmobile here. What provisions are there for motorized and non motorized crossing.</li> <li>2. Why isn't there an open house in Anchorage. A lot of the property owners live in Anchorage and a lot of Anchorage citizens recreate in this area.</li> </ol>	10/5/2007
Stu Grenier	<p>I am a big fan of the Red Shirt Lake area. I am glad to see the rail line goes west of Red Shirt Lake rather than through it. I hope that the Willow route is not developed. My opinion would change if ARRC would offer reasonable flag stop rates but since it cost \$40 to go from Anch to the Willow area for a flag stop I consider you guys high way robbers.</p>	10/8/2007

Thursday, December 20, 2007

Page 7 of 23

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
Earl Ramsey	<p>Red Shirt Lake Landowners Association                      4330 Seeley Court                      Anchorage, Alaska 99502</p> <p>October 9, 2007</p> <p>HDR Alaska Inc.                      Port Mackenzie Rail Extension Comment                      836 S Colony Way, Suite 2                      Palmer, Alaska 99645</p> <p>To Whom It May Concern:</p> <p>As President of the Red Shirt Lake Landowners Association, I am submitting the following comments to object to the rail corridor being placed behind the ridge that parallels Red Shirt Lake.</p> <p>For thirty years, as an association, we have fought to keep Red Shirt a quiet and tranquil part of the Nancy Lake Park System. We have cooperated with the State Department of Parks and Recreation to accomplish this. There is no road to Red Shirt. It is accessible by trail and air only. By agreement with the state, we have a lake where personal watercraft is not allowed and we have placed restrictions on the boating times and aircraft landings on the lake. As far as we know it is the only lake in the valley with these restrictions.</p> <p>Red Shirt has a wonderful history as the most prolific subsistence lake in the Susitna Valley with Native villages located on the lake during the 1800's. The history of the lake is recorded in the book "Shem Pete's Alaska".</p> <p>There are several reasons for our objections for this proposed location of the proposed rail.</p> <ol style="list-style-type: none"> <li>1. The noise factor in an area that has been set aside as a tranquil and quiet place for canoeing and recreational activity.</li> <li>2. The large concentration of moose that gather on the back side of Red Shirt Ridge seeking shelter for the hunting pressure from the Susitna River Area. Opening up the rail corridor will simply put more pressure on them and the winter kill will be greatly increased.</li> <li>3. The coal dust pollution is also objectionable this close to a large outdoor recreational Area.</li> <li>4. The Willow corridor is the only one that requires crossing the Little Susitna River and bridging Willow Creek, among many other small streams that will require special handling. This corridor is also the most expensive and difficult to be constructed. It is estimated to cost an additional \$35 million dollars to construct.</li> </ol> <p>After considering these objections, we feel that the Houston Corridor would be more acceptable to us.</p> <p>Yours truly,</p>	10/9/2007

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
Don Hepler	It appears from the map that all proposed routes either go directly through or skirt current residential areas-- except possibly the Willow route, which would go through a major recreational area. The best route appears to be the Willow route and it seems to be the most direct, but would require over- or underpasses to accommodate the snowmachiners, dog mushers and others that access those areas, particularly in the winter months. All the proposed routes will make some groups unhappy, but routing potentially hazardous cargoes away from residential areas and schools would seem to be a priority over inconveniences to recreational users.	10/11/2007
Randy Tashjian	<p>To whom it may concern,</p> <p>I'm writing to express my concern for the possible Alaska Railroad spur line extension through the communities of Willow, Houston, Big Lake, Wasilla and Knik. Many issues remain to be addressed properly and fully, and these include:</p> <ol style="list-style-type: none"> <li>1) The impact that the spur will have on the communities bisected by the rail line.</li> <li>2) The impact that the spur will have on recreational trails, wildlife habitat, wetlands, rivers, and streams nearby.</li> <li>3) The cost to taxpayers.</li> </ol> <p>Before the extension of the spur line can proceed any further, I urge that these issues and concerns be addressed to minimize potential conflicts of interest. A response with your position on this important matter would be greatly appreciated. Thank you for your consideration.</p>	10/11/2007

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
Richard Gaffey	<p>I attended two of the five 'Open House' meetings regarding the Port Mackenzie Rail Extension last week, Big Lake Elementary and Houston Middle School. It was enlightening to learn of the extensive effort required to create a rail corridor that leaves all people and government entities happy. A daunting task.</p> <p>I am not a wacko environmentalist. I understand the need for the rail extension and besides, trains are cool.</p> <p>I am, however, very concerned about impact upon trails. To Friday's meeting, at Houston Middle School, I brought along maps to help me illustrate my concerns. Maps that I made and use when I, my family and friends, venture out on snowmachines crisscrossing the very same areas that are slated for the rail extension study. I, and many like me, are very concerned that we will loose something we hold most dear...unfettered access to the trails, swamps and marshes of the Big Lake-Houston-Willow area. Many of us choose to live here for just that reason. I myself could not afford a trailer for my two snowmachines, so I bought a home in Big Lake.</p> <p>You will note that from my back yard I have access to the trail system that spreads over 275,000 acres. That's just the length and breath of the area I mapped...with enough gas I can ride to Nome.</p> <p>Several of you expressed interest in my data. Well here it is.</p> <p>GPS Snowmachine waypoints by RLG.txt is a CSV file type that can be massaged into working with any GIS software. The waypoints are the result of field observation with a Garmin GPSmap76, map-spotting utilizing either GoogleEarth or National Geographic TOPO and then verified by field proofing.</p> <p>Snowmachine Trails.kmz is a GoogleEarth executable file</p> <p>The four JPG images are the NW, NE, SE, SW Big Lake-Houston-Willow area...sorry my computer could not handle the write from TOPO to JPG. Break out the Scotch tape.</p> <p>WOTRR.tpo is opened by National Geographic TOPO state series software.</p> <p>These trails are nothing new to the thousands of folks that use the trails...be it mushing, XC skiing, snowmachine, ATV or bicycle in the Big Lake-Houston-Willow area...These are the trails ridden by myself, Rita Loomis and friends during the winters of '05-'06 &amp; '06-'07 via snowmachine. I did not make these trails. They are just plotted as found. The old timers here about could, no doubt, add greatly to this data. I hear that many of these trails have names.</p> <p>Your friends and neighbors use these trails, be they seismics, legally described section line ROW's or just critter trails that have been used by dog musher's for fifty years. Great care must be taken in placement of crossings to ensure continued and safe access to both sides of the track for all users. Should the crossings be inadequate in number or not in very close proximity to historical trails and limited only to those trails with a legal description, folks will cross and do so at risk. No one desires this issue to be tied up in the courts</p>	10/11/2007

Thursday, December 20, 2007

Page 10 of 23

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
Lori Okuley	<p>where only the blood-sucking lawyers win. Make no mistake about it, we are your neighbors.</p> <p>Thank you for the opportunity to present this wealth of free data. I am sure you will appreciate the time I invested to collect this data and the effort entailed to beat into submission the software to present it. Therefore I pray you will honor my request that my maps, plots, GPS point data and images not be used for any other purpose than the planning of the best possible rail corridor for your fellow Alaskans. Oh yeah...if I can be of any assistance with regard to my data presented here for your review, do not hesitate to call. It would be my pleasure to help.</p> <p>I would like to comment on the proposed rail route from Port Mackenzie to Houston. I am vehemently opposed to this route.</p> <p>My family has owned property on the northwest corner of Horseshoe Lake since 1955. Our property remained recreational until 2004 when my husband and I finished construction and moved into our retirement home. One of the reasons we choose to build our retirement home on the property was because of the peace and solitude we have in that location. We also heavily use the surrounding area for a variety of seasonal activities. Hiking and 4-wheeling during summer months as well as cross country skiing and snowmachining during winter months are just a few of the uses we consistently participate in. The Houston rail route would drastically change our way of life.</p> <p>There are also concerns regarding wildlife habitat. Some of those affected may be the moose that calve in the spring and sandhill cranes that nest in the summer throughout this area. There is also a large population of moose that winter here.</p> <p>Another concern I would like to address is the watershed. The drainage from Horseshoe Lake to the Little Susitna River would be blocked by the proposed Houston route. I have no doubt that water levels in the lake would raise due to the restricted drainage, the only question would be how much. The potential for flooding would increase for existing structures along the shoreline and could potentially cause homeowners to suffer repair or replacement expenses due to the placement of the rail line along the Houston route.</p> <p>Thank you for your consideration of my comments in opposition to the Houston route.</p>	10/12/2007

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
Steve Charles	What is there a deadline for comments on this initial fall comment period?	10/15/2007
Kevin Hite	As a landowner to the west of the northernmost route, it is very concerning to me that this alternative may affect access to my property. The Alaska RailRoad has never been very accomodating to providing access through it's right of way, and I see nothing to this point that would contradict that position. In addition, we utilize this entire area trail system for both summer and winter access and recreation. Any net loss of that access and availability will by defination, negatively affect our property and recreation. We will be continuing to participate in this process as it moves along. Please include the supplied address in your mailing list. Thank you.	10/18/2007
Robert Sexton	I do not want to see the RailRoad come through Willow. I believe it would deteriate the way of life in Willow, and I see no economic benefit to Willow.	10/21/2007
Joe Guana	I like and agree with the project but must be assured that you will not be putting up barriers to snowmobile travel to the west. Installing full-blown corssings would be a bit much but if you would just put timbers between the tracks at intervals and sign them, snowmobilers would be able to safely and easily cross the tracks. Let's work together on this instead of opposing each other, as has happened in Broad Pass and other locations.	10/21/2007
Ken Ludy	I am a professional land surveyor who has lived, worked and recreated in this area for the past 30 yrs. I also own an ag parcel in Willow, and while not directly affected by the alignment there, do not want that choice because of it's impact on other farms and year round recreation in the entire area. It appears to me that the optimal cost/benefit ratio would be the Mac East/Conn 3/Houston/Houston North alignment. Respectfully Submitted, Ken Ludy	10/21/2007

Thursday, December 20, 2007

Page 12 of 23

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
Steve Charles	<p>I am strongly opposed to the Port MacKenzie Rail Extension being built through Willow as described in the 2003 Rail Corridor Study and currently proposed in the latest study. The many reasons are listed below:</p> <ol style="list-style-type: none"> <li>1. A second rail line in a small, rural community would place undue hardship on residents, their property values, safety and quality of life. According to a 2004 survey by Willow2020 and the MSB, 90% of respondents chose Willow as a place to live and recreate because of the open country, beauty and quiet. Having two tracks within a recreation area is inviting trespass and presents a clear danger. Last year, a child was killed sleeping on the tracks after fishing in Willow Creek.</li> <li>2. A Willow rail extension will bisect the heart of the Willow Creek State Recreation Area, home to an internationally renowned salmon and trout fishing stream, dramatically effecting the extraordinary natural and aesthetic qualities so easily accessible to the public.</li> <li>3. The rail line will bisect the heart of the West Gateway Trail System, a highly popular recreation area and venue for the Iditarod, Junior Iditarod, the Iron Dog, Willow Winter Carnival and many other community and State events that depend on open spaces and a contiguous trail system.</li> <li>4. The rail corridor would be adjacent to the Nancy Lake State Recreation Area, impacting recreational, natural and aesthetic qualities stated in the mission of Alaska State Parks.</li> <li>5. The line will bisect a very important and concentrated moose wintering area, thus increasing moose mortality.</li> <li>6. The proposed railroad, by compromising noise, viewsheds, and the semi wilderness character of the country in which visitors from around the world come to visit, would adversely impact local recreational and tourism related businesses.</li> <li>7. Since the purpose of the rail extension is to haul freight, any local freighting business could conflict with the recreational value of the area. Any commuter or tourism business still has the existing rail line to build upon.</li> </ol>	10/23/2007



<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
-----------------------	----------------	---------------------

There are much better options for the location of the rail extension, locations where it could enhance a community's development. The Willow route will not be an enhancement to me, nor according to the Willow Area Community Organization, Alaska State Parks, Willow Dog Musers Association, the Iron Dog, Deshka Landing and many local business. Pleas pick a better route.

*Page 14 of 23*

*Thursday, December 20, 2007*

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
Steve Charles	<p>I am strongly opposed to the Port MacKenzie Rail Extension being built through Willow as described in the 2003 Rail Corridor Study and currently proposed in the latest study. The many reasons are listed below:</p> <ol style="list-style-type: none"> <li>1. A second rail line in a small, rural community would place undue hardship on residents, their property values, safety and quality of life. According to a 2004 survey by Willow2020 and the MSB, 90% of respondents chose Willow as a place to live and recreate because of the open country, beauty and quiet. Having two tracks within a recreation area is inviting trespass and presents a clear danger. Last year, a child was killed sleeping on the tracks after fishing in Willow Creek.</li> <li>2. A Willow rail extension will bisect the heart of the Willow Creek State Recreation Area, home to an internationally renowned salmon and trout fishing stream, dramatically effecting the extraordinary natural and aesthetic qualities so easily accessible to the public.</li> <li>3. The rail line will bisect the heart of the West Gateway Trail System, a highly popular recreation area and venue for the Iditarod, Junior Iditarod, the Iron Dog, Willow Winter Carnival and many other community and State events that depend on open spaces and a contiguous trail system.</li> <li>4. The rail corridor would be adjacent to the Nancy Lake State Recreation Area, impacting recreational, natural and aesthetic qualities stated in the mission of Alaska State Parks.</li> <li>5. The line will bisect a very important and concentrated moose wintering area, thus increasing moose mortality.</li> <li>6. The proposed railroad, by compromising noise, viewsheds, and the semi wilderness character of the country in which visitors from around the world come to visit, would adversely impact local recreational and tourism related businesses.</li> <li>7. Since the purpose of the rail extension is to haul freight, any local freighting business could conflict with the recreational value of the area. Any commuter or tourism business still has the existing rail line to build upon.</li> </ol>	10/23/2007

Thursday, December 20, 2007

Page 15 of 23

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
	<p>There are much better options for the location of the rail extension, locations where it could enhance a community's development. The Willow route will not be an enhancement to me, nor according to the Willow Area Community Organization, Alaska State Parks, Willow Dog Musers Association, the Iron Dog, Deshka Landing and many local business. Pleas pick a better route.</p>	
John Scudder	This area is highly used for winter recreation. Protect and provide access to the trails for dog teams and snowmobiles	10/23/2007
Jason Kuehn	I think this is a critical project for Alaskan transportation infrastructure. In addition to providing a far more efficient port outlet for Usibelli coal and other future bulk traffic, it could well become the new mainline for the ARR if the Point MacKenzie Bridge gets built which will probably happen some day. For the long-term the most efficient routing is the Willow routing which also skirts the population areas (Willow, Houston, Big Lake)impacted by the other routes. Conn1 and Mac West also provide options to skirt the Port area with a future mainline that might use a future Port MacKenzie bridge. I would imagine this might be the more expensive routing as it is probably the longest route. it appears to me to be the best route for the long-term benefit of the ARR and the local area.	10/24/2007

Thursday, December 20, 2007

Page 16 of 23

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
Charles Wade	<p>DO NOT BUILD A RAILROAD LINE AT THE PROPOSED WILLOW LOCATION.</p> <p>We would like to add our names to the list of those OPPOSING the proposed Willow rail line extension to Port MacKenzie. We live on Little Lonely Lake, which is very near the proposed Willow route.</p> <p>We moved to Willow and this little lake in 2001. We purchased a lot and were the first to build a house on the lake. We live here year round. We selected this location because it was remote and quiet. Until now it has remained that way.</p> <p>If this extension is built using the proposed Willow route. We will clearly be able to hear the railroad activity (construction and operations). This is unacceptable to us. There is no doubt that any such construction will reduce the attractiveness of our home; and very likely reduce its property value. I know this from personal experience. When we were looking for a home back in 2001, we considered a home on Cheri Lake (just north of Houston). We both liked it. However, we changed our minds quickly when a train came by. Should the line be built here others will do the same when they someday look at our house.</p> <p>Additionally, we make extensive use of the "Swamp" area to the south and west of our home throughout the year. Building this extension here will restrict access to this area, and as a result our quality of life will decrease.</p> <p>There are other options. We urge choosing one of the others.</p>	10/25/2007
Stu Grenier	<p>I am still waiting for a comment meeting to be held in Anchorage. Why has not one been announced yet? The vast majority of property owners on Red Shirt lake live in Anchorage and would like a chance to comment at a meeting. Thank You.</p>	10/28/2007

Thursday, December 20, 2007

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
Laura Bedard	I am opposed to the Port Rail Extension through Willow. This will destroy moose habitat as well as the recreation. I am concerned about the proposed trail tunnels: Tunnels flood, especially built in a swamp. It will be a hazardous to the multiple users motorized and non motorized.	10/28/2007
Mary Miller	My husband and I bought land on Little Lonely Lake in Willow in 2001. We have built our retirement home there and plan on moving out there in 5 years, we go out there every weekend now to regain our sanity. We purposely bought there due to the tranquility and serenity of the small quiet lake with the charm of the sound of loons. All the homeowners got together to have lake management to protect those issues. With a railroad so close, that will be destroyed. Not only that, but the access to the open swamp area will be cut off. And there goes the whole freedom of snowmachining. I understand that Houston would love to have the route, please let them have it and don't go the Willow route. Thanks for listening.	10/29/2007
Kevin Gray	As a homeowner in the MatSu Borough on Nancy Lake, I am deeply concerned with the western-most corridor that is also marked Corr3. This area is a highly used area during the wintertime by a variety of traditional users. Dog mushing, the iditarod, the Iron Dog and regular "weekend warrior" cabin and snowmachine types utilize this area for access to the Susitna drainage. A North-South railroad access easement would create an effective "block" of these activities given the ability to cross the tracks. Some discussion has been present regarding "crossings" at some intervals, but would require going some distance out of the way to get to the "crossing". In addition, this area is a great North-South transit for snowmachines from the Susitna Landing area to Big Lake. This corridor needs to be maintained as an "open" area for these traditional uses. The other two routes proposed are much closer to civilization, and would provide much-needed access to these areas, rather than ruin a pristine wetlands area in favor of "progress". Please look seriously at the other two routes.	10/29/2007
Earl Ramsey	I am a Red Shirt Lake land owner. I support the following letter from the Red Shirt Lake Owners Association:	10/30/2007

Thursday, December 20, 2007

Page 18 of 23

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
James Miller	<p>Please do not put the railroad through Willow. The route goes through what we call the big swamp which is home to the largest number of moose and wild game in the area. The deep snow will drive the moose to the easiest trail available which will be the railroad track. The number of moose kill will be to high a price to pay.</p> <p>The railroad will need to cross the road to the Deshka Landing which is an extremely busy road in the summer when people from all over Alaska come to fish the Deshka River for Kings, Silvers and Pinks. It will cause many traffic problems.</p> <p>The railroad will need to cross Willow Creek home to a King salmon and Rainbow trout potential damage to the creek and fish are not worth the risk.</p> <p>The Willow route will run right behind the lake we built our retirement home on. We worked for several years to get a Lake Management Plan to protect the quiet enjoyment of the lake. Putting the rail line about 1/4 mile away will damage our quiet enjoyment.</p>	10/31/2007
Steve Wells	<p>I am totally in favor of the Willow route and the sooner the better. I have lived in Alaska for 50 years and have been in Willow for the last 29 years. Have built 3 homes in the area. I favor responsible development and improved access.</p>	11/1/2007
Duane Mathes	<p>Houston would be the best location to connect to the main line. They could use the business and the influx of money it'll bring. Maybe clean it up a little there too. It's a shorter run to construct, should save some money for the railroad as well with a minimal amount of private property to purchase.</p>	11/1/2007
Chris Grabowski	<p>Specifically, how will the Point Mac Rail Extension benefit Houston, Alaska?</p> <ul style="list-style-type: none"> <li>•Will all new rail beds be elevated over all road crossings?</li> <li>•Why would the AKRRR agree to put a depot in Houston, Alaska if the AKRRR currently pursues projects that reduce run times?</li> <li>•If:             <ul style="list-style-type: none"> <li>oThe Point Mac Rail Extension is a passenger/freight line</li> <li>oThe South Entrance to Denali National Park &amp; Preserve is going to be in Willow</li> <li>Isn't it likely that there will be a depot in Willow at the new South Entrance?</li> <li>oA depot in Houston would add time onto rail schedules.</li> <li>Why would the AKRRR add a depot in the City of Houston?</li> </ul> </li> </ul>	11/1/2007

Thursday, December 20, 2007

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
Keith Dobson	<p>I attended the meeting in Palmer last night (11/20/07) and have the following questions regarding the cost matrix.</p> <ol style="list-style-type: none"> <li>1. Have you factored in the cost of delays for a particular route due to unexpected wetland run-off causing personal property damage...</li> <li>2. Cost of delays due to litigation associated with personal property damage.</li> </ol>	11/21/2007
Yvonne Sumner	<p>I feel that the rail spur is an extremely important infrastructure development. The rail spur will benefit all Alaskans, especially those in the interior, by giving them access to shipping at lower costs. Given the current fuel costs this rail spur will make the difference for many businesses, industries, and villages as to viability. As Alaskans we should support this rail spur it is essential and strategic, and it is cruel to do otherwise.</p>	11/25/2007
Florence Pitcher	<p>It appears that we would be among the biggest losers if the Point Mackenzie West route is chosen. We own tract 9. The one with all the nice lake frontage, and wildlife refuge frontage.</p> <p>Point Mackenzie Agricultural District is not "Port Mackenzie Agricultural District".</p> <p>Our farm there is privately owned. The farm is also our primary life investment.</p> <p>The Western Pt. Mac route would devastate the integrity of our own planning as owners of this land.</p> <p>What makes planners think that they can plan commercial developments across Agricultural lands? Because if they do indeed try to negotiate for a commercial rail line, rest assured that they will indeed pay commercial prices (NOT ag land prices) for the entire property effected. Cutting into a parcel with this type of development effects its value as a whole, and effects it's entire value, and will be considered as such. Be ready to see savvy commercial appraisers from other States and Professionals well versed in Congressional law, and a few hard working land owners with their heels dug in real deep.</p>	11/28/2007
Jordan	<p>It seems that the more easterly route would make more sense. There is already development, there appears to be less wetland, and it even looks more direct. The more westerly routes would also have an effect that is not so easily quantified, and that is the idea that we will no longer have the feeling of wide open spaces to the west. There would always be the rail barrier. Just a thought.</p>	11/29/2007

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
Duane Mathes	I believe the port mckenzie rail spur should intersect the main rr line at Houston. Houston is a city that could certainly use some economic help and this could be a way to help that community get on the map. Maybe with the help of putting in a rr station of some type. I would think they would want this too.	12/10/2007
Dan Mayfield	<p>The clear choice is the MacEast-Big Lake route. Your latest publication utilizes a criteria matrix to grade the individual routes using a plus and minus for positive and negative attributes of each. The results of this grading indicates the MacEast-Houston to Houston South route as scoring the highest. Analysis of this area is flawed and suggest an agenda out side the interest of the people who live in this area. The MacEast-Houston route clearly fragments recreational areas that are used almost everyday by the majority of recreational user who live and visit the Big Lake areas. A vast portion of this route crosses wetlands and would have a negative impact on designated land use.</p> <p>The least impact to quality of life, recreational opportunities, wild life and the choice that provides for the least noise impact would be the MacEast-Big Lake route because this would establish the railroad traffic around the already developed road system.</p> <p>Your Matrix Criteria missed the most important measurement. How does the choice impact the identity of the communities around it? Big Lake is a residential community with a recreational community purpose. Building the railway to the west of the lake impacts that purpose in a gigantic way by cutting off access to the areas west of the Susitna River. This area is hugely popular and critical to the recreational nature of this community.</p>	12/10/2007



<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
Keith Dobson	<p>To all concerned, I have previously submitted my comments via the Port Mackenzie web site. My previous comments and concerns raised issues that I perceive with selecting either of Houston routes (Houston North or Houston South). However, I want to make it clear that I am in full support of this project and will support the final decision of the STB and the conclusions of the EIS. I do however request that a fair and comprehensive approach be used in determining the most viable route/routes to go forward with in an EIS study. It is my opinion that the current evaluation matrix clearly does not satisfy this request. A comprehensive evaluation matrix would include the following new categories/considerations:</p> <ol style="list-style-type: none"> <li>1. A weighting system associated with each column. I contend that train power should not be weighted the same as the amount of wetlands disturbed in constructing the line.</li> <li>2. Furthermore, I contend that the current evaluation matrix should be expanded to include the following criteria.               <ol style="list-style-type: none"> <li>a. Amount of residential property impacted or potentially impacted</li> <li>b. The potential for future road expansion</li> <li>c. Unforeseen hidden costs (i.e. caused by unstable soil conditions; damage caused by flooding to personal property; impact on commercial fishing)</li> <li>d. Alignment with valley economic development plan</li> </ol> </li> </ol> <p>Finally, concerning this last item that I raised above, if the Matsu Borough has not determined an economic development plan for the next 25-50 years than I would suggest that a plan be determined prior to selecting any of these routes under consideration. If such a plan is already in place or when a plan is determined, I would ask that a selection criteria be added to the evaluation matrix to reflect the impact that any of the proposed routes will have on this plan.</p> <p>Thank you for your consideration.</p>	12/10/2007

<i>Commenter Name</i>	<i>Comment</i>	<i>Comment Date</i>
Keith Dobson	<p>To all concerned, I attended the assembly meeting last evening and would like to make just a few comments regarding the Port MacKenzie Rail Extension Project.</p> <p>First off, my family has owned property on West/Little Horseshoe Lake for over 30 years. I have snow-machined, hiked and canoed much of the area west of the lake around Mule Lake along the proposed Houston rail route. This area is primarily one big marsh with a very sensitive eco-system. Any disruption in this eco-system will most certainly disturb the local loon and crane bird population which are highly sensitive to any eco-system changes. A proposed rail track in this area would require massive amounts of material to be added to establish a firm base for trains, producing in affect an extremely large dam cutting through the heart of this sensitive eco-system. We can use the best available science to estimate the impact of this dam, but I am sure that no-one really knows what the long term impact will be on the bird and wildlife population and the personal property owners who may and most likely will see a change in their soil conditions (which will affect well and septic systems, foundations could be flooding as a result of the railroad dam...). In reviewing the evaluation matrix last evening, I suspect that the following criteria were not included in the cost estimates for this project.</p> <ol style="list-style-type: none"> <li>1. Additional costs associated with unexpected water drainage issues resulting from rail extension dam.</li> <li>2. Additional costs associated with unexpected soil conditions.</li> <li>3. Cost over-runs as a result of the above delays which require additional engineering.</li> <li>4. Litigation costs due to personal property damage.</li> <li>5. Cost of delays associated with personal property litigation issues.</li> </ol> <p>Now for the social and economic consideration for this project. I am in full support of economic development for Alaska as long as we consider both the short term and long term costs and benefits associated with any project like this. I would submit to you that over the next 50 years the Matsu valley area around Big Lake and Nancy Lakes will be highly desirable locations for Alaskans to both work, recreate and live. With proper management and infrastructure (roads, business development, schools...) this area will likely grow significantly and become a wonderful place for people to live. With that said, I would like you to consider the following questions in light of a 20-50 year plan for this area.</p> <ol style="list-style-type: none"> <li>1. What would be the social impact of a rail project slicing through the heart of the Matsu Valley as proposed in the Houston route?</li> <li>2. What will be the social impact of rail accidents and coal dust and other airborne debris for any proposed route?</li> <li>3. Should a future car carrying ferry or bridge connecting Anchorage to Port MacKenzie be built, have you considered obtaining a large enough right-of-way to accommodate a future highway along the rail extension line for car commuters to the valley?</li> <li>4. If the above is desirable, which of the proposed routes is best suited to accommodate a future road project along the rail extension line.</li> </ol> <p>Thank you for your consideration.</p>	12/10/2007

# MAP COMMENTS

*(This page intentionally left blank.)*

Port MacKenzie Rail Extension Project  
 Wasilla Meeting, October 1, 2007  
 Map Comments

All map comments are entered moving from South to North. Geographical references are used when possible.

*Map 1 (Agricultural Area)*

No Comments

*Map 2 (West)*

Willow

- On WACO web site (?) Willow West Gateway Winter Trail System
- Willow West Gateway Winter Trail System = Map
- (connected to comment above) Check for snowmachine trails
- Please in considering the Willow Route stay west of the Red Shirt/Cow Lake/Fish Creek ridge line. The ridge line would be a good sound barrier for Red Shirt and Cow Lake property holders Jon McCracken and Cathy McCracken, Cow Lake property holders
- (Cow Lake indicated south of Red Shirt Lake)
- (Ridge line noted west of Red Shirt and Cow lakes)
- (snowmachine trails marked)

*Map 3 (East)*

Big Lake

- This is awfully close to my house (marked) would like to see it moved a little father back to cut down on noise

Houston

- Major BL (Big Lake) recreation sites and winter trails

Houston South

- (Horseshoe Lake Firewise Community outlined)
- This is an emergency runway for any aircraft
- Also crosses B.L. Winter trails
- Cathi Kramer 892-7067 Horseshoe Lake Firewise Community Concerns:
  - Decreased property value in an area the (sic) has worked hard
  - Fire potential from rail sparks in a high risk fire area
  - Trail access
- Private Runway 15 acres Emergency access and evacuation. This route cuts property and runway in half and makes property unuasable (sic) Recomend (sic) Willow furtherest (sic) west route
- These routes would ruin the quiet serenity of living in this area
- Gas well (indicated)
- Noise not compatible with homeowners

Houston North

- (Castle Mountain Earthquake Fault indicated)

*Map 4 (Central)*

No comments

Port MacKenzie Rail Extension Project  
 Big Lake Meeting, October 2, 2007  
 Map Comments

All map comments are entered moving from South to North. Geographical references are used when possible.

*Map 1 (Agricultural Area)*

Comment regarding the port

- This is not an open port in the winter.-

*Map 2 (West)*

Willow

- (Small trails noted periodically along the entire route)
- Please notice this is on the best land to build. Look at other lines proposed.
- Each community (sic) should do everything possible to document trail (sic) so bridges can be built in for trail. That is what this meeting is for (face icon)
- Willow grows/maintains a series of trails in this area (broad area indicated on both sides of the alignment)

Houston

- Anadromous stream from Crooked Lake to Little Su River (drawn in)
- Iron Dog Trail and snow machine trails—Big Lake to Little Susitna River (drawn in)
- Road access to Papose (sic) Lakes (drawn in)
- Wetlands (indicated directly west of Big Lake)

*Map 3 (East)*

Big Lake

- Notice the roads and subdivisions? This area will only continue to grow—which will be a BIG impact in the future.
- Private, gated community (marked) 16+ full time families. We are here for the solitude and QUIETNESS!
- Designated non-motorized lake for nesting loons.
- Bee yard
- (Parcel noted on North shore of Big Lake) my house. \*\*HUGE Noise pollution to all properties. We hear police and ambulance on Big Lake Road at our house. Whole community will be impacted. Ditto on the other (Houston) route.
- Too close to our homes this would be a wistle (sic) point.
- Business, planned subdivision (outlined)

Houston

- \*\*See comment (on Big Lake alignment above)
- This area floods
- The railroad would cause extensive flooding potential.

Houston South

- Very high risk hazard rating. Get 2006 CWPP—Horseshoe Lake Firewise Community

- This area is used extensively by showmachiners and dog mushers! I would be very concerned about what would happen to this wilderness area, habitat, etc. Please not here! Yuck! (area along Houston South and Little Susitna Recreational Area indicated by arrows).
- 

*Map 4 (Central)*

Houston

- (Snowmachine trails noted periodically)
- Not here (property circled)
- How will your culverts work for trails/crossings in the wetlands, poorly need alternative (arrows leading from the Little Susitna Recreational River area).
- Private sea plane dock and ramp—5000ft floatplane sea port.

Houston South

- 1200 ft. private emergency access and runway. This route ruins our quality of life and the reason we moved out in the Valley.
- Lot 17 N04W11 A012, 17 N04W11 A010 (?) We are building our retirement home use the track and see numerous mushers in our area. We object to having a rail down our driveway and across our property (marked).
- (Snowmachine trails marked)

Houston North

- (Snowmachine trails marked)



Port MacKenzie Rail Extension Project  
Willow Meeting, October 3, 2007  
Map Comments

All map comments are entered moving from South to North. Geographical references are used when possible.

*Map 1 (Agricultural Area)*

Near Port Area

- Why is a route not being considered to run on the opposite side of Knik arm from the existing railroad and connecting to the existing railroad just south or around #150. I know this would require updating crossings—but isn't that just a cost of doing business?
- I agree (referring to above comment). Stay close to existing roads. Wouldn't that help minimize environmental impact?

Center of connector areas (along Ayrshire Ave. at the boundary of the Point MacKenzie Agricultural Area)

- Snowmachine parking lot (circled)

*Map 2 (West)*

Willow

- How could it possibly be feasible to build a RR through all those wetlands (Willow corridor) when there are other options?
- 2<sup>nd</sup> this concern (referencing above comment)!!! Willow Rt. considerable more wetlands impact and necessary fill material. Houston: least impact and they want it!!!
- You should put the Nancy Lake State Recreation Area boundary more clearly on all maps. Make it obvious how the routes would impact it. That Rec Area is mostly NON-MOTORIZED and a RR is at odds with the management intent.
- Big Swamp (outlined)
- \*See Willow Trail Plan for better representation [of trails]
- \*Lines indicated in red represent trail crossings of the West Gateway Trail System, documented in the Willow Trails Plan submitted to the MSB.
- Corridor 3 will need 10 overpasses for trail access as well as overpasses at roads x3
- For your consideration
  - Fire safety concerns along the rail corridor—
  - New fault lines
  - Environmental impacts
  - Protection from terrorism
  - Economic feasibility
- Willow swamp (indicated)
- Give us trails along the tracks and places to cross often
- Move to existing state of AK right-of-way by MSB Assembly Jan 2003 (possibly 2005)
- Mitigation—wide, how gradient above grade xings at all trails
- Mitigation—parking lots for snow machines and recreational users
- Go through Big Lake—Willow doesn't need another railway dissecting (sic) it!!
- We recreate heavily in this area—my favorite swamp. It will need a culvert. Value our lifestyle as you create changes.
- Let's not sandwich Willow community between two railroads
- What are you going to give us when you take our park?

- Better soils here (marked)
- Private property w/ext(xxxxx) trail system. Please consider moving north or south (area at Y junction/connection to existing line marked)
- This are is a natural for the crossing (indicated)
- Highway curvature and site (sic) lines speed limit 65mph at present

Houston North

- This is Willow, not Houston (area marked)

*Map 3 (East)*

- Togo Woods Girl Scout Camp noted in area east of Big Lake alignment
- Aurora Dog Musers (indicated west of Big Lake alignment)

*Map 4 (Central)*

Houston

- Floods! (area near Y of Houston, Houston South, and Houston North)

Port MacKenzie Rail Extension Project  
 Knik Meeting, October 4, 2007  
 Map Comments

All map comments are entered moving from South to North. Geographical references are used when possible.

*Map 1 (Agricultural Area)*

Mac West

- Baskin Home noted. B&B Farms established 1982. Highly productive hay fields. Irrigation system ¼ mile center pivot.
- 1) Migratory flight plans and 2) use of Point MacKenzie Agricultural Area by migratory critters, as food source?

Mac East

- How provide (sic) access to property owners along RR ROW?

Midway between Mac West and Mac East

- Shown on Board as State-owned; not so.

*Map 2 (West)*

Willow

- The western route would have fewer at-grade crossings and would be safer and have least impact to traffic—go west!
- (immediately beneath above comment) Not if you consider snow machine traffic—potentially even more danger of serious accidents.
- Free right-of-way
- The eastern routes are better than the Willow one. The area from Red Shirt Lake south is better off undeveloped.
- Properties that are remote need to remain remote for safety/security.
- No noise along remote lakes.
- (Three areas along Red Shirt Lake are marked as 1800s Native Village)
- Very quiet area. We like to keep it quiet/remote. No road access. People around the lake have resisted roads, opening lake up to jet skis etc. So keeping it quiet and remote would be great!
- Worried about vandalism from 4-wheelers coming down the RR right of way (arrow to west of Red Shirt Lake).
- We don't allow personal watercraft. There is no road into Red Shirt. We have fought to keep it as part of the canal trail and more calm—

Suggested Route between Houston and Houston North, removing curves of Houston North

- Less land impact and shorter route.

*Map 3 (East)*

Big Lake

- 16 mile trail noted (dedicated easement?)
- Iditarod Trail noted (dedicated easement?)
- Wildlife area and fish creek habitat (areas near junction of Iditarod trail and Big Lake alignment circled)

- Aurora dog mushing trails area (circled area south of Big Lake and west of Big Lake alignment).
- Girl Scout/scout lake areas circled
- Mental Health Trust land circled
- Moose area
- Mental Health Trust and Knikatnu land circled.
- MSB land circled
- Herning Trail outlined (dog mushing trails area)
- Lots of moose in burn area

*Map 4 (Central)*

Houston

- Not here (property on lake west of Houston alignment circled)

Houston South

- Route very close to many recreational/residential properties. Many Anchorage owners—do they know about these meetings? Notice was very short (arrows pointing to lots south of Houston South).

Port MacKenzie Rail Extension Project  
Houston Meeting, October 5, 2007  
Map Comments

All map comments are entered moving from South to North. Geographical references are used when possible.

*Map 1 (Agricultural Area)*

No comments

*Map 2 (West)*

Willow

- (Small trails noted periodically along the entire route)
- Major habitat disruption.
- Robinson 373-1112
- Willow Creek State Park
- Willow Creek—salmon, guides, rafting
- Parks Highway

Houston

- (Small trails noted)

Houston North

- (Snow machine trails area noted north of Houston North/South divergence)
- Some high spots not so bad
- Wetland swamp

*Map 3 (East)*

- Area near Knik—cemetery sites marked
  - Lot 1 V551726
  - Lot 8 V551726
  - Lot 11 V551726
  - Lot 7 V551726
  - V55239B
  - Gov Lot 1
- Federally recognized Native village (Knik)

Big Lake

- Water rights impact? Mapped by BLM
- RS2477 Trail, Indian trail?
- Water rights corridors up to Chase Homesteads—state grant and am??r lakes north subdivision—state grant need to be shown (1911 BLM Map)
- Dana Olson, Nansen ?ten
- Judicial notice A-97219 Federal in comprehensive MSB planning disclaimer: DNR, DOT, DEC, NOAA, EPA
- Olson vs Gamon/USDA excerpt of record is MSB document showing trail on water rights doc establishing apportioned water rights for horses.
- Big Lake Rd to be upgraded to 4 lane

- Good!! (crossed out, "Bad" written)
- 4,000 cars/day (at Big Lake Road junction with Big Lake alignment)
- Busses and H.S. kids crossing tracks
- 11,000 cars/day (at Parks Highway junction with Big Lake alignment)
- Park Cemetary
- Very Congested
- Houston Jr Hi and Hi (circled)

Park areas noted north of existing alignment roughly between the Big Lake and Houston South alignments

#### Houston

- This Dam (referring to potential dam created by new alignment) will flood this area
- (Referencing above) Agreed! with personal knowledge

#### Houston South

- Big Lake #2 (arrow)
- Potential development industrial (circled)

#### Map 4 (Central)

#### Houston

- (area marked) Salmon spawning stream

#### Houston South

- VOR approach used for air guard and other training traffic, flying over 2 times daily with large planes @ 500ft. (flight path noted, roughly parallel to Houston South and ending at an area just south of junction)
- The best route

#### Houston North

- Ducks,, swan, yellowlegs nesting, very wet, water height fluctuates, fish migrate (salmon) major wetland (area near connection to existing alignment)
- Land is wet but doable (something I can't read)

### Map Comments from the Willow Community

A green line and some blue pen lines (indicating a possible alignment(s)?) were drawn starting approximately 6 miles from the divergence of Houston and Willow alignments. The line ran north and east, above the residential development around unnamed lakes, through the Little Susitna Recreational River, and reconnecting to the Houston North Alignment approximately two miles southwest of the Parks Highway.

Notes on the green line read:

- This pathway does not cross the Parks Highway
- Not crossing Parks

The Nancy Lakes State Recreation Area name is circled and boundary outlined.

An area to the northwest of the northern Willow route is circled "our park".

Notes at the Y intersections of both the Willow and Houston Routes at their junctions with the Parks Highway request that approximately highway mile marks be used "so that people have an idea where the railway is in relation to the Parks Highway."

Extensive trail system is circled in an area along the Willow route near the developed lakes.

Questions in the margins read:

- Is there fire mitigation included for corridor 3?
- Are you aware that the economic profitability of coal is only 15 years due to increasing federal regulations due to environmental factors of coal—carbon dioxide production & mercury.
- How much land will be impacted by corridor 3—the 2003 survey says 4,556. Is that still accurate?
- Originally an 800 foot right of way was included to accommodate 4-lane divided highway and included a possible fuel storage area—what is present status?
- If the 4-lane divided highway is not included, what would be the economic benefit for Willow?
- Is there a possible commuter/passenger rail line included?

# COURT REPORTER COMMENTS



*(This page intentionally left blank.)*

1  
2  
3  
4  
5  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25

PORT MACKENZIE RAIL EXTENSION PROJECT  
PUBLIC COMMENTS

October 1, 2007  
Commencing at 6:00 p.m.

Taken at  
Lake Lucille Inn  
Wasilla, Alaska

Reported by:  
Britney Chonka, CSR

Page 2

1 PUBLIC COMMENTS  
 2 MR. WHEDBEE: Kenneth M. Whedbee, W-H-E-D-B-E-E.  
 3 I live at 450 Brazil Circle, Big Lake. It's where  
 4 the Houston -- you've got two intersections there,  
 5 you've got the Houston extension and then one coming  
 6 out of -- right there. I live right there.  
 7 Okay. The Houston North and the Houston  
 8 intersection is where I live. I live on West Lake  
 9 with a 1200-foot runway that extends -- if this map  
 10 is correct, it extends 400 foot north of where  
 11 the -- of where the railroad is coming through. So  
 12 they would take out my runway. And they would come  
 13 right through my 15 acres of property.  
 14 But West Lake and this runway is used for  
 15 emergency evacuation to fly people out of that area;  
 16 example, the '96 fires. And it's really the only  
 17 runway in that area. And where I offered the VOR,  
 18 you know, that -- I closed escrow 30 days ago, there  
 19 was never disclosure there that there was a railroad  
 20 issue. I just bought it. I gave them \$1 million  
 21 for it. And 30 days later they said, oh, they're  
 22 going put a railroad through.  
 23 I feel that the Willow route is the best route  
 24 to take. It would impact the least amount of  
 25 recreation, snowmachine trails and everything in

Page 3

1 that whole Big Lake area. I guess that's the  
 2 conclusion.  
 3 MR. SEAVER: Carl Seaver. He said things a lot  
 4 I would want to say. I live way over here by Knik.  
 5 You want my address and all that?  
 6 Carl Seaver, S-E-A-V-E-R, Post Office Box  
 7 877171, Wasilla, Alaska 99687. Everything -- what's  
 8 your name again? Everything Kenneth said would be  
 9 just -- is just about what I would have liked to  
 10 say, exception being I don't own any land over there  
 11 or nothing.  
 12 But my views are the same as his. And I believe  
 13 you said you agree with the most rural route, which  
 14 is the Westward route, which is the same feeling I  
 15 would have. It just seems the most sensible, that's  
 16 supposed to have, as I understand it, the most  
 17 gravel supply and stuff like that, too, better  
 18 bedding for the railroad and all the whole thing.  
 19 That's what I wanted -- he took the words right  
 20 out of my mouth, except for the airport thing. And  
 21 I didn't know about the emergency stuff. He has got  
 22 an excellent point there, his area being used for  
 23 emergencies.  
 24 MR. TURINSKY: Anthony Charles Turinsky,  
 25 T-U-R-I-N-S-K-Y. I own a 500-acre parcel of

Page 4

1 property. And originally they've drawn lines in  
 2 various places. And they drew a line right through  
 3 the center of my property. And my big concern is if  
 4 it takes 30 years or 50 years, whatever line they  
 5 draw on my property, it diminishes the value of my  
 6 property today. Because if I go to sell it to  
 7 somebody and I have to show them the line that's  
 8 been drawn, but not decided upon, but it's a line,  
 9 it impacts the value of my property now.  
 10 Some day when they decide they may move it,  
 11 maybe not anywhere near my property. And if they  
 12 take a large enough piece of property, mine is one  
 13 of the parcels of 500 acres that is impacted pretty  
 14 severely.  
 15 And I'm in favor of the railroad, I think the  
 16 process of the railroad is important. But I'm --  
 17 concerned about the environment, but I would prefer  
 18 that they push it as far west away from private  
 19 property and trade some land the Borough has that's  
 20 wetlands to the refuge. And I understand the cost  
 21 of having to go through wetlands is more. And if  
 22 they have to go through wetlands, I certainly want  
 23 to make sure -- I mean, if they don't go through  
 24 wetlands, I certainly want to make sure that they  
 25 make me whole and all the other property owners

Page 5

1 whole. That's it. Thank you.  
 2 MR. CIZEK: Joe Cizek, C-I-Z-E-K. I guess my  
 3 big concern is most of this land on the south side  
 4 of this development Port Mackenzie, that's all  
 5 dedicated as ag land. This ag land, according to  
 6 the papers that I -- I own one of the pieces. But  
 7 what I'm reading from the papers, ag land doesn't  
 8 really allow me to construct a railroad on it. How  
 9 can an -- a railroad be constructed across the  
 10 property? I mean, is this going to set some sort of  
 11 precedent for future development? I mean, housing  
 12 projects to hotels to whatever? I mean --  
 13 MS. GOJMERAC: Mary, last name is  
 14 G-O-J-M-E-R-A-C. The Willow route, is it higher  
 15 elevation than the Wasilla route? Number 2, has the  
 16 Willow route considered the possibility of flooding,  
 17 because of the Susitna River and the Willow Creek?  
 18 That thing just really stuck out at me when I seen  
 19 that I said, oh, man.  
 20 Because you have -- Talkeetna has three rivers  
 21 that flow into it. And once in a blue moon they  
 22 flood. And when they flood, then it sort of works  
 23 its way down the range. You understand? Thank you.  
 24 MR. MCCrackEN: Jon, J-O-N, McCracken,  
 25 M-C-C-R-A-C-K-E-N. I just want the railroad, when

Page 6

1 they're considering the Willow route, to stay well  
2 west of the Red Shirt, Cow Lake, Fish Creek ridge  
3 line, I mean, for the most part.

4 And then recognizing that the ridge line is a  
5 good sound barrier for any rail traffic going back  
6 and forth from Willow to Port Mackenzie. It would  
7 block all the property owners along that corridor  
8 from obviously inevitable sounds, railroad sounds  
9 and so forth.

10 That's it.  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25

Page 7

1 REPORTER'S CERTIFICATE  
2

3  
4 I, Britney E. Chonka, Court Reporter, hereby  
5 certify:

6 That I am a Court Reporter for Southcentral Court  
7 Reporters and Notary Public in and for the State of  
8 Alaska at large. I certify Hereby that the forgoing  
9 transcript is a true and correct transcript of said  
10 proceedings taken before me at the time and place stated  
11 in the caption therein.

12 I further certify that I am not of counsel to  
13 either of the parties hereto or otherwise interested in  
14 said cause.

15 In witness whereof, I hereunto set my hand and  
16 affix my official seal this 15th day of October, 2007.  
17  
18  
19  
20

21 BRITNEY E. CHONKA, REPORTER  
22 Notary Public - State of Alaska  
23  
24  
25

*(This page intentionally left blank.)*

1  
2  
3  
4  
5  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25

PORT MACKENZIE RAIL EXTENSION PROJECT  
PUBLIC COMMENTS

October 2, 2007  
Commencing at 6:00 p.m.

Taken at  
  
Big Lake Elementary  
Big Lake, AK

Reported by:  
Rosie S. Scott, CSR

PUBLIC COMMENTS

1 MR. ORIEN: Oric Orien, O-R-I-E, O-R-I-E-N  
 2 resident on Crooked Lake. I'd like to make the following  
 3 observations and comments about the Houston route.  
 4 The Houston route is an endorsing to the north  
 5 Big Lake area, and passes through to the east of the  
 6 Pappoose Twin Lakes, and west of Crooked Lake down through  
 7 its intersection at Connection 3.  
 8 It passes through significant wetland areas.  
 9 It also passes and cuts through the heart of the  
 10 well-established Big Lake snowmachine trail area that has  
 11 intersecting trails, including the Iron Dog Trail that  
 12 goes west past Little Susitna River, over to the Susitna  
 13 River. The area between Pappoose, Twin Lakes and Crooked  
 14 Lake is a very small corridor with significant open  
 15 wetlands.  
 16 Also, the stream that flows out of Crooked Lake  
 17 that flows over to the Little Susitna River is also a  
 18 migratory salmon stream designated previously.  
 19 Therefore, basically, my comment is that the  
 20 Houston route basically disrupts entire recreational  
 21 area, and also disrupts a wetland area, and I would not  
 22 favor that selection. Thank you.  
 23 MS. THOMAS: Good evening. My name is Monica  
 24 Thomas. I'm a resident here in Big Lake. And after  
 25

1 looking at your maps and figuring where things are, it  
 2 seems to me that even though you outline the two ways of  
 3 accessing the Parks Highway on through Big Lake, and one  
 4 on the other side of Houston the -- none of the plans  
 5 shows the fact that the railroad already goes through  
 6 like Houston proper. And by hooking on below Houston  
 7 between Big Lake and Houston to that railway, whatever  
 8 you're bringing down that railway is going to go through  
 9 those communities.  
 10 I think if you use the one that goes out by Red  
 11 Shirt Lake it doesn't impact as many areas that have  
 12 already been settled. And I know it may cost more, but  
 13 we look at environment, and the things that it's  
 14 disturbing.  
 15 Besides looking at wetlands and all that, we  
 16 have a lot of communities that have children in them, and  
 17 industrial waste is something we don't want. And I'm  
 18 sure if that -- what they want to do with this port,  
 19 eventually, they will want to make some money, so that  
 20 means they're going to travel back and forth on that  
 21 railroad.  
 22 So it would be nice if they could do it in a  
 23 way that does not, once they access the railroad, it  
 24 doesn't go through these major communities. That's my  
 25 thoughts. Thank you.

1 MR. WILSON: My name is Scott Wilson. I'm a  
 2 Big Lake resident. I'd like to make sure that they look  
 3 at an impact study in regards to the moose population on  
 4 the Big Lake, Houston and Willow, as well as the snow  
 5 levels in those three different areas because the snow  
 6 depth is going to be different in each one of those, and  
 7 you don't want to see too much mortality in regards to  
 8 the moose on the railroads. That's my primary concern is  
 9 the moose habitat. Yeah, my main concern is the moose  
 10 habitat in that area.  
 11 The trails have been covered already, but I  
 12 haven't seen any of them talk about the moose habitat,  
 13 and make sure that there were enough studies done on that  
 14 MS. WHITFIELD: My name is Janet Whitfield.  
 15 I've got several questions. I'd like to know if the  
 16 Willow route would open up access to an undeveloped area  
 17 promoting expansion? And I'm talking major commercial,  
 18 residential, tourism and industrial.  
 19 Would there be less impact on more densely  
 20 populated residential areas for the Willow route? I'm  
 21 looking at the map, and it really looks like a much  
 22 better route. Another thing is I want to know if the  
 23 Willow route would require less acquisition of private  
 24 land.  
 25 Looking at the map it shows it going around

1 basically the developed residential areas, and also  
 2 basically the developed areas, period. And it just seems  
 3 to me like it would be a major benefit to the borough to  
 4 have the Alaska Railroad and, you know, the federal  
 5 government open up a new area for the borough at -- I  
 6 don't really know if it's even going to cost the borough  
 7 much of anything to have this route go in. I don't know  
 8 if the federal government is picking it up. That would  
 9 be nice, too.  
 10 And I didn't know if the Willow route would  
 11 also have less impact on wetlands. I know it all seems  
 12 pretty wet in this whole area. I didn't know if that  
 13 would be a better route environmentally as well. But  
 14 absolutely I'm in favor of the Willow route. I think  
 15 it's a much better route than thinking about Big Lake or  
 16 Houston where they have to go through residential  
 17 populated areas, and definitely the Wasilla route I think  
 18 would be out if that's what someone mentioned tonight.  
 19 And that is because the Alaska Railroad is already  
 20 talking about re-routing through Wasilla anyway in a  
 21 different area, so it would not make sense, plus it would  
 22 not -- you would not have the benefit of opening up new  
 23 areas. That's it. Thank you.  
 24 MS. THOMAS: Good evening. My name is Monica  
 25 Thomas and I'm back for a second time after listening and

Page 6

1 looking at more things. I spoke to a man by the name of  
 2 Tom Brooks who explained to me the rail route, and why  
 3 you have Conn 1, 2, and 3.  
 4 And one of the things we discussed was that  
 5 currently, when you move coal from up at Healy, it's  
 6 currently going through all these cities. It goes  
 7 through Willow, Houston, Big Lake, Wasilla, right into  
 8 Eagle River and Anchorage to get to the port currently.  
 9 You're moving industrial things through heavily  
 10 populated areas. It doesn't make any sense to look at --  
 11 I don't know, I guess hazards we don't need to have,  
 12 which makes the Willow extension look better because that  
 13 route would, once it hits the Willow extension, would go  
 14 around all these populated areas which, you know, seems  
 15 safer to me all the way around, which I realize it does  
 16 have impact on plant life and animals.  
 17 In looking at your map and studying it more,  
 18 the Big Lake route within the first five miles of the new  
 19 rail line, it will be attempting to integrate within  
 20 existing neighborhoods and a community proper in which  
 21 the rail line will impact, not only its noise, sound,  
 22 dust, impurities in the air, generally changing our  
 23 current recreational clean air, natural environment.  
 24 And, you know, you're saying well, why should I  
 25 have it? Well, if I don't have to, I just assume leave

Page 7

1 it the way it is.  
 2 There's also new neighborhoods between Knik  
 3 Goose Bay Road and Marian and West Susitna Parkway that  
 4 your maps do not address. And I think it's mainly  
 5 because there's been a big push in the Valley with  
 6 contractors and subdivisions, and they're basically the  
 7 Knik Goose Bay subdivisions are moving back up over the  
 8 little hill towards the backside of Big Lake. And it  
 9 would really impact huge neighborhoods.  
 10 I think that your Mac West and the Conn 1, and  
 11 the Willow route is a -- looks much better to me because  
 12 it's streamlined, it's available to the agriculture area,  
 13 even though they don't think that's positive for them  
 14 now, in the long run it could be very positive.  
 15 And the land in that area, at least around --  
 16 has the least amount of wetlands. If you look at the  
 17 topography of that land, it's much more stable land than  
 18 what we have here in Big Lake and the Houston area.  
 19 Because in those two areas the railroad would  
 20 not only cross the subdivisions, but it's looking at a  
 21 tremendous amount of waterway issues that you don't seem  
 22 to have when you go over this Willow route.  
 23 I think that the other thing that comes to mind  
 24 is that the railroad is incoming through Houston and the  
 25 Big Lake sites. It won't only cross the subdivisions,

Page 8

1 but there's an excellent animal, people, plant habitat  
 2 that we've all learned to coexist in, and it's getting  
 3 populated.  
 4 Your rail line I think would upset the delicate  
 5 balance that we're working towards right now. I really  
 6 do.  
 7 I think it's already -- the growth we've seen  
 8 in the last two years is really scrunching. And since I  
 9 spent some of the day picking berries, I would like to  
 10 know that my grand kids are going to pick berries some  
 11 day, too.  
 12 It seems to me that Willow route will impact  
 13 some animals and plant habitat, yet it's sparsely  
 14 inhabited by humans. So I think that this project could  
 15 help open up that area using the latest and best  
 16 practices for expansion, which aren't available or  
 17 weren't available, and were not used with expansion of  
 18 the borough that has happened in the last three or four  
 19 years.  
 20 So I would really like to have that site looked  
 21 at really closely, due to the fact that it impacts the  
 22 least type of human, animal, plant elements, versus --  
 23 just has less people living over there, folks. So thank  
 24 you.  
 25 MS. WOODBURY: Kim Woodbury, W-O-O-D-B-U-R-Y.

Page 9

1 And what I'm concerned about are the trails and the trail  
 2 crossings. I know that it's only the recognized trails  
 3 that you'll be addressing. And your proposed crossings  
 4 using a culvert or a below grade -- or grade separated  
 5 crossing needs to be addressed because with wetlands,  
 6 which is primarily the areas 4 or 3, where many of our  
 7 trails are below the grade separated crossing would put  
 8 the crossing under water basically.  
 9 And I don't see any alternative means of  
 10 addressing that, or will there be some type of crossing  
 11 to go over the railroad because where a lot of these  
 12 trails or unrecognized trails, if the access is  
 13 terminated at the point of the railroad that's another  
 14 issue we need to look at, too because a lot of people  
 15 know their travel from A to B, but if from A to B is not  
 16 a direct route anymore, they have to find alternative  
 17 means, and there's going to be a lot of lost people out  
 18 there trying to find their destination point because  
 19 they've now been put with means of trying to find their  
 20 way to their destination. And I think I just repeated  
 21 myself.  
 22 So I think again, we're going to have to look  
 23 at addressing posting signs that show what the trail  
 24 system is because a lot of pioneer people know the way  
 25 around, know the trails, but now with the new rail system



Page 10

1 that's out there, that's going to change a lot of things.  
 2 And access is one that -- which ones -- I'm not sure  
 3 exactly which ones are established trails. And I'm sure  
 4 a lot of other people don't either.  
 5 I'm very active in the community, and I also do  
 6 a lot of riding, so that will be something new that we  
 7 will have to contend with. And hopefully we can just  
 8 kind of put some foresight into it, and make sure that we  
 9 don't have these problems and make it a fight for  
 10 survival out there for people who get lost due to new  
 11 routes. I think that's it.  
 12 Another issue I have would be the noise issue  
 13 with Corridor 7, the one that's near Big Lake. I know  
 14 right now I'm able to hear ambulances and other fire and  
 15 rescue vehicles when they turn the sirens on. I can hear  
 16 them all the way across Big Lake, and I'm on the  
 17 northwest side of Big Lake. So I'm, you know, a good  
 18 couple of miles away, but I do hear them. I hear them  
 19 coming from where their station is down towards NAPA.  
 20 And knowing that the railroad would be that  
 21 close to a community, the noise pollution is going to  
 22 highly impact the entire community, not just the people  
 23 who are in the immediate vicinity because of the way the  
 24 sound travels. So that's an issue whether it's there or  
 25 even near Horseshoe Lake, being the medium spot where I'm

Page 11

1 at between the two proposed rails, I think noise would  
 2 travel definitely throughout the Horseshoe Lake area, the  
 3 Big Lake community, and then also the southeast area is  
 4 going to be highly impacted, which a lot of us come out  
 5 for the recreational time, the quiet time, our weekends.  
 6 And we live here year round. We would all highly be  
 7 impacted by that and not enjoy our time. And it would  
 8 reduce the quality of life, and also the value of what we  
 9 own, our homes due to noise degradation.  
 10 (Proceedings concluded at 8:15 p.m.)  
 11  
 12  
 13  
 14  
 15  
 16  
 17  
 18  
 19  
 20  
 21  
 22  
 23  
 24  
 25

Page 12

1 REPORTER'S CERTIFICATE  
 2 I, ROSIE S. SCOTT, CSR, hereby certify:  
 3 That I am a Certified Shorthand Reporter  
 4 for Southcentral Court Reporting and Notary Public for  
 5 the State of Alaska; that the foregoing proceedings were  
 6 taken by me in computerized machine shorthand and  
 7 thereafter transcribed by me; that the public comments  
 8 constitutes a full, true and correct record of said  
 9 proceedings taken on the date and time indicated therein.  
 10 Further, that I am a disinterested person to  
 11 said action.  
 12 IN WITNESS WHEREOF, I have hereunto  
 13 subscribed my hand and affixed my official seal this  
 14 \_\_\_\_\_ day of \_\_\_\_\_, 2007.  
 15  
 16  
 17  
 18  
 19 \_\_\_\_\_  
 20 ROSIE S. SCOTT  
 21 Certified Shorthand Reporter  
 22 My Commission Expires  
 23 8/16/08  
 24  
 25

(This area is blank in the original document)

1

2

3

4

5

PORT MACKENZIE RAIL EXTENSION PROJECT

6

PUBLIC COMMENTS

7

8

9

October 3, 2007

10

Commencing at 6:00 p.m.

11

12

Taken at

13

14

Willow Community Center

15

Willow, AK

16

17

18

19

20

21

22 Reported by:

23 Rosie S. Scott, CSR

24

25

Page 2

1 PUBLIC COMMENTS  
 2 SANDRA DELUCIA: My name is Sandra  
 3 DeLucia, D-E-L-U-C-I-A. And I live at Nancy Lake. And  
 4 I am very much in favor of the railroad corridor going  
 5 around and behind Red Shirt Lake, due to the fact that I  
 6 don't believe the residents would like the constant  
 7 commercial type railroad going in their backyards.  
 8 And I think it would be more beneficial to us  
 9 to have it outside the actual city. And I guess that's  
 10 all. I just want to go on record as being in favor of  
 11 the corridor.  
 12 JIM NORCROSS: My name is Jim Norcross. I'm  
 13 the road supervisor for the community of Willow. I work  
 14 for the Mat-Su Borough as a volunteer. And we are in  
 15 favor of the rail corridor running from Willow to  
 16 MacKenzie Point.  
 17 If built from Willow to MacKenzie Point it will  
 18 reduce the amount of traffic on on-grade crossings  
 19 between Willow and the Glenn Highway. And I feel that it  
 20 would be an economic benefit for the community, as well  
 21 as everything north of us for the next 100 years.  
 22 JOEIN WOOD: I'm a dog musher. And unlike a lot  
 23 of verbal opposition to this, I don't oppose it. And  
 24 there are several of us that don't. Having said that,  
 25 there's no reason given to us to support it either. And

Page 3

1 that's where there's a huge deficiency that can be  
 2 addressed.  
 3 If you were to take the corridor for the  
 4 railroad and the maintenance road you have 200 feet to  
 5 play with, allow vegetation to grow on either side of  
 6 those two structures, and allow a clearing path to be  
 7 down one side of the tracks to be used by motorized  
 8 recreational vehicles, and the other side of the tracks  
 9 have to be used by non motorized, so then you've got a  
 10 separation between the mushers, the skiers, et cetera and  
 11 the snowmachiners, and the motorized vehicle users.  
 12 And you would open up a lot of new country for  
 13 recreational use. That is what this community is built  
 14 upon. You'd probably open up cottage industries that  
 15 would want to service those particular recreational  
 16 users. It would give people a reason to get behind the  
 17 project rather than just being bystanders watching what  
 18 is taking place. And you're only hearing one side, the  
 19 opposition because they are motivated. The people that  
 20 aren't opposed are not motivated. So give them something  
 21 to work with.  
 22 MR. STANCULESCU: Vic, Stanculescu,  
 23 S-T-A-N-C-U-L-E-S-C-U. My comments involve the trail  
 24 corridor with Willow as the selection site. I feel it's  
 25 a bad idea because it cuts a large number of trails that

Page 4

1 arc part of the west gateway trail system in the Willow  
 2 area. This is a recognized trail system that heads from  
 3 Willow out west. And it allows mushers, snowmachiners,  
 4 skiers, and other recreational and professional trail  
 5 users access out into the Susitna drainage, and all the  
 6 trail systems that lead out into the Alaska Range. And  
 7 these trails are necessary for training sled dogs,  
 8 they're necessary for the recreational life blood of this  
 9 community.  
 10 And there's a lot of kennels in this area that  
 11 rely on the local trail systems that are historical  
 12 trails that have been used for many, many years,  
 13 generations in fact. They rely on those trails to train  
 14 their sled dogs and to access areas that are off the road  
 15 system.  
 16 And by cutting through these areas with the  
 17 rail corridor, what I feel is going to happen is the  
 18 trails will be forced into a single main line, and that  
 19 main line as per the tunnels will prove to be deleterious  
 20 to the mushing community, and the recreational community  
 21 here in Willow. And I am a trail's advocate in this  
 22 area.  
 23 Part of the need in mushing kennels is to have  
 24 a variety of trails. You don't train a dog team to run a  
 25 thousand-mile race by running around the same trail over

Page 5

1 and over and over. What mushers need to train their dogs  
 2 is to have a choices of trails, so that the dogs are not  
 3 getting in a rut, so that the -- they're able to  
 4 constantly be looking on new horizons. And what's good  
 5 for dogs is good for people.  
 6 I oppose Willow as the site for the rail  
 7 corridor. I think a site further south would be much  
 8 more appropriate and much less costly. The lands to the  
 9 west of Willow are predominately swamp. Swamp is very  
 10 expensive to build road and rail systems on. And I feel  
 11 it's a poor choice. And I don't think Willow has been  
 12 adequately represented in the previous selection  
 13 procedure. Thank you.  
 14 MR. MCCAIN: Edward McCain, M-C-C-A-I-N. What  
 15 is -- what's the purpose of shipping coal for 220 miles  
 16 from Healy, when the Hunts are going to build a conveyer  
 17 at Tyonek on the Shuyak River which is going to be  
 18 10 miles long. And when they get that up and running, is  
 19 this going to be a white elephant that is going to be a  
 20 waste of money? They're going to build this without any  
 21 government money. And we're going to get a lot  
 22 government money tied up in this. And then it's not  
 23 going to economically feasible when they get this Beluga  
 24 or Tyonek or Shuyak River operation going because we're  
 25 not going to be able to compete with them shipping coal

Page 6

1 200 miles, when they only have got to ship it 10 or 20.  
 2 That's why I'm questioning the whole  
 3 feasibility of this project. You know, the government is  
 4 all too good at rewarding failure and penalizing success.  
 5 I think that will do it for right now.  
 6 MR. MYLARENON: My name is Paul McLamon,  
 7 M-C-L-A-R-N-O-N. And my comment is I would like to see  
 8 above-grade, low gradient wide crossings at all trails,  
 9 all of the recognized trails for the westgate system as  
 10 mitigation.  
 11 I also would like to see parking lots added for  
 12 mitigation for snowmachiners and dog mushers. I would  
 13 like to see habitat improvements to Willow Creek as part  
 14 of mitigation for the project, boardwalks, improvements  
 15 for sport fishermen.  
 16 I would also like to see as part of mitigation  
 17 for the project for Willow some type of crossing of the  
 18 Parks Highway so kids don't have to cross the highway on  
 19 their snowmachines and risk getting hit.  
 20 I would also like to see a crossing put in for  
 21 the Emil, Stansik and Espinosa Trail as part of  
 22 mitigation and that's it.  
 23 MR. HALTMANN. My name is Sid Haltmann,  
 24 H-A-L-T-M-A-N-N. And I live at Mile 72.8 where one of  
 25 the three routes crosses the highway and goes right

Page 7

1 through my dog lot, or in other words, my private  
 2 property.  
 3 It would destroy our dog kennel, which we built  
 4 for the last two or three years, and it would destroy our  
 5 plans to maybe build a tourist business.  
 6 It would be very much appreciated if you could  
 7 move the railroad line about 700 feet south, also across  
 8 private property, but undeveloped.  
 9 MS. BARCOME: My name is Lynda Barcome,  
 10 B-A-R-C-O-M-E. And my biggest concern is trails. That's  
 11 why I moved to this community was for trails. And I'm  
 12 very concerned about what I see that they're proposing  
 13 because I don't believe they have taken into account all  
 14 the trails that exist out in this area. And I do not  
 15 agree that underground trails crossings is the way to go.  
 16 They're all going to flood. There's too much water  
 17 around here to think that an underground crossing is  
 18 going to be acceptable to either snowmachines, dog teams  
 19 or any kind of recreational use. I think it's just going  
 20 to make everything impassable.  
 21 If they're going to do it, I mean, they've got  
 22 to come up with some way that we can go up and over and  
 23 above ground safely over those tracks. I just don't see  
 24 how they are going to be able to take care of all the  
 25 trails that are already in inclusive. And I don't want

Page 8

1 to lose any of the trails. There's such a variety out  
 2 here and I think that's why a lot of us are here is  
 3 recreational uses.  
 4 MS. STEIGE: My name is Pat Steige,  
 5 S-T-F-I-G-E. I'm on Shirley Lake. I think it's a real  
 6 shame that Willow, the designated capital site of Alaska  
 7 is becoming the thoroughfare for dirt moving, rather than  
 8 the capital. I just think that's a sin.  
 9 I think they ought to consider that. It's  
 10 still on the books. I know that there's all this other  
 11 discussion in our papers anyway, but it is the only site  
 12 that is designated as the capital site. And I would like  
 13 to see that remain and given that same consideration when  
 14 they're doing this railroad.  
 15 MR. PALMATIER: Dick Palmatier. I believe that  
 16 anywhere this railroad, particularly through Willow here,  
 17 that there should be trails alongside for the use of  
 18 snowmachines, four wheelers, whatever, dog mushers.  
 19 At present the railroad has no specialty signs  
 20 along the railroad. If a new railroad is built that will  
 21 be more of Alaska's lost use to the public. And I also  
 22 think that the existing railroad now should have trails  
 23 for the many motorized vehicles and for pedestrians, dog  
 24 mushers running alongside of it. And they should put  
 25 that in if they want to keep people off the tracks

Page 9

1 because the only way to get to certain parts of the  
 2 country is to go across the tracks, or go down the tracks  
 3 to a particular place you want to be at.  
 4 But I'm mainly interested in this new building  
 5 of the railroad that will have a place for pedestrians,  
 6 snowmachines, four wheelers, and dog mushers to cross  
 7 frequently, and to run alongside the tracks so that  
 8 amount of Alaska -- and it's a big amount -- with a  
 9 proposed railroad going out of Willow. It's a big amount  
 10 that would be lost to the use of the Alaskan public.  
 11 MR. STANCULESCU: Vic Stanculescu. And  
 12 following the presentation another concern came to me.  
 13 And I see that the plan addresses certain trails,  
 14 including the historic Iditarod Trail. The functional  
 15 Iditarod Trail for the past 7 out of 10 years for the  
 16 Iditarod Race restart has left from the Willow Community  
 17 Center. And it departs to the west and the rail corridor  
 18 does not address the fact that the Iditarod Race Trail  
 19 will be blocked if special considerations aren't made for  
 20 crossing at that location.  
 21 I have concerns about this because the Iditarod  
 22 is -- dog mushing is the Alaska State sport. The  
 23 Iditarod is an important -- well, it's more than  
 24 economic -- it's an important aspect to this community.  
 25 And I have concerns that the Iditarod Race Trail, the one

1 that has been used for the majority of years is not being  
 2 taken into consideration in the new plan. Thank you.  
 3 (Proceedings concluded at 8:15 p.m.)  
 4  
 5  
 6  
 7  
 8  
 9  
 10  
 11  
 12  
 13  
 14  
 15  
 16  
 17  
 18  
 19  
 20  
 21  
 22  
 23  
 24  
 25

REPORTER'S CERTIFICATE

1 I, ROSIE S. SCOTT, CSR, hereby certify:  
 2 That I am a Certified Shorthand Reporter  
 3 for Southcentral Court Reporting and Notary Public for  
 4 the State of Alaska; that the foregoing proceedings were  
 5 taken by me in computerized machine shorthand and  
 6 thereafter transcribed by me; that the public comments  
 7 constitutes a full, true and correct record of said  
 8 proceedings taken on the date and time indicated therein.  
 9

10 Further, that I am a disinterested person to  
 11 said action.

12 IN WITNESS WHEREOF, I have hereunto  
 13 subscribed my hand and affixed my official seal this  
 14 \_\_\_\_ day of \_\_\_\_\_, 2007.  
 15  
 16  
 17  
 18

19 \_\_\_\_\_  
 ROSIE S. SCOTT  
 Certified Shorthand Reporter  
 My Commission Expires  
 8/16/08  
 20  
 21  
 22  
 23  
 24  
 25

- 1
- 2
- 3
- 4
- 5
- 6
- 7
- 8
- 9
- 10
- 11
- 12
- 13
- 14
- 15
- 16
- 17
- 18
- 19
- 20
- 21
- 22
- 23
- 24
- 25

PORT MACKENZIE RAIL EXTENSION PROJECT  
PUBLIC COMMENTS

October 4, 2007  
Commencing at 6:00 p.m.

Taken at  
Knik Elementary School  
Wasilla, Alaska

Reported by:  
Britney Chonka, CSR

Page 2

1 PUBLIC COMMENTS  
 2 MS. GOJMARAC: My name is Mary Gojmarac,  
 3 G-O-J-M-A-R-A-C. And my question is what can be  
 4 done to eliminate or drastically reduce critter or  
 5 animal kills? One example would be to really widen,  
 6 and I mean really widen the level gravel areas  
 7 adjacent to both sides of the railroad tracks. This  
 8 can allow critters the chance to move off of the  
 9 railroad tracks and still be on solid footing and  
 10 not forced to jump into deep snow drifts. Anything  
 11 that will help the moose.  
 12 B&B FARMS: What's interesting is that this  
 13 proposed route cuts through our farm from one end to  
 14 the other -- B&B Farms, Baskin. B-A-S-K-I-N.  
 15 I feel, how do I say it, fairly happy in the  
 16 fact that three years ago up in Houston when I put  
 17 my comments in about them going through our barn, at  
 18 least that route is not happening. So I think maybe  
 19 somebody actually read what I wrote. Makes me think  
 20 that.  
 21 MS. GOJMARAC: Mary Gojmarac. Okay. I have a  
 22 concern with the Mac West corridor. It goes between  
 23 Point Mackenzie Agriculture area, which has farms on  
 24 one side and the Susitna Flats State Game Refuge,  
 25 which is on the other.

Page 4

1 route which runs west of the agricultural land and  
 2 east of the game refuge, the one in yellow.  
 3 Okay. First concern is that it runs the entire  
 4 length of our farm from north to south. In doing  
 5 this route, it runs right through an established  
 6 irrigation system, which is a quarter-mile center  
 7 pivot, so it cannot be moved.  
 8 These are established hayfields and they're very  
 9 productive hayfields.  
 10 Let's see. This north/south run would separate  
 11 our home from the business of the farm operation.  
 12 B&B Farms -- they're talking about economics of  
 13 the Valley and economics of -- we had talked about  
 14 cost of this railroad and development. And B&B  
 15 Farms has actually contributed millions of dollars  
 16 into the economics of the Borough and the State over  
 17 the last 25 years, millions of dollars.  
 18 If we had a choice of routes, I'm sure our  
 19 preference would be the one that would run down Port  
 20 Mackenzie Road, even though we have a farm on that  
 21 route, too. And it would be impacted, but  
 22 noticeably less of an impact than on the proposed  
 23 route to the west, which runs right through the  
 24 farm.  
 25 And I just want to say that B&B Farms is one of

Page 3

1 Is this area, the refuge, involved in the flight  
 2 paths of migratory birds? Also farming can involve  
 3 no-till farming, which involves leaving residue in  
 4 the fields for migratory critters. This no-till  
 5 farming can also result in helping to replace the  
 6 loss of topsoil and erosion.  
 7 Okay.  
 8 MR. RUEBLING: Ed Ruebling, R-U-E-B-L-I-N-G. We  
 9 have a place on Red Shirt Lake. And we're concerned  
 10 about the noise around Red Shirt Lake. And it's  
 11 currently no road access.  
 12 And I guess the other concern is maybe  
 13 additional four-wheeler traffic coming down the  
 14 right-of-way into the lake area, possible vandalism  
 15 and stuff. I just prefer that it be kept, you know,  
 16 remote and isolated, the way it is right now. So I  
 17 guess we're worried about the noise at night from  
 18 train traffic. And that's about all I can think of  
 19 right now.  
 20 Okay.  
 21 MS. BASKIN: Lynne D. Baskin, B-A-S-K-I-N,  
 22 representing B&B Farms, owner is Merlene Baskin, my  
 23 mother-in-law, and her son, Lance Baskin, I'm  
 24 married to. He's my husband.  
 25 And this concern the proposed -- one proposed

Page 5

1 the original farms out there. We cleared the lands  
 2 in 1982 and we have been out there working that for  
 3 25 years. So, "right to farm." How's that?  
 4  
 5  
 6  
 7  
 8  
 9  
 10  
 11  
 12  
 13  
 14  
 15  
 16  
 17  
 18  
 19  
 20  
 21  
 22  
 23  
 24  
 25

2 (Pages 2 to 5)

REPORTER'S CERTIFICATE

1  
2  
3  
4  
5  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25

I, Britney E. Chonka, Court Reporter, hereby certify:

That I am a Court Reporter for Southcentral Court Reporters and Notary Public in and for the State of Alaska at large. I certify Hereby that the forgoing transcript is a true and correct transcript of said proceedings taken before me at the time and place stated in the caption therein.

I further certify that I am not of counsel to either of the parties hereto or otherwise interested in said cause.

In witness whereof, I hereunto set my hand and affix my official seal this 15th day of October, 2007.

BRITNEY E. CHONKA, REPORTER  
Notary Public - State of Alaska



*(This page intentionally left blank.)*

1  
2  
3  
4  
5  
6  
7  
8  
9  
10  
11  
12  
13  
14  
15  
16  
17  
18  
19  
20  
21  
22  
23  
24  
25

PORT MACKENZIE RAIL EXTENSION PROJECT  
PUBLIC COMMENTS

October 5, 2007  
Commencing at 6:00 p.m.

Taken at  
  
Houston Middle School  
Houston, AK

Reported by:  
Rosie S. Scott, CSR

PUBLIC COMMENTS

1 MR. ROBINSON: David Robinson. Me and my wife  
 2 Corinne own the property bordering the McCain Farm  
 3 Willow. And I can appreciate the route, but if you're  
 4 going to have to make somebody -- you know, if you're  
 5 going to put it in, please credence to dividing the  
 6 burden rather than shove it all off on one person, maybe  
 7 try to come down the line where it impacts us both a  
 8 minimal amount, rather than one person the whole amount.  
 9 And be fair with the amount that you pay for.

10 This property means a lot to us. It's our  
 11 retirement. I've lived here my whole life. My folks  
 12 homesteaded up here in 1948. And my aunt, her ashes are  
 13 spread on this property. And we plan to when we retire  
 14 to move on this property and have your farm. And we've  
 15 expanded quit a bit of money out of our pocket to  
 16 clearing and planting, and keeping it clean.

17 MEA strong armed us on it. And my wife while I  
 18 was out of town in order to gain access to bring power  
 19 across there and didn't live up to what they promised.  
 20 And I would like to have some sort of -- I'd like to be  
 21 treated fairly if we decide to go through this and be  
 22 kept in the loop of what the plans are if I'm going to be  
 23 impacted by it.

24 I think that's only fair. And I do appreciate

1 resorts, rafting companies, fishing guides, and then  
 2 again, of course, it's got across the Parks Highway this  
 3 Corridor 3 and then connect with the railroad. And to me  
 4 to have this even considered is a travesty as a route it  
 5 will bisect and dissect the community of Willow so that  
 6 it will eventually turn out like Wasilla, which is just  
 7 an unplanned -- the real feeling of Willow to me is it's  
 8 a rural recreational wilderness community that values  
 9 small family owned businesses.

10 I just attended the Alaska Travel Industry  
 11 Association convention in Juneau this week. Of course,  
 12 they have thousands of members. 58 percent of the  
 13 tourists businesses in ATI, Alaska Travel Industry have  
 14 five employees or less. That's the definition of Willow;  
 15 our bed and breakfast, or fishing, our guiding, our  
 16 rafting. And this rail corridor will mess that up. And  
 17 of course the trails, the mushing, the Iditarod re-start,  
 18 of course, has made all -- the last 8 of the 10 Iditarod  
 19 Races have been here in Willow. And, of course, we'd  
 20 like to see that continue. And the whole concept of the  
 21 Iditarod Race is wilderness.

22 And to have a commercial rail corridor come  
 23 through that section of Willow is just flat wrong. So  
 24 I'm opposed to the Corridor 3. I think the railroad can  
 25 meet its goals of having a commercial spur that's far

1 growth and progress in my Valley, however I want it done  
 2 equitably and fairly. That's all I have to say.

3 My address is 5061 West Hollow Nook Drive,  
 4 Wasilla, 99654, 907-373-1112 and then our e-mail address  
 5 is KBAB@MTAonline.net.

6 MR. HALTER: I'm Glen Halter. I'm a long-time  
 7 Willow resident. I live at Mile 64 and a half of the  
 8 Parks Highway. I own and operate a tourist business  
 9 there. I've been -- my wife and I have run 26 Iditarod  
 10 Races, Yukon Quest Races. We value the community of  
 11 Willow. I'm on the Borough Planning Commission. And I  
 12 am 100 percent, if not 200 percent opposed to what we  
 13 call Corridor 3, which would come up west of Red Shirt  
 14 Lake. First of all, you've got to cross Little Su River,  
 15 which is a legislative designated river -- park type  
 16 river. You would then have to come up through Morain  
 17 Ridge, which was a legally designated a residential area  
 18 in the Fish Creek Management Plan, and then you would  
 19 come west Red Shirt up through the most swamp land along  
 20 this route and cross through what I think is disgraceful  
 21 of any planner from the borough or the railroad would  
 22 plan a commercial rail corridor through Willow straight  
 23 state park Willow Creek, which is our pride and joy up  
 24 here at Willow.

25 It supports family-owned businesses Willow

1 shorter, connects to the Port as they want without  
 2 damaging the environment like would if they came up  
 3 through Corridor 3.

4 And to me, not to listen to the community of  
 5 Willow -- the original Tryck, Nyman, Hayes' report that  
 6 came out and actually said that this Willow corridor was  
 7 preferred was a travesty. There was never one meeting  
 8 held in Willow. There was never even a notice.

9 The Alaska Railroad wrote a letter to Tryck,  
 10 Nyman, Hayes and didn't even bother copying the community  
 11 of Willow. And I am just 100 percent opposed to Corridor  
 12 3. I think it's going to change the community of Willow  
 13 into a -- will change the community of Willow forever.  
 14 It's something we can never get back.

15 The renewable resource that is the best  
 16 interest for the community of Willow is recreation,  
 17 tourism, that's our second leading industry, and that  
 18 stacks up against a non renewable resource which is  
 19 hauling coal. And I guess it really irks me to death to  
 20 have the Alaska Railroad Corporation -- I was just down  
 21 in Juneau at the Alaska Travel Industry. They had seven  
 22 or eight people down there. Of course, they're all  
 23 talking about tourism, how to maintain things in the  
 24 green state, how to enhance tourism, and how to keep  
 25 things wow, how to keep the atmosphere that visitors and

Page 6

1 guests that come to Alaska want to see. And then, of  
 2 course, at the same time they're proposing this  
 3 commercial rail corridor to haul coal. And I guess that  
 4 really bothers me that all of a sudden you can switch  
 5 horses in the middle of the stream so quickly.  
 6 And so I would hope that somebody in the  
 7 railroad comes to their senses and really takes a look at  
 8 Corridor 3, and see what it's going to do to this  
 9 community.  
 10 And I guess it bothers me a lot too, that the  
 11 railroad is how they align themselves with Holland,  
 12 Princess, Celebrity Cruises, all these big cruises they  
 13 never stop in Willow. And all of a sudden they want to  
 14 use Willow and destroy the fundamentals that support us  
 15 smaller tour operators with this corridor. And that's  
 16 exactly what it's going to do. It's going to bisect and  
 17 dissect Willow, and it's going to change us forever. And  
 18 it's going to affect a renewable resource.  
 19 So that's my comments, and I would like to have  
 20 a chance to comment further, too.  
 21 MR. STRONG: My name is Greg Strong. I live at  
 22 3559 South Horseshoe Lake Road. My concern is that the  
 23 structure that would support the railroad basically, the  
 24 earth and the dam is in the middle of the watershed from  
 25 Beaver Lake, Horseshoe Lake and West Horseshoe Lake and

Page 7

1 Coal Lake, Pear Lake and Blanket Lake. Those lakes all  
 2 flow to the west. They flow from east to west to the  
 3 Little Sue for natural drainage.  
 4 So the mountains of -- the Talkeetna Mountains  
 5 melt and that water flows into Pear Lake and Blanket  
 6 Lake. From there they go into Horseshoe Lake, Hourglass  
 7 Lake, West Lake, Coal Lake. From there the water runs  
 8 out into the Little Su. The earth and dam created by the  
 9 railroad for the railroad will, in fact, restrict  
 10 virtually entirely the movement of water into the Little  
 11 Sue from the Talkeetna Mountains through the lakes that I  
 12 just mentioned. And that would cause considerable  
 13 flooding to the properties that are to the east of the  
 14 earth and dam.  
 15 Last year we had between August 1st and  
 16 September 1st rain every day. And it caused the water  
 17 level at Horseshoe Lake to go up 18.75 inches. That  
 18 height was caused in part by merely a beaver dam between  
 19 Hourglass Lake and the Little Susina River. And if that  
 20 little beaver can cause that kind of consequence, you can  
 21 imagine what a structure as long and tall and thick as  
 22 the rail line would cause, and the flooding and loss of  
 23 the environment that would be the result of that  
 24 construction. And that concludes my comment. Thank you.  
 25 MR. THISTLE: I'm Richard Thistle. I've lived

Page 8

1 in Alaska my whole life. And I use the area west of Big  
 2 Lake often for hunting and recreation in the winter. I  
 3 think the railroad should stay as far to the west as  
 4 possible to minimize impact with existing people and  
 5 land, and come into the Willow area where the existing  
 6 railroad and the airport already is to minimize impact  
 7 for the whole area as a whole. Thank you.  
 8 MR. NORTHY: My name is Ben Northy. And I'm a  
 9 property in the area. And I would like to voice  
 10 opposition to the Houston connector routes, either one of  
 11 those. It doesn't seem feasible to me to build it from  
 12 an economic standpoint due to the wet ground in the area,  
 13 especially the area right near the VOR site. It's an  
 14 extremely wet and flooding area year round.  
 15 I'm also concerned about what it would do to  
 16 the traffic -- heavily used traffic of snowmobiles in the  
 17 winter. And it will cause many unsafe conditions.  
 18 You're not going to limit the number of riders in that  
 19 area from crossing the tracks and it's asking for  
 20 trouble.  
 21 I think also the railroad has probably learned  
 22 from what it's done around the Fort Wainwright area in  
 23 Fairbanks that putting a railroad through a populated  
 24 area is going to cost a lot of money later to some day  
 25 move it, which you're faced with up there now. And it

Page 9

1 doesn't rail make any sense at all to put a rail right  
 2 through one of the fastest growing areas in the state of  
 3 Alaska. So either one of the routes, Big Lake or the  
 4 Houston connection doesn't make any sense. The preferred  
 5 route to me would be the 2003 recommended route to the  
 6 west.  
 7 So once again I don't believe it would be smart  
 8 to build a railroad through populated areas and cut off  
 9 one of the fastest growing, not only population areas,  
 10 but winter recreation areas year round. It's the play  
 11 ground of Alaska, if you will, and put the rail corridor  
 12 through it regardless of the impact on property values  
 13 and quiet enjoyment of the same out there. Thanks for  
 14 the consideration.  
 15 MS. GAMEL: My name is Beth Gamel. One of my  
 16 comments is concerning tourism. And I'd like to know how  
 17 they would like to use the railroad for tourism? I know  
 18 that it's going to be shortest route to Denali. A lot of  
 19 tourists would prefer to travel that way other than the  
 20 longer route, which they're presently using.  
 21 I think that a lot of tourists who have been  
 22 here before would like to see that area as an alternative  
 23 to things they've seen before. And I think of lot of  
 24 Alaskans might like to use it in a recreational way. And  
 25 so that's one of my questions.

Page 10

1 Also, I'm very concerned about the route that  
 2 goes through the wetland, which is south of Nancy Lake  
 3 adjacent to the Parks Highway. There's a lot of peat in  
 4 there. It's a bog. It's a nesting area for swans,  
 5 greater yellow legs and various ducks. And it is the  
 6 passage way for salmon to get to their spawning area in  
 7 the lake. There's also trout in there.  
 8 The water level fluctuates there at different  
 9 times during the year. I believe that's due to the  
 10 runoff of Hatcher Pass, so when they're studying that  
 11 route they might want to go at different times of the  
 12 year to see what the bog looks like. It's very wet.  
 13 Otherwise, I think it's positive that the railroad will  
 14 be going from the Port north.  
 15 MR. HALTER: This is Glen Halter again of  
 16 Willow, Alaska. I just listened to the presentation here  
 17 in Houston, Alaska about the various railroad corridor  
 18 routes. And I just wanted to add some more comments.  
 19 First of all, the people at Houston, the city  
 20 of Houston want the railroad corridor to come in south  
 21 I Houston route. And I guess the north Houston route would  
 22 be a second choice. The official community of Willow has  
 23 written an opposition to Corridor 3.  
 24 And it seems to me that the railroad, to meet  
 25 all its needs with the Houston corridor. And the cost if

Page 11

1 you're going through the wetland is shorter, the cost has  
 2 got to be shorter. And, of course, you don't really have  
 3 the impact on the community of Willow that no doubt that  
 4 the Corridor 3 would have.  
 5 Also I wanted to comment too that lately the  
 6 Willow Chamber of Commerce -- I'm a member of the Willow  
 7 Chamber of Commerce. I have been for several years. The  
 8 Willow Chamber of Commerce which is a small group has come  
 9 out and said they want Corridor 3. That is just exactly  
 10 the opposite of the great majority of the people in the  
 11 community of Willow. And I think that should be  
 12 minimized because the first time they voted on that there  
 13 was never public notice. There was not even a notice  
 14 that they were going to take up a railroad corridor.  
 15 Three or four people brought it up and did it in a bar  
 16 setting without notice to anybody.  
 17 And I'm a member of that organization, and I  
 18 didn't receive notice. And I just think it's very  
 19 discouraging when weight is given to a Chamber of  
 20 Commerce when it's not really the real feelings of the  
 21 community, and not the feelings of the business  
 22 community.  
 23 The business community, the true business  
 24 community of Willow would be Willow Resort, Pioneer  
 25 Lodge, the rafting, the guiding organizations, the people

Page 12

1 like me that run the family-owned businesses, the  
 2 tourism -- the backbone of tourism alone. We are opposed  
 3 to Corridor 3 because of the impact it's going to have.  
 4 And I hope people recognize that. And to have the Willow  
 5 Chamber Commerce on the radio and interviewing on TV when  
 6 it doesn't have community support I think is a travesty.  
 7 I wanted to also say something about  
 8 mitigation. I was a public defender attorney for many,  
 9 many years. When the prosecutor came up to me and told  
 10 me, hey, you guys plead guilty and I'll talk to the judge  
 11 about mitigation, you knew you were doomed.  
 12 And that's exactly what's going on here. The  
 13 railroad throws out these words, "Don't worry Willow,  
 14 we'll mitigate this." Well that's a trap.  
 15 The impact that this railroad corridor would  
 16 have on the community of Willow would be devastating.  
 17 You're bisecting this community so you've got two  
 18 railroads coming together at basically Mile 73, you're  
 19 going through our state park, you're crossing our salmon  
 20 streams and things like that. So you cannot mitigate  
 21 that in the long run. And I think that's just kind of a  
 22 superficial statement. And so those are my comments. I  
 23 hope they're added on. Thank you.  
 24 MR. GAMEL: My name is William Gambel. I go by  
 25 Bill. That's G-A-M-E-L. Okay. And I would like to

Page 13

1 comment that I think that the Willow junction or the  
 2 Willow trail looks the best for the railroad. But I  
 3 question the ability to cross -- have right-of-ways  
 4 crossing this area for future expansion.  
 5 20 years, 30 years from now we will probably be  
 6 expanding and have to expand with that area as the  
 7 environmental problems of the world seem to be growing,  
 8 we will have water, we will have cheap electricity and we  
 9 will have oil. And we will be the closest state to the  
 10 Orient for world trade. Anchorage and the Valley could  
 11 be a huge metropolitan area.  
 12 The railroad and their present attitude about  
 13 access and crossing their property is going to be done on  
 14 their terms, where they want it. And they it should be  
 15 well defined in advance where we have crossings. And we  
 16 should have lots of crossings across that area that would  
 17 be guaranteed to us at a later date.  
 18 I'm dealing with the railroad right now. And  
 19 they are very possessive. Rightly so, if I was in their  
 20 shoes I would be an SOB also. But if we were a citizen  
 21 and were a city or community that needs to be developed  
 22 in the future, we need to have some really firm  
 23 understandings about our access across those areas.  
 24 MR. SWAN: My name is Glen Swan. I'm on the  
 25 board of Alaska State Association. And we are in the

1 process right now of making a trail that connects Big  
 2 Lake to the Petersville riding area. And we're concerned  
 3 that this railroad proposal might block access to the  
 4 north if it's not incorporated in the plan to give us  
 5 proper crossing. That's about it.  
 6 As far as my personal comment would be that I  
 7 would prefer the MacKenzie west connecting to Conn 1 and  
 8 going out to Willow. To me that would make more sense  
 9 because it stays on the high dry ground mostly. That's  
 10 it. If we had a vote, that's my vote. That's not the  
 11 Association's feeling though, that's mine.  
 12 MS. OLSON: My name is Dana Olson, O-L-S-O-N.  
 13 I live in Knik, Alaska. And I'm here opposing this  
 14 process on guides of several grounds. One, an EIS is not  
 15 normally required when there's a supreme court decision.  
 16 And I claim that there is a supreme court decision.  
 17 Alaska Survival versus DNR is a supreme court opinion  
 18 that was codified in law in 1987 by Senate bill 196.1987,  
 19 which is the environmental impact standard review  
 20 ability.  
 21 The second grounds for contention is that  
 22 former DOT Commissioner Perkins had privileged  
 23 information concerning my historical right-of-way when he  
 24 worked for the Department of Transportation. And he is  
 25 now attempting to reinvent the wheel, so to speak, by

1 recording district. And I got this information out of  
 2 the BLM's office here in Anchorage.  
 3 The recordings there are during the time frame  
 4 that the legislature codified the EIS standard which  
 5 includes subsistence. And I am at odds with these  
 6 subsistence standards because I've had two children  
 7 killed at different times under the jurisdiction of the  
 8 Matanuska-Susitna Borough, where the courts have gone in  
 9 and interpreted what the legal standard is. And in 1986  
 10 the federal judge and the Alaska Supreme Court determined  
 11 that the issue was outside of the zone of the roadway  
 12 itself.  
 13 So I feel compelled not to want to impair  
 14 anything, but I also feel necessary to bring this to your  
 15 attention, that unless you can provide some reasoning why  
 16 a partnership should override legislative law I would ask  
 17 that it cease and the normal process begin.  
 18 The Alaska Railroad was allowed to leave its  
 19 jet fuel spill potentially interfering with my aquifer  
 20 and the chase area. And this contradicts with the EIS  
 21 standard that was codified by the Alaska Legislature in  
 22 1987.  
 23 I also raise the issue that the borough is not  
 24 a first class borough. And that its partnership is  
 25 fraudulently conveyed as being -- as having greater

1 creating a process potentially reinterpreting what his  
 2 position was when he was within DOT.  
 3 I claim that this is conflict of interest. I  
 4 have a portion water rights granted by BMR and they are  
 5 for horses, five of them, and another livestock. And  
 6 these apportions of water rights ran to other legislative  
 7 grants that I got at the time that they were issued  
 8 including the Chase III Agriculture Homestead Lottery.  
 9 The Amber Lake North subdivision area which is a  
 10 contested land issue there. And they tie back to my Knik  
 11 area, my Knik property. And that this constitutes a  
 12 region and not simply an issue of routes going through  
 13 the Mat-Su Borough, that these are protected under the  
 14 1866 Mining Act Amendment, and that the borough  
 15 officials, the state officials are all aware because of  
 16 the correspondence, the documentation, the meetings that  
 17 have been ongoing for quite a few years.  
 18 And this process is attempting to use a  
 19 division of the state to interpret law while under  
 20 federal delegation is an unlawful profess.  
 21 I also have a pending lawsuit against Alaska  
 22 Railroad for fraud and misrepresentation. One of the  
 23 things that is recorded on my land title in Knik is a  
 24 reservation for a federal railroad. And in that federal  
 25 railroad reservation it's recorded in the Talkeetna

1 capacity than it has because a federal judge in 97-219CV,  
 2 ruled that the borough was a division of the state. So  
 3 if the borough is a division of the state, it cannot  
 4 simply reinvent a federal legal process in order to  
 5 overturn issues that were prior decided and how they  
 6 would be resolved.  
 7 That's a violation of the full faith and credit  
 8 clause and Article 3, Section 2. Full case and  
 9 controversy, the borough is trying to piecemeal and take  
 10 away from the controversy and attempt to partner with  
 11 other people for the purposes of apparently an economic  
 12 goal.  
 13 The public process that was embodied was not to  
 14 comment on particular routes, but it was to recognize the  
 15 uses in the process of vesting and the uses that had  
 16 vested under state law. To simply deny this continuance  
 17 if a rationalization for government to rationalize the  
 18 right to own property which is prohibited by the  
 19 Fourteenth Amendment. And there is no capacity for  
 20 partnerships to rationalize through partnerships -- to  
 21 violate the substantial notice requirement that is  
 22 embodied in Moore versus State, incorporated in Alaska  
 23 Survival versus DNR1986. And codified law Senate Bill  
 24 196.1987.  
 25 The requirement that I come and tell you

Page 18

1 without being paid for my time as a consultant or someone  
 2 who is giving information is that once something is of  
 3 continuance and it's vested the right becomes the state  
 4 or the borough or whoever to come to me and not me to  
 5 come to them.  
 6 The public process of not being able to answer  
 7 questions is a violation of the first Amendment, right of  
 8 debate, by asking the simple question of what authority  
 9 they were operating under and debating that would have  
 10 been productive because now people have the belief that  
 11 this process is lawful process.  
 12 One of the questions that I wanted to answer  
 13 was what was the readdress ability if I didn't agree with  
 14 the assessments, agreements or whatnot in state law  
 15 because we don't normally sue federal officials in the  
 16 state, nor do we sue state officials in federal law.  
 17 So obviously the readdress ability of not  
 18 agreeing to some of these historic claims cannot be  
 19 obscured. It must be well defined in order for the  
 20 process to continue.  
 21 The reason I really solely object to the  
 22 borough attempting to be the sponsor of this or  
 23 participant is that the borough doesn't address  
 24 low-income housing, evacuation due to spill or fire or  
 25 whatnot. There is no state law specifically authorizing

Page 19

1 the right to sue the borough for any of these things. In  
 2 fact, for fire, for example, the duty falls upon the  
 3 state. And so in an EIS, having the borough acting as  
 4 the liaison is unlawful.  
 5 While I was in Montana this summer there was  
 6 the Sealy Lake fire where Alaska firefighters came down  
 7 to fight this horrific fire interspersed with beautiful  
 8 houses along this lake front. The newspaper in Great  
 9 Falls, Montana articulated the comments concerning how  
 10 the fire was being fought. And they said in Montana that  
 11 the local firefighters were considered more important  
 12 than the range land firefighters because of their  
 13 experience in fighting house fires.  
 14 This is directly contradictory to the Miller's  
 15 Reach fire lawsuit, which I have opted out of. And my  
 16 home was damaged by water in the -- how the evacuation  
 17 took place. So I'm very, shall I say, insistent that I  
 18 know what the process is for evacuation.  
 19 Back in 1996 the state even refused to let me  
 20 stay at a state campground even though I was evacuated  
 21 and had no place to go. There's no housing, rental  
 22 housing units, there's no shelter. There's nothing. So  
 23 the borough can't control what is carried on the  
 24 railroad. And to have them be the facilitator of this is  
 25 crazy. A person has the right to know what the potential

Page 20

1 impact to them are.  
 2 Moore versus State says notice is not  
 3 regulatory. They have a right to make informed decisions  
 4 and have a political process of how they will be impaired  
 5 and where they will go and who will pay for it. As far  
 6 as I understand right now, the issue of who would be  
 7 responsible has not even been decided. It is not  
 8 clarified by state law for any railroad spills,  
 9 catastrophes, train derailment, or even taking hazardous  
 10 material down to the Port for processing.  
 11 In the early '90s an iron ore plant proposed to  
 12 go down to point MacKenzie. It's called Medricks. They  
 13 wanted to ship all the way from South America and take it  
 14 to point MacKenzie for processing and leaving the  
 15 tailings in our state. There was opposition to this  
 16 project.  
 17 And there was a newspaper about my tree farm.  
 18 The associated press did a story that a child in the  
 19 Mat-Su School district read -- wrote a winning essay on  
 20 my apple tree farm and briefed it at the assembly meeting  
 21 of which the Medricks people were invited.  
 22 I have a long history of opposing things mainly  
 23 because they impact my agriculture use and my traditional  
 24 way of living. I am adamant that regulatory law does not  
 25 consider agriculture, and that this process to

Page 21

1 potentially impact me is opposed.  
 2 I can't imagine anyone who's lost two children  
 3 to the Mat-Su Borough would want to have this sort of  
 4 thing ongoing without a clear expectation of what would  
 5 be expected.  
 6 I am insisting that before any of those routes  
 7 are even considered that if I have to walk 8 miles to go  
 8 vote in Knik, Alaska in the borough election addressing a  
 9 public referendum that didn't even go before the Attorney  
 10 General which potentially is affecting me water rights,  
 11 which are state rights and not the borough. I would have  
 12 to say that I need -- I would to have to raise the issue  
 13 unless there are a facilities where people can go in  
 14 their own community that this route should be denied.  
 15 Thank you.  
 16 MR. DILLEY: My name is Rick Dilley,  
 17 D-I-L-L-E-Y. And my comment is I like the Houston south  
 18 the best. I feel that one would work the best for our  
 19 town of Houston anyway. And I would also like them to  
 20 put in a trestle that we could dump coal cars on because  
 21 I'm the coal man for Valley and Anchorage. And right now  
 22 the cost of transpiration is prohibitive for a lot of  
 23 customers.  
 24 So if we would have coal siting to dump the  
 25 railroad cars it would save a whole lot of people a whole

1 lot of money. I can foresee people saving at least 40  
 2 percent on their heating bills. And nowadays with fuel we  
 3 need to do that. And basically that's my comment, make  
 4 me a trestle and I will save them some money.  
 5 (Proceedings concluded at 8:00 p.m.)  
 6  
 7  
 8  
 9  
 10  
 11  
 12  
 13  
 14  
 15  
 16  
 17  
 18  
 19  
 20  
 21  
 22  
 23  
 24  
 25

1 **REPORTER'S CERTIFICATE**  
 2 I, ROSIE S. SCOTT, CSR, hereby certify:  
 3 That I am a Certified Shorthand Reporter  
 4 for Southcentral Court Reporting and Notary Public for  
 5 the State of Alaska; that the foregoing proceedings were  
 6 taken by me in computerized machine shorthand and  
 7 thereafter transcribed by me; that the public comments  
 8 constitutes a full, true and correct record of said  
 9 proceedings taken on the date and time indicated therein.  
 10 Further, that I am a disinterested person to  
 11 said action.  
 12 IN WITNESS WHEREOF, I have hereunto  
 13 subscribed my hand and affixed my official seal this  
 14 \_\_\_\_ day of \_\_\_\_\_, 2007.  
 15  
 16  
 17  
 18  
 19 \_\_\_\_\_  
 20 ROSIE S. SCOTT  
 21 Certified Shorthand Reporter  
 22 My Commission Expires  
 23 8/16/08  
 24  
 25



*(This page intentionally left blank.)*



## MATANUSKA-SUSITNA BOROUGH

### Office of Public Affairs

350 East Dahlia Avenue, Palmer, AK 99654  
 (907) 745-9577 • [psullivan@matsugov.us](mailto:psullivan@matsugov.us)



Port MacKenzie Rail project

Public Comments from Nov. 20, 2007 Assembly meeting  
 Transcribed by Kathy Rocci, Mat-Su Borough

1. Linda Oxley – I am Chair of WACO and I am speaking to the rail spur. It has come to my attention that some of you may erroneously believe that Willow does want the rail spur to come through Willow. We do not. I thought I would share a few more words on that. We believe that this is not just a Mat-Su issue but a regional issue. Ms. Woods was saying that people need a place to recreate; we already have a place to recreate. I was just wondering if you all might think through if you yourselves have not come to Willow to play, fish, hunt, canoe or ski, snowmobile or any of the other hundreds of outdoor activities that Alaska allows. As Mr. Kluberton stated, 60% of the state's population lives close to us within a day's drive and we think it is really important to protect that. We do strongly believe in the economic development of this Borough. There is no question that the Borough can't survive with 80% of its cash coming from outside the Borough, as many of us compute to get most of our money. On the other hand, the same value of recreation coming to a place of peace and quiet and helps from the stress of life must be entertained and supported; and we do believe that those two elements can be side by side. I want to point out that Willow is going to bear a burden no matter what. Where ever the rail line joins the main line, we will get extra traffic, that is a given. I would also like to say that the Houston North route actually ends in Willow at about mile 63.7 on the Parks, which is two miles into the Willow area. A couple of dedicated community members have put together a packet of materials, some of which was previously submitted to Ms. Sullivan and Ms. Wheeler, and there is an addendum. The second page lists all the groups that have provided documents in opposition to Willow being the site for the rail spur. (She lists all the groups) The City of Houston has not come to an official position, but in private conversations with some of their leadership groups, they would like to have it there as long as the siding comes there with it. They will be getting with you folks later. In the attachment, we have documents from the Alaska Travel Industries, etc. (she lists them). **Mayor Menard** asked if there were any groups in Willow in support of the rail spur. Linda answered, "Yes, the Chamber of Commerce and Willow Wildlife and Historical Foundation support the Willow route. **Cindy Bettine** asked about comments from the Nancy Lake Association? Linda answered she had not heard officially from the Nancy Lake home owners so could not answer for them. **Michelle Church** asked if different user groups would be supportive if the right-of-way along the rail would be available for motorized on one side and Non-motorized on the other side for sled dogs, skiing, snowmobiles, etc. Linda answered that if the rail came through Willow we would work with whatever groups to come up with the best solutions. At this time, the corridor cuts through our big swamp, which is the circle loop to major connective trail systems,

and they also connect through Big Lake north and further north and through the whole region. There seems to be serious flaws in the way the engineers would have to design such a creature, like a high speed freight train, that would interrupt the water streams, and the way the animals come and cross over; so we are not only concerned about the trails, but the rest of the story in our community. We can't foresee any possible mitigation which is technologically available, or that would be so cost prohibitive, as you saw in the matrix. The only thing I can tell for sure is the dollar bill; and Willow would cost between a fifty and a hundred million dollars more to build than any of the other routes. Linda was asked which route would be preferred and she answered the south route would be preferred over the north route although the homeowners were surprised to see that their road was the exit road coming off the swamp, but as a community, it would harm us less to have it come in willow south versus willow north.

2. Ed McCain – live in Willow. Would like to address the rail road and I am the farmer that three or four years ago you moved the railroad off my property and I appreciate that. In looking at this there is nothing in this railroad for Willow and anywhere that the railroad goes, it is dividing the area in half in the state right now. There is real access problems across the tracks. One of the things you people want to keep in mind when you are looking at this railroad is, What are you going to do with the section line easement every mile across these tracks? It has been near impossible to get a section line easement vacated without providing equal or better access as your criteria. What do you intend to do about 20, 30, 40 miles of section easement across these railroads? I would like to see that the railroad is going to be the big money maker that it is. The railroad ought to be required to build access roads on both sides and put in their grade separations crossings at ½ million a pop, so the private developer or road service area doesn't have to pay for these services.
3. Greg Strong – I'm here to speak about the railroad. Last night there was a meeting put on by Dr. Haeussler of the USGS. He indicated that the south Houston route was on a fault line called the Castle Mountain fault. That fault is expected to fail in the next 40-50 years at a 7.2 magnitude. I looked at the matrix and the matrix looks at stream issues and soil issues but it doesn't look at subsurface estate and along that route is considered fragile. The issue that I have is that the Assembly is very competent and has dealt with issues such as the prison, methane gas, etc. and there is really no public participation as you had with the prison and other issues. I have a rhetorical question, "Why, if you have been so effective in dealing with these issues in the past, are we now allowing people in Washington DC, six thousand miles away, who are nameless and faceless, to all of us, decide incredibly important issues for this Borough. I think that the public somewhere along the line is expecting to come before you as they have in the past to make their feelings known; and let you wrestle with them and come up with a recommendation to the STB that allows the board to realize that you as the local elected officials are not allowing this to become a Forrest Gump situation. (we don't want this to be a box of chocolates where we never know what we are going to get). We would like to have you folks look at and address this comprehensively and make some recommendations, that all of us might not be happy with, but at least we know there's been community input and participation.

-end-



## MATANUSKA-SUSITNA BOROUGH

### Office of Public Affairs

350 East Dahlia Avenue, Palmer, AK 99654  
 (907) 745-9577 • [psullivan@matsugov.us](mailto:psullivan@matsugov.us)



Port MacKenzie Rail project

Public Comments from Tuesday, Dec 11, 2007 Assembly meeting  
 Transcribed by Kathy Rocci, Mat-Su Borough

1. Greg Strong - Wisdom of bringing a route (south Houston route) that is on an active earthquake fault. Might be acceptable for the Railroad to live within a mile of the fault with a \$330 million improvement, but I suspect the guarantor and issuer of that \$330 million bond won't be comfortable with it.  
 Houston North route as it bisects the watershed of key residential and recreational lakes and bisects Castle Mt. fault. On related transportation issue, the Parks Hwy (bet Big Lake and Wasilla) is considered to be the most dangerous highway in the State of AK. What we are looking at is possibly bringing additional traffic down that hwy corridor to go to the port and will add more congestion to an already documented dangerous stretch of roadway. The only way to avoid this is with a transportation corridor from Willow to Pt. MacKenzie. This would include a two-lane blacktop roadway along side the rail line. The corridor would open for the Borough tens of thousands of acres of Borough and State lands for development and enrich the Borough's real property tax base for the next 100 years. The other corridors will do the opposite. As a rail line, in and of its self, it will limit development and growth because of the high cost of railroad crossings. The road next to the other corridors is not likely because it will not expedite truck traffic to the port. Please select Willow route with a recommended road way along rail.
2. Kevin Berg – Like the idea of port and expansion of rail road. Alaska Railroad Corp letter in 2002 from Bruce Carr supporting corridor 3, Willow route because of geography and geology and had added benefit of connecting Knik Arm crossing as a transportation clink to Anchorage and Fairbanks. I support it because of wide corridor and preserving space for railroad with sidings, utilities, bike paths, 4 land hwy. It is thinking into the future
3. Ted Berry– Encourage us to have great vision for design of rail so there will be sighting locations that would have potential for development of properties private and Borough owned, and future highway that would align with rail. Supports Willow mac west.
4. John Himmelright - Support Willow route. Building a retirement home on West Lake and proposed Houston South route would go right thru our driveway and we would lose access and we would have to build a road to get to our house, and noise factor since it is a 1000 ft. from lake.
5. Patrick Sherrock– has recreation property. Appreciative of public forum to speak. Not here to promote one route over another but there only appear to be 3 options under review. Matrix shows 8 routes which are determined by 4 linkage routes (Mac East, Mac

West, Houston North and Houston South), combined, these linkage routes cover more distance than the Willow route, and that distance does not include the more than 10 miles for the leg shown on the map as Con 1, Con 2 and Con 3. If there is an analysis of routes then the linkage routes, including the Con routes, should be reviewed and evaluated on their own merit and shown separately on the Matrix sheet. That would leave the 3 primary routes to be evaluated and also shown separately and that is not the case now. However on the current hand out Matrix sheet there is no definition for a plus, minus or zero for each criteria. The Matrix should be clear, meaningful and reflect as much detail that can be easily understood. Suspect we all agree rail way is a monumental project for Alaska and the valley and it a new rail line that can encourage development of the West, promote construction of the Knik Arm Bridge and promote development in the valley. In the short view, don't want to discourage current public expansion, and in the long view recognizes westerly movement across the Susitna River. There are 5 issues that should be included in our resolution. 1. Any rail line should have the least impact on resident and recreational users of the area and not deter development, 2. The rail line shall not degrade or stifle natural surface or subsurface distribution of water that sustains lakes, ponds and streams and wildlife breeding habitat, 3. Public safety be a primary concern to dangerous or illegal railroad crossings, 4. The STB consider the issues raised in June of 2003 MSB Rail Corridor Study, and 5. the STB maintain its own website for the project to assure accuracy, completeness and timeliness of information, and notices contained therein, and also show there is not an appearance of conflicts of interest. Last item he feels very frustrated on because of inconsistency and accuracy on website. Wants to make sure all the issues raised in the June, 2003 MSB Rail Corridor study are included and sent to STB

6. Keith Dobson – My concerns come down to two points: I have concerns over the use of the evaluation of the Matrix at all because at the end of the Matrix, it is all tabulated where it is +6, -2, a +1. It would lead possibly the STB to review the plus and minuses and taint their perception of which route is superior to another route. If you are not going to actually add a weighting criteria to these pluses and minuses, I wouldn't even tabulate that, I wouldn't total it up, in fact, I might even not suggest that you put a plus or minus there. I would simply put strengths and weaknesses or concerns. If you are not going to wait it, I wouldn't bother totaling it and tabulating it and then persuading one route over the other; and the other one, if you insist on weighting it or not weighting it, but insist on having an evaluation of the criteria of the Matrix, then I would suggest that the Matrix be expanded to include items such as a future road construction along the path way; but most importantly I would think that the one item that is missing is the alignment with a valley economic development plan. I haven't seen it; we've talked about individual plans, but I haven't seen a plan for the valley. And how will any of these routes impact that economic plan. Why go forward with a recommendation if we haven't determined the impact to the valley's economic plan; if there is no plan, then let's make a plan prior to making a selection.
7. Tina Dobson – I believe on the website, at one point, it stated that public comments were going to be listed on the website and I don't think they've been added and I think a lot of people would like to see what's been said at the meetings. I also wanted to know will all the routes considered have an EIS study or one route? I know it's very expensive to do studies on all the routes, is there going to be one proposed route that gets the study because all the routes that have been proposed, the EIS study that has been done on all them would determine whether that route is viable or not. I also read the 2003 study and it pointed out that private property owners will be more affected than the occasional recreational users; and as Greg Strong mentioned, the huge impact of additional truck

traffic from the Houston route or the Big Lake route on the highway, making the Willow route more acceptable. My question is, where are these products being transported from? They are coming from the interior past Willow; does it make sense to put the route as close to those locations as possible? Last, I want to bring up the letter that Kevin Berg brought up from Bruce Carr, that he's already stated that the Willow route was their choice, so I'm confused about why this whole process is being rehashed and why that route isn't just getting the EIS statement and consideration to the STB.

8. Cathi Kramer – I live on Horseshoe Lake and I am the Horseshoe Lake Fire wise coordinator. We have the only fire wise community in the whole Mat-Su Borough and are the first in the State of Alaska. Our community of Horseshoe Lake, the Beaver Lakes area and North shore and Big Lakes were devastated in the 2006 Miller's Reach fire. My family lost their home, as did many of my neighbors. We made the difficult decision to rebuild our home in that area and that looked like a battle zone. It's hard to go back and tell the kids we are going to rebuild here, but we did. We enjoy the quiet, we enjoy the trail access and quality of life that the community has put back together in that area. Areas around us have been less fortunate. They have not been resilient like the Horseshoe Lake area, and that is something I have been trying to work with the fire wise. The Houston route would destroy many of the things that brought us back to this area. After so many years of rebuilding and bringing back our property values, the choice of the railroad route would take away much of what we've worked for. I urge you to look at the maps of where the Miller's Reach fire burnt. The Houston route and the Big Lakes routes entail much of those areas. Those people have already been hit and they are struggling to get back on their feet. The scientific consideration such as the Castle Mt. fault and the water flow concerns should be enough on their own to rule out these routes as options. I urge you to reconsider the Willow route as the best option for this rail line.
9. Bill O'Hara – I am president of the Big Lakes Community Council. The council has always supported the rail line and the port and we always supported the Willow route; and we think that the reasons that have already been stated here and there is no reason to rehash here, but I think that the impact on the future of opening that western route is going to be tremendous to the valley; and when we send a letter to the STB with our recommendations and our support and our concerns, so we will continue to monitor it and do what we have to do.
10. Ed McCain - I see nothing in this rail road spur thru Willow, for Willow, except it divides the area in half. In my experience with the rail road over the last 30 years is that they have not been a good neighbor. They are God and you do things their way or else. Could something put this rail road spur out of business? Are we going to put the Seward coal loading facility out of business? Let's remember what happened with the big studies they did at the Delta barley project some 25 years ago. They always say this is going to be a great thing but then the state found out they couldn't control the world barley price so it was all a bust for nothing. What could put this project out of business? There was an article in the paper 8 to 10 months ago about the Hunts were going to try a conveyor belt at the Tionic Shore River cove ten miles to tide water. Is there going to be any economic competition there? Those people shipping at ten miles compared to us shipping at 220 miles. And I remember what this Borough did 15, 20 years ago with the boat wash at Willow Creek where they spent a lot of money putting in a boat launch and then the other landing in the area decided they didn't want any competition, took the Borough to the cleaners, and the Borough went out there and put some big rocks in front of it so nobody could use it. Could the Hunts decide they want no competition and shut this down? My

other concern is, they said a 200 foot right of way, but I think Mr. Perkins, before the Knik Arm Bridge Toll Authority, said eventually they were going to go for the whole 800 feet, and we need to know what you are talking about. Is it 800 feet or 200 feet, or are you going to eventually go for the 800 feet? And who is going to pay for all these crossings? You know that TNH plan done in 2003, there wasn't any crossings for 16 miles from the Deshka landing road to the South Big Lake Rd., and the rail road is just going to divide these people in half; and the private individuals or the road service areas are going to have to pay for these crossings? And they keep saying "at grade" crossings- my last experience with the rail roads were they weren't going to allow any "at grade" crossings, they were all going to be grade separation @ million and a half a pop. Who is going to pay for this? Or are we just going to divide the area in half?

11. Doug Smole- I support the rail road spur to the port. One of the things I would like you to look at is appendix M to the 2003 study and it is a letter from the strategic planner of the rail road. I had intended to give a longer presentation but most of the things have been covered. Within the letter I have highlighted 9 points where Bruce Carr, strategic planner for the railroad, indicates where the most westerly route is the preferred route in his opinion as of December, 2002. And that is the study that \$300,000 was spent on and I believe it was a study that was very well done for the type of study that it was. I also know we are going to have in depth studies and the environmental impact statement as a result of the \$10 million. So this in one of the things I wanted to say to you, but as the discussion went on today other concerns have been raised; and one of those is that the simplistic plus and minus matrix thing can be very misleading. The comments was made that you want to lead the STB and I believe the simplistic approach can leave them with inaccurate information because it is not waited. Let me give you an example: If all the criteria that you list have been given equal value, then answer for me, how many saved horse power hours equal impacted parcels of property, or why numbers, like parcel properties don't tell the whole picture; not all parcels are the same size or the same value, so how can you use wild numbers to lead the STB? The poor scale of the map that was indicated here concerns me because the public that has been going thru this "show and tell" process didn't really have a precise picture of the routes that related to their immediate areas. So I caution you to be very careful, please, in using this matrix as it now exists, and I ask you to look carefully at the results of that 2003 study. Thank you
  
12. Jim Huston - I am the Willow Chamber of Commerce President, and I would like it read into the record that I have joint resolutions from the Big Lake Chamber of Commerce and the Willow Chamber of Commerce, for distribution to the right sources that we are backing the Willow West line as the best line for least impact on all of our residents. I would like to read another letter here that we wrote as a chamber of commerce. On behalf of the Willow Chamber of Chambers and the silent majority of the Willow residents, I would like it known that we strongly are in favor of the Willow railroad corridor. The Willow corridor is an opportunity to plan and build for the future. Presently, Willow is right on track to become another Wasilla. Both communities settled around the railroad for convenience sake, only to found out thousands of cars, homes, people later, that though the love affair is not over, it is better to be a little further apart. With the probability of many new freight trains per day running by our homes and across our access roads, we have definite quality of life and safety concerns, the other corridor alignments do nothing to lessen these conflicts. We sincerely hope that the Pt. MacKenzie to Willow railroad route is the start of a responsible development, the teams where the new highway that will lessen the growing pressure on our present transportation system. Recreation trails are important to our locals, so much so that some

Willow-ites have gone so far as to back the Houston rail alignment to steer clear of the recreation trails. The business leaders of Willow, however, have confidence that the underpasses will be built along the best alignment possible, which we feel is the Pt. MacKenzie to Willow route. Willow never became the capital of Alaska, which may or may not be a good thing, but if we had been the capitol, I'm sure that our transportation systems and access roads would have received more attention. We view the possibility of a new rail and new roads through Willow as a favorable addition to our community. Let's plan them and build them.

13. Tony DeLucia -- We're going on assumptions. First of all I'm going to assume that Willow, Meadow Lakes and Houston are going to become good groom communities of Wasilla, there's going to be a tremendous amount of growth in that area. I'm going to assume that we are going to get a Knik Arm crossing. When you look at that, and start looking at this map here, you wonder, "Why do you want to put everything right down the Parks Hwy where all the congestion is?" Eventually, we are going to have to have another road coming through. That road needs to go along the rail corridor, especially the Willow corridor. This gives traffic an opportunity to flow to Fairbanks without going through stop light after stop light after stop light. In conclusion, I don't want to create another Wasilla in our area.
14. Jerry Howard -- passed
15. Larry Jacobson -- Location, location, location. I'm not here to speak about location. As the rail road knows, I've spoken to them before about the environmental concern that I'm concerned about, not the location. Federal regulations and the Alaska railroad rules require the trains to sound their horns when approaching a public crossing and to continue sounding that horn until the crossing is reached. In June of 2005, the FRA (Federal Railroad Admin) began enforcing this new rule for train engineers to sound their horn at least 15-20 seconds before crossing and to continue sounding the horn until crossing is reached. Given significant consequences for not complying, monetary fines and a suspension of train crews, train engineers have no choice but to blow their horn, as required, day or night. That is the environmental issue I look at. The least crossings that you have, the less noise that you have. Trains in motion that go up to a 150 miles an hour, that is the way that the FRA takes the Federal Railway Act, can not exceed 90 decibels. These horns have a 130 decibel rating, and at a 100 feet, exceed the federal standards of 96 decibels. That is the noise at the crossing. There is also stringent rules and regulations when it comes to locomotives, how much sound they can produce, 90 decibels max. Interstate rail roads, the noise control act also required the EPA to establish noise control standards for trains and rail way stations engaged in interstate commerce and is authorized by the FRA to enforce them. The level of noise is limited to 73 dbs, and the stationary operations (idle speed), is limited to 96 dbs at cruising speed. Locomotives built after 1979 are more stringent and limited noise and stationary operations at idle speeds 78 dbs and a cruising speed 90 dbs. Noise from railroad cars may not exceed 88 dbs at speeds of 45 mph or less; and must not surpass 93 dbs at speeds greater than 45 mph. Noise from car coupling activities at any time at railway stations is limited to 92 dbs. It is a lot of figures, a lot of facts and I'm going to give them to the court clerk to disseminate; however, I was an air traffic controller for 36 years with the FAA, and every day it was noise, noise, noise. And that is why I brought up this issue and not the place where the track goes. Thank you for your time.



16. Roger Purcell – with the City of Houston and the Chamber of Commerce. We've held 3 public hearings on these different routes. The Houston Chamber of Commerce resolution supports the south route. The City of Houston has dropped off a resolution this Thursday to also support the south route. We have concerns with the Big Lake route because it's going so close to the high school and junior high crossing both Kenlar and Hawk Lane. It crosses on two different ones. We had a lot of concern and impact on danger to the kids going to school on that route and so we are totally against the Big Lake route. We wanted to make sure the Borough knew that. Our comprehensive plan actually shows that we, ten years ago, put together a plan for an industrial and also the rail route in our plan. That is why it is one of the reasons we looked at the economic development of a train coming through Houston, and the Houston south route is the least amount of impact on any residential areas, including an area that is also an industrial area for our planning to put in place things like a depot and an off and on rail route. We have a truss plan in Houston and its location where we can start loading trusses onto the train and transporting to all the state and enhance our economic growth. I am also here to answer any questions from the council or assembly. QUESTION from Bettine: Are you here representing the City of Houston? ANSWER: And the Chamber of Commerce. CINDY: OK, but the City has not taken a position yet? ANSWER: Thursday, we got the resolution drafted, and will be voting on it on Thursday. We had a work shop last week on it and another work shop. We had 3 public hearings already. CINDY: OK, but you don't know what the resolution says yet. ANSWER: I am pretty sure it is going to pass. CINDY: OK, and what about the Chamber? ANSWER: Chamber voted unanimously to support the Houston south route. CINDY: Do you have a letter? ANSWER: The letter was sent to the Borough. The Borough has a copy of the resolution.
17. Doyle Holmes – One of the things that I had a serious problem with when I was on the assembly was trying to determine the real public support or opposition to a issue when it came before the assembly. The Willow route that has been proposed over the years will probably not affect more than two or three hundred people. Then we have the groups that always have the perceived problems with any of these routes, or nothing at all. That leaves about 98% of the people in the valley that are kind of quiet on this issue; and I think that what they are probably interested in is responsible development that will lower their taxes. One of the comments that I used to make was when that 98% did show up, I'd call them the normal people, and I don't see too many of those people here objecting to a development of a rail corridor, period. The choice, of course, is going to be up to you (the assembly?). The resolution that you have before you today is close to what I call "onerous", and you need to make sure that when you put requirements on people that are trying to develop projects, that it doesn't stop the project totally; especially something of this value, in my opinion, to the people of the valley. I really feel that over the years the assembly has tried to pick that route time after time after time, and money seemed to be the biggest problem. But in any case, I feel that the Willow route has the most potential for the Borough and the state. One thing that I feel we made a bad error on over the years is when the assembly picked the route that they thought was best for a rail or highway corridor, that we never ever platted it. And if nothing else comes out of all of this, I think we need to sit down, take a look at the railroad corridor that exists now. That was simply drawn out by the federal government and it was platted by messy bounds, and I think that is probably what we need to start doing to these routes. They have been identified now, and in the next 10-50 years they will probably be developed as transportation corridors for highways, trails, the whole mess. We need to sit down, and really look at seriously and put some money into it, and time and direction and administration to start

blocking those corridors out so that they will be saved and the land protected from sell and protected for future transportation needs.

18. Ken Ray -- I work on the tab board and we have heard this presentation from the railroad a month ago at the tab board. One of the suggestions we made was the consideration of the transportation corridor concept was first brought to my attention under Mr. Perkins administration from the Dept. of Transportation. It has been explained to me that the 800 ft right-of-way is not, perhaps, a doable thing working with the STB, but it does seem like a great opportunity for the Mat-Su Borough to plan for future corridors in conjunction with this project. The idea of dedicating corridors in the Mat-Su Borough, the fastest growing borough in the state, would indicate future planning. Prices will be sustainable, either through acquisitions or options, or planning and proposed regulations; but the Borough should, as part of this, consider transportation corridors rather than continue expanding the existing ones. The railroad was laid out in 1917 and did a great job with what they had at hand; but with Alaska thinking of the future, we have a great opportunity with this rail corridor for future transportation needs.

-end-

*(This page intentionally left blank.)*

Testimony of  
Patrick L. Sharrock (345-5353)  
Matanuska-Susitna Borough Assembly

December 11, 2007 – 3pm  
Palmer, Alaska

Good afternoon Madam Chair and assembly members,

My name is Patrick Sharrock. My wife and I have owned recreation property in the rail routes area for almost 38 years. We live in Anchorage.

Thank you for allowing testimony from the public. I also want to publicly thank Patty Sullivan for her very efficient help.

I understand that you will not be determining a route preference recommendation to the Surface Transportation Board.

I am not here to promote one route over another. However, there are clearly only three alternative routes under review, as identified on the map that you viewed at your meeting of November 20. Those routes are Big Lake, Houston, and Willow. In discussions that I have had with people those are the names used. The matrix sheet you reviewed at that meeting shows eight routes. Those routes are determined from what I call four linkage routes – Mac-West, Mac-East, Houston-North and Houston-South. Combined, these linkage routes cover more distance than the Willow route and that distance does not include the more than ten miles for the legs shown on the map as Conn 1, Conn 2 and Conn 3.

If there is an analysis of routes, then the linkage routes, including the Conn routes should be reviewed and evaluated on their own merits and shown separately on the matrix sheet. That would then leave the three primary routes to be evaluated and also shown separately. However, regarding the current matrix sheet, there is no definition for what a "+," "-" or "0" is for each criteria.

I read in a recent document that *"The matrix, as part of a longer project background report, will lay the foundation for the STB's environmental*

1. I attended two open houses, the Planning Commission meeting on November 5 and your assembly meeting on November 20.
2. Poster boards at the open houses showed that the Planning Commission would hold a "public hearing." Additionally, I recall hearing at one open house that the Planning Commission would make a recommendation of a rail route to the assembly.
3. The Planning Commission meeting was a "work session" only and no public comment was allowed. I asked the Committee Chairwoman about both items and she told me that that information was a mistake.
4. The next morning I called Patty Sullivan about the problem. She seemed stunned and embarrassed and apologized for the error. I believe it was the next day when I checked the MacRail website that I saw a revision to the poster and a footnote apology.
5. I received handouts at the Planning Commission and assembly meetings.
6. On October 29, I wrote a letter to Patty Sullivan and Stephanie Wheeler. The letter was my input concerning the alternative rail routes. I know my letter was timely filed because I received a very nice reply from Patty Sullivan dated November 2. On a page from the MacRail website that I printed on November 26 (MacRail Home Page), a notice in a box at the bottom of the page reads *"Comments received through November 2, 2007, were considered in alignments and preparing the project application to the Surface Transportation Board. Comments received after this deadline will be forwarded to the STB."*
7. As of today the website shows the handout distributed at the assembly meeting, held on November 20, but not the handout distributed at the Planning Commission meeting, held on November 5. So I compared the pages in each handout. One page struck my attention. The page in the Planning Commission handout is titled "Public Comment Statistics (As of Oct. 16, 2008)." The page in the assembly handout is titled "Results of Public Involvement Activities, Public Comment Statistics (As of November 2, 2007)." The information on each page is identical. I refer to two rows in the "Written Comments" column. Those rows are "Mailed Comments" and "Letters." Because of my letter mentioned above (dated Oct. 29), I

In my opinion, anyone raising a family, not retired or reporting on this particular issue would have a terrible time keeping informed of this, seemingly, fast-track process.

In closing, I would like to incorporate by reference <sup>and</sup> ~~or~~ have submitted for the record the "Matanuska-Susitna Borough Rail Corridor Study" dated June 2003 to assure that the issues contained therein are included in the application to the Surface Transportation Board.

I would be happy to answer any questions.

**Appendix B:  
Comments Received**







*Port MacKenzie Rail Extension Project  
Iditarod Dog Sledding Historic District Workshop Summary  
and Implementation Plan*

**Prepared for:**

Surface Transportation Board  
395 E Street SW  
Washington, DC 20423

Alaska State Historic Preservation Officer  
550 West 7<sup>th</sup> Avenue, Suite 1380  
Anchorage, AK 99501

**Prepared by:**

HDR Alaska, Inc.  
with the  
Port MacKenzie Rail Extension Working Group

Alaska Railroad Corporation  
327 W. Ship Creek Avenue  
Anchorage, AK 99501

November 19, 2012

*This page intentionally left blank*

## **Contents**

<b>1</b>	<b>Introduction .....</b>	<b>1</b>
1.1	Project Overview .....	1
1.2	Purpose of the Document .....	1
<b>2</b>	<b>Method for Developing the Implementation Plan .....</b>	<b>3</b>
2.1	Consultation .....	3
2.2	Workshops/Meetings .....	5
2.3	Identification of Mitigation Measures .....	9
<b>3</b>	<b>Descriptions .....</b>	<b>9</b>
3.1	Area of Potential Effect (APE).....	9
3.2	Iditarod Dog Sledding Historic District (IDSHD).....	10
3.3	Officially Recognized Trails .....	12
<b>4</b>	<b>Summary of Issues, Outcomes, and Mitigation Commitments.....</b>	<b>13</b>
4.1	Participant Comments/Concerns .....	13
4.2	Constraints.....	13
4.3	Implementation Measures based on Workshops/Meetings .....	14
4.4	Additional Implementation Measures .....	29

## **Tables**

Table 1: Summary of Mitigation Measures .....	2
Table 2: IDSHD Meeting Summary .....	6
Table 3: Summary of IDSHD Workshop Comments/Issues and ARRC Mitigation Commitments .....	33
Table 4: Trail/Crossing Summary.....	37

## **Exhibits**

Exhibit 1: Iditarod NHT crossing design before and after the IDSHD Workshops .....	16
Exhibit 2: Approach example .....	17
Exhibit 3: Crossing 5 profile view .....	20
Exhibit 4: Proposed Iron Dog and Flat Lake Connector Trail Reroutes and Crossing (Crossing 5) .....	21
Exhibit 5: Possible Iditarod Race Trail Reroute and Crossings.....	23
Exhibit 6: Connectivity between Crossings 7 and 10 .....	25
Exhibit 7: Signage examples presented at the MSB Trail User Open House (10/27/11).....	26
Exhibit 8: Willow Trail System Kiosk.....	27
Exhibit 9: Kiosk schematic (Department of Natural Resources).....	27
Exhibit 10: Big Lake Trail #2 Reroute .....	30
Exhibit 11: Houston Lake Loop Trail Reroute .....	31

**Figures**

Figure 1: Project overview and crossing locations ..... A-3  
Figure 2: Area of Potential Effect (APE)..... A-4  
Figure 3: Constraints..... A-5  
Figure 4: Trail connectivity ..... A-6

**Attachments**

Attachment A ..... Figures  
Attachment B ..... Glossary  
Attachment C ..... IDSHD Consulting Parties and Participants  
Attachment D ..... Workshop 1 Summary and Materials  
Attachment E ..... Workshop 2 Summary and Materials  
Attachment F ..... MSB Trail User Open House Summary and Materials  
Attachment G ..... Workshop 3 Summary and Materials  
Attachment H ..... Draft Document Reviewer Comment and Response Summary

**List of Acronyms/Abbreviations**

ACHP.....	Advisory Council on Historic Preservation
ADNR.....	Alaska Department of Natural Resources
APE.....	Area of Potential Effects
ARRC.....	Alaska Railroad Corporation
ATV.....	All-Terrain Vehicle
BLM.....	Bureau of Land Management
BMBF.....	Bulk Material Bi-modal Facility
CBD.....	Concrete Ballast Deck
CFR.....	Code of Federal Regulations
Corps.....	U.S. Army Corps of Engineers
DMLW.....	Division of Mining, Land and Water
EIS.....	Environmental Impact Statement
EPA.....	Environmental Protection Agency
FAA.....	Federal Aviation Administration
HDR.....	HDR Alaska, Inc.
HTK.....	Happy Trails Kennels
IDSHD.....	Iditarod Dog Sledding Historic District
KTC.....	Knik Tribal Council
LRTP.....	Long Range Transportation Plan
MEA.....	Matanuska Electric Association
MHLT.....	Mental Health Land Trust
MOU.....	Memorandum of Understanding
MP.....	Milepost
MSB.....	Matanuska-Susitna Borough
NEPA.....	National Environmental Policy Act
NHPA.....	National Historic Preservation Act
NHT.....	National Historic Trail
NPS.....	National Park Service
NRHP.....	National Register of Historic Places
PA.....	Programmatic Agreement
PMRE.....	Port MacKenzie Rail Extension
ROD.....	Record of Decision
ROW.....	Right-of-way
SCRO.....	Southcentral Regional Office
SHPO.....	State Historic Preservation Officer
SnoTRAC.....	Snowmobile Trail Advisory Committee
SOI.....	Secretary of Interior
SPUD.....	Special Land Use District
SRB&A.....	Stephen R. Braund & Associates
STB.....	Surface Transportation Board
UA.....	University of Alaska
USGS.....	U.S. Geological Survey
VORTAC.....	VHF Omnidirectional Range/Tactical Aircraft Control
WDMA.....	Willow Dog Musers Association

*This page intentionally left blank*

# 1 Introduction

## 1.1 Project Overview

The Alaska Railroad Corporation (ARRC), in partnership with the Matanuska-Susitna Borough (MSB), has undertaken planning for construction of the Port MacKenzie Rail Extension (PMRE) Project. In a November 2011 decision, the Surface Transportation Board (STB) approved a license allowing ARRC to construct and operate approximately 35 miles of new rail line connecting the Port MacKenzie District to a point on ARRC's existing main line near Houston, Alaska, using the Mac East Variant-Connector 3 Variant-Houston-Houston South alignment (Attachment A, Figure 1). That authorization was issued after completion of an Environmental Impact Statement (EIS), and is subject to environmental mitigation conditions, including a Programmatic Agreement<sup>1</sup> (PA; executed 5/25/2011) executed by STB, the Alaska State Historic Preservation Officer (SHPO), and the Advisory Council on Historic Preservation (ACHP).

The PA identifies mitigation measures to address possible effects to historic properties<sup>2</sup>, including the Iditarod Dog Sledding Historic District (IDSHD). Stipulation II of the PA identifies the process for developing an Area of Potential Effect (APE), within which effects to historic properties such as the IDSHD shall be considered. Stipulation V of the PA (*Treatment of the Iditarod Dog Sledding Historic District*), as well as Mitigation Measures 92 and 93 in the Record of Decision (ROD) for the Final EIS, identify mitigation for adverse effects to the IDSHD, that are addressed through preparation of this *Workshop Summary and Implementation Plan (Implementation Plan)* that has been developed in consultation with STB, SHPO, and parties interested in the IDSHD.

## 1.2 Purpose of the Document

ARRC, in consultation with the Working Group<sup>3</sup> and SHPO, has drafted this document to report on compliance with the requirements of the PA (Stipulations II.D and V) and ROD (Mitigation Measures 92 and 93). This document combines reporting for the PA and ROD mitigation requirements into a single document rather than multiple separate documents because ARRC believed that these requirements include overlap such as discussion of trail use and impacts to users of the existing trail system in the APE as well as consultation with the same interested parties. In addition, combining the requirements into one document is supported by the PA, which required that ARRC submit plans for grade separation and other mitigation measures (e.g., relocations of officially recognized trails and design changes) included in the ROD during the IDSHD workshop(s) and summarize the outcome of the workshop(s) in an Implementation Plan. Table 1 summarizes PA and ROD mitigation measures addressed in this document.

---

<sup>1</sup> The PA was developed through the Section 106 process under the National Historic Preservation Act (NHPA)

<sup>2</sup> See Attachment B, *Glossary*, for definitions of selected terms used throughout the document.

<sup>3</sup> The Working Group is comprised of ARRC, MSB, and KTC representatives with input from STB and SHPO.

**Table 1: Summary of Mitigation Measures**

<b>Mitigation Measure or PA Stipulation</b>	<b>Location in Document - How Addressed</b>
<p><b>PA Stipulation V.A.1</b></p> <p>Hold a workshop with parties interested in the IDSHD (within 60 days of STB licensing an alternative) to delineate the boundaries of contributing features within the APE, how they continue to be used for their historic function, and how that function could be maintained during and after construction.</p>	<p><b>Sections 2.1, 2.2, and 3.2</b></p> <p>ARRC held a series of workshops, and MSB held an open house, with dog sledders and other trail users interested in the IDSHD and trails impacted by the project. These meetings were held in consultation with the STB and SHPO. Based on comments at the meetings, it became clear that the trail users were more concerned with continued access and connectivity within the IDSHD than in identification of contributing trails. Therefore, the focus of the workshops shifted from delineating the boundaries of contributing features, historic function, and how to maintain this function to identifying ways to maintain safe access and connectivity for trail users.</p>
<p><b>PA Stipulation II.D</b></p> <p>Delineate the proposed final APE for the Undertaking using ARRC's final construction plans and the APE definition in PA Stipulation II.C.</p>	<p><b>Section 3.1 and Figure 2</b></p> <p>ARRC, in consultation with IDSHD workshop participants, the Working Group, SHPO, and STB, developed a proposed final APE that was submitted to STB and SHPO and approved in January 2012.</p>
<p><b>ROD #92, ROD #93, and PA Stipulation V.B</b></p> <p>Prepare a draft report that:</p> <ul style="list-style-type: none"> <li>• Identifies sources and parties consulted on proposed trail relocations and trail use</li> <li>• Summarizes discussions/meetings with interested parties</li> <li>• Identifies officially recognized trails to be relocated, rather than provided with grade-separated or at-grade crossings, the rationale for the relocations and potential impacts to existing trail users if the trails are relocated rather than provided grade-separated crossings</li> <li>• Identify location and use of trails contributing to the IDSHD</li> <li>• Develop an Implementation Plan</li> </ul> <p>Provide a copy of the draft report (30-day review/comment period) to consulting parties; prepare a final report (that also summarizes substantive comments/ responses) and submit to consulting parties.</p>	<p><b>Sections 2.1, 2.2, 3.2, 3.3 and 4.0; Tables 2 and 3; Attachments D - H</b></p> <p>ARRC, in consultation with the Working Group and SHPO, prepared a draft <i>IDSHD Workshop Summary and Implementation Plan</i> that:</p> <ul style="list-style-type: none"> <li>• Identifies consulting parties</li> <li>• Summarizes the IDSHD Workshops and MSB Trail User Open House</li> <li>• Identifies officially recognized trails that may be relocated, rationale for their relocation, and potential impacts to trail users from the relocations</li> <li>• Describes the IDSHD trail system and use (see response for PA Stipulation V.A.1)</li> <li>• Provides implementation measures/ commitments to maintain access and connectivity</li> </ul> <p>This report was submitted to all parties to the PA and workshop/open house participants for a 30-day review in March 2012. The final report, which includes a summary of comments received and ARRC's responses, will be provided to all PA parties and workshop/open house participants.</p>



Mitigation Measure or PA Stipulation	Location in Document - How Addressed
<p><b>PA Stipulations V.A.2, V.A.3, and V.C; ROD #93</b></p> <p>Submit plans for design changes, modifications, and refinements (e.g., grade separation and other mitigation measures specified in the ROD) for review and discussion during the workshop. These proposed design changes shall seek to avoid, mitigate, or minimize adverse effects on the IDSHD. Treatment of contributing trails to the IDSHD that are used for dog sledding and are necessary to maintain the connectivity of the IDSHD could include grade-separated crossings, relocation of trails, and/or other mitigation to allow for continued access and connectivity.</p>	<p><b>Sections 2.2 and 4.3, Table 3 and Attachments D - G</b></p> <p>This plan does not identify trails contributing to the integrity of the IDSHD for reasons previously mentioned. However, it does discuss trails that are being used for dog sledding and recreational activities and crossings to be provided to allow continued connectivity of these trails. ARRC provided plans for crossings, trail improvements, and trail relocations at the various IDSHD workshops and open house. Changes to project design consider workshop participant comments and endeavor to minimize adverse effects to the IDSHD by maintaining access and connectivity. Project design minimizes adverse effects to the IDSHD by maintaining safe access and connectivity through at-grade and grade-separated crossings, trail relocations, and trail improvements.</p>

ROD Mitigation Measure 93 and PA Stipulation V focused on trails contributing to the IDSHD; however, consultation with the SHPO and parties interested in the IDSHD indicated identifying a way to maintain access and connectivity of the current trail network should be the priority of mitigation measures. This is a shift in focus from identifying individual trails as contributing elements to a historic district to maintaining the current and historic use of these trails. Further discussion regarding the change in focus of the IDSHD consultation is included in Section 2.2.1; further discussion of the IDSHD is included in Section 3.2.

## 2 Method for Developing the Implementation Plan

### 2.1 Consultation

Consultation began with identification of appropriate consulting parties. The initial list was based on consulting parties identified in the PA as being interested in the IDSHD (e.g., Happy Trails Kennels [HTK], Willow Dog Musher's Association [WDMA], Knik Tribal Council [KTC], and MSB; see PA Stipulation V). In addition to SHPO, the Working Group, and consulting parties identified in the PA, the Project Team<sup>4</sup> contacted other dog mushers and parties known to be associated with dog sledding and other trail use in the project area. The consulting party list for the IDSHD workshops and open house is included in Attachment C.

Project Team members have been meeting with key property owners (e.g., Alaska Department of Natural Resources [ADNR], University of Alaska [UA], Mental Health Land Trust [MHLT]), key interests (e.g., Federal Aviation Administration [FAA]), and trail users throughout project development. ARRC consulted with the Working Group and SHPO, and their representatives participated in planning for and attended the IDSHD workshops and the MSB Trail User Open House. They also participated in developing the *Comment Resolution Summary* provided to consulting parties as part of IDSHD Workshop 3 (Attachment G) and this *Implementation Plan*.

---

<sup>4</sup> The Project Team consists of ARRC, MSB, and their consultants.

Agencies and organizations contacted for participation in the IDSHD workshops and the MSB Open House included:

- ADNR
- Aurora Dog Mushers Club
- Big Lake Trails, Inc.
- Bureau of Land Management (BLM), Iditarod National Historic Trail (NHT)
- Dream a Dream Dog Farm
- Homestretch Kennel
- HTK
- Iditarod Historic Trail Alliance
- Iditarod Trail Blazers
- Iditarod Trail Committee
- Iron Dog Race
- Junior Iditarod Sled Dog Race
- Knik Dog Mushers Association
- Knik 200 Joe Redington, Sr., Memorial Sled Dog Race
- KTC
- MSB
- Mat-Su Trails Council, Inc.
- Northern Lights 300 Sled Dog Race
- Perseverance Springs Farm
- SHPO
- WDMA
- Willow Trail Committee

The Project Team used a revised consulting party list for the invitation for the first IDSHD workshop and asked invited participants to forward the invitation to other individuals they thought might be interested in attending. This process was followed for each IDSHD workshop and meeting. Persons who received forwarded invitations were added to the consulting party list. In addition, the consulting party list was revised based on sign-in sheets at each IDSHD workshop and meeting. Communications with consulting parties were conducted in person as well as via email and telephone. The Project Team posted IDSHD-related events and associated materials on the project website and notified consulting parties when the materials had been posted.

A draft *Implementation Plan* was sent to PA Signatories, Invited Signatories, Concurring Parties, and persons interested in the IDSHD (i.e., workshop participants) for a 30-day review and

comment period on March 1, 2012. Comments were received from STB; SHPO; and the ADNR, Division of Mining, Land and Water (DMLW), Southcentral Regional Office (SCRO). No comments were received from Concurring Parties or workshop participants. ARRC, in consultation with the Working Group, STB and SHPO, reviewed these comments and incorporated revisions to this document as deemed appropriate (see Attachment H for further detail regarding comments and responses)<sup>5</sup>. In a meeting on April 10, 2012, STB and SHPO requested that ARRC make the following revisions to the document to address comments received:

- Add additional detail and graphics into the body of the document to provide further clarification regarding mitigation measures being implemented;
- Provide firm commitments (e.g., remove “will continue to discuss” language where possible);
- Provide contingencies for resolutions to issues that are not currently resolved (e.g., MSB obtaining easements for specific trails or trail relocations);
- Provide additional narrative regarding change in workshop focus from individual trails as contributing elements and historic use (see Table 1) to access/connectivity of the current trail network;
- Provide additional narrative to explain why ARRC combined reporting for PA and ROD mitigation measures into a single document;
- Add a glossary of terms (e.g., cultural landscape, contributing element, etc.) as an attachment; and
- Provide timeframes that address the project and when the public will be expected to participate in the future.

ARRC is providing this final *Implementation Plan* to PA Signatories, Invited Signatories, and other interested consulting parties and has posted this document on the project website (<http://www.portmacrail.com/library.html>). Should further changes to the *Implementation Plan* be required because of changes to crossings, that information will be conveyed to all parties prior to final project design and construction.

## **2.2 Workshops/Meetings**

The IDSHD workshop series and the MSB Trail User Open House addressed both dog sledding and officially recognized trails relative to crossings types, trail relocations, and maintenance of connectivity to inform mitigation measures for addressing impacts to the IDSHD. Section 2.2.1 provides a summary of the methodology and discussions at the workshops/open house. Attachments D through G provide workshop/meeting materials, including materials presented or distributed and sign-in sheets. A discussion of workshop/meeting participants’ substantive comments as well as the Project Team’s responses and ARRC’s mitigation commitments are included in Section 4.3. Table 2 includes a summary of the IDSHD meetings held to date. A

---

<sup>5</sup> Mitigation Measures 92 and 93 require that all substantive comments from reviewers on the draft document and ARRC’s responses be summarized and included in the final document.

summary of workshop/meeting participants’ substantive comments with the Project Team’s responses and mitigation commitments is included in Table 3.

**Table 2: IDSHD Meeting Summary**

<b>IDSHD Workshop #</b>	<b>Workshop Date</b>	<b>Workshop Location</b>	<b># of Attendees</b>
1	6/28/11	Willow Area Community Organization Center Willow, Alaska	26
2	7/7/11	Big Lake Library Big Lake, Alaska	20
N/A <sup>6</sup>	10/27/11	Houston High School Houston, Alaska	41
3	1/11/12	Houston Middle School Houston, Alaska	23

### **2.2.1 Summary of the IDSHD Workshop Series and MSB Trail User Open House**

The PA stipulates that ARRC, in consultation with STB and SHPO, hold a workshop<sup>7</sup> with parties interested in the IDSHD, including HTK, WDMA, KTC, and MSB. The initial purpose of the workshop was to “delineate the boundaries of contributing features within the APE of the alternative licensed by the STB; discuss which are still used for their historic function; and determine how that historic function could be maintained during and following construction of the Undertaking” (PA Stipulation V.A.1). The PA also stipulates that the Working Group consult with HTK, WDMA, and other concurring parties interested in the IDSHD to delineate the proposed final APE for the project (PA Stipulation II.D).

The goals/purpose of IDSHD Workshop 1 (see Attachment D for workshop materials) were designed to fulfill the goals of the PA and included:

- Providing a project update and background;
- Soliciting input/comments on the APE;
- Identifying/discussing important features, functions, and uses of IDSHD trails/structures in the APE;
- Identifying/discussing potential mitigation measures for IDSHD trails/structures in the APE to ensure continued functionality and connectivity of trails; and
- Identifying the need for future workshops/meetings.

---

<sup>6</sup> This MSB-sponsored open house was held for trail users at the request of the IDSHD Workshop participants.

<sup>7</sup> While the PA only requires one workshop with parties interested in the IDSHD, SHPO advised ARRC during planning that the workshop’s goals likely could not be met in a single meeting. Therefore, ARRC held a series of workshops.

Following a formal presentation, participants were divided into smaller working groups. Each group had a facilitator who posed a series of questions<sup>8</sup> to participants.

- Does the proposed APE encompass the areas where impacts would likely occur? If not, where should the APE be altered to include these areas?
- What are important features, functions, and uses of IDSHD trails/structures in the proposed APE (e.g., access and connectivity, visual way finders, or different/distinct uses [freight, training, races])?
- Do you feel that the PMRE project will affect dog sledding or the features, functions, and uses of IDSHD trails through the proposed APE? If so, how?
- What can be done to further reduce these impacts?

Following the break-out session, participants reconvened to present questions and comments raised by each group. While break-out session questions were designed to obtain information on IDSHD contributing features and use, participant discussions and comments focused primarily on effects of the project including current and future access, safety, and connectivity concerns. The Project Team did not receive substantive comments from participants regarding the APE, except that some participants suggested that the APE should be broader to include potential indirect effects (i.e., foreseeable effects caused by the action that is later in time or farther removed in distance; see Attachment B, Glossary), particularly in areas where trails cross the project right-of-way (ROW). Further discussion of the APE is included in Section 3.1. Participants stated that future workshops were desired. As a follow-up to this, Workshop 1 participants were asked to self-identify as being interested in participating in a smaller focus group whose original purpose was to further discuss the APE and provide information regarding historic trails and features within the IDSHD (see Attachment D).

Based on Workshop 1 participant comments and further consultation with SHPO and the Working Group, the focus of the IDSHD workshops was shifted from the purpose stipulated in the PA and ROD Mitigation Measure 93 (i.e., to identify contributing features of the IDSHD within the APE and historic function/use) to maintaining continued access, use, and connectivity of trails. Because the proposed rail line bisects the IDSHD, SHPO emphasized that the continued functionality of the IDSHD and use for dog sledding were more important than identifying individual contributing features. It was clear from participant comments that the significance of the IDSHD lies not in individual features, although trails such as the Iditarod Race Trail and the Iditarod NHT did stand out as being of particular importance to participants in general, but rather in the network of trails and other features (see Section 3.2 for further discussion of the IDSHD).

In line with the modified focus from historic to continued/future use of the trail system that comprises the IDSHD, the goals/purpose of IDSHD Workshop 2 (Attachment E) were to:

- Review and clarify Workshop 1 comments;

---

<sup>8</sup> Questions were developed in consultation with the Working Group and SHPO.

- Develop and address the location and design of trail crossings to minimize or mitigate impacts to important characteristics of the IDSHD focusing on connectivity and use of the landscape; and
- Discuss the proposed APE (revised per input from Workshop 1).

Participants were provided with a summary of Workshop 1 comments/issues and revised APE maps prior to the meeting (see Attachment E). The Project Team presented each comment/issue and asked participants for further information or clarification. Comments focused primarily on access, safety, and connectivity, including recommendations for additional crossings and trails to maintain access and connectivity and revisions to crossing and approach designs to enhance safety. Workshop participants indicated that they wanted to ensure that trail users other than dog mushers were allowed a chance to comment on maintaining trail access and connectivity. Therefore, they requested that Workshop 3 be postponed until after a larger meeting was held with all trail users. As MSB was taking the lead on addressing general trails issues associated with the project, the Project Team decided to have MSB organize and sponsor this larger trail users meeting with input and participation from Working Group and SHPO representatives.

Based on the input from IDSHD Workshop 2 participants, the goals/purpose of the MSB Trail User Open House (Attachment F) were to:

- Ensure that trail users other than dog mushers were being heard regarding project impacts to trail connectivity and use;
- Present trail network connections for continued connectivity and use; and
- Listen and gain feedback from trail users.

The open house included stations for the different use areas within the project area (e.g., Willow, Big Lake, and Point MacKenzie) as well as stations dedicated to MSB trail designation and Section 106 activities. The use area stations were staffed by local users. Project Team members and SHPO circulated between stations to answer participants' questions and receive comments. Similar to the IDHSD workshops, participant comments focused primarily on access, connectivity, and safety issues.

The IDSHD Workshop 3 was held after the MSB Trail User Open House. The purpose of this workshop (Attachment G) was to:

- Present/discuss the Project Team's proposed resolutions to participant comments received to date; and
- Create understanding – the Project Team has heard, considered, and addressed participant comments where practicable.

The format of this workshop was a presentation of summarized issues identified at the two previous IDSHD workshops and the MSB Trail User Open House with an opportunity for participant questions and comments following each issue. Comments again focused primarily on maintaining access and connectivity as well as safety issues. Workshop participants indicated that they felt the Project Team had accurately captured their comments and had provided adequate responses to the majority of these comments (see Section 4.3 for a discussion of issues and project commitments and Table 3 for a summary of participant comments/issues and Project Team responses/commitments).

## **2.3 Identification of Mitigation Measures**

Comments received at the IDSHD workshops focused primarily on maintaining and ensuring access, connectivity, and safety. Through comments and discussions at the IDSHD workshops/open house, as well as the Project Team and Working Group meetings, the Project Team developed plans for grade separation and other minimization or mitigation measures (e.g., crossing design changes, modifications, and refinements as well as trail improvements and reroutes). Additional information regarding minimization or mitigation measures is included in Section 4 (Summary of Issues, Outcomes, and Mitigation Commitments), Table 3 and Table 4.

## **3 Descriptions**

### **3.1 Area of Potential Effect (APE)**

STB, in consultation with SHPO, established an APE for the project during studies conducted under Section 106 and the EIS. The PA stipulates that the Working Group, in consultation with HTK, WDMA, and other consulting parties interested in the IDSHD, shall review ARRC's final construction plans, apply the APE definition provided in PA, and delineate the proposed final APE for the Undertaking within 60 days of STB issuing a license for the project (PA Stipulation II.D). As the target audience for the IDSHD and APE discussions involved the same consulting parties, the Working Group and SHPO decided to consult with these parties regarding APE at the IDSHD workshops.

The Project Team provided an explanation of APE and presented a proposed project APE to Workshop 1 participants (see Attachment D). This proposed APE included a 200-foot ROW (100 feet on either side of the rail centerline) and other ground disturbance areas that may be outside the 200-foot ROW (e.g., staging areas, cut and fill areas, material sources, etc.).

Further discussion was conducted at Workshop 2, including additional explanation of APE and presentation of maps showing the Working Group's proposed delineation of the project APE (Attachment E). Participant's requested that the Working Group consider indirect effects (e.g., visual, noise, increased trail user-moose conflicts, and increases in population resulting in loss of trails) while defining the final APE. As a result of this request and consultation with SHPO, the Working Group defined an indirect APE, in addition to the previously delineated 200-foot APE for direct effects, that includes an area of one mile maximum<sup>9</sup> on either side of the rail centerline. No objections were raised to the proposed final APE by workshop participants.

Following the Working Group's presentation of the proposed APE, discussion with workshop participants, and consultation with SHPO, ARRC submitted the proposed final APE to STB and SHPO for review and comment on December 30, 2011. SHPO (January 9, 2012) and STB (January 17, 2012) indicated that they had no objections to the proposed APE.

Changes in project design required a modification to the APE in the northernmost portion of Segment 6 (e.g., access and material storage) and in Segment 5 near Muleshoe Lake (realignment of the proposed rail ROW; see Section 4.2 for further discussion). ARRC submitted proposed modifications of the proposed final APE to STB and SHPO on March 9, 2012. SHPO (March 16,

---

<sup>9</sup> The APE for indirect effects may be less than the proposed one-mile maximum in areas where views are impeded by dense vegetation or topography.

2012) and STB (March 29, 2012) indicated that they had no objections to the proposed modifications to the APE.

Due to the likelihood of additional changes, ARRC proposed a revised approach to STB and SHPO for dealing with future modifications to the APE. Modifications that are within the overall APE (i.e., including direct and indirect effects) will only result in a change from indirect to direct effects, and a request for modification and approval of a revised APE is not necessary. STB and SHPO agreed that ARRC shall notify STB and SHPO of the change in designation from indirect to direct effects. The Working Group would address the need for cultural resources surveys in any new areas that would have direct effects in accordance with the PA and Working Group Memorandum of Understanding (MOU; Section 5). These areas would require cultural resource surveys if they are in areas of medium or high probability for cultural resources, or selected areas of low probability identified during consultation. In the annual report, ARRC shall provide updated APE maps that incorporate the new areas of direct effects. This annual report shall be provided to Signatories, Invited Signatories, and Concurring Parties to the PA and shall be posted on the project website for review and comment. The current APE is illustrated in Figure 2 of Attachment A.

### **3.2 Iditarod Dog Sledding Historic District (IDSHD)**

The PMRE project bisects the historic and current dog sledding trail system between the Knik Arm coastline and the Susitna River. These trails are used by dog mushers as well as multiple other users, including all-terrain vehicles (ATV), snow machines, horseback riders, hikers, and skiers.

In association with studies conducted under Section 106 and the EIS, Stephen R. Braund & Associates (SRB&A 2010<sup>10</sup>) examined a dog sledding cultural landscape in the project area by conducting literature and archival research, site visits, and interviews with people knowledgeable about dog sledding in the project area.

A cultural landscape is a geographic area associated with a historic event, activity, or person or that exhibits other cultural or aesthetic values (SRB&A 2010). Cultural landscapes are generally listed on the National Register of Historic Places (NRHP) as sites or districts. The National Park Service (NPS) recognizes four types of cultural landscapes: 1) historic designed landscapes (e.g., cemeteries, parks, gardens, etc.); 2) historic vernacular landscapes (e.g., IDSHD, rural villages, industrial complexes, agricultural landscapes, etc.); 3) historic sites (e.g., presidential properties, battlefields, etc.); and 4) ethnographic landscapes (e.g., religious sacred sites, massive geological features with cultural significance, etc.). Cultural landscapes are defined by multiple characteristics such as land use, patterns of spatial organization, and circulation networks (i.e., systems of movement such as roads and trails).

The IDSHD is a historic vernacular landscape that illustrates the historic significance of dog sledding in the study area. The IDSHD consists of trails, travel routes, kennels, clubs, roadhouses, and natural features that demonstrate use of natural systems and features, spatial organization, circulation, buildings/structures, and cultural traditions associated with dog sledding. As reported by SRB&A (2010), dog sledders frequently use seismic lines, section lines, power line easements, open wetland areas and frozen rivers and creeks in addition to

---

<sup>10</sup> SRB&A. 2010. *Port Rail Extension Project Report of 2009 Cultural Resources Fieldwork*. Anchorage, Alaska.



established trails. Trails are not generally identifiable during non-winter months as ground surface and vegetation is protected from permanent damage due to snow cover. Because the trail system is dependent on natural features, trail locations fluctuate from year to year due to changes in the landscape. Therefore, movement of trails to take advantage of landscape conditions from year to year is a characteristic of the IDSHD.

STB supported, and SHPO concurred with, the recommendation and finding (SRB&A 2010) that the IDSHD (ANC-03326/TY0-00203) was eligible for listing in the NRHP under Criterion A at the national level of significance for its association with events that have made a significant contribution to broad patterns of Alaska history, specifically the theme of recreation related to the Iditarod Race and its development (1967-1978). The start of this period marks the inaugural run of the Iditarod Race that traced the route of the historic Iditarod Trail between the Knik and Big Lake areas. The historic Iditarod Trail was designated as a NHT in 1978.

SRB&A (2010) recommended multiple individual trails, buildings/structures, and natural features as contributing elements/features to the IDSHD. The majority of these trails and buildings/structures are located outside of the APE. Recommended contributing trails within the APE include: the Iditarod NHT<sup>11</sup>, Iditarod Race Trail, United States Geological Survey (USGS) Transmission Line Trail, and Basemap Winter Trails 2<sup>12</sup>, 4, and 5 (see Attachment D, Crossing Locations map and APE map series for locations of SRB&A's recommended contributing trails). Not all of these trails are still used for dog sledding. For example, workshop participants stated that Basemap Winter Trail 2 is no longer used for dog sledding; it was located on private/agricultural lands and no longer exists. Other recommended contributing trails in the southern most end of the project are located within the Port MacKenzie District and access/use is restricted due to port security and land ownership constraints. The Project Team proposed one crossing, an oversized culvert for the Figure 8 Lake Loop Trail (Crossing 18; see Figure 1 and Table 4), at the southern end of the project, and participants raised no objections. Additional accommodations for recreational trail users have been proposed since the IDSHD workshops/open house (see Section 4.4).

The Iditarod NHT and Iditarod Race Trail are individual trails in the project area that were specifically referenced by workshop/open house participants as important to the IDSHD and noted for continued use for dog sledding. These trails are necessary to maintain the connectivity of the district, and have separated grade crossings (see Section 4.3 and Table 4 for further discussion of these crossings).

Continued use of the IDSHD for dog sledding is dependent on maintaining access and connectivity across/through the railroad embankment, not on whether a trail is labeled as contributing or non-contributing to the historic district. As workshop participants pointed out, trails of more recent origin currently used by dog mushers are important to connectivity and access, even if they are not contributing elements to the historic district. As discussed in SRB&A (2010), it is the interconnectedness of the trails and easy access that makes the IDSHD unique to other areas of the state where dog sledding occurs. Therefore, following Workshop 1

---

<sup>11</sup> This trail was designated as a National Historic Trail [NHT] in 1978 but has had no determination of eligibility for the NRHP to date.

<sup>12</sup> Based on 2011 consultation, USGS Base Map Winter Trail 2 trail is no longer in use, so the Working Group recommends that it is no longer a contributing element to the IDSHD.

and consultation with the Working Group and SHPO, the focus of mitigation for the IDSHD shifted from addressing impacts to individual trails and other features to addressing the access and connectivity of the IDSHD trail network and related infrastructure through maintaining access and connectivity and addressing safety issues for dog sledding within the IDSHD at crossings and crossing approaches. ARRC has developed multiple commitments to avoid, minimize, or mitigate impacts to the IDSHD from the project (see Section 4.3, Table 3 and Table 4).

### **3.3 Officially Recognized Trails**

Mitigation Measure 92 requires ARRC to report on any officially recognized trails that it proposes to relocate rather than provide grade-separated or at-grade crossings, along with the rationale for the relocations and impacts to trail users if the trails are relocated rather than being equipped with grade-separated crossings. Per the Final EIS, “Where the proposed rail line would cross an officially recognized trail, ARRC has stated it would provide public access by a grade-separated crossing. Alternatively, the trail could be relocated by ARRC to avoid crossing the rail line.” The Project Team has not committed to providing crossings for unofficial trails<sup>13</sup>.

Officially recognized trails in the project area include:

- Big Lake Trail #2 (Crossing 2)
- Big Lake Trail #1 (Crossing 4)
- Houston Lake Loop Trail (Crossing 4)
- Big Lake Trail #5 – Iron Dog Connector Trail (Crossing 5)
- Flat Lake Connector Trail – Iron Dog Trail (Crossing 5)
- Crooked Lake Trail – West Papoose Twins Road (Crossing 7)
- Big Lake Trail #14 – Iditarod Race Trail (Crossing 9)
- Iditarod NHT (Crossing 10)

All officially recognized trails have planned crossing structures with the exception of three trails that are planned to be rerouted under nearby bridges to facilitate a crossing: 1) Big Lake Trail #5 - Iron Dog Connector Trail, 2) Flat Lake Connector - Iron Dog Trail, and 3) the Houston Lake Loop Trail. Further discussion of these reroutes is included in Sections 4.2 and 4.3.

During the IDSHD workshops/open house, participants stated that relocating trails rather than keeping the trails in place and providing each with grade-separated crossings would result in potential impacts to trail users. Of particular concern to trail users was reroutes that would consolidate multiple trails onto one trail and crossing; these types of reroutes were considered to have higher potential for “bottlenecking” trail users, increasing potential for collisions, and creating user conflicts. Participants indicated that impacts could be minimized by improving line

---

<sup>13</sup> Per the Final EIS, an unofficial trail is any trail “that is not specifically established within currently adopted plans by ADNR and/or MSB or is established within these plans at the time of construction or ROW conveyance (whichever occurs first), and whose location is not provided for by recorded ROW or easement. ARRC does not propose to provide crossings for unofficial trails. Unofficial trails would be blocked, and ARRC’s trespassing regulations would prohibit the public from crossing of the ROW without first obtaining approval from ARRC.”

of sight at approaches, increasing the radius of curves for approaches and trail relocations, and providing grade-separated crossings with increased horizontal clearance.

ARRC has proposed to grade separate officially recognized trails where possible; however, grade separations are not feasible for all crossings due to terrain, adverse impacts to other environmental resources, or other constraints (see Section 4.2). IDSHD workshops/open house participants stated that they would prefer to have at-grade crossings rather than no crossing at all in cases where grade separation is not feasible.

At-grade crossings will be provided for roads and utility corridors currently crossing the alignment; some of these corridors share the same alignment as officially recognized trails. Pursuant to ROD Mitigation Measures 84D and 92, ARRC is conducting ongoing coordination with recreational trail users, regulatory agencies, landowners, and the STB to comply with STB's requirements for the location, design, and construction of any at-grade rail crossings. Upon acquisition of all project ROW and associated easements, ARRC will submit a final At-Grade Crossing Plan to the STB.

Some of the officially recognized trails still do not have a formal ROW instrument/easement in place despite being included in a local trails plan (e.g., Houston Lake Loop Trail, Big Lake Trail #2, Iron Dog Connector Trail and Flat Lake Connector-Iron Dog Trail). Crossings are contingent on adjacent landowners granting trail easements. The Project Team is working diligently with trail users, property owners and agencies to secure easements or other potential legal instruments for the officially recognized trails crossing the new rail embankment. Negotiations are currently underway with UA and MHLT to secure easements for existing trails on their lands. This effort is ongoing and progress will be reported to user groups and agencies prior to final design and construction. Further discussion of these efforts is included in Section 4.3.

## **4 Summary of Issues, Outcomes, and Mitigation Commitments**

### **4.1 Participant Comments/Concerns**

The Project Team has worked to identify design changes, modifications, and/or refinements to the project to address mushers' and other trail users' concerns and mitigate impacts identified during IDSHD workshops and the MSB Open House. Participant comments/concerns provided at the workshops and open house focused primarily on access, connectivity, and safety issues. Table 3 summarizes comments/concerns raised by participants at the IDSHD-related workshops/meetings. Further information regarding these comments/concerns and resulting ARRC mitigation commitments is included in Section 4.3.

### **4.2 Constraints**

Multiple constraints (see Attachment A, Figure 3) have affected project design including development of avoidance, minimization, or mitigation measures:

- FAA VORTAC<sup>14</sup> radar site construction restrictions

---

<sup>14</sup> VHF Omnidirectional Range/Tactical Aircraft Control.

- Wetlands impacts (Environmental Protection Agency [EPA] and the U.S. Army Corps of Engineers [Corps])
- Land ownership
- Lack of legal easements for some trails affected by the project

The Project Team has been working with FAA regarding project and construction constraints associated with the FAA VORTAC radar site, which controls the approach for all Anchorage air traffic. FAA defined a three-mile radius where construction restrictions will apply (see Figure 3 in Attachment A). Within this restricted area, the rail embankment and passing trains cannot block the VORTAC electronic signals. FAA denied ARRC's request for an easement across the 160-acre parcel where its VORTAC is located. In light of FAA's decision, a potential modification to the alignment was made to avoid the FAA VORTAC site that included shifting approximately two miles of the alignment to the west of Muleshoe Lake. As originally proposed, the rail alignment was located on the east side of Muleshoe Lake and was going to require a slight realignment to the Houston Lake Loop Trail. The proposed modification would eliminate the need to realign this segment of the Houston Lake Loop Trail and would shift the proposed grade separated crossing of the Houston Lake Loop Trail (Crossing 4) approximately 420 feet north of the previous location. The currently proposed alignment is illustrated on Figure 1 in Attachment A. The Project Team will continue to work with FAA regarding design needs within the three mile radius where construction restrictions apply (see Attachment A, Figure 3), and will report any significant design changes to consulting parties.

Wetland complexes between Crossings 4 and 6 have restricted the locations of crossings (see Attachment A, Figure 3). In consultation with the Project Team, the Corps and EPA requested that there be no increases in the project footprint in wetlands areas to accommodate a widened embankment area. As a result, the Project Team has worked to keep the rail embankment as low as possible to minimize impact in wetlands areas. In the case of the Iron Dog Connector Trail (an officially recognized trail), adding a grade-separated crossing would require raising the embankment high enough to provide 12' to 14' of clearance for trail users and would increase the fill footprint of the embankment over several miles. Adding an at-grade crossing would increase the impact to wetlands as the crossing would require additional embankment at the approaches to the proposed railroad crossing.

Land ownership and lack of legal easements are also challenges. The Project Team must consider property ownership when planning crossing locations, so as to not promote trespass or provide a crossing that may not be usable into the foreseeable future. This is a particular concern between Crossings 1 and 3 and Crossings 7 and 10 (see Attachment A, Figure 3). The MSB is currently in discussions with UA and MHLT to attempt to establish easements for some of the currently used trails in the project area (e.g., the Iditarod Race Trail). See Section 4.3 for further discussion.

### **4.3 Implementation Measures based on Workshops/Meetings**

The Project Team has tried to strike a reasonable balance between the needs of trail users, obligations to property owners and State and Federal agencies, and safety considerations. Based on comments received at the IDSHD workshops and MSB Open House, participants seemed to agree that the Project Team had made good progress toward resolving the reported issues and that these proposed measures would avoid, minimize, or mitigate adverse effects on the IDSHD.

These comments received during the workshops, and measures to address them, are summarized in Table 3 (Summary of IDSHD Workshop Comments/Issues and ARRC Mitigation Commitments), and crossing details are included in Table 4 (Trail/Crossing Summary). Identified issues and ARRC commitments are discussed further in the following section.

Several comments/issues were resolved during the IDSHD workshops and MSB Open House. They require no further implementation and/or commitment and are not discussed further in this *Implementation Plan*. These comments/issues are summarized in Table 3 and include:

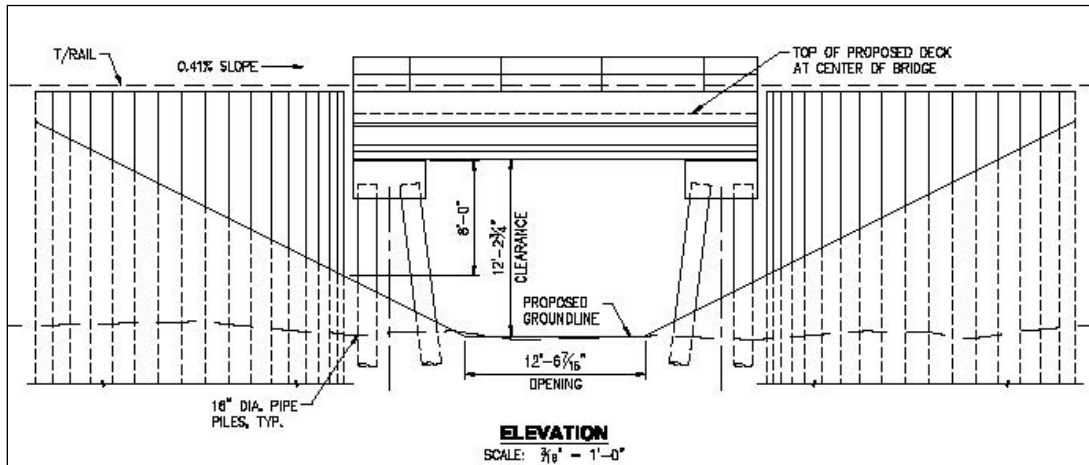
- Providing participants with a constraints map illustrating where crossings cannot be located (Issue 6);
- Adding a crossing for the trail between current Crossings 12 and 13 (Issue 13);
- Holding a meeting with other trail users and postponing Workshop 3 until after that meeting (Issue 17);
- Providing better/more detailed information on each crossing for the larger trail user meeting (Issue 18); and
- Explaining the process for how the final decision will be made on the crossings (Issue 20).

The following discussion summarizes the key commitments made by the Project Team in response to comments/issues raised by workshop participants. Most of these commitments will be implemented during final design and construction, or as indicated in Table 3. The exhibits/figures provided below are based on 60% design and are not final, so some refinements may occur. Changes to crossings and reroutes from what is described/illustrated below will be reported to PA Signatories, Invited Signatories, and other interested consulting parties and posted on the project website.

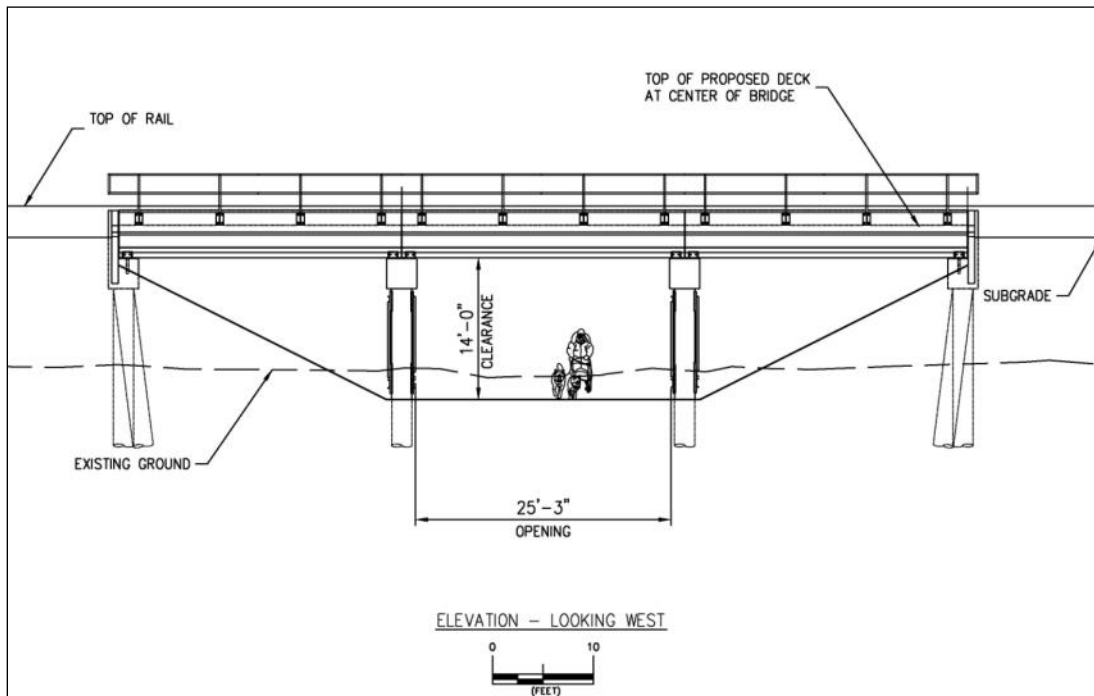
***Redesign bridges/crossings with the vertical clearance increased to a minimum of 14' whenever possible (Issue 1)***

Workshop participants stated that the height (vertical clearance) of crossings needs to be increased to more than 12' to account for snow depth, accommodate equipment for grooming of trails, and enhance safety. The initial clearance presented at Workshop 1 was as low as 10' for some crossings. The Project Team committed to redesigning bridges/crossings with the vertical clearance increased to a minimum of 14', whenever possible. Vertical clearances will range from 12' to over 18' (see Table 4). Only two crossing structures, located south of the Agricultural District, will have a vertical clearance less than 14'. Baker Farm Bridge (Crossing 16) will be a three-span bridge providing between approximately 12' vertical and 25' horizontal clearance. This crossing is adjacent to the Baker Farm Road at-grade crossing (Crossing 17), which provides an alternative crossing option in the vicinity. The Figure 8 Lake Loop trailhead crossing (Crossing 18) will be accommodated by a 19' diameter plate pipe, providing between approximately 9' and 14' vertical by 16' horizontal clearance. A bridge crossing in this location is not practicable due to the poor soil conditions and high-railroad embankment through the area. Alternative access for over-height vehicles is provided by the access road from Lu Young Lane, south of the Bulk Material Bi-modal Facility (BMBF).

Exhibit 1 shows a comparison of the Iditarod NHT crossing (Crossing 10) proposed at Workshop 1 and the currently proposed crossing that has been refined based on comments from trail users. The vertical clearance was increased from approximately 12' to 14'.



June 2011 Planned Crossing

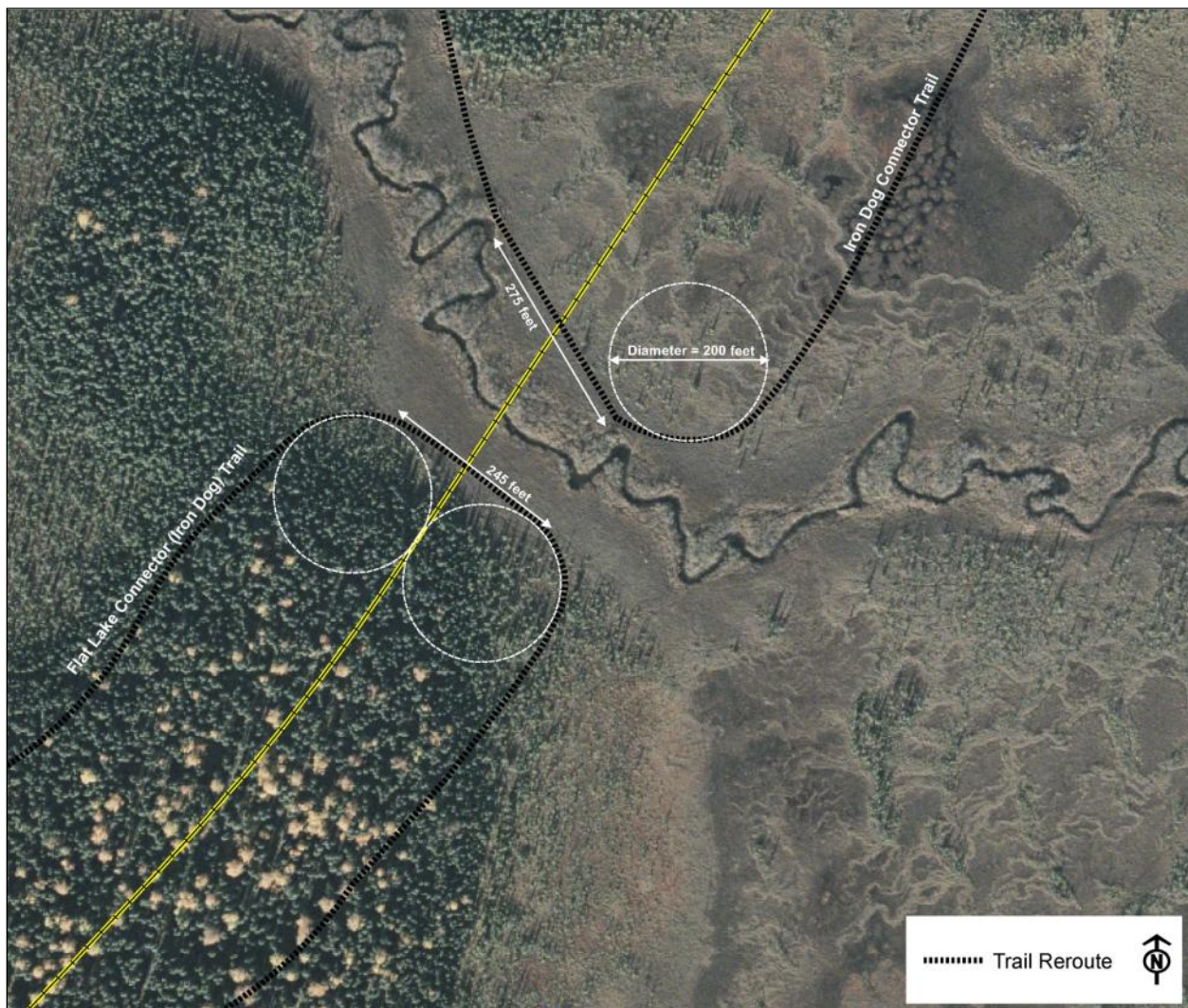


December 2011 Planned Crossing

Exhibit 1: Iditarod NHT crossing design before and after the IDSHD Workshops

**Develop standards for sight distance needs and crossing approaches to accommodate a 100'-long vehicle/dog team (minimum 60' turn radius; Issue 2)**

As stated by workshop participants, in reference to the rerouted trails (specifically the separated-grade crossing at Milepost (MP) K22.8 Tributary of the Little Susitna River [Crossing 5]), the length of a sled dog team with 16 to 20 dogs (approximately 85') needs to be considered during design of trail approaches to crossings to ensure curves are not too tight. Exhibit 2 illustrates typical design changes to approaches to address line of sight issues for trail reroutes. The Project Team worked with Sustainable Design Group to develop standards for sight distance needs and crossing approaches. A 100' long vehicle/dog team requires a minimum 60' turn radius. The design standards for trail approaches to crossings include gentle curves with turn radii between 75' and 90', which will allow for a 100' long dog team to turn and safely pass through the structure while allowing adequate line of sight for trail users prior to passage.



**Exhibit 2: Approach example**

***Redesign bridges/crossings with minimum of three spans to provide a minimum of 20' of horizontal clearance and provide relatively straight trail approaches (Issue 3)***

As presented at Workshop 1, some proposed crossings were single span bridges, and participants stated that these types of crossings do not provide adequate visibility to see if anything is approaching from the other direction. The participants recommended that the length of these crossings be increased and trail approaches to crossings be designed to allow adequate line-of-sight. The Project Team eliminated the 28' single-span concrete ballast deck (CBD) bridges that were originally proposed for some trails. Bridges are now a minimum of three spans, and the spans designated specifically for existing and or rerouted trails will generally provide a minimum of 20' of horizontal clearance (Table 4). Adequate visibility/line-of sight will also be provided with relatively straight trail approaches to these longer bridges with higher vertical clearance (minimum of 14' whenever possible). The December 2011 design for the Iditarod NHT crossing (Crossing 10) is typical of design changes undertaken by the Project Team to address workshop participants' concerns (see Exhibit 1). Exhibit 2 illustrates typical design changes to approaches to address line of sight issues for trail reroutes.

***Consider future development and population growth during project design (Issue 4)***

Participants commented that planning for at-grade road crossings (e.g., West Susitna Parkway and Ayrshire Avenue) should take population growth and resulting development into consideration, as conflicts between users could increase. They were concerned that at-grade trail crossings could potentially be turned into road crossings as growth and development increases. The Project Team confirmed that designs for trail crossings are not adequate for roadways, and trail crossings will not be turned into road crossings. The project includes sufficient road crossings to address needs identified in the MSB Long Range Transportation Plan (LRTP), and future growth is being considered during design and ROW acquisition for roads such as West Susitna Parkway and Ayrshire Avenue. The Project Team developed and will implement design standards for trail crossings that are separated from the roads at at-grade crossings.

***Easements do not exist but are needed for some trails (Issue 5)***

Participants noted that easements are not currently in place for some trails that they regularly use. These easements are needed to avoid possible trespass situations and to ensure that as population increases, trails are not sold into private ownership/development. In situations where ARRC proposes a trail crossing at a location where a legal trail easement does not currently exist, the MSB is requesting easements from adjoining landowners to ensure that trail use associated with the crossing structure is authorized and that future trespass is avoided. MSB Community Development and Land Management continues to work with trail users, agencies, and land owners to obtain easements for the Iditarod Race Trail, the Houston Lake Loop Trail and Big Lake Trail #2. MSB is also working with agencies to try to secure easements or other legal instrument for recognized trails that are proposed to be rerouted as part of the project including the Iron Dog Connector Trail and Flat Lake Connector (Iron Dog) Trail. If MSB is unsuccessful in acquiring legal easements, trails will be relocated to an area where an easement can be obtained such as along section line easements or re-routed through the nearest MSB-owned property in an effort to maintain existing trail connectivity and provide safe, legal crossing opportunities along the new rail line.

Negotiations with UA and MHLT are ongoing and the Project Team has worked with these land owners to minimize trail reroute impacts while also emphasizing that trail easements are of the



upmost importance to preserve access. The Project Team is currently in negotiations with UA to obtain an easement for the Iditarod Race Trail. This process will take several months and requires MSB assembly approval (anticipated to occur at the end of 2012). The Project Team has been working closely with MHLT on the relocation of trails and easements. No estimate of time is currently known for this process.

***Provide additional crossings between Crossings 2 and 10 to maintain connectivity (Issue 7)***

Limiting the number of crossings and relocating and/or combining trails in the area between Crossings 2 and 10 may “bottleneck” multiple user groups onto fewer trails, thereby increasing the potential for collisions and user conflicts (e.g., between snow machiners or other users and mushers). The Project Team has tried to strike a reasonable balance between the needs of trail users, our obligations to property owners and State and Federal agencies, and safety considerations. We have investigated inclusion of additional grade-separated crossings between Crossings 2 and 10, and have been working with user groups, agencies, and landowners to find a solution.

Safety is a significant concern to both the Project Team and IDSHD workshop/meeting participants and is a constraint for crossing locations. Unfortunately, the area between Crossings 2 and 10 does not present good opportunities for separated grade crossing structures (e.g., bridges) because of safety concerns (e.g., visibility issues caused by topography and vegetation), regulatory issues related to wetlands impacts, design issues associated with a high water table in the area, property ownership, existing property/easement constraints, and construction restrictions associated with the FAA VORTAC radar site (see Section 4.2).

Much of the terrain in the area between Crossings 2 and 10 is undulating with patches of trees making it difficult for users to see an oncoming train. A train cannot stop quickly, and it generally takes one mile or more for a train to come to a stop. Sight distance for train operators is also a potential safety issue in areas where a curve in the tracks, dense vegetation, and topography may affect visibility. In addition, some users may be wearing helmets and listening to headphones while traveling in the area, or be operating loud vehicles, thus impeding their ability to hear a train coming.

The Project Team will continue to investigate inclusion of additional crossings between Crossings 2 and 10. At this time, however, the Project Team believes that we have identified all possible crossings in this area and we make no commitments regarding additional crossings between Crossings 2 and 10. Two crossings have been added between Crossing 11 (Outflow of Diamond Lake) and Crossing 16 (Baker Farm Bridge) to improve connectivity and safety in that area (see Section 4.4).

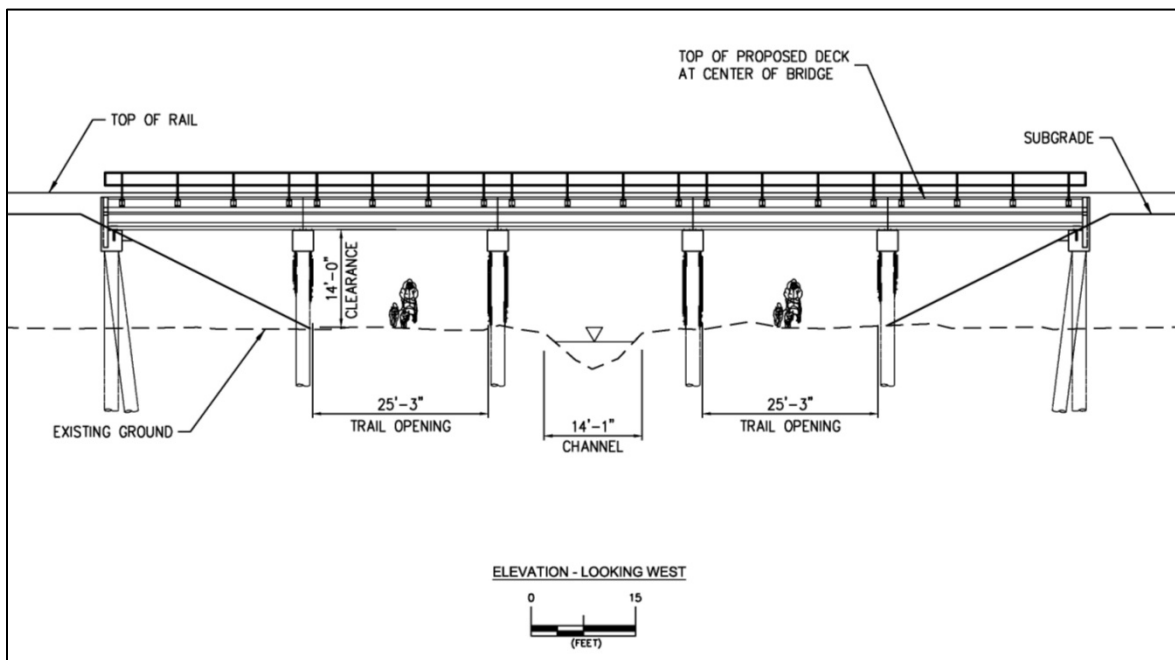
***Provide a crossing for the Iron Dog Connector Trail and the Flat Lake Connector Trail (Issues 8 and 9)***

Participants requested that the Project include crossings for the Iron Dog Connector and Flat Lake Connector trails. The Iron Dog Connector Trail (Big Lake Trail #5) is identified in MSB trail plans as a winter use only trail that traverses wetlands and lakes. Due to safety concerns, wetland and regulatory concerns, tree cover, and topography, a crossing is not a good option at its current location (see Section 4.2).

The Flat Lake Connector Trail (official Iron Dog trail) is a well-established trail with a legal easement that needs an at-grade crossing and other improvements, such as cutting trees at the

crossing to allow a wider field of view for approaches. A minimum height of 14' to allow for snow depth, accommodate equipment for grooming of trails, and enhance safety cannot be achieved at this location; no more than 10' of clearance is possible without interfering with the shallow underlying groundwater table. An at-grade crossing cannot be used at this location due to safety concerns; the available line of sight is minimal due to area topography even if trees are cleared.

To meet trail user crossing needs and allow for a safe crossing, the Project Team proposes to reroute the Iron Dog Connector and Flat Lake Connector trails to a multi-use crossing structure at MP K 22.8 over an unnamed tributary to the Susitna River (Crossing 5; Figure 1). This five-span bridge will have approximately 25' of horizontal clearance for three spans and a minimum of 14' of vertical clearance (Exhibit 3).

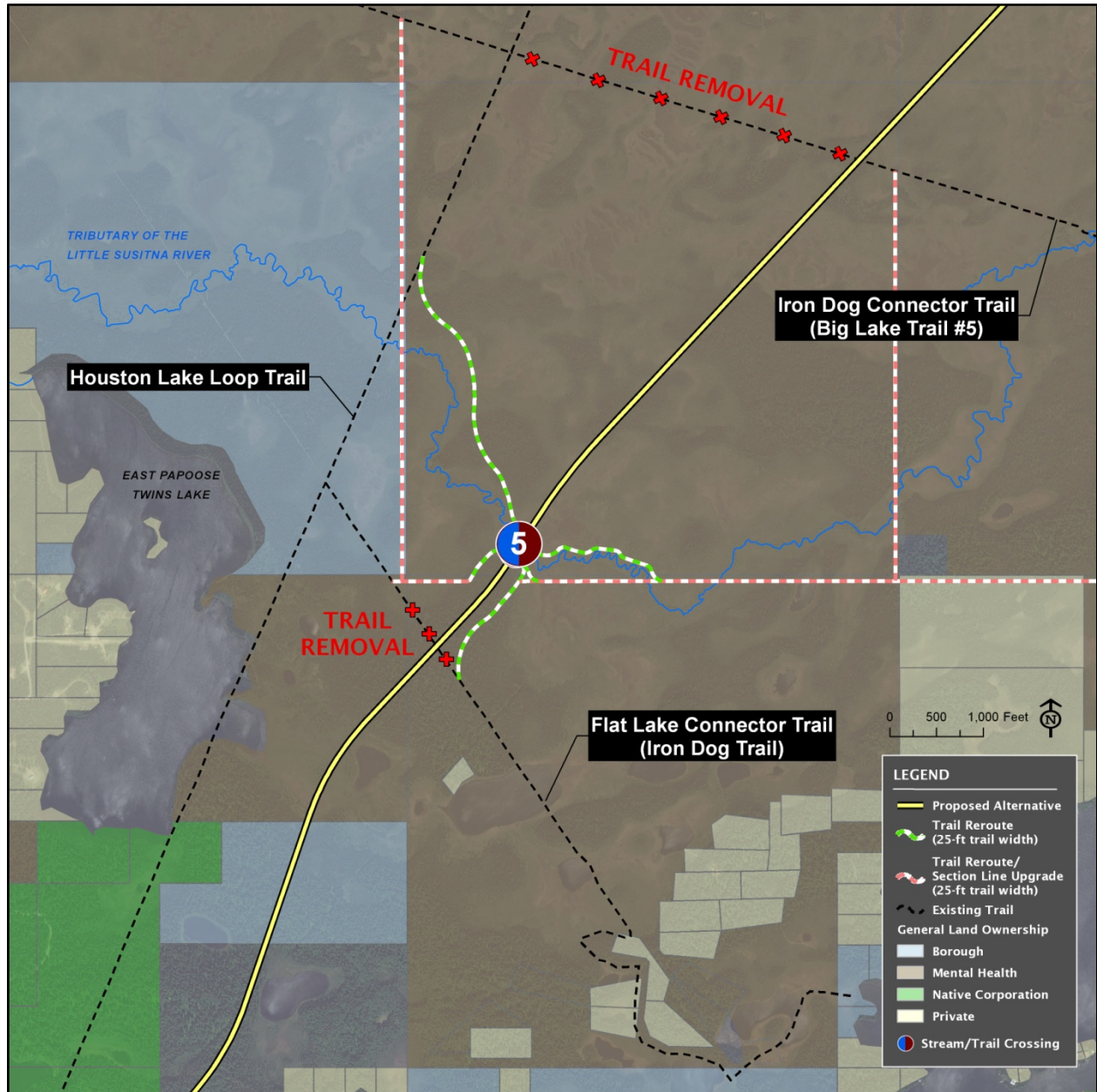


**Exhibit 3: Crossing 5 profile view**

The design of the reroutes will allow for sufficient line of sight for all users and traffic will be accommodated by two parallel trails, one on each side of the creek. The Iron Dog Connector trail will be rerouted under the northern span of the bridge and will be reconnected with the Houston Lake Loop Trail north of the rail embankment (Exhibit 4). The MSB worked with the Iron Dog Race Director on the Iron Dog Connector Trail reroute to Crossing 5. The MSB will work with the Race Director and/or Race officials to provide adequate safety coverage at this location during races.

The Flat Lake Connector trail will be routed under the southern span and will be reconnected with the existing trails west of the rail embankment (Exhibit 4). MHLT previously issued a six year revocable license for the Flat Lake Connector Trail. MSB is currently working with the MHLT to try and establish legal easements for both of these trails and their planned reroutes. We are confident that the proposed reroutes will address the needs and concerns of trail users and

the MHLT. If MSB can not acquire an easement for the Iron Dog Connector and Flat Lake Connector trail reroutes, the Project Team will notify workshop participants and provide a revised proposal for continued access and connectivity prior to final design and construction.



**Exhibit 4: Proposed Iron Dog and Flat Lake Connector Trail Reroutes and Crossing (Crossing 5)<sup>15</sup>**

<sup>15</sup> This exhibit does not reflect all the trail reroutes in the area; it is focused on the reroutes to Crossing 5.

***Incorporate safety considerations into design for Crossing 5 (MP K 22.8 Unnamed Tributary to the Little Susitna River; Issue 10)***

All officially recognized trails in the APE have planned crossing structures with the exception of three trails that are planned to be rerouted under nearby bridges: 1) Big Lake Trail #5 - Iron Dog Connector Trail, 2) Flat Lake Connector - Iron Dog Trail, and 3) the Houston Lake Loop Trail. These trails will be rerouted to Crossing 5 (MP K 22.8, Unnamed Tributary to the Little Susitna River; see Exhibit 4).

The Project Team has developed design standards for Crossing 5 and these reroutes to provide safe crossings (e.g., increased horizontal and vertical clearance for better line of sight). Crossing 5 has been modified since Workshop 1 based on input from participants. The original design for Crossing 5 was a three-span bridge with 12'-7.5" of vertical clearance, two spans with 14' horizontal clearance, and a center span with over 25' of horizontal clearance. The design presented in the draft *Implementation Plan* for Crossing 5 included vertical clearances of more than 17' and horizontal clearances of more than 25'. However, the current vertical clearance will be a minimum of 14' to reduce wetland impacts. The structure will accommodate a trail on both the north and south side of the creek, each with a horizontal clearance of more than 25'. Exhibit 3 shows the most recent proposed design for Crossing 5.

The Project Team is committed to continuing access and connectivity in association with these trails by obtaining easements for the reroutes. The design of the reroutes will allow for sufficient line of sight for all users and traffic will be accommodated by two parallel trails, one on each side of the creek. The trail groomer will be able to maintain access on the north and south spans of this crossing structure via existing trail connectivity. Crossing approaches will follow standards for sight distance needs that will accommodate a 100' long vehicle/dog team (i.e., turn radii between 75' and 90'; see Exhibit 2). Trails will be aligned to provide increased site distance. Trail embankments will be designed to minimize impacts from overflow.

***Provide crossing for the Iditarod Race Trail; obtain an easement from the University of Alaska (Issue 11)***

Workshop participants stated that the Project needs to provide a crossing for the Iditarod Race Trail (Crossing 9) and obtain an easement from the University of Alaska for this trail should one not currently exist. It was learned during the course of the workshops/meeting that an easement does not currently exist for the Iditarod Race Trail. This trail has reportedly hosted 20 Iditarod races and is used more frequently/regularly than the Iditarod NHT. Participants indicated that a separated grade crossing (bridge) is preferred, but if a bridge is not possible, an at-grade crossing would be better than no crossing at all.

Approaches to a crossing for the Iditarod Race Trail would be on UA land. Since no easement currently exists at this location, the MSB continues to work with UA to secure an easement. This issue is anticipated to be resolved by the end of 2012. The proposed Iditarod Race Trail crossing (Crossing 9) would be a typical three-span bridge with a minimum 14' vertical clearance, and approximately 25' of horizontal clearance in the center span (see Exhibit 1 as an example of a typical three-span bridge). This crossing will involve excavation and grading of the trail to obtain 14-feet of vertical clearance and 20' of horizontal clearance at the current crossing location. Approach improvements would include widening the trail in the vicinity of the crossing.

The Project Team is committed to continuing access and connectivity in association with the Iditarod Race Trail, a trail that IDSHD workshop/open house participants indicated was important to the IDSHD. If negotiations with UA to secure an easement are unsuccessful, a crossing cannot be provided for the Iditarod Race Trail at its current location as it would encourage trespassing on UA property. The Iditarod Race Trail would instead be rerouted to the Iditarod NHT, which would be upgraded/improved to accommodate the potential increase in users (Exhibit 5).

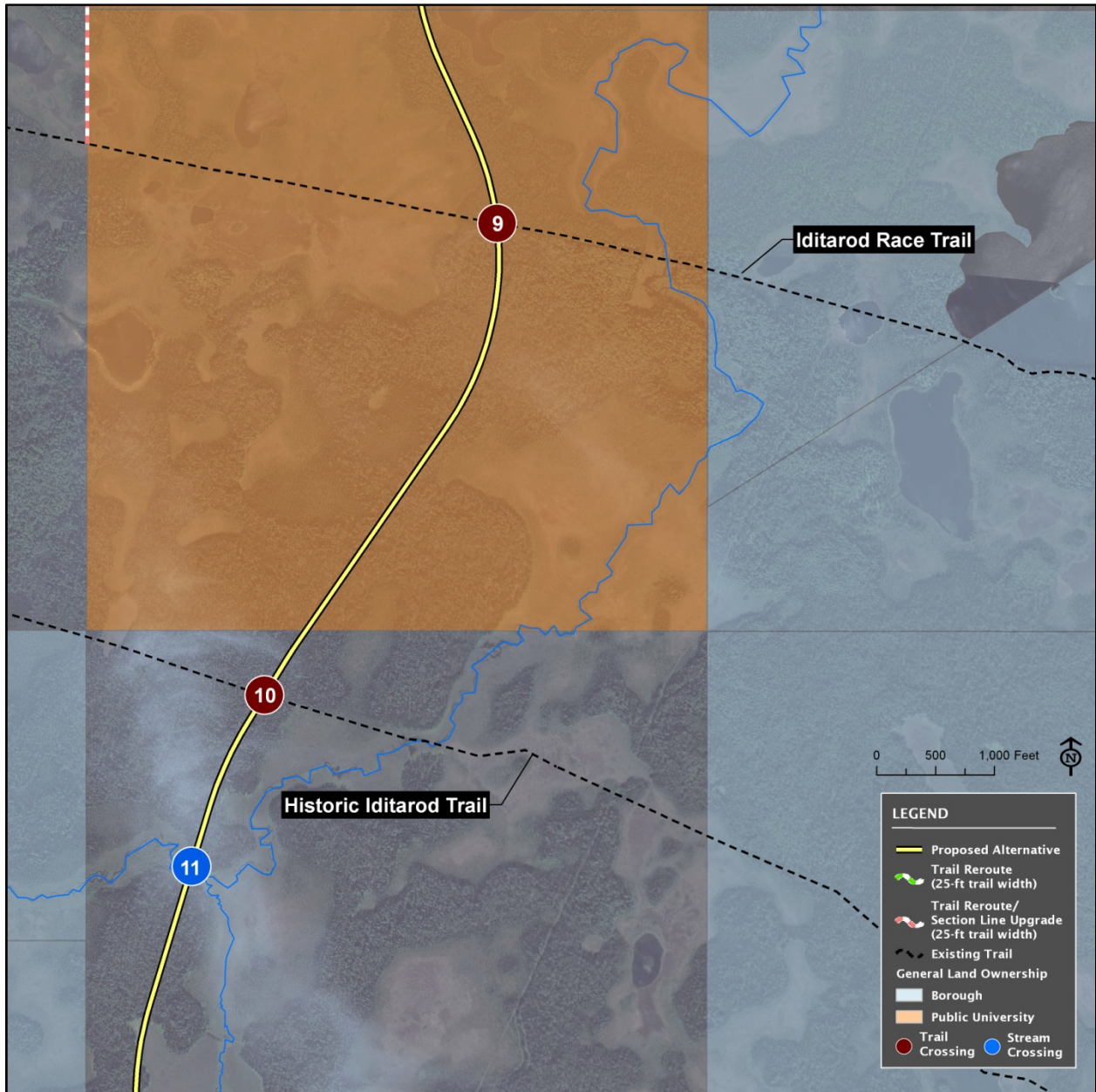


Exhibit 5: Possible Iditarod Race Trail Reroute and Crossings

***Provide separated grade crossing (bridge) and trail improvements for the Iditarod NHT (Crossing 10; Issue 12)***

IDSHD workshop participants stated that the project needs to provide a separated grade crossing (bridge) and trail improvements for the Iditarod NHT (Crossing 10). The Iditarod NHT is reportedly not used as frequently as the Iditarod Race Trail and needs improvement if it is to be regularly used. According to participants, it is currently too narrow for current and potential increased use and the portion of the trail west of the crossing location near the Little Susitna River washes out and is in ill repair.

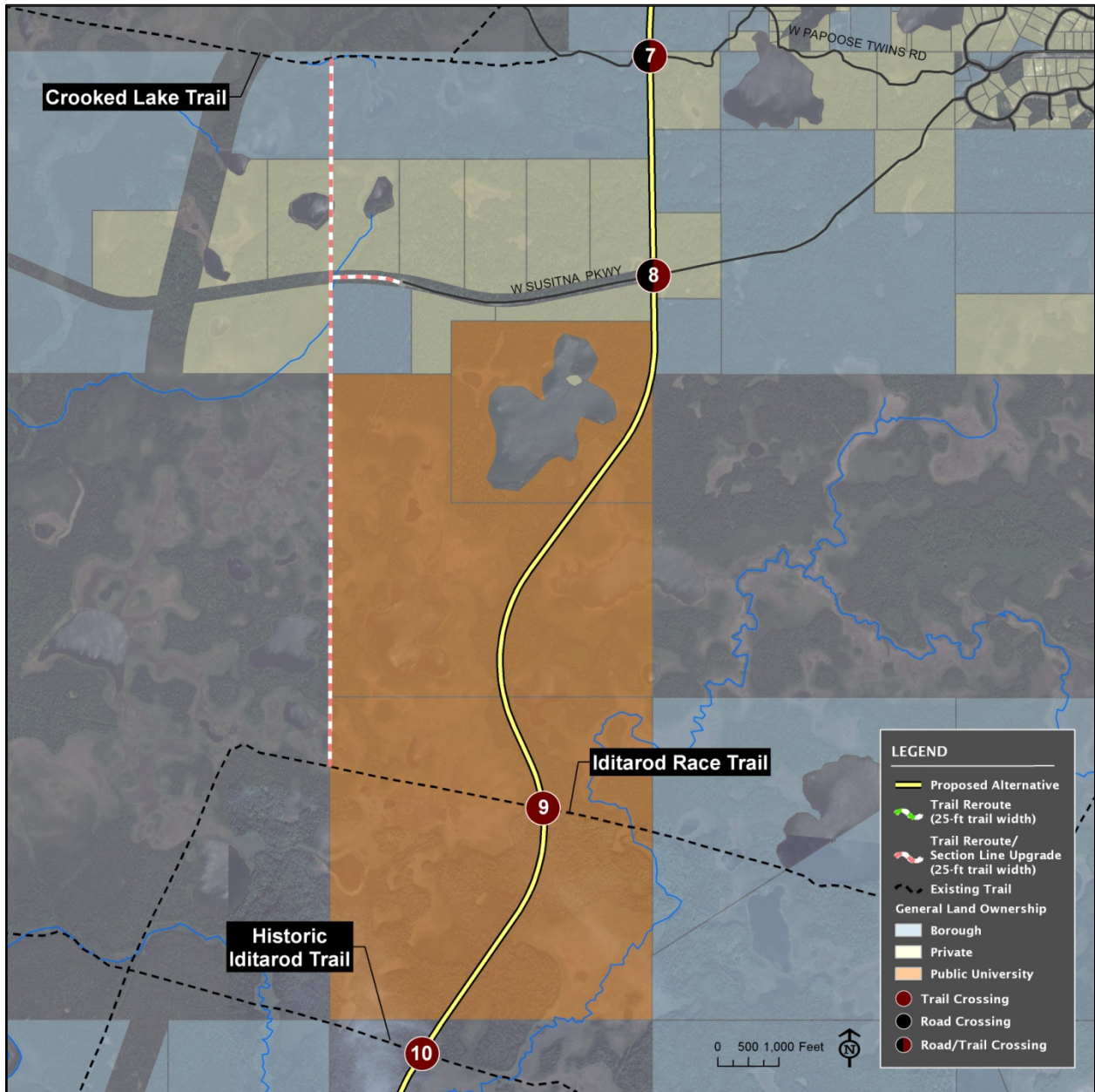
The Project Team will provide a separated grade crossing for the Iditarod NHT (Table 4 and Attachment A, Figure 1). Although the original crossing designs for this trail included a single span crossing with 12' of vertical and 14' of horizontal clearance, the crossing structure was redesigned to include a three-span bridge with 14' of vertical clearance and over 25' of horizontal clearance (see Exhibit 1 for a comparison of early and current crossing design). Although only the central span of the bridge will be passable for trail users, the north and south spans increase visibility for potential oncoming traffic. The Iditarod NHT will be improved where it passes beneath the rail embankment using standards for sight distance needs and crossing approaches to accommodate a 100' long vehicle/dog team (i.e., turn radii between 75' and 90'; see Exhibit 5).

***Provide a trail(s) along one or both sides of the proposed ROW to maintain connectivity (Issue 14)***

Participants stated during the IDSHD workshops/meeting that the project should include a north-south trail(s) along one or both sides of the proposed ROW to help maintain connectivity. They indicated that even if there is not a dedicated trail, people would still likely travel along this area.

The Project Team evaluated participants' recommendation for a north-south trail along one or both sides of the proposed ROW to help maintain connectivity. Trail reroutes, enhanced crossing structures and strategies to maintain connectivity in a safe manner are all measures the Project Team has and continues to work toward as this project moves forward. The proposed railroad alignment is orientated predominately north-south, and most of the trails identified both in the Final EIS and IDSHD workshops that provide north-south movement will remain intact. Providing additional north-south corridors along one or both sides of the rail line has many challenges such as FAA VORTAC issues, safety concerns, wetland and regulatory concerns, land ownership, tree cover, topography and cost of constructing additional bridge structures (see Section 4.2). Based upon current north-south connectivity (Attachment A, Figure 4) and the issues identified above, a new trail(s) paralleling the proposed ROW for the project is not planned.

The existing trail system west of the proposed rail corridor provides a north-south corridor (Attachment A, Figure 4). However, one area of concern for north-south connectivity has been identified between Crossings 7 and 10 (Exhibit 6). This area demonstrates the biggest gap in connectivity and is owned primarily by UA. The Project Team has developed an alternative corridor for north-south travel in this area that may offer a solution for trail users and property owners (see Exhibit 6 and Attachment A, Figure 4). We will continue to work with local trails groups/users, regulatory agencies, land owners and other interested parties to maintain connectivity in the IDSHD.



**Exhibit 6: Connectivity between Crossings 7 and 10**

***Provide adequate signage to warn trail users (Issue 15)***

Participants requested use of adequate signage to warn trail users. They suggested the Project Team consider using standard snow machine/dog sledding trail signage as seen on Alaska State Parks Snowmobile Trail Advisory Committee (SnoTRAC) website.

The Project Team has developed design standards for signage and kiosks to aid trail users. Appropriate signage would be placed at key points on trails, trail reroutes, and at crossings and could include: trail direction (e.g., North/South arrows), “Caution - Dog Teams on Trail,” “High

Traffic Area,” “Clearance 14’,” “Slow – Keep Right,” “Trail Closed Ahead,” or “Slow – Trail Rerouted.”

Examples of signage are provided in Exhibit 7, and are posted on the project website (<http://www.portmacrail.com/library.html> ). To address user concerns, the Project Team will develop signage that is generally consistent with the Alaska State Parks SnoTRAC guidelines.

Kiosks with trail maps will be placed at key locations and will aid trail users by identifying trail reroutes and showing available trail connections. An example of a kiosk located on the Willow trails system is illustrated in Exhibit 8. A proposed design for the kiosks is illustrated in Exhibit 9.

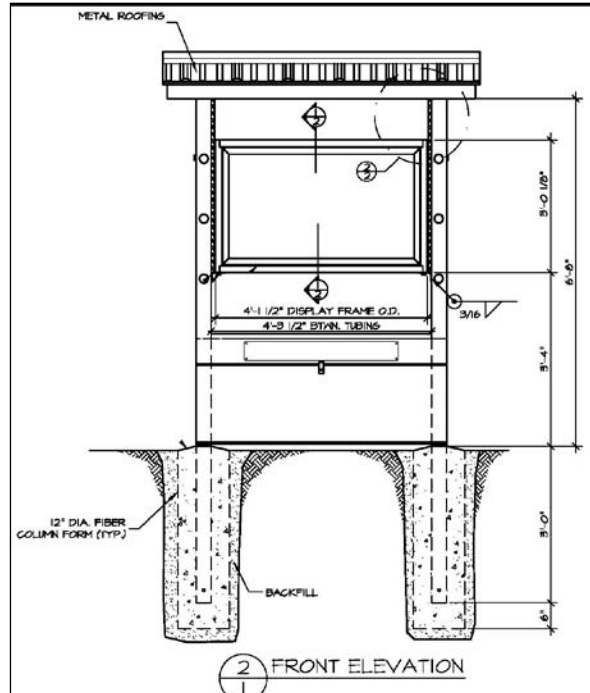


Exhibit 7: Signage examples presented at the MSB Trail User Open House (10/27/11)





**Exhibit 8: Willow Trail System Kiosk**



**Exhibit 9: Kiosk schematic (Department of Natural Resources)**

***Consider creating a special use district (SPUD) or formalizing designation of the IDSHD for the NRHP (Issue 16)***

IDSHD workshop participants asked the Project Team if the IDSHD would work the same way as the Knik Sled Dog and Recreation Special Land Use District (SPUD) in the MSB. Participants wanted to know if creating a SPUD for the IDSHD area would require future development in the area to go through a formal review process similar to what is required under Section 106 of the NHPA. Participants asked about delineating an IDSHD SPUD similar to the Knik SPUD to create a formal procedure/process for review of projects planned in the IDSHD area and/or to solidify/formalize designation of the IDSHD for the NRHP.

The Knik SPUD was implemented to preserve and protect the existing community of Knik and the homestead lifestyle (includes outdoor, historical, traditional recreational and agricultural uses), protect legal trails, and to provide for planning and growth in the Knik area. The Knik SPUD ordinance implements land use regulations to ensure compatibility between various land uses and encourage developers to recognize and dedicate legal trail ROWs where appropriate. For example, developers are required to identify legal and known trails that will be impacted by their project and work with MSB and the community to protect or relocate those trails. Any new SPUD or expansion of an existing SPUD would have to be initiated by a Community Council and approved by the MSB Assembly.

The IDSHD was found eligible for the NRHP as a result of Section 106 (NHPA) review carried out as part of the National Environmental Policy Act (NEPA) process (see Section 3.2). A NRHP nomination is currently underway for the Iditarod NHT which may include the portion of the trail in the project area. Development using Federal funds or requiring a Federal permit

and/or oversight would require potential developers to go through the Section 106 review process, which includes identifying historic properties (i.e., cultural resources eligible for the NRHP) in the project area, assessing effects to historic properties from the project, and resolving adverse affects. Consultation (i.e., seeking, discussing, and considering the views of interested parties, and seeking agreement when feasible) occurs at all stages of the process. As the IDSHD has been found eligible for the NRHP, Federal agencies would be required to consider the impact of any projects in the area on the IDSHD and avoid, minimize or mitigate any potential adverse effects. Listing and/or nomination of the IDSHD for the NRHP would provide no additional protection or review requirements than are currently in place.

Consultation with SHPO identified the need for a clearly defined IDSHD boundary and guidelines for identifying contributing elements to the IDSHD to facilitate Section 106 consultation for entities considering future development. As currently defined, the IDSHD presents challenges for future undertakings in the general project area. ARRC, in consultation with SHPO and the Working Group, will address these challenges by preparing a separate report that refines the boundaries of the IDSHD, focusing on areas that best retain features representative of the IDSHD, and developing guidance for identifying and documenting property types that could be contributing elements. The draft report will include:

- Brief overview/description of the IDSHD (e.g., name, site number, general location/setting, ownership, function/use, types of associated resources, period of significance, area of significance);
- Summary of consultation and studies done to date regarding the IDSHD;
- Brief overview of the historic context (based on SRB&A 2010) – what did the district look like/use during its period of significance (includes maps and photographs as appropriate);
- Explanation of why/how the district is significant/eligible for the NRHP;
- Discussion of the features that a contributing element to the district would have (e.g., what types of features, physical characteristics of these features that provide association with the district, important aspects of integrity needed to maintain association with the district);
- Identification of district boundaries (e.g., method for identifying the boundary, description of the boundary, justification for the boundary, maps); and
- Summary form that includes the essential elements of NRHP nomination form 10-900.

The draft report/form will be submitted to STB and SHPO for review and comment within one year of finalizing the *Implementation Plan*. Following a 30-day review and comment period, ARRC will address comments received as appropriate and submit a final report/form. Interested consulting parties will be notified that the final report is available and it will be posted on the project website.

***Ensure that trails in the MSB trails plan have continued connectivity (Issue 19)***

Participants noted during the IDSHD workshops/meeting that when the project was in the earlier phases of the EIS, the Project Team stated that trails in the MSB trails plan would have continued connectivity. Participants questioned if this was still the case.

Although certain east-west trails through the area will change, east-west connectivity will be maintained. The Project Team has been working with various trail user groups to identify and address access/connectivity concerns. We believe we have addressed the major concerns to insure continued access for mushers as well as other users, consistent with the need to maintain connectivity across the rail corridor in the IDSHD. In fact, the project will enhance connectivity by resolving multiple ROW issues on existing trails, ensuring that they will be maintained for the long-term.

As discussed in Section 3.3, the Project Team has committed to providing crossings where the proposed rail line would cross an officially recognized trail or relocate officially recognized trails that could not be provided with crossings. The Project Team has maintained throughout the EIS process that we will not provide crossings for “unofficial trails.” The Project Team has stated in the IDSHD workshops/meeting that trails that cannot be maintained in their current location will be rerouted. If there is a legal easement for the trail (e.g., the Iditarod NHT), the trail location will be maintained and a crossing will be provided in its current location if possible. In addition, the Project Team will work with regulatory agencies and or property owners to attempt to obtain similar legal access and easements for any rerouted trail (e.g., the Flat Lake Connector and Iron Dog Connector trails).

***Consider potential conflicts with moose during project design (Issue 21)***

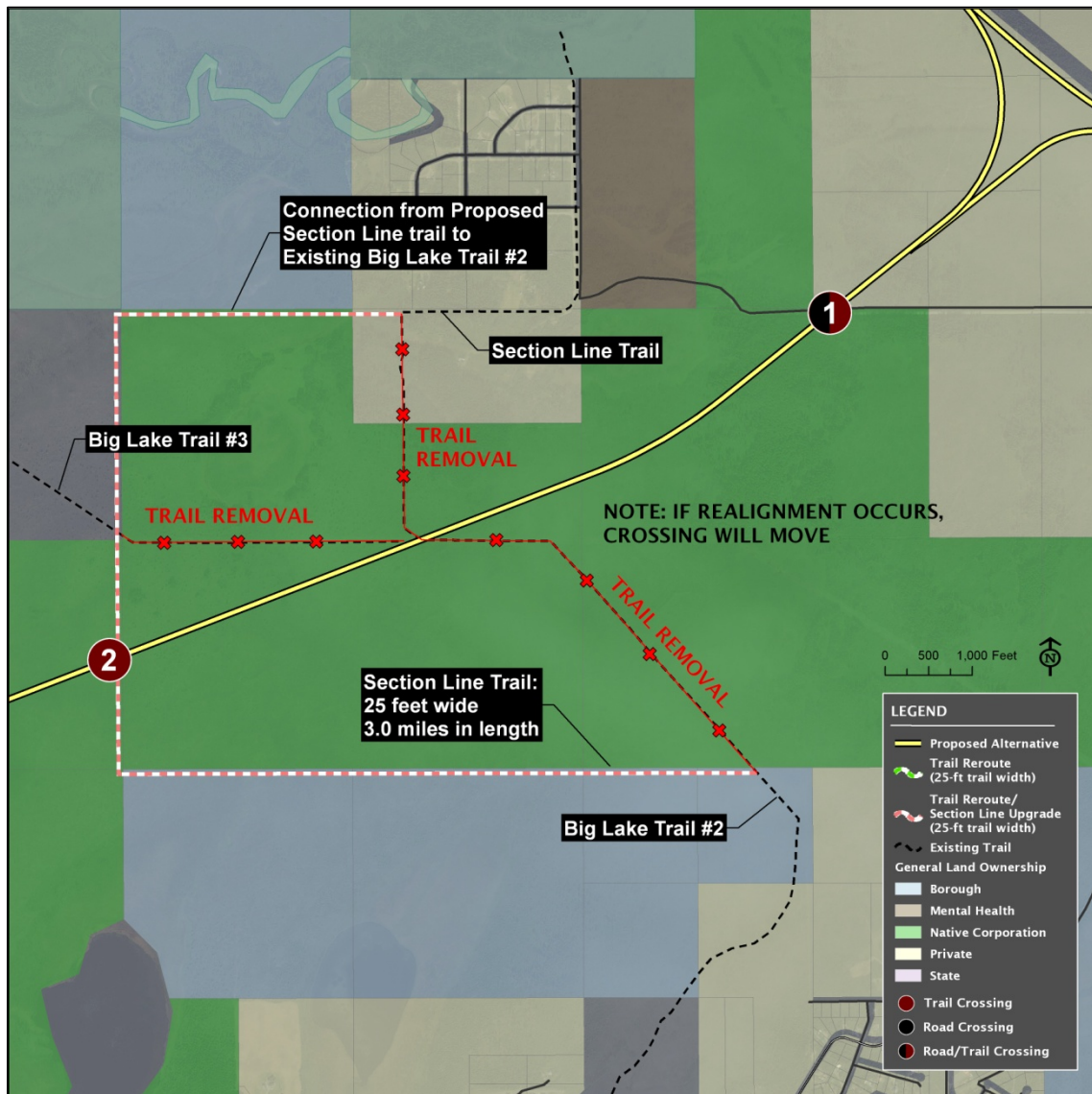
Workshop and Open House participants stated that moose will also use the project crossings, and that funneling wildlife and trail users onto fewer crossings/trails would increase conflicts between users and wildlife. Moose often go up and over the railroad embankment, although they may also use the crossings. On many trails, moose are unlikely to be present due to the presence of human users. The Project Team has developed design standards that will minimize the potential for trail users and wildlife such as longer bridges and larger bridge openings to improve visibility. The Project Team believes that longer bridges, with wider horizontal and vertical openings, will improve visibility for trail users and reduce the potential for wildlife conflicts. Moose conflicts on the existing trail system are not common and the number of moose encounters with dog teams, snowmachines or other user groups is not expected to increase as a result of this project. The length and width of the crossing structures provides ample space for moose and other wildlife to move away from the rail embankment.

**4.4 Additional Implementation Measures**

Following the IDSHD workshops/meetings and submittal of the draft Implementation Plan to consulting parties, there have been modifications to the proposed project design. Currently there are 18 planned crossing locations identified for the project (Figure 1). While the majority of these crossings are designed to accommodate existing or relocated trails, three of the crossings (Crossings 6, 11 and 12) are designated as stream crossings and are not connected to the established trail system. However, these crossings have been designed to meet the height and width requirements to support multiple-user groups in the project area (Table 4). Additional trail relocations and crossings are summarized below.

**Additional proposed trail/crossing relocations**

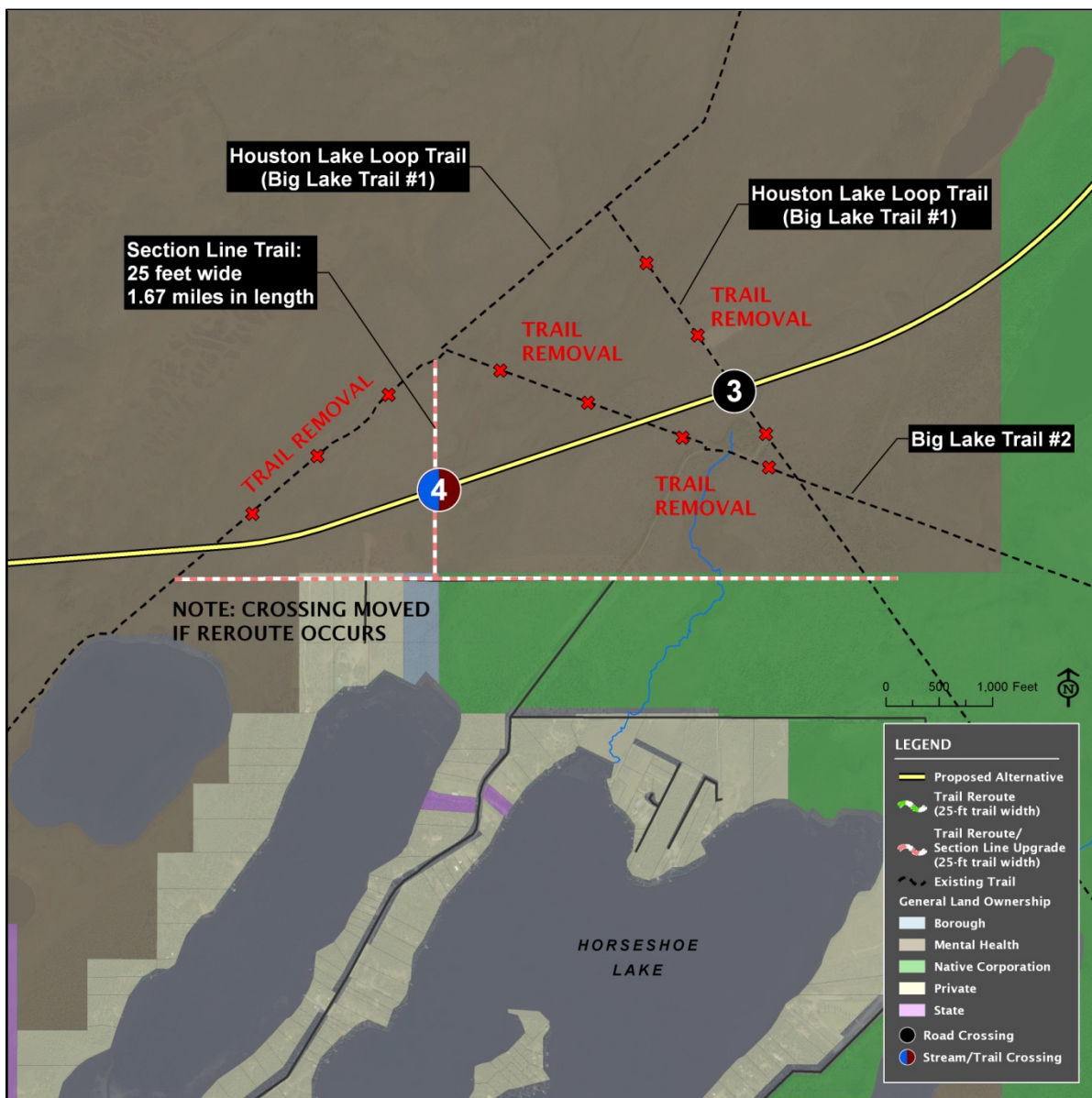
Big Lake Trail #2 is located in an existing Matanuska Electric Association (MEA) power line license on Knikatnu Corporation lands<sup>16</sup>. To facilitate continued connectivity/access for this trail and avoid conflicts between trail users, the land owner and MEA, the Project Team has proposed rerouting Big Lake Trail #2 to a section line easement adjacent to the existing trail. This rerouted trail would use Crossing 2, an at-grade crossing, which would be moved west of its current location to the section line easement where it crosses the proposed rail alignment (see Exhibit 10 and Table 4). Grade separation is not feasible at this location due to the relatively short distance to the Millers Reach Road at-grade crossing (Crossing 1) and the need for the MEA power lines to cross.



**Exhibit 10: Big Lake Trail #2 Reroute**

<sup>16</sup> Cook Inlet Regional Incorporated [CIRI] owns subsurface rights to this parcel.

Big Lake Trail #1 is currently located on the Gas Pad Access Road associated with Crossing 3 (see Figure 1 and Table 4). The Houston Lake Loop Trail is currently located on MHLT to the north of an unnamed lake west of Little Horseshoe (or West) Lake. Currently, extensive portions of the Houston Lake Loop Trail are in trespass, including the location of the previously proposed bridge crossing (Crossing 2). Accordingly, the MSB has proposed a new crossing location and trail reroute that is along an existing section-line easement, allowing for legal, safe access across the new rail line (Exhibit 11). There will be a separated grade crossing at MP K 27.9 for the relocated trail that will include a four-span bridge with the second span providing approximately 14-feet of vertical and over 25-feet of horizontal clearance. The second and third spans will provide additional line of sight for approaching trail users and will convey a small anadromous fish stream.



**Exhibit 11: Houston Lake Loop Trail Reroute**

### ***Additional crossings***

Since the draft *Implementation Plan*, two crossings have been added to improve connectivity and safety. The first crossing is in Segment 3 between the proposed bridge at Crossing 11 (Outflow of Diamond Lake) and the Ayrshire Avenue at-grade crossing (Crossing 13). Several cleared seismic survey lines converge in the vicinity of MP K 15.2, between Crossings 11 and 13, and are reportedly used as unofficial recreational trails. The Project Team anticipates the area may be subject to unauthorized crossings at this location and wants to allow for safer crossings at this location. In an effort to dissuade ARRC ROW trespass and ensure a safe crossing opportunity in a location where crossings are expected, even though the cleared lines are not within a formal trail plan, the Project Team proposes to install an oversized culvert structure to maintain access for recreational users of these trails (Crossing 12; see Table 4 and Attachment A, Figure 1).

A second crossing has been added in Segment 2 at Reddane Avenue (Crossing 15) within the Point MacKenzie Agriculture District (Attachment A, Figure 1). Reddane Avenue (Crossing 15) is a farm road that numerous landowners in the area have requested remain open and unobstructed due to both safety and traffic concerns. This crossing will be grade separated (railroad over the road) due to the landowner concerns and grading required for the Terminal Reserve. No trail is associated with this crossing, and this area is reportedly not heavily used by recreational trail users; however, the crossing will include accommodations for recreational users (see Table 4).

**Table 3: Summary of IDSHD Workshop Comments/Issues and ARRC Mitigation Commitments**

Issue #	Comment/Issue Summary	ARRC/Project Team Mitigation Commitment	Implementation
1	Increase height/vertical clearance of grade-separated crossings to 12'+	Grade-separated trail crossings will provide a minimum vertical clearance of 14', with two exceptions: Crossings 14 (~12') and 16 (10-14') (see Table 4 for crossing details).	Design standards have been developed (see Exhibit 1) and communicated to the Project Team, and will be implemented during final design and construction of crossings.
2	Consider length of a sled dog team (~85') during design of trail crossing approaches	Trail approaches will allow, at a minimum, a 100'-long team to turn/pass safely at approaches (e.g., Iron Dog Connector Trail and Flat Lake Connector, where trail approaches will be modified).	Design standards have been developed (see Exhibit 2) and communicated to the Project Team, and will be implemented during final design and construction of trail approaches to crossings.
3	Consider line of sight during crossing design; single-span bridges do not provide adequate line of sight	Bridges will include a minimum of 3 spans (20'+ horizontal clearance) and trail approaches to these crossings will allow adequate line of sight (see Table 4 for crossing details).	Design standards have been developed (see Exhibit 1) and communicated to the Project Team, and will be implemented during final design and construction of crossings.
4	Consider growth and development in crossing design	At-grade trail crossings will not be turned into road crossings. Officially recognized trails co-located with roadways will include separated crossing panels for the trails along the roads (see Table 4 for crossing details).	Design standards have been communicated to the Project Team and will be implemented during final design and construction of crossings.
5	Obtain easements for trails that do not have them	The Project Team/MSB will continue efforts to secure easements for certain existing trails and rerouted legal trails (Big Lake Trail #5 - Iron Dog Connector Trail, Flat Lake Connector - Iron Dog Trail, Houston Lake Loop Trail, Big Lake Trail #2, and Iditarod Race Trail).	The Project Team /MSB is working with land owners to obtain easements for the specified existing trails in the project area that do not currently have legal easements and for recognized trails that are proposed to be rerouted as part of the project.
6	Provide participants with constraints map illustrating where crossings cannot be located	NA – Resolved. A constraints map was presented at the MSB Open House (Attachment F) and IDSHD Workshop 3 (Attachment G). The map was posted on the project website ( <a href="http://www.portmacrail.com/maps/Constraints_v2%20web.jpg">http://www.portmacrail.com/maps/Constraints_v2%20web.jpg</a> ) and is included in Attachment A (Figure 3).	No further implementation needed.
7	Add crossings between Crossings 2 and 10 to maintain connectivity	The Project Team will continue efforts to identify additional crossings between Crossings 2 and 10. Constraints for crossings in this area include: safety concerns, regulatory issues, design issues, property ownership, and property/easement constraints.	The Project Team is working with landowners, agencies, and trail users to identify additional crossings in the project area between Crossings 2 and 10.
8 and 9	Provide crossing for the Iron Dog Connector Trail and the Flat Lake Connector Trail	The project will include a crossing and reroute for the Iron Dog Connector Trail and the Flat Lake Connector Trail (Crossing 5).	The Project Team/MSB is working with MHLT to acquire trail reroute easements (Exhibit 4). If MHLT does not grant an easement for the reroute, the Project Team will notify workshop participants and provide a revised proposal for continued access and connectivity.

<b>Issue #</b>	<b>Comment/Issue Summary</b>	<b>ARRC/Project Team Mitigation Commitment</b>	<b>Implementation</b>
10	Incorporate safety considerations into design for Houston 6.3 crossing (Crossing 5)	Crossing 5 will incorporate safety considerations, including trail crossings on either side of creek that exceed minimum design standards (see Table 4 for crossing details).	These design standards have been developed (see Exhibits 3 and 4) and communicated to the Project Team and will be implemented during final design and construction of Crossing 5.
11	Provide crossing for the Iditarod Race Trail; obtain an easement from the University of Alaska	A crossing and reroute (if needed) will be provided for the Iditarod Race Trail. If an easement is granted for the trail, the crossing will exceed minimum safety/design standards (see Table 4 for crossing details).	The Project Team/MSB is working with UA to obtain an easement for the Iditarod Race Trail. If an easement can not be obtained at its current location, Crossing 9 will be eliminated and the Iditarod Race Trail will be routed to the Historic Iditarod Trail and Crossing 10 (see Exhibit 5).
12	Provide separated grade crossing (bridge) and trail improvements for the Iditarod NHT (Crossing 10)	A separated grade crossing will be provided for the Iditarod NHT that exceeds minimum safety/design standards (see Table 4 for crossing details).	Standards for crossings have been developed (see Exhibits 1 and 2) and communicated to the Project Team and will be implemented during design and construction of Crossing 10 and its approach.
13	Add a crossing for the trail between current Crossings 12 and 13	NA – Resolved. Based on consultation during the IDSHD Workshops/MSB Open House, this former trail is on private agricultural land and is no longer used. Therefore, no crossing will be provided at this location. Other crossings are available for use including a separated-grade crossing at an unnamed stream at MP K5.5 (Baker Farm Bridge, Crossing 16), an at-grade crossing at Baker Farm Road (Crossing 17), and a culvert at the Figure 8 Lake Loop Trail (Crossing 18).	No further implementation needed.
14	Provide trail along one or both sides of the proposed right-of-way (ROW) to maintain connectivity	The Project Team believes that the existing trail system west of the proposed rail corridor provides an adequate corridor for north-south connectivity with one exception (see Attachment A, Figure 4). The project will include development of an additional corridor between Crossings 7 and 10 (Exhibit 6) to fill this gap in north-south connectivity. Continued connectivity and accessibility will be considered during design of crossings and trail reroutes and the Project Team will continue to work with users to maintain connectivity.	The importance of continued connectivity and accessibility has been communicated to the Project Team and will be addressed to the extent practicable as part of final design of crossings and trail reroutes.
15	Provide adequate signage to warn trail users	Signage posted as part of the project will be consistent with SnoTRAC guidelines, and kiosks will be placed at key points on trails, trail reroutes, and crossings.	Standards for signage have been developed (see Exhibits 7-9) and communicated to the Project Team and will be implemented during design and construction.



Issue #	Comment/Issue Summary	ARRC/Project Team Mitigation Commitment	Implementation
16	Consider creating a special use district (SPUD) or formalizing designation of the IDSHD for the NRHP	ARRC, in consultation with SHPO and the Working Group, will prepare a report that refines the boundaries of the IDSHD and develops guidance for identifying and documenting property types that could be contributing elements. (Note: designation of a new SPUD/expansion of an existing SPUD must be initiated by Community Council and approved by MSB Assembly. The IDSHD was found eligible for NRHP which is same level of consideration as being listed.)	ARRC will prepare a report to refine IDSHD boundaries and develop guidance for identifying and documenting property types that could be contributing elements to the IDSHD. The report will be developed in consultation with the Working Group, SHPO and IDSHD interests; it will be submitted to STB and SHPO for review and comment within one year of finalizing the <i>Implementation Plan</i> . Following a 30-day review and comment period, ARRC will address comments received as appropriate and submit a final report to STB, SHPO and appropriate consulting parties.
17	Hold a meeting with other trail users and postpone Workshop 3 until after this meeting	NA – Resolved. MSB sponsored a Trail User Open House (see Section 2.2.1) that included participants from multiple trail user groups. Comments/questions from participants were compiled, responded to, and posted on the project website ( <a href="http://www.portmacrail.com/library.html">http://www.portmacrail.com/library.html</a> ) following the meeting. Workshop 3 was completed following this meeting (see Section 2.2.1 and Attachment G).	No further implementation needed.
18	Provide better/more detailed information on each crossing for the larger trail user meeting	NA – Resolved. The Project Team provided detailed information on crossings, approaches, and reroutes at the MSB Open House (see Attachment F). These maps/graphics include proposed crossing design schematics and aerial maps showing relocations and approaches for each crossing and were posted on the project website ( <a href="http://www.portmacrail.com/library.html">http://www.portmacrail.com/library.html</a> ).	No further implementation needed.
19	Ensure that trails in the MSB trails plan will have continued connectivity	The Project Team will continue efforts to maintain connectivity for official trails such as those included in the MSB trails plan. Official trails that cannot be maintained in their current location will be rerouted, and easements will be acquired for the proposed rerouted trails.	The Project Team/MSB is working with user groups, agencies, and land owners to maintain connectivity for official trails and acquire easements for relocated trails.
20	Explain the process for how the final decision will be made on the crossings	NA – Resolved. The process for decision making for the crossings was explained to Workshop 3 participants (see Attachment G).	No further implementation needed.
21	Consider potential conflicts with moose during project design	The Project Team will design and construct crossings/approaches that will increase visibility for trail users and provide space for moose/wildlife to move away from rail embankment.	Design standards have been developed (see Exhibits 1-3) and communicated to the Project Team and will be implemented during final project design and construction

*This page intentionally left blank*

**Table 4: Trail/Crossing Summary**

Status	Original No. <sup>17</sup>	New No. <sup>18</sup>	Milepost (Segment)	Common Name/ (Trail Name)	Crossing Type	Crossing Description	Comment
Important for Continued Functionality of IDSHD	1	1	K 31.9 (Segment 5)	Millers Reach Road and Trail Crossing	At-Grade (road/trail)	<ul style="list-style-type: none"> <li>• 80' crossing width</li> <li>• 32' road surface</li> <li>• 10' separation, 8-10' trail (north)</li> <li>• crossing panels for road and trail</li> </ul>	Crossing will accommodate an existing heavily used trail on the north side of Miller's Reach Road.
Important for Continued Functionality of IDSHD	2	2	K 30.2 (Segment 5)	Big Lake Trail #2 Crossing (Big Lake Trail #2*)	At-Grade (trail)	<ul style="list-style-type: none"> <li>• minimum 20' crossing width</li> <li>• crossing panels</li> </ul>	Big Lake Trail #2 is located in trespass in an existing MEA power line license on private land. The new trail crossing is planned for an area immediately west of the MEA corridor and along a section line easement.
	3	3	K 28.4 (Segment 5)	Gas Pad Access Road Crossing	At-Grade (road)	<ul style="list-style-type: none"> <li>• 32' gravel drive width</li> </ul>	The existing trail at this location is in trespass and will be relocated to Crossing 4. This road crossing will be for private use only to access a natural gas development area.
Important for Continued Functionality of IDSHD	4	4	K 27.9 (Segment 5)	Big Lake Trail #1 & Houston Lake Loop Trail Crossing (Houston Lake Loop Trail*, Big Lake Trail #1*)	Bridge (trail)	<ul style="list-style-type: none"> <li>• 4-span bridge</li> <li>• 14' vertical clearance (center span)</li> <li>• 25'-3" horizontal clearance (center span)</li> </ul>	No trail easement in place for these trails. The Houston Lake Loop Trail and Big Lake Trail #1 will be relocated to the section line easement west of a gravel (gas) pad on land leased from MHLT.
Important for Continued Functionality of IDSHD	5	5	K 22.8 (Segment 4)	Flat Lake Connector (Iron Dog)/Iron Dog Connector (Big Lake Trail #5)/Houston Lake Loop trail Crossing (Flat Lake Connector Trail-Iron Dog Trail*, Big Lake Trail #5-Iron Dog Connector Trail*, Houston Lake Loop Trail*)	Bridge (stream/trail)	<ul style="list-style-type: none"> <li>• 5-span bridge</li> <li>• Minimum 14' vertical clearance (spans 2 and 4)</li> <li>• Minimum 25' horizontal clearance (spans 2 and 4)</li> <li>• Opening 3 is a stream channel</li> </ul>	No trail easement is in place for these trails. Project Team is currently rerouting trails to section lines and negotiating with MHLT to obtain trail easement for approach to bridge/undercrossing; Project Team will reroute trail if easement not obtained. This crossing (spans 2 and 4) will accommodate the reroute of these trails. Vertical clearance was recently reduced to 14' minimum to minimize wetland impacts.

<sup>17</sup> This column (Original No.) represents the crossing numbers presented at the IDSHD workshops/meetings and the draft *Implementation Plan*.

<sup>18</sup> This column (New No.) represents the revised crossing numbers; this number is used throughout this document.

Status	Original No. <sup>17</sup>	New No. <sup>18</sup>	Milepost (Segment)	Common Name/ (Trail Name)	Crossing Type	Crossing Description	Comment
Additional Crossing – no current trail	6	6	K 20.7 (Segment 4)	Tributary of the Little Susitna River	Bridge (stream)	<ul style="list-style-type: none"> <li>• 5-span bridge</li> <li>• 14'-6" vertical clearance</li> <li>• 25'-3" horizontal clearance (opening 2-4)</li> <li>• Opening 3 is a stream channel</li> </ul>	Not connected to the established trail system. Spans 2-4 designed to meet height/width requirements for access to/along waterway for various user groups and allow continued overland winter travel in the project area.
Important for Continued Functionality of IDSHD	7	7	K 20.4 (Segment 4)	West Papoose Twins Road and Trail ( <i>Crooked Lake Trail-West Papoose Twins Road*</i> )	At-Grade (road/trail)	<ul style="list-style-type: none"> <li>• 52' crossing width</li> <li>• 32' road surface</li> <li>• 10' separation, 10' trail (north)</li> <li>• Crossing panels for road and trail</li> </ul>	
Important for Continued Functionality of IDSHD	8	8	K 19.8 (Segment 3)	West Susitna Parkway	At-Grade (road/trail)	<ul style="list-style-type: none"> <li>• 72' crossing width</li> <li>• 32' road surface</li> <li>• 10' separation, 10' trail (north and south)</li> <li>• 80' of crossing panels for road and trail</li> </ul>	
Contributing to IDSHD	9	9	K 18.0 (Segment 3)	Iditarod Race Trail ( <i>Iditarod Race Trail*, Big Lake Trail #14*</i> )	Bridge (trail)	<ul style="list-style-type: none"> <li>• 3-span bridge</li> <li>• 14' vertical clearance</li> <li>• 25'-2" horizontal clearance (center span)</li> </ul>	No trail easement exists. MSB is working with UA to develop an easement or will reroute trail to Crossing 10 if necessary. Bridge was raised to provide additional vertical clearance for trail grooming equipment.
Contributing to IDSHD	10	10	K 17.1 (Segment 3)	Historic Iditarod Trail ( <i>Iditarod NHT*</i> )	Bridge (trail)	<ul style="list-style-type: none"> <li>• 3-span bridge</li> <li>• 14' vertical clearance</li> <li>• 25'-3" horizontal clearance (center span)</li> </ul>	Bridge was raised to provide additional vertical clearance for trail grooming equipment.
Additional Crossing – no current trail	11	11	K 16.8 (Segment 3)	Outlet of Diamond Lake	Bridge (stream)	<ul style="list-style-type: none"> <li>• 5-span bridge</li> <li>• 18'-5" vertical clearance</li> <li>• 21'-11" horizontal clearance (spans 2 and 4)</li> <li>• 60' horizontal clearance (span 3; stream channel)</li> </ul>	Not connected to the established trail system. Spans 2-4 designed to meet height/width requirements for access to/along the waterway for various user groups and allow continued overland winter travel in the project area.
Important for Continued Functionality of IDSHD		12	K 15.2 (Segment 3)	Unnamed Trail	Oversized Culvert (trail)	<ul style="list-style-type: none"> <li>• 18' diameter culvert</li> </ul>	Seismic survey lines used as unofficial recreational trails. Culvert to allow access for various user groups and allow continued overland winter travel in the project area.

Status	Original No. <sup>17</sup>	New No. <sup>18</sup>	Milepost (Segment)	Common Name/ (Trail Name)	Crossing Type	Crossing Description	Comment
Important for Continued Functionality of IDSHD	12	13	K 13.9 (Segment 2)	West Ayrshire Avenue	At-Grade (road/trail)	<ul style="list-style-type: none"> <li>• 72' crossing width</li> <li>• 32' road surface</li> <li>• 10' separation, 10' trail (south)</li> <li>• 80' of crossing panels for road and trail</li> </ul>	Considerations made for at-grade accommodation of trail vehicles on south side of the road for the annual Susitna 100 dog sled and multi-sport races.
Additional Crossing– no current trail	13	14	K 10.0 (Segment 2)	West Holstein Avenue	Bridge (road/trail)	<ul style="list-style-type: none"> <li>• Road bridge over railroad with approximately 32' crossing width</li> </ul>	Road over railroad; designed to allow access for various user groups.
Additional Crossing– no current trail		15	K 8.0 (Segment 2)	West Reddane Avenue	Bridge (road/trail)	<ul style="list-style-type: none"> <li>• 3-span bridge</li> <li>• 60' width (32' deck with 12' on each side for trails)</li> </ul>	Railroad over road.
Additional Crossing– no current trail	14	16	K 5.5 (Segment 1)	Baker Farm Bridge (Unnamed Stream)	Bridge (stream)	<ul style="list-style-type: none"> <li>• 3-span bridge</li> <li>• 11'-9" to 12' vertical clearance</li> <li>• 19'-7" horizontal clearance (opening 1)</li> <li>• 25'-3" horizontal clearance (opening 2)</li> <li>• 19'-2" horizontal clearance (opening 3)</li> </ul>	Not connected to the established trail system, but designed to allow access to/along the waterway for various user groups and allow continued overland winter travel in the project area.
Additional Crossing– no current trail	15	17	K 5.5 (Segment 1)	Baker Farm Road	At-Grade (road/trail)	<ul style="list-style-type: none"> <li>• Existing road width</li> </ul>	Emergency access route; no current consideration for trails.
Contributing to IDSHD	16	18	K 2.3 (Segment 1)	Figure 8 Lake Loop Trail Crossing (Figure 8 Lake Loop Trail, USGS Transmission Line Trail, Basemap Winter Trail Nos. 4 & 5)	Oversized Culvert (trail)	<ul style="list-style-type: none"> <li>• 146' long multi-plate culvert (19" diameter)</li> <li>• 9' to 14' vertical clearance</li> <li>• 16'-3" horizontal clearance</li> </ul>	A crossing, trail embankment and parking lot improvements are planned for the Figure 8 Loop Trail. Other contributing trails are in the same general area in the Port MacKenzie District. They apparently receive little use for dog sledding and would be consolidated with the Figure 8 Lake Loop Trail crossing.

\* Officially Recognized Trail

*This page intentionally left blank*

## **Attachment A: Figures**

*This page intentionally left blank*



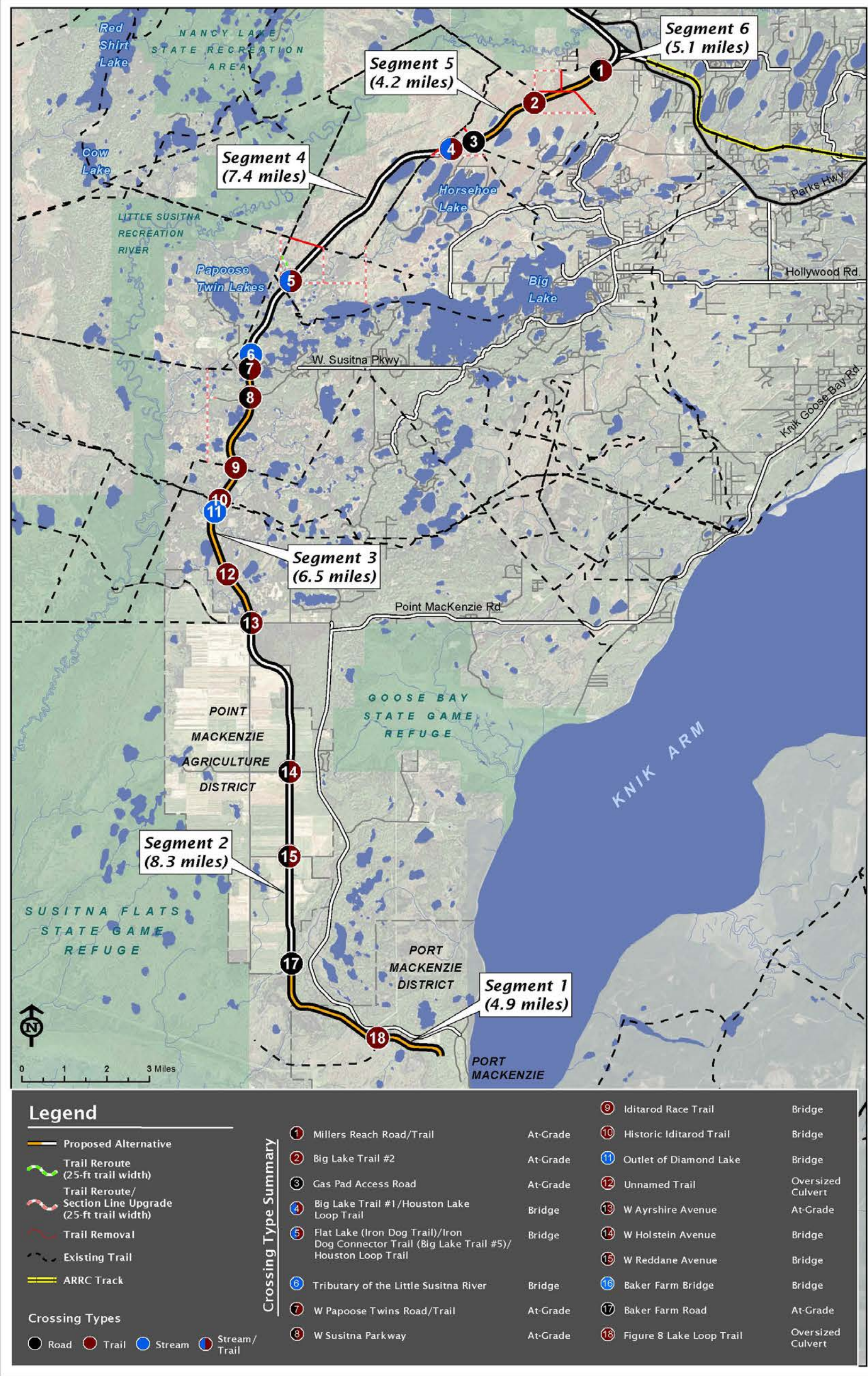


Figure 1: Project overview and crossing locations

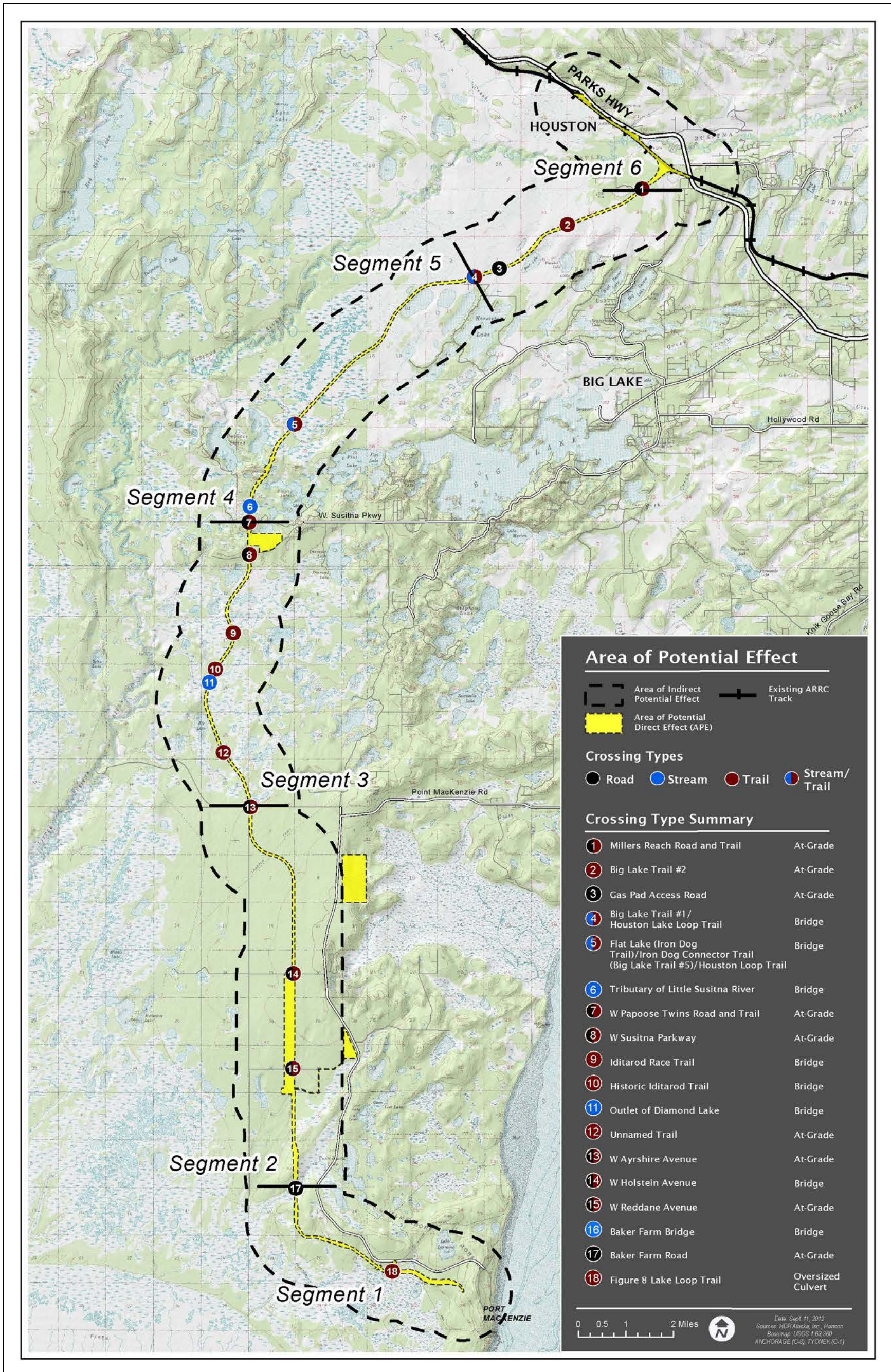


Figure 2: Area of Potential Effect (APE)

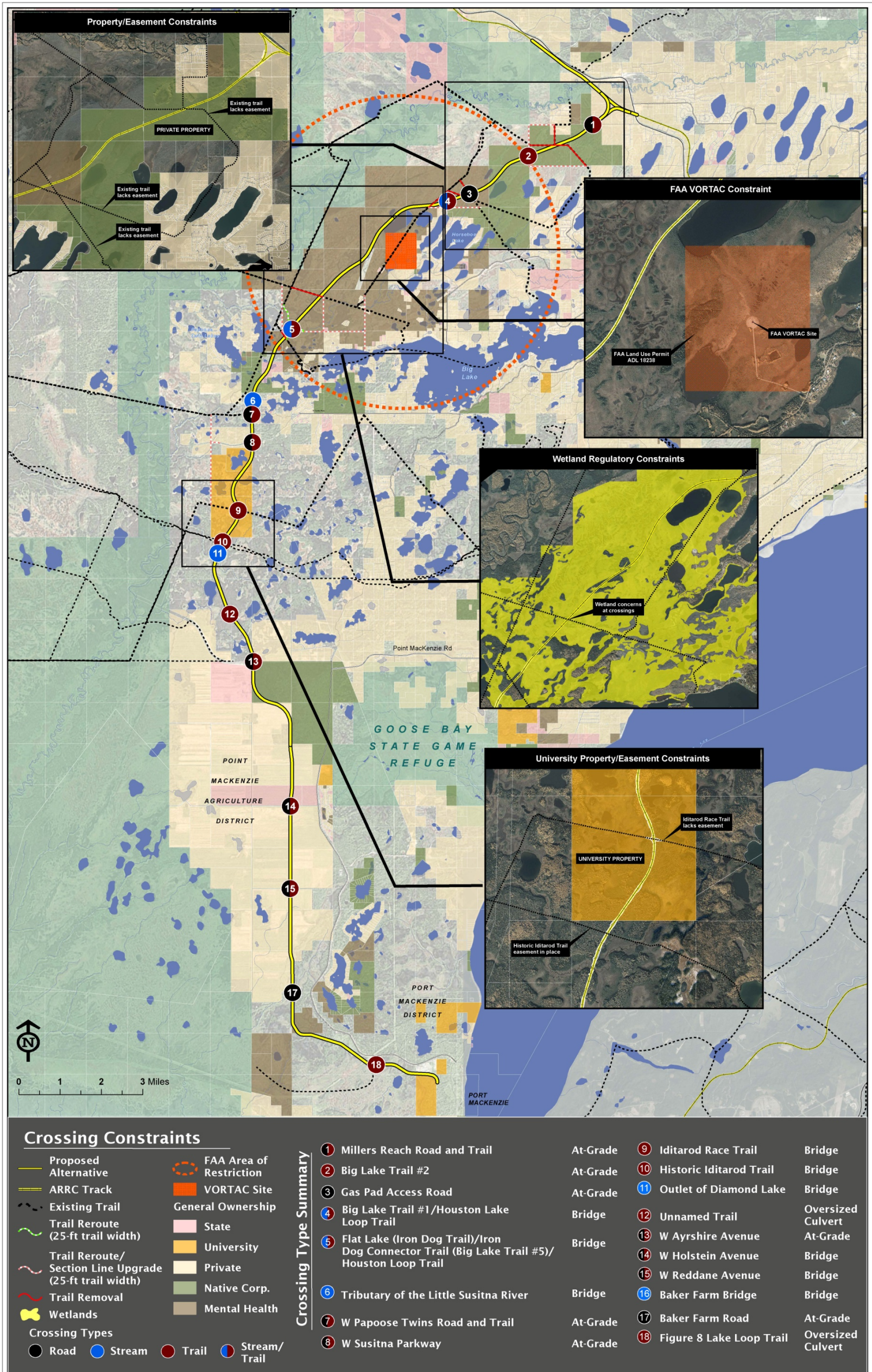


Figure 3: Constraints

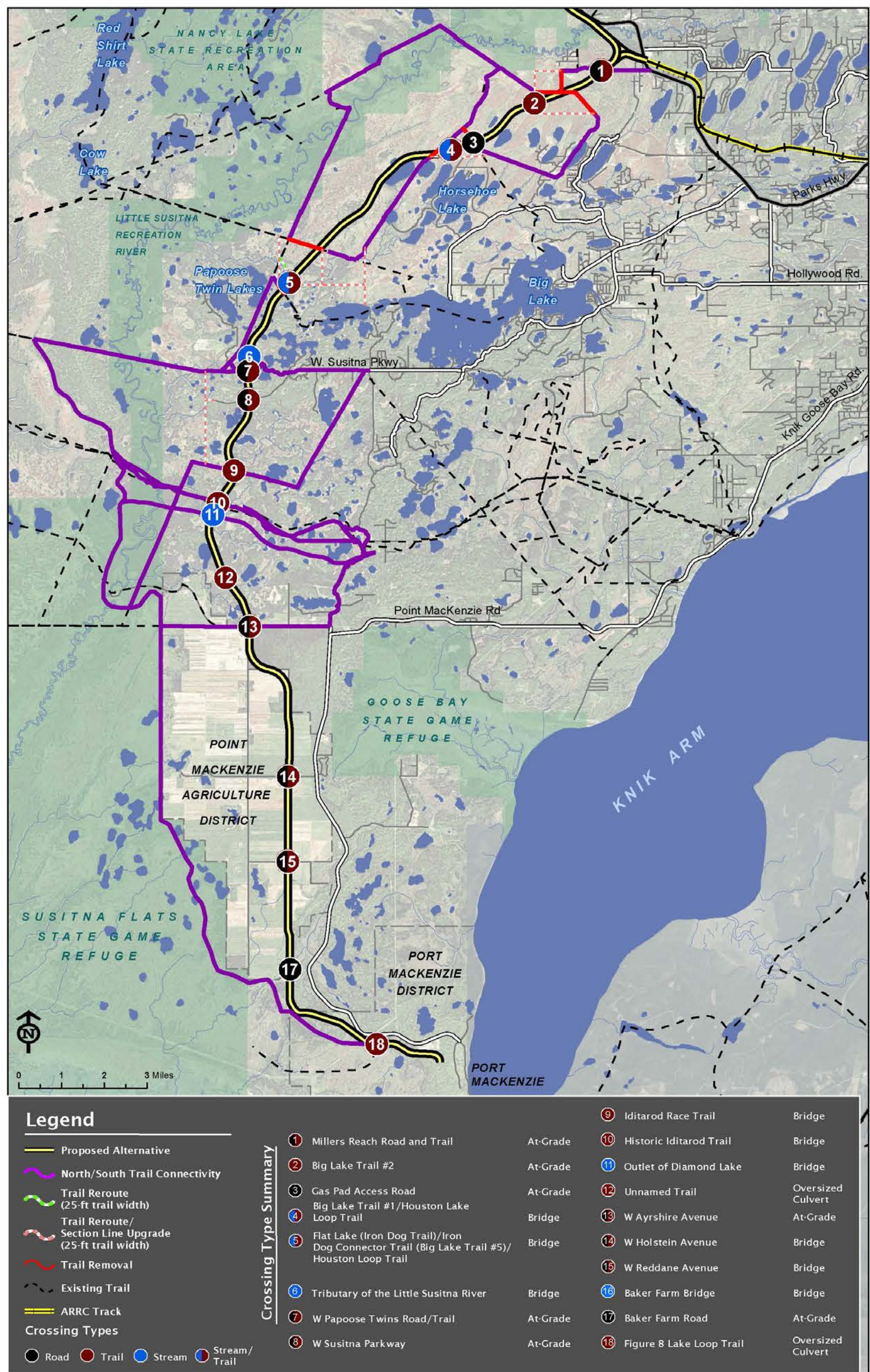


Figure 4: Trail connectivity

## **Attachment B: Glossary**

*This page intentionally left blank*

**Archaeological sites/resources:** Any physical evidence of past human life/activity; generally refers to resources that pre-date written history (i.e. prehistoric period) but may include newer materials; under the Archaeological Resources Protection Act, these are any material remains of human life/activity more than 100 years old

**Area of Potential Effect (APE):** Area where an undertaking may directly or indirectly cause changes in the character or use of a historic property.

**Building:** Any construction created principally to shelter human activity (e.g., house, cabin, barn, church, hotel/motel, shed, store, garage, etc.); should include all basic structural elements

**Character defining feature:** A prominent or distinctive aspect, quality, or characteristic of a historic property that contributes significantly to its physical character. Land use patterns, structures, objects, vegetation, spatial relationships, views, furnishings, decorative details, and materials may be such features.

**Consultation:** The process of seeking, discussing, and considering the views of other participants, and, where feasible, seeking agreement with them regarding matters arising in the Section 106 process (36 CFR 800.16.f)

**Contributing elements/features:** A building, site, structure, or object that adds to historic significance of a cultural resource.

**Cultural resource:** Resource of cultural interest; consist of (alone or in combination) artifacts, sites, districts/landscapes, buildings, structures, etc.; cultural resources are any resource that is of a cultural character (e.g., historic places and cultural sites, prehistoric sites/features, artifacts, documents, traditional places and ways of life, social institutions, etc.)

**Cultural landscape:** A geographic area (including both cultural and natural resources and the wildlife or domestic animals therein) associated with a historic event, activity, or person or exhibiting other cultural or aesthetic values. Cultural landscapes are defined by and understood through multiple characteristics including land use and activities; patterns of spatial organization; response to the natural environment; cultural traditions; circulation networks (i.e., systems of movement); boundary demarcations; vegetation related to land use; buildings, structures and objects; and clusters/groupings of features; archaeological sites; and small-scale elements.

**Direct effect:** An effect caused by the action and occur at the same time and place (40 CFR 1508.8).

**Historic district:** A property category (e.g., site, building, structure, object, district) in the NRHP that possesses a significant concentration, linkage, or continuity of sites, buildings, structures, or objects united historically or aesthetically by plan or physical development (NPS NRHP Bulletin 15)

**Historic property:** Any prehistoric or historic district, site, building, structure, or object included in, or eligible for inclusion in, the National Register of Historic Places (36 CFR 800.16(l)).

**Historic sites/resources:** Any physical evidence of past human life/activity; generally refers to resources that post-date written history (i.e., historic period)

**Indirect effect:** An effect caused by the action and later in time or farther removed in distance, but still reasonably foreseeable. Indirect effects may include growth inducing effects and other

effects related to induced changes in the pattern of land use, population density or growth rate, and related effects on air and water and other natural systems, including ecosystems (40 CFR § 1508.8).

**Integrity:** The authenticity of a property's historic identity, evinced by the survival of physical characteristics that existed during the property's historic or prehistoric period. The seven qualities of integrity as defined by the NRHP are location, setting, feeling, association, design, workmanship, and materials.

**National Historic Trail (NHT):** A designation under the National Parks and Recreation Act of 1978 meant to protect the remains of significant overland routes that reflect national history. National Historic Trails may only be designated by an act of Congress.

**Object:** Constructions that are primarily artistic in nature or small in scale and simply constructed (e.g., monument, sculpture, signs, survey monument, etc.)

**Officially recognized trail:** A recreational trail that has been specifically established within currently adopted plans by DNR and/or MSB or is established within these plans at the time of construction or right-of-way (ROW) acquisition by ARRC or MSB (whichever occurs first). In addition, an officially recognized trail is used primarily for recreational purposes. The locations of officially recognized trails may or may not be provided for by recorded easements or ROW instruments. In some cases, officially recognized trails may be adopted by or mapped in a recognized trail plan, but a recorded easement or ROW instrument may not exist. The presence of a recorded easement or ROW easement is not sufficient alone to make the property an officially recognized trail.

**Significance:** The meaning or value ascribed to a cultural landscape based on the National Register criteria for evaluation. It normally stems from a combination of association and integrity.

**Site:** Location of a significant event, prehistoric or historic occupation or activity, or a building/structure where the location itself possesses historic, cultural, or archaeological value (e.g., campsite, battlefield, village site, designed landscape, rock shelter, shipwreck, etc.)

**Structure:** Constructions usually made for purposes other than creating human shelter (e.g., aircraft, automobile, bridge, dam, water tower, tunnel, etc.)

**Unofficial trail:** Any trail that is not specifically established within currently adopted plans by DNR and/or MSB or is established within these plans at the time of construction or ROW conveyance (whichever occurs first), and whose location is not provided for by recorded ROW or easement.



## **Attachment C: IDSHD Consulting Parties and Participants**

*This page intentionally left blank*

*Port MacKenzie Rail Extension Project  
Iditarod Dog Sledding Historic District Workshop Summary and Implementation Plan*

<b>Name</b>	<b>Organization</b>	<b>6/28/11 Workshop</b>	<b>7/7/11 Focus Group</b>	<b>10/27/11 MSB Open House</b>	<b>1/11/12 Workshop</b>
<b>Workshop/Meeting Participants</b>					
Bud Smyth	Aurora Dog Musers Club	x	x		
Carol Tyler	Aurora Dog Musers Club			x	
Vern Cherneski	Aurora Dog Musers Club		x		
Margaret Billinger	Big Lake Chamber of Commerce			x	
Dan Mayfield	Big Lake Trails		x	x	
Cathy Mayfield	Big Lake Trails			x	
Mike Donald	Big Lake Trails			x	
Cole Donald	Big Lake Trails			x	
Richard Gaffey	Big Lake Trails			x	
Jeff Bruno	DNR			x	
Kyle Kidder	DNR	x	x	x	x
Lesli Schick	DNR	x	x	x	x
Doug Gasek	DNR/SHPO	x	x		
Shina duVall	DNR/SHPO	x	x	x	x
Summer Rickman	DNR/SHPO				x
Judy Bittner	DNR/SHPO, Iditarod Historic Trail Alliance	x	x	x	x
Vern Halter	Dream a Dream Dog Farm	x	x	x	x
Martin Buser	Happy Trails Kennels				x
Ramey Smyth & Becca Moore	Homestretch Kennel	x	x		
Ralph Buzard	Houston Running			x	
Kevin Keeler	Iditarod National Historic Trail Administrator (BLM)			x	x
Terry Langholz	Iditarod Trail Blazers	x	x	x	x
Chas St. George	Iditarod Trail Committee	x	x		
Darrell Davis	Iditarod Trail Committee			x	
Greg Bill	Iditarod Trail Committee	x	x	x	x
Mark Nordman	Iditarod Trail Committee			x	x
Richard Plack	Jr. Iditarod Trail Manager	x		x	
Jon Brautigam	Knik Dog Musers Association, Iditarod Trail Blazers	x	x	x	x
Kelley Griffin	Knik Dog Musers Association	x	x		x
Angie Wade	Knik Tribal Council	x		x	x
Jaik Campbell	Mat-Su Trails Council			x	

*Port MacKenzie Rail Extension Project  
Iditarod Dog Sledding Historic District Workshop Summary and Implementation Plan*

<b>Name</b>	<b>Organization</b>	<b>6/28/11 Workshop</b>	<b>7/7/11 Focus Group</b>	<b>10/27/11 MSB Open House</b>	<b>1/11/12 Workshop</b>
Sue Allen	Northern Lights 300 Dog Race			x	
Cim & Corinne Smyth	Perseverance Springs Farm	x	x	x	
Toby Riddell	Point MacKenzie Community Council			x	
Dale & Jenny Evans	Willow Dog Musers Association	x			
Donna Quante	Willow Dog Musers Association	x			
Sue Morgan	Willow Dog Musers Association	x			
Vic Stanculescu & Tina Owen	Willow Dog Musers Association			x	
Steve Charles	Willow Trial Committee	x	x	x	
Bill Johnson				x	
Chris Kosinski				x	
Elaine & Gene Martin			x		
Ellen Halverson				x	
Jim Clemensen				x	
Kathie Smith		x			
Mike & Anna Stephan		x			
Noreen Austermuhl				x	
Scott Lanene					x
Sharon Berg				x	
<b>Project Team Members Participating in Workshops/Meeting</b>					
Barbara Hotchkin	ARRC	x	x		x
Brian Lindamood	ARRC	x	x	x	x
Brad Sworts	MSB			x	x
Bruce Paulsen	MSB	x	x	x	x
Fran Seager-Boss	MSB	x	x	x	x
Heather Ralston	MSB	x			x
Todd Rinaldi	MSB			x	
Elizabeth Grover	HDR	x	x	x	x
Jessica Manifold	HDR	x		x	x
RaeShaun Schmidt	HDR	x	x	x	
Rosetta Alcantra	HDR	x	x	x	x
Zoe Meade	HDR				x
<b>Invited Parties (did not attend)</b>					
Bob Chlupach	Aurora Dog Musers Club				

*Port MacKenzie Rail Extension Project  
Iditarod Dog Sledding Historic District Workshop Summary and Implementation Plan*

<b>Name</b>	<b>Organization</b>	<b>6/28/11 Workshop</b>	<b>7/7/11 Focus Group</b>	<b>10/27/11 MSB Open House</b>	<b>1/11/12 Workshop</b>
Larisa Meyers-McCoin	Aurora Dog Musers Club				
Kathy Chapoton	Happy Trails Kennels				
Joanne Potts	Iditarod Trail Committee				
Kevin Kastner	Iron Dog				
Harry Caldwell	Knik Dog Musers Association, Knik 200 Organization				
Barb Redington	Knik Dog Musers Association				
Richard Porter	Knik Tribal Council				
Debra Call	Knik Tribal Council				
Helen Hegener	Northern Lights Media				
Jody Simpson	Senator Huggins Office				
Bruno & Joan Bryner	Willow Dog Musers Association				
DeeDee Jonrowe	Willow Dog Musers Association				
Erin McLarnon	Willow Dog Musers Association				
Robert Sexton	Willow Dog Musers Association				
Cindy Bettine					
Dan & Alice Huttunen					
Frank Sihler					
Joe Meehan					
Kevin Saiki					
Lynn McCoin					
Roxann Dayton					
Sam Amato					

*This page intentionally left blank*

## **Attachment D: Workshop 1 Summary and Materials**

[see Attached CD]

## **Attachment E: Workshop 2 Summary and Materials**

[see Attached CD]



**Attachment F: MSB Trail User Open House Summary and  
Materials**

[see Attached CD]

## **Attachment G: Workshop 3 Summary and Materials**

[see Attached CD]

## **Attachment H: Draft Document Reviewer Comment and Response Summary**

*This page intentionally left blank*

<b>Comment #</b>	<b>Comment Date</b>	<b>Comment Author (Agency)</b>	<b>Document Section</b>	<b>Comment/Issue Summary</b>	<b>ARRC Response/Proposed Resolution</b>
1	3/30/12	Catherine Glidden (STB)	General	I think that because of the broadened range of participants, the workshops and comments tended to focus more attention on the current recreational use(s) of the area, than on the impacts of your undertaking to the Iditarod Historic District and contributing resources thereof. The adverse effects to historic properties that would occur as a result of the undertaking are handled almost as a footnote compared with discussion of current recreational uses of the project area for dog sledding. While it is wonderful that you have taken into consideration the concerns of the dog sledders, I think the Implementation Plan should provide a fuller discussion of impacts to historic properties (see Section 4.3 as an example).	<p>Participant discussions and comments at the IDSHD Workshops and MSB Open House focused primarily on access, safety, and connectivity concerns. While Workshop 1 was open to all persons (dog sledders) with an interest in the IDSHD, and Workshop 3 and the MSB Open House was open to trail users in general, Workshop 2 participants self-selected to be part of a focus group to discuss historic trails/features within the IDSHD. Even so, the discussions during Workshop 2 focused on access, safety, and connectivity.</p> <p>In addition, while SHPO concurred that the IDSHD is eligible for the NRHP under Criterion A at the national level of significance for the period of significance of 1967–1978 (i.e., the Iditarod Race and its development), there has been no concurrence on the boundary of and contributing elements to the IDSHD. SHPO has emphasized the need for connectivity and continued functionality of the district for dog sledding.</p> <p>ARRC added/revised text to include a general discussion of potential impacts to the IDSHD as a whole from the project (both adverse and beneficial).</p>
2	3/30/12	Catherine Glidden (STB)	General	For instance, existing trails that do not contribute to the district are discussed at length. This is useful for implementing other conditions in the Board's decision, but the focus of this document should be on the historic properties, impacts of the undertaking to historic properties, and measures that ARRC will put in place to mitigate any adverse effects to historic properties. I would start out with providing a discussion of the various terms used to define a historic district and impacts thereof (what is a contributing vs. non-contributing property?; what is integrity?).	<p>The purpose of this document is to fulfill the requirements of the PA (Stipulation V) and ROD (Mitigation Measures 92 and 93) which are summarized in Section 1.2.</p> <p>In addition, the focus of this effort shifted following the initial Workshops and consultation with SHPO away from discussions of IDSHD (and potential contributing elements) historic significance, integrity, etc. to one of continued access and connectivity of the larger trail system in the project area. Therefore, mitigation was aimed at trying to maintain connectivity and access and ensure safety on these trails to the extent possible.</p> <p>ARRC added/revised text to include a general discussion of what a historic district and cultural landscape are and potential impacts to the IDSHD as a whole from the project (both adverse and beneficial). The revised text also explains why the focus of the document is on access/connectivity rather than individual contributing elements to the district. ARRC also added a Glossary (Attachment B) to provide definitions of terms used in the document.</p>
3	3/30/12	Catherine Glidden (STB)	General	I think you should also explain that causing an adverse effect to a historic property (such as a bridge) is sometimes necessary for safety reasons and/or to ensure the continued use of the bridge for whatever reason(s). However, in most places in the document, it is hard to understand how a planned alteration would impact a given historic property from a Section 106 perspective and/or why an adverse effect was nevertheless viewed beneficially.	See ARRC responses to Comments 1 and 2. Design changes, modifications and refinements of the project to avoid, mitigate, or minimize adverse effects on the IDSHD are identified in this document (see Section 4.3 and Table 3 and Table 4).

Comment #	Comment Date	Comment Author (Agency)	Document Section	Comment/Issue Summary	ARRC Response/Proposed Resolution
4	3/30/12	Catherine Glidden (STB)	Table 2	Table 2 (page 15) is hard to follow and I am not sure what it is saying in regards to the Section 106 process. It is also unclear what the dollar amounts mean. What is the "original design description" and how is this heading different from the "revised design description"? Why have the dollar amounts changed from one to the other? Are these crossings historic properties? If so, will there be an adverse effect to the resource(s) as a result of any alteration?	The purpose of former Table 2 (now Table 4) was to show how project design has changed (i.e. a side-by-side comparison of "Original" design to "Revised" design) based on discussions with/input from the dog sledders and other trail users during the Workshops and Open House.  ARRC revised this table to remove references to costs and for clarity.
5	3/30/12	Catherine Glidden (STB)	New section	I would add a section at the end (Section 4.5) that says: <b>Agreed Upon Implementation Measures</b> Under this heading, ARRC should specify exactly what actions it will be taking to mitigate impacts to historic properties. This is only alluded to in Table 2, and elsewhere, but nowhere specifically stated in writing. This can be included in bulleted form, if desired.	ARRC revised former Table 1 (now Table 3) to include ARRC commitments and how these commitments will be implemented as well as revised the body of the document to make these connections more apparent
6	3/30/12	Catherine Glidden (STB)	P. 2, Sect. 2.1	Delete "were contacted" at the end of the third sentence in Section 2.1.	Text deleted
7	3/30/12	Catherine Glidden (STB)	P. 8, Sect. 3.3	In the 7th bullet in Section 3.3, is the "Iditarod Race Trail" (Crossing 9) the same as the "Iditarod Dog Sled Race Trail" mentioned on Page 7? If it is, please keep the name consistent throughout.	Reference to the Iditarod Race Trail and Iditarod National Historic Trail (NHT) has been made consistent throughout the body of the document
8	3/30/12	Catherine Glidden (STB)	P. 11	2nd to last paragraph, delete the word "at" from the phrase "Assuming the Corps at issues."	This text is no longer in the revised document
9	4/2/12	SHPO	Summary (Sect. 4?)	Condense the summary. The information needs to be succinct and lead the reader to the recommendations. Right now it is exhaustive and the relevant information is lost. Keep this section to one page, two at the most.	ARRC reviewed the document to ensure that discussion is succinct.
10	4/2/12	SHPO	Purpose (Sect. 1.2)	The purpose of the document is to create a workshop summary and implementation plan. (V.B.) Many of the purposes outlined in this document are not necessary such as identifying consulting parties, identifying contributing trails, identifying officially recognized trails, etc.	Section 1.2 is a summary of the purpose of the document which is based on the requirements of the PA (Stipulation V) and ROD (Mitigation Measures 92 and 93). These measures/stipulation specifically require identification of parties consulted, identifying contributing trails, providing rationale for relocating officially recognized trails, etc. The purpose of this document was agreed to in consultation with SHPO during the drafting of this document. Text has been added to clarify that this document meets the requirements of the PA and the ROD.
11	4/2/12	SHPO	Workshops (Sect 2.2)	Workshop summaries need to focus on the results, not goals. Goals and purposes are for the meeting, now we need to know results and issues. If there is desire to receive "credit" for the number of meetings, then identify that workshops were held on these days with a certain number of people participating. This is an example of course.	ARRC reviewed the document to ensure that discussion is succinct.

<b>Comment #</b>	<b>Comment Date</b>	<b>Comment Author (Agency)</b>	<b>Document Section</b>	<b>Comment/Issue Summary</b>	<b>ARRC Response/Proposed Resolution</b>
12	4/2/12	SHPO	APE (Sect. 3.1)	Simply say APE issue is resolved, see map.	The PA (Stip. II.D) required the Working Group to delineate a final APE with IDSHD parties – in consultation with SHPO, it was determined that this could be done during the IDSHD Workshops. ARRC disagrees that the language regarding APE should be removed; however, ARRC reviewed the text in this section to ensure that the discussion is succinct.
13	4/2/12	SHPO	IDSHD Description (Sect. 3.2)	Describe it briefly, say why it is eligible, and features found in it. There seems to still be a general confusion between character defining features and contributing elements.	Based on consultation with SHPO and IDSHD interests, it was determined that the purpose of the Workshops was not to identify contributing elements or character defining features. The focus of the participants was on continued access, connectivity and safety.  ARRC added/revised text in Sect. 3.2 (and elsewhere as appropriate) to include a general discussion of what a historic district and cultural landscape are, what the IDSHD is, and potential impacts to the IDSHD as a whole from the project (both adverse and beneficial; see Comment 2).
14	4/2/12	SHPO	Officially Recognized Trails (Sect. 3.3)	We acknowledge the attempts made to minimize impacts to trails in EIS. However, this document is looking at impacts for a specific historic property under the National Historic Preservation Act. All the information was not present at the time of the EIS so all character defining features of this district will be discussed to ensure they are considered to the greatest extent feasible.	As summarized in Section 1.2, the purpose of this document is to meet the requirements of the PA and ROD, which is more than just a summary of the impacts to the IDSHD. As discussed above, it was determined in consultation with SHPO and IDSHD interests that the purpose of the Workshops was not to identify contributing elements or character defining features. This is why ARRC proposed the additional work on the IDSHD in Sect. 4.4. The scope for this work, which includes developing methods for identifying character defining features to the district and district boundaries, was developed in consultation with the Working Group and SHPO.
15	4/2/12	SHPO	Constraints (Sect. 4.2)	Again simplify. Include map in text and briefly describe each constraint with one or two sentences. Do not identify specific locations in the text.	Constraints were a significant point of discussion during the Workshops and Open House as they provide the basis for what (and why) the Project Team can and cannot do to mitigate general impacts to the IDSHD related to access and connectivity.  ARRC reviewed the text in Sect. 4.2 again to ensure that discussion is succinct.
16	4/2/12	SHPO	Outcomes (Sect. 4.3)	Specify how ARRC will commit to specific changes. An example is “ARRC will ensure that all bridges/crossing will have a vertical clearance of 14’.”	ARRC revised former Table 1 (now Table 3) to include ARRC commitments and how these commitments will be implemented as well as revised the body of the document to make these connections more apparent
17	4/2/12	SHPO	Outcomes (Sect. 4.3)	Specify how the different trail crossing locations will be handled.	ARRC revised former Table 2 (now Table 4) to address each crossing

<b>Comment #</b>	<b>Comment Date</b>	<b>Comment Author (Agency)</b>	<b>Document Section</b>	<b>Comment/Issue Summary</b>	<b>ARRC Response/Proposed Resolution</b>
18	4/2/12	SHPO	Outcomes (Sect. 4.3)	Include the how and when.	ARRC revised former Table 1 (now Table 3) to include ARRC commitments and how these commitments will be implemented. It may not be possible to provide specifics such as schedule as some decisions are not under ARRC control (e.g., negotiations with agencies and landowners). ARRC will commit to resolution of issues prior to final design and construction.
19	4/2/12	SHPO	Outcomes (Sect. 4.3?)	Mitigation to the entire historic property is necessary. The boundary and NRHP-like document needs to be carefully outlined so it is useable for the people living in the area, MSB, and SHPO. How it is written, it appears that yet another report will be produced. It also needs to be produced by someone that has experience writing NRHP nominations, not DOEs	The additional IDSHD documentation will be developed in consultation with SHPO, STB, the Working Group and IDSHD interests. This additional IDSHD documentation is a mitigation measure that ARRC agreed to at SHPO's request. The documentation will be prepared by a professional that meets SOI Professional Qualification Standards.
20	4/2/12	SHPO	Outstanding Commitments/Issues (Sect 4.4)	There should be no outstanding commitments/issues at this point. Either commit, don't commit, or outline procedures. This is the final document, there is no more discussion.	ARRC reviewed and modified language in the document to clarify commitments. For issues that can not be resolved at this time (e.g., the MHLT and UA trail easements), a procedure for moving forward will be made clear. Added language that says the Project Team shall ensure that the issue is resolved prior to final design and construction?
21	4/2/12	SHPO	Table 2	Remove dollar figures. Historic preservation concerns are focused on the outcomes. There should never be a discussion if enough or not enough has been spent to mitigate.	ARRC modified former Table 2 (now Table 4) to remove costs (see Comment 4)
22	4/2/12	SHPO	General	Overall, this should be simplified so it is as useable as possible to the general public. Make sections digestible with maps and drawings included by the references in the text	ARRC reviewed the document for clarity and added text and exhibits/figures where appropriate.
23	4/2/12	SHPO	General	Consider restructuring the document so it is easier to use.	ARRC made no significant changes to the structure of the document. SHPO was involved throughout the drafting of this document (e.g., preliminary reviews of the proposed outline and drafts of the document) and was at Working Group meetings where this document was discussed. SHPO edits/comments were addressed/included in revisions of the draft document prior to its distribution to PA Signatories, the Working Group, Concurring Parties, and other IDSHD interests. Text and illustrations from the <i>Comment/Response Summary</i> (former Attachment G in the previous draft) have been incorporated into the body of the document as appropriate.



Comment #	Comment Date	Comment Author (Agency)	Document Section	Comment/Issue Summary	ARRC Response/Proposed Resolution
24	4/2/12	Lesli Schick (DNR, DMLW, SCRO)	General	The Project Overview states that this document is to comply with Stipulation V of the PA, Mitigation Measure 92 and Mitigation Measure 93. Mitigation Measure 92 deals with reports required to relocate any <i>officially recognized trail</i> , while Mitigation Measure 93 is to identify the location and use of all trails contributing to the Iditarod Dog Sledding Historic District (IDSHD) and determine which are needed for connectivity. By combining these items into one report it is difficult for a reader to separate these different issues (historic/non-historic dog mushing trails in the IDSHD and non-dog mushing or nonhistoric officially recognized trails that are being rerouted). Please separate the documents so that the IDSHD implementation plan contains items specific to the district, not just the rail project in general.	This document was developed in consultation with the Working Group and SHPO. ARRC feels that there is sufficient overlap in the requirements of the PA and ROD mitigation measures to make combining reporting into one document. In addition, it became apparent during consultation with SHPO, the Working Group and IDSHD interests that mitigation for impacts to the IDSHD should not be limited to “historic” trails only and should address continued access and connectivity to trails throughout the network (including non-historic trails). ARRC has reviewed and modified the document to clarify mitigation in association with the IDSHD.
25	4/2/12	Lesli Schick (DNR, DMLW, SCRO)	General	An implementation plan usually is one of the final document produced, but according to the draft the following items still need to be completed/resolved: <ul style="list-style-type: none"> <li>• Additional crossings between Crossing 2 and 10</li> <li>• Iron Dog Connector Trail crossing resolution</li> <li>• Changes in the alignment following the FAA decision, and any effects this may have on trails in the area. There is a request for review and comments by the consulting parties on the new alignment in the implementation plan. An implementation plan usually implements the review and comments, not requests it.</li> <li>• A draft report one year after the implementation plan on the IDSHD that will identify the district boundaries, what is significant and contributing.</li> <li>• Additional consultation on various trails and <i>possible</i> reroutes</li> </ul> How will the above items be implemented by the plan, if they have yet to be resolved?	<p>The implementation plan outlines ARRC commitments and how these commitments will be implemented.</p> <p>ARRC has reviewed and modified language in the document to clarify commitments (e.g., Section 4.3 and Table 3 and Table 4; see Comment 20). In summary:</p> <ul style="list-style-type: none"> <li>• The Project Team is committed to maintaining access and connectivity for the IDSHD. However, there are multiple constraints that prevent crossings from being added between Crossings 2 and 10. It is unlikely that crossings will be added in this area. If additional crossing locations are identified, the Project Team will notify consulting parties.</li> <li>• The Project Team is committed to maintaining access and connectivity for the Iron Dog Connector Trail, and believes that the reroute will maintain continued use.</li> <li>• This information regarding the FAA VORTAC issue was included in the draft <i>Implementation Plan</i> because this was a change in the project since Workshop 3. The Project Team is committed to seeking input from trail users as project design changes.</li> <li>• Regarding the additional IDSHD reporting, this document outlines the process for this additional documentation, which is an appropriate approach for an implementation plan.</li> <li>• The Project Team is committed to providing information to, and seeking input from, trail users as the project design changes. There are outstanding issues associated with trails on MHLT and UA lands that were not resolved before the draft <i>Implementation Plan</i> was required by the PA to be submitted (within 60 days of the IDSHD workshop), and these issues continue to be unresolved.</li> </ul>

<b>Comment #</b>	<b>Comment Date</b>	<b>Comment Author (Agency)</b>	<b>Document Section</b>	<b>Comment/Issue Summary</b>	<b>ARRC Response/Proposed Resolution</b>
26	4/2/12	Lesli Schick (DNR, DMLW, SCRO)	General	The workshops were primarily focused on the Officially Recognized Trails (Mitigation Measure 92), not the question of connectivity and what properties contribute to the district. At the January 11, 2012 meeting, John Brautigan of the Knik Dog Musers Association and Knik Iditarod Trail Blazers formally commented on this and asked when Mitigation Measure 93 would be discussed. He inquired if the meetings were meant to comply with Mitigation Measure 93, because the group had not been asked where all the trails that could contribute to the district are and which are needed for connectivity. Instead, the group had been addressing the Officially Recognized Trails (Mitigation Measure 92). His comment is not listed in the enclosed table, nor is it addressed.	ARRC disagrees that a primary focus of the Workshops was on Officially Recognized Trails, and not connectivity. Overall, participant discussions and comments at the IDSHD Workshops and MSB Open House focused primarily on access, safety, and connectivity concerns. This was not the original intent of the Workshops (see Section 2.2.1), but this is what participants wanted to discuss even when directed in break out groups and asked about contributing/historic trails. Workshop 1 was open to all persons (dog sledders) with an interest in the IDSHD, and Workshop 2 participants self-selected to be part of a focus group to discuss historic trails/features within the IDSHD. Even so, the discussions during Workshop 1 and 2 focused on access, safety, and connectivity. Based on these discussions, it was decided in consultation with SHPO to shift focus.  As discussed during the NEPA process, ARRC is obligated to provide crossings/relocations for officially recognized trails. However, ARRC has considered unofficial trails in developing crossing and relocation design to facilitate continued access and connectivity and the Project Team has been working to obtain easements for trails that currently do not have them.  The Project Team did not record a formal comment from Jon Brautigan regarding Mitigation Measure 93 at the January 11 meeting. The Project Team believes that Mr. Brautigan's comment is addressed under Issue 16. In addition, this document was sent to all IDSHD Workshop/MSB Open House participants and no participant comments on or objections to the IDSHD document have been provided to the Project Team or ARRC to date. The proposed additional documentation for the IDSHD will include a discussion of the features that a contributing element to the district would have (e.g., what types of features, physical characteristics of these features that provide association with the district, important aspects of integrity needed to maintain association with the district).
27	4/2/12	Lesli Schick (DNR, DMLW, SCRO)	General	The enclosed maps appear to indicate that the stream crossings that do not have trails on them are being counted as crossing locations for the Iditarod Dog Sledding Historic District. Is this project proposing to construct trails to these structures? What do they have to do with the district? This may reflect the first issue listed above about combining trail issues outside of the district into the implementation plan designed specifically for it.	The maps count as crossings all at-grade or separated grade crossings (including, road, trail, and stream crossings) that are large enough to facilitate safe passage by people and trail grooming equipment. During IDSHD Workshops, the participants asked that these crossings be made usable by mushers for continued access and trail connectivity and noted that they travel along streams during the winter when they are frozen. This project will not be constructing new trails with the exception of trails that have to be relocated as a result of the project. ARRC has revised former Table 2 (now Table 4) to reflect whether a trail is associated with the crossing or designed for use possible recreational use.

<b>Comment #</b>	<b>Comment Date</b>	<b>Comment Author (Agency)</b>	<b>Document Section</b>	<b>Comment/Issue Summary</b>	<b>ARRC Response/Proposed Resolution</b>
28	4/2/12	Lesli Schick (DNR, DMLW, SCRO)	P. 4	Page 4 states: “ <i>The Project Team did not receive substantive comments from participants regarding the APE, except that some participants suggested that the APE should be broader to include potential indirect effects, particularly in areas where trails cross the project right-of-way.</i> ” What is meant by indirect effects? Are they concerns about connectivity, future development (both were brought up by participants) or something else? The effects and how they interact within the APE could be important to the district and the implementation plan.	One purpose of the IDSHD Workshops was to discuss and receive input on APE. The presentation in Workshop 1 included an explanation of direct versus indirect effects. Text has been added in Section 3.1 to clarify what is meant by “indirect effects” and a definition is included in Attachment B (Glossary).
29	4/2/12	Lesli Schick (DNR, DMLW, SCRO)	P. 9	Page 9 states: “ <i>The restrictions also resulted in the loss of one previously planned crossing (e.g., the Outflow of Muleshoe Lake could not meet the 14’ of vertical clearance required).</i> ” Given the description on Page 11, isn’t the loss of this crossing due to the FAA denial for the rail line? If so, why is it in this section, as the rail line is not crossing at this location.	The loss of this crossing occurred prior to FAA’s decision and was based on conditions at this crossing being adverse for providing a minimum 14’ vertical clearance as requested by IDSHD Workshop participants. ARRC has revised the text in Section 4.2 to provide clarity and reflect current plans for this area; this sentence has been deleted.
30	4/2/12	Lesli Schick (DNR, DMLW, SCRO)	General	Additionally, some of the maps in the main document have the old rail alignment and some have the new one west of Mullshoe [sic] Lake. Please standardize the maps so that the current rail alignment is used throughout.	ARRC has revised maps in the document to reflect the latest alignment.

*This page intentionally left blank*



## **Iditarod Dog Sledding Historic District Workshop**

**Tuesday, June 28, 2011**

**6:00-8:00 pm**

Willow Area Community Organization Center, Willow, Alaska

### **AGENDA**

- I. Welcome and Introductions - Rosetta Alcantra, HDR
- II. Purpose/Format of the Iditarod Dog Sledding Historic District (IDSHD) Workshop - Rosetta Alcantra, HDR
  - a. Overall Goals of the IDSHD Workshop
    1. Provide project update and background
    2. Develop Area of Potential Effect (APE)
    3. Identify/discuss important features, functions and uses of IDSHD trails/structures in the APE
    4. Identify/discuss potential mitigation measures for IDSHD trails/structures in APE
  - b. Participant Goals?
  - c. Alaska State Historic Preservation Office (SHPO) – Involvement and Goals
  - d. Workshop Format
- III. Project Update and Background - Brian Lindamood, ARRC
- IV. Section 106 Process Overview - Elizabeth Grover, HDR
  - a. What is the consultation process for the PMRE project?
  - b. What is an APE?
    1. What is the proposed APE for the PMRE project?
  - c. What is a historic district/cultural landscape?
    1. What is the IDSHD?
- V. Break into Small Working Groups
- VI. Small Working Groups Report Back
- VII. Next Steps
- VIII. Adjournment



# Section 106 Facts

## Project Description

The Matanuska-Susitna Borough (MSB) and the Alaska Railroad Corporation (ARRC) propose to construct a 32-mile rail line in the Susitna River Valley to connect the Borough's Port MacKenzie to the Alaska Railroad Corporation's (ARRC) existing mainline track just south of Houston. The proposed rail line would provide freight services between the Port and Interior Alaska.

On March 25, 2011, the Surface Transportation Board (STB) recommended the Mac Central and Houston South as the selected route in the Final Environmental Impact Statement for the Port MacKenzie Rail Extension.

The federal board approves all new rail line construction in the nation. The 32-mile rail extension will link the deepwater Port MacKenzie to the mainline of the Alaska Railroad. The STB finding finalizes a rigorous environmental impact study, field work and public involvement process that began in 2007.

## Project Schedule

March 2011:	Release of Final EIS.
April 2011:	Permit Application Package Submittal
Late Spring 2011:	Record of Decision
2011-2012:	Construction of Mac Central Segment anticipated ending near Ayrshire Avenue.
2012-2013:	Construction of Modified Connector 3, Houston and Houston South segments.
2013-2014:	Track and railroad facilities to be constructed.

## Purpose of the Iditarod Dog Sledding Historic District Workshop

The new rail line will cross several officially recognized recreational trails within the Iditarod Dog Sledding Historic District (IDSHD).

The Programmatic Agreement (PA)—a document that describes the actions that will be taken by the parties in order to meet their responsibilities under the National Historic Preservation Act (NHPA)—between the Surface Transportation Board, Advisory Council on Historic Preservation, and Alaska State Historic Preservation Officer determined that the Iditarod Dog Sledding Historic District is eligible for listing in the National Register of Historic Place at the national level of significance between 1967 and 1978.

This workshop is being held to review the important features, functions and uses of IDSHD trails/structures in the project area, and to determine how trail access will remain intact during and after construction.

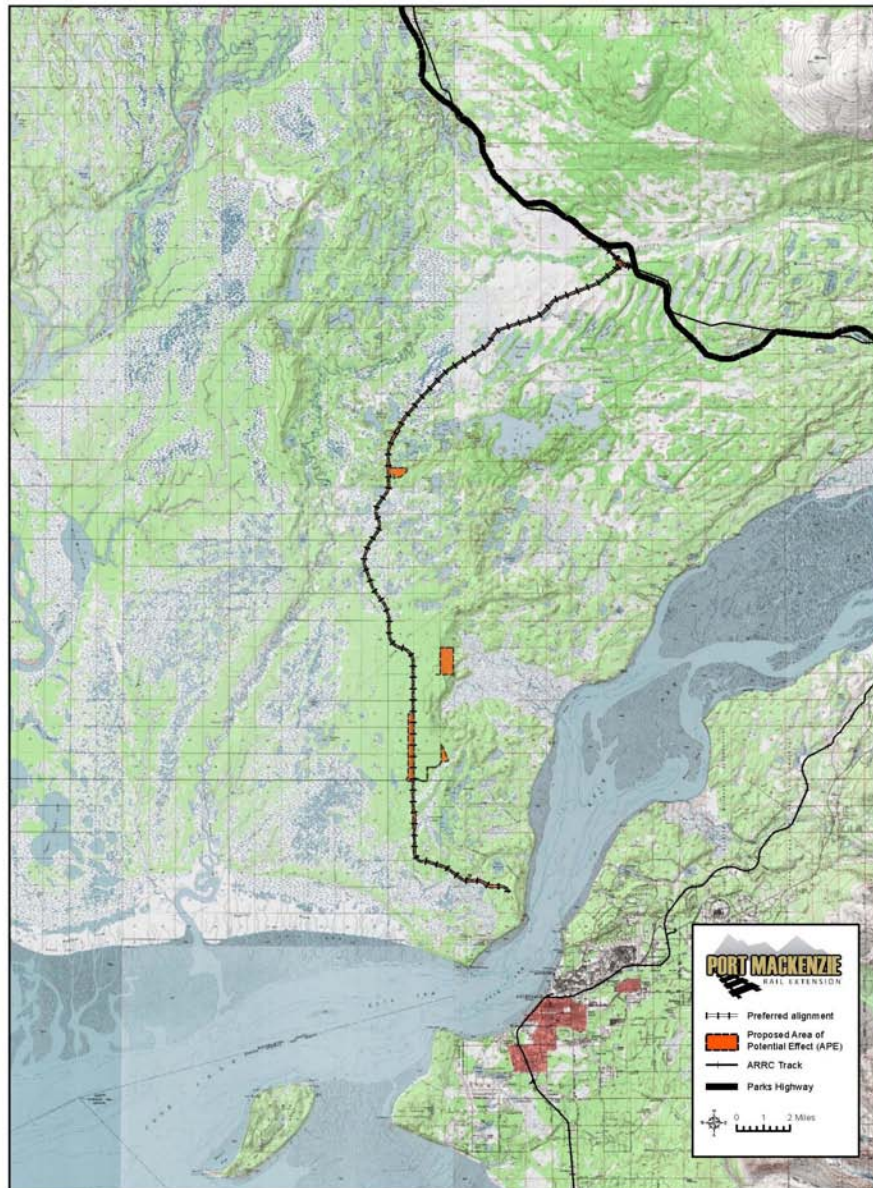
During the workshop, participants will review ARRC's plans for trail crossings and other mitigation measures. Within 60 days after the workshop ARRC will summarize the outcome of the workshop and develop an implementation plan.

In addition, the workshop will identify those individuals who wish to become consulting parties\* under Section 106 of the NHPA, and who are interested in working with the ARRC as part of a focus group to fully review and understand the historic significance of the district. This group will review the proposed final Area of Potential Effect (the geographic area or areas within which a project, activity, program or practice may cause changes in the character or use of any cultural resources present).

\* A consulting party can be any person or group that has demonstrated a legal/economic relation to a project or demonstrated interest in the project's effects on historic properties (or cultural resources eligible for listing on the National Register).



## Proposed Area of Potential Effect Map



### General Contact Information

Borough Contact:  
Patty Sullivan, MSB Public Affairs Director  
Phone: (907) 745-9577  
Email: psullivan@matsugov.us

Railroad Contact:  
Stephenie Wheeler, ARRC Corporate  
Communications Officer  
Phone: (907) 265-2671  
Email: wheelers@akrr.com

### Section 106 Contact Information

Alaska State Historic Preservation Office Contact:  
Doug Gasek, Architectural Historian  
Phone: (907) 269-8726  
Email: Doug.Gasek@alaska.gov

HDR Alaska, Inc. Contact:  
Elizabeth Grover, Cultural Resources Specialist  
Phone: (907) 644-2077  
Email: Elizabeth.Grover@hdrinc.com

## Please discuss the following questions:

- Does the proposed APE encompass the areas where impacts would likely occur? If not, where should the APE be altered to include these areas?
- What are important features, functions and uses of IDSHD trails/structures in the proposed APE (e.g., access and connectivity, visual way finders, or different/distinct uses [freight, training, races])?
- Do you feel that the PMRE project will affect dog sledding or the features, functions and uses of IDSHD trails through the proposed APE? If so, how?
- What can be done to further reduce these impacts?




# PORT MACKENZIE

## RAIL EXTENSION

Have more to contribute? Join the

# Focus Group

Are you a long-term user of the trail system in the project area? Are you familiar with the trails use and function during the historic period of significance, 1967 - 1978? If so, please join the Port MacKenzie Rail Extension Focus Group to help the project team identify information about the historic trails and features within the Iditarod Dog Sledding Historic District. In addition, this group will assist in delineating the proposed Area of Potential Effect. The focus group is tentatively scheduled to meet on July 7, 2010. Please attend and be a part of one of the largest infrastructure projects the Matanuska-Susitna Borough has seen since 1971. For more project information visit:

[www.portmacrail.com](http://www.portmacrail.com)

---

## Have Questions?

### Section 106 Contacts

Alaska State Historic Preservation Office Contact:

Doug Gasek, Architectural Historian

Phone: (907) 269-8726

Email: [Doug.Gasek@alaska.gov](mailto:Doug.Gasek@alaska.gov)

HDR Alaska, Inc. Contact:

Elizabeth Grover, Cultural Resources Specialist

Phone: (907) 644-2077

Email: [Elizabeth.Grover@hdrinc.com](mailto:Elizabeth.Grover@hdrinc.com)





## WELCOME to the Iditarod Dog Sledding Historic District (IDSHD) Workshop

June 28, 2011

Willow Area Community Organization  
Center  
Willow, Alaska

[www.portmacrail.com](http://www.portmacrail.com)



## Agenda

- Welcome and Introductions - Rosetta Alcantra, HDR
- Purpose/Format of Workshop - Rosetta Alcantra, HDR
  - Overall Goals
    - Provide project update and background
    - Area of Potential Effect (APE)
    - Identify/discuss important features, functions, and uses of IDSHD trails/structures in the APE
    - Identify/discuss potential mitigation measures for IDSHD trails/structures in the APE
    - Future workshops/meetings if necessary (July 7 – focus group, July 19 – workshop)
  - Participant Goals
  - Alaska State Historic Preservation Office (SHPO) – Involvement and Goals
  - Workshop Format

[www.portmacrail.com](http://www.portmacrail.com)





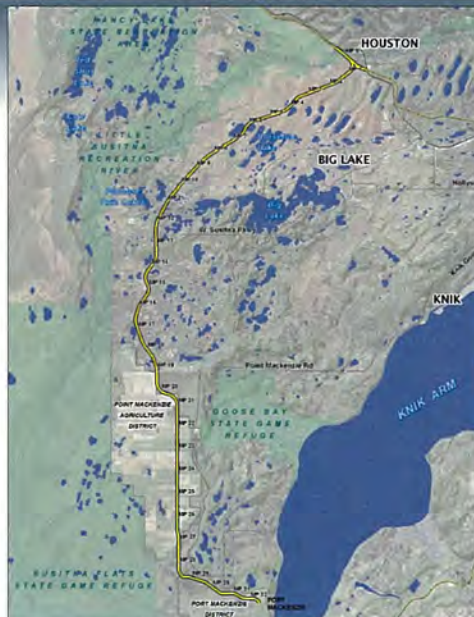
## Agenda (Cont'd.)

- Project Update and Background - Brian Lindamood, ARRC
- Section 106 Process Overview - Elizabeth Grover, HDR
  - What is the consultation Process for the PMRE Project?
  - What is an APE?
    - What is the proposed APE for the PMRE project?
  - What is a historic district/cultural landscape?
    - What is the IDSHD?
- Break into Small Working Groups
- Small Working Groups Report Back
- Next Steps
- Adjournment

[www.portmacrail.com](http://www.portmacrail.com)



## Project Background/Update



[www.portmacrail.com](http://www.portmacrail.com)



# Project Schedule



- March 25, 2011 – Release of Final Environmental Impact Statement
- Spring 2011 – Permit Applications submitted to State and Federal agencies and MSB for review and comment
- Summer 2011 – STB provides Record of Decision
- Construction (in segments) scheduled to begin in Summer 2011

[www.portmacrail.com](http://www.portmacrail.com)



## Crossing Summary



[www.portmacrail.com](http://www.portmacrail.com)





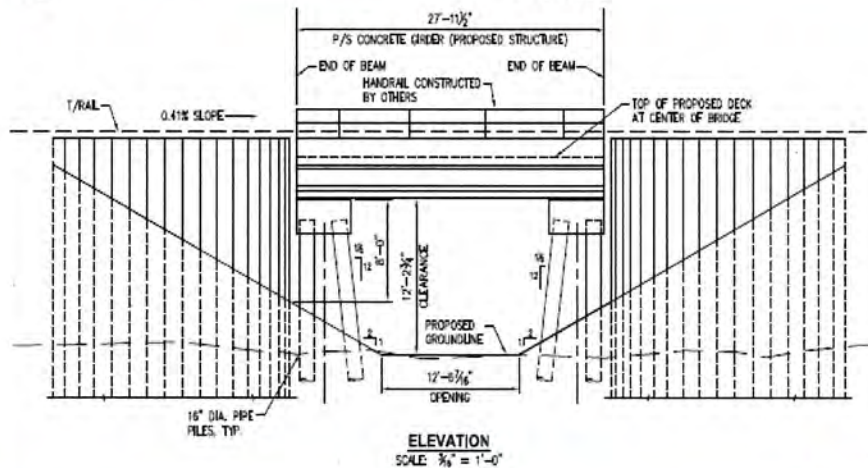
## Mac Central, Houston South Alignment Key Construction Elements

- 32.2 miles of rail
- Approx. 200 foot right-of-way
- 16 crossings, including:
  - 8 at-grade crossings of roads and trails
  - 4 grade separations (3 bridges and 1 large culvert) of trails
  - 5 multi-purpose crossing structures
- Terminal reserve area
- Drainage structures to maintain drainage
- Rail bridge over Little Susitna River (120-foot span)
- 6.57 miles of rail line will cross Agricultural Project area
- Constructed by end of 2014 depending on funding

[www.portmacrail.com](http://www.portmacrail.com)



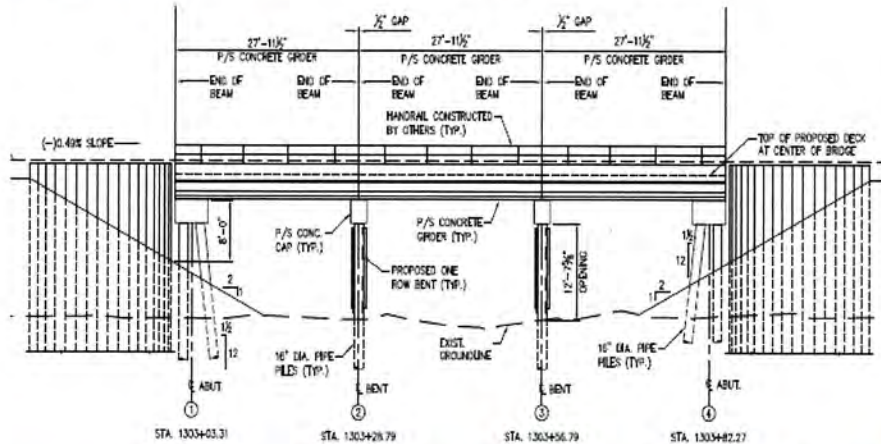
## Trail Crossing – Iditarod Trail



[www.portmacrail.com](http://www.portmacrail.com)



## Triple-Span Multi-Purpose Crossing



## What is Section 106?

- Section 106 of the National Historic Preservation Act and its implementing regulations - 36 CFR 800 (Protection of Historic Properties)
- Section 106:
  - ✓ Applies to projects where there is Federal funding; permits, licenses or approvals; or oversight
  - ✓ Requires Federal agencies to "take into account" the effects of their undertakings on "historic properties"
  - ✓ Ensures that preservation values are considered in Federal project planning and decisions





## Four Steps of the Section 106 review

1. Initiate Section 106 process (e.g., establish undertaking, identify consulting parties)
2. Identify historic properties (e.g., determine scope of efforts, identify historic properties)

*A **historic property** is any cultural resource that has been listed on or determined eligible for the National Register of Historic Places*

3. Assess Effects (i.e., determine if historic properties will be adversely affected by the undertaking)
4. Resolve Adverse Effects (i.e., continue consultation to resolve adverse effects)

[www.portmacrail.com](http://www.portmacrail.com)



## What is consultation?

The process of seeking, discussing, and considering the views of other participants, and, where feasible, seeking agreement with them regarding matters arising in the Section 106 process (36 CFR 800.16.f)

[www.portmacrail.com](http://www.portmacrail.com)





## History of Consultation and the PMRE Project

- STB is responsible for identifying Section 106 consulting parties
- STB initiated consultation:
  - Government-to-Government consultation with Tribes and Tribal organizations (2008) and Section 106 consultation (2/5/2009)
  - Section 106 consultation with the SHPO's office (6/19/2008); STB met with SHPO prior to this (3/2008) to discuss identification efforts
  - Section 106 consultation with other parties (2/5/2009): MSB Historic Preservation Commission, Cook Inlet Historical Society, Wasilla-Knik Historical Society, Willow Creek Historical Society, Museum of Alaska Transportation and Industry, Palmer Historical Society, Willow Dog Musers Association, Happy Trails Kennels, and Dream a Dream Dog Farm
  - Section 106 consultation with ACHP (8/2/2010)
- Consulting parties met in October, 2010 to discuss draft Programmatic Agreement and participated in a follow-up teleconference (2/24/2011)

[www.portmacrail.com](http://www.portmacrail.com)



## Section 106 Consultation Participants

Participants in Section 106 consultation include:

- Federal agencies (e.g., STB)
- Advisory Council on Historic Preservation (ACHP)
- State Historic Preservation Officer (SHPO)
- Tribal governments and corporations (e.g., Knik Tribal Council)
- Local government (e.g., MSB)
- Applicants for Federal assistance/approval (e.g. ARRC)
- Other consulting parties
  - demonstrated legal/economic interest in the project
  - demonstrated concern about potentially affected historic properties
- Public

[www.portmacrail.com](http://www.portmacrail.com)







## Identifying/Inviting Consulting Parties

- Federal agency consults with SHPO to identify consulting parties
- Consulting parties can be identified throughout Section 106 process
- Federal agency invites consulting parties in writing
- Federal agency must consider and respond to all written requests for participation as a consulting party
- Communication between Federal agency and consulting party occurs throughout the Section 106 process

[www.portmacrail.com](http://www.portmacrail.com)



## Why be a Consulting Party?

- Learn more about the prehistory/history of the Project area
- Share information about local prehistory/history
- Share information about specific cultural resources in the Project area
- Share your concerns with the Federal agency regarding potential effects to cultural resources from the Project
- Let the Federal agency know what cultural resources are important to you
- Share your thoughts on how effects to historic properties can be mitigated or minimized

[www.portmacrail.com](http://www.portmacrail.com)



## What is an APE?

The Area of Potential Effect (APE) is an area where an undertaking may directly or indirectly cause changes in the character or use of a historic property

- Direct effects (e.g., places where ground-disturbing activity will occur such as right-of-ways)
- Indirect effects (e.g., visual, noise, vibration)

### Proposed APE\* for the PMRE Project

\*see map series in Participant packet for further detail



## PMRE Project APE

- Preliminary APE was defined as 1 mile on either side of the rail centerline
  - Broad study area for identification of historic properties that could be sensitive to visual or noise effects
  - Broadly encompassed the anticipated maximum extent for visual effects that could affect features that make a historic property eligible for the NRHP, especially in areas with low vegetation and flat topography
- Final APE will be based on final construction plans for the licensed alternative and may be closer to the rail centerline

## What is a Cultural Landscape?

“...a geographic area, including both cultural and natural resources and the wildlife or domestic animals therein, associated with a historic event, activity, or person or that exhibit other cultural or aesthetic values”  
(Page, Gilbert, and Dolan 1998:12 as cited in SRB&A 2010)

## What is a Cultural Landscape?

- NPS recognizes four types of cultural landscapes:
  - Historic designed landscapes (e.g., cemetery, parks, gardens)
  - Historic vernacular landscapes (e.g., IDSHD, rural villages, industrial complexes, agricultural landscapes)
    - evolved through human activities/occupancy
  - Historic sites (e.g. presidential properties, battlefields)
  - Ethnographic landscapes (e.g., religious sacred sites, massive geological features with cultural significance)
- Listed on the NRHP as sites or districts

## Characteristics of a Cultural Landscape

- Natural systems/features
- Spatial organization
- Buildings, structures and/or archaeological sites
- Land use
- Views/vistas
- Cultural traditions
- Circulation (i.e., systems of movement)
- Topography

## Contributing Features/Resources

- Does the feature add to the historic significance of the landscape?
- A contributing resource is:
  - A building, site, structure, or object that adds to historic significance
  - An independent cultural resource that could be significant outside of the cultural landscape
- A contributing feature is:
  - Vegetation, water bodies and other natural features, views/vistas, spatial organization
  - Not significant outside of the cultural landscape

## What is a Historic District?

### A Historic District:

- Is a property category (e.g., site, building, structure, object, district) in the NRHP
- Possesses a “significant concentration, linkage, or continuity of sites, buildings, structures, or objects united historically or aesthetically by plan or physical development” (NPS NRHP Bulletin 15)

## What is a Historic District?

- A Historic District:
  - Is comprised of multiple resources that are unified in some way (e.g., event/activity, person, architecture, site type)
  - Gets its identity from the interrelationship of its resources
  - Can reflect a single event/activity or several interrelated events/activities
  - Can include features that are individually distinct and/or lack individual distinction

## What is a Historic District?

- A district must be a definable geographic area that can be distinguished from surrounding areas
- District boundaries are based upon a shared relationship among properties constituting the district
- Discontiguous districts may be composed of two or more definable significant areas separated by nonsignificant areas such as:
  - Elements are spatially discreet
  - Space between elements is not related to the significance of the district
  - Visual continuity is not a factor in the district's significance
  - Not appropriate for including isolated elements that have become separated due to demolition or new construction
  - An example could include a canal system where man-made sections are interspersed with natural sections of river



## What is the IDSHD?

- Illustrates the historic significance of dog sledding in study area
  - SHPO concurred with NRHP eligibility for the 1967-1978 period associated with the Iditarod Race (9/1/2010)
    - 1967 marked the inaugural run of the Iditarod Race
    - 1978 marked the nomination of the Iditarod as a National Historic Trail

[www.portmacrail.com](http://www.portmacrail.com)



## IDSHD Documentation

- SRB&A conducted interviews in August/September 2009
- SRB&A researchers interviewed members/personnel of:
  - Aurora Dog Musers Club
  - Dog Mushing Program of the Little Su 4-H Club
  - Knik Musers
  - Willow Dog Musers Association
  - Iditarod Trail Sled Dog Race Headquarters
  - Musers Feed and Supply
  - Alaskan Kennels
  - Happy Trails Kennels

[www.portmacrail.com](http://www.portmacrail.com)





# IDSHD Recommended Contributing Trails

- SRB&A 2010 recommended 15 contributing trails:
  - **INHT (Knik to Susitna Station; 1898-1925/1967-1978)**
  - **Iditarod Sled Dog Race Trail (1967-1978)**
  - Lucky Shot Trail (1898-1925/1967-1978)
  - Corral Hill Trail (1898-1925/1967-1978)
  - **Flat Horn Lake Trail (1967-1978)**
  - Nancy Lake-Susitna Trail (1898-1925)
  - Red Shirt Lake-Nancy Lake Trail System (1967-1978)
  - Herring Trail (1898-1925/1967-1978)
  - Aurora Dog Mushing Club Trail System (1967-1978)
  - **USGS Transmission Line Trail (1967-1978)**
  - **Five USGS Base Map Winter Trails (1967-1978)**

Note: Trails in bold text are crossed by the APE

[www.portmacrail.com](http://www.portmacrail.com)



Recommended Contributing Resources (SRB&A 2010: Map 6)



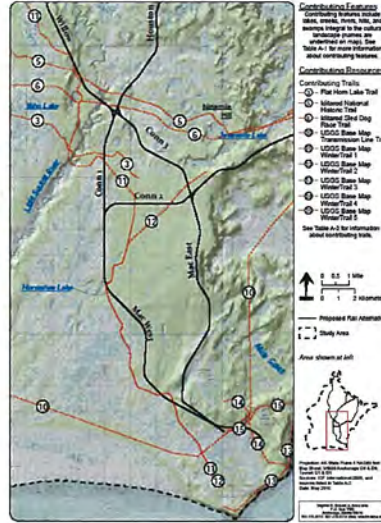
[www.portmacrail.com](http://www.portmacrail.com)





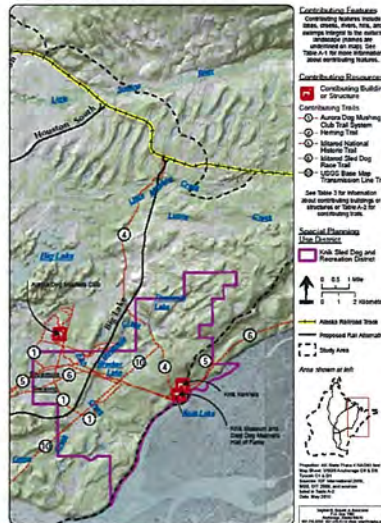
Recommended  
Contributing Resources  
(SRB&A 2010: Map 8)

Map 8 Contributing Features and Resources, Part 2 of 5



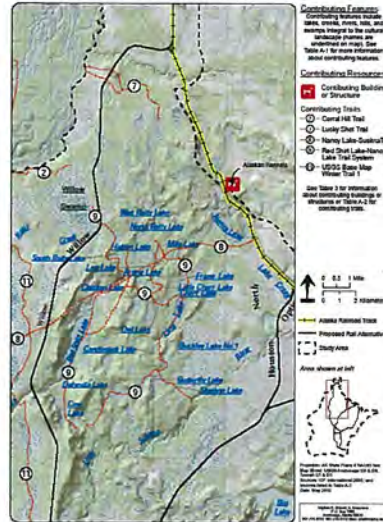
Recommended  
Contributing Resources  
(SRB&A 2010: Map 9)

Map 9 Contributing Features and Resources, Part 3 of 5



Recommended  
Contributing Resources  
(SRB&A 2010: Map 10)

Map 10 Contributing Features and Resources, Part 4 of 5



www.portmacrail.com



## Summary of IDSHD

- Iditarod Dog Sledding Cultural Landscape/IDSHD consists of:
  - Trails/travel routes
  - Buildings/structures
  - Specified natural systems/features
- Is eligible for listing in the NRHP under Criterion A (important events) and Criterion Consideration G (properties that have achieved significance in the last 50 years) at the national level
- Eligible for its association with events (1967-1978) that have made a significant contribution to broad patterns of Alaska history, specifically the theme of recreation related to the Iditarod Race and its development

www.portmacrail.com





## Additional Information

- PMRE Project website <http://www.portmacrail.com/>
- Citizen's Guide to Section 106 – ACHP 2010  
<http://www.achp.gov/docs/CitizenGuide.pdf>
- NPS Preservation Brief "Protecting Cultural Landscapes: Planning, Treatment and Management of Historic Landscapes"  
<http://www.cr.nps.gov/hps/tps/briefs/brief36.htm>
- PMRE Final Environmental Impact Statement (FEIS)  
<http://www.stbportmacraileis.com/feis-document-list.html>
- Port MacKenzie Rail Extension Project Report of 2009 Cultural Resources Fieldwork – Stephen R. Braund & Associates 2010



## For more information

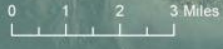
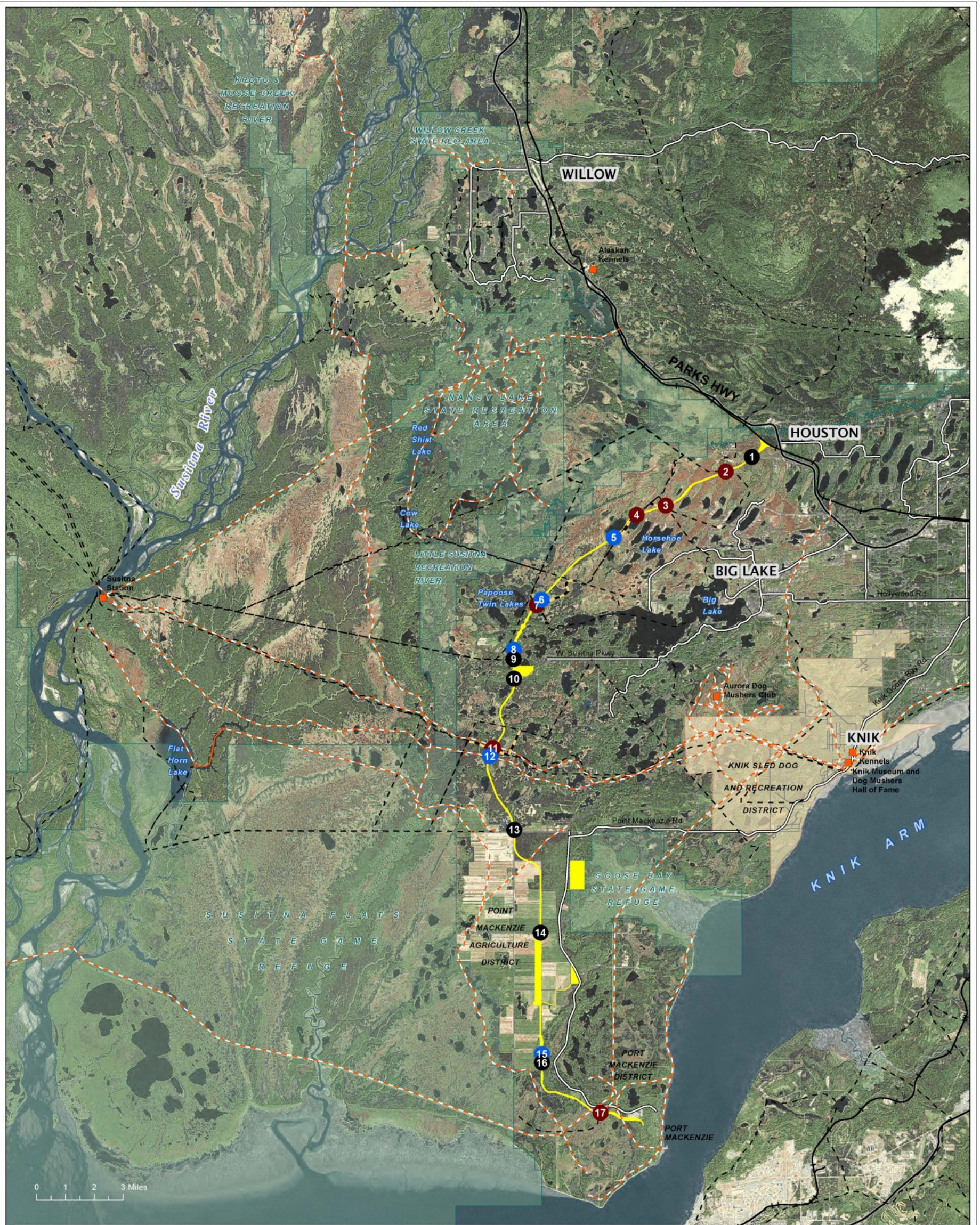
Please contact:

*Elizabeth Grover, Cultural Resources Specialist*  
Phone: (907) 644-2077  
E-mail: [Elizabeth.Grover@hdrinc.com](mailto:Elizabeth.Grover@hdrinc.com)

*Doug Gasek, Architectural Historian, Alaska State Historic Preservation Office*  
Phone: (907) 269-8726  
E-mail: [Doug.Gasek@alaska.gov](mailto:Doug.Gasek@alaska.gov)

**Visit: [www.portmacrail.com](http://www.portmacrail.com)**





## Crossing Locations

- Proposed Area of Potential Effect (APE)
- Proposed trail re-route
- Contributing trail
- Contributing structure
- Trail

### Crossing Types

- Road
- Multi-purpose
- Trail

### Crossing Type Summary

① Millers Reach Rd.	At-Grade	⑩ W Susitna Pkwy	At-Grade
② Unnamed Big Lake Rec.Trail	At-Grade	⑪ Iditarod Trail	Bridge
③ Unnamed Big Lake Rec.Trail	At-Grade	⑫ H-0.8 - outlet of Diamond Lake	Bridge
④ Houston Lake Loop Trail (Crossing)	Bridge	⑬ W Ayrshire Ave.	At-Grade
⑤ H-9.6 - Outflow of Muleshoe Lake	Bridge	⑭ W Holstein Ave.	At-Grade
⑥ H-6.3 - Tributary of Little Su	Bridge	⑮ MC-4.5 - Unnamed Stream	Bridge
⑦ Flat Lake Connectors Trail (Iron Dog Trail)	Bridge	⑯ Baker Farm Road	At-Grade
⑧ H-4.3 - Tributary of Little Su	Bridge	⑰ Figure 8 Lake Loop Trail	19' Culvert
⑨ W Papoose Twins Rd.	At-Grade		

# Unnamed Big Lake Recreation Trail



# Unnamed Big Lake Recreation Trail



## Crossing Locations

Proposed Area of Potential Effect (APE)



Proposed trail closure



Contributing Trails



Trail

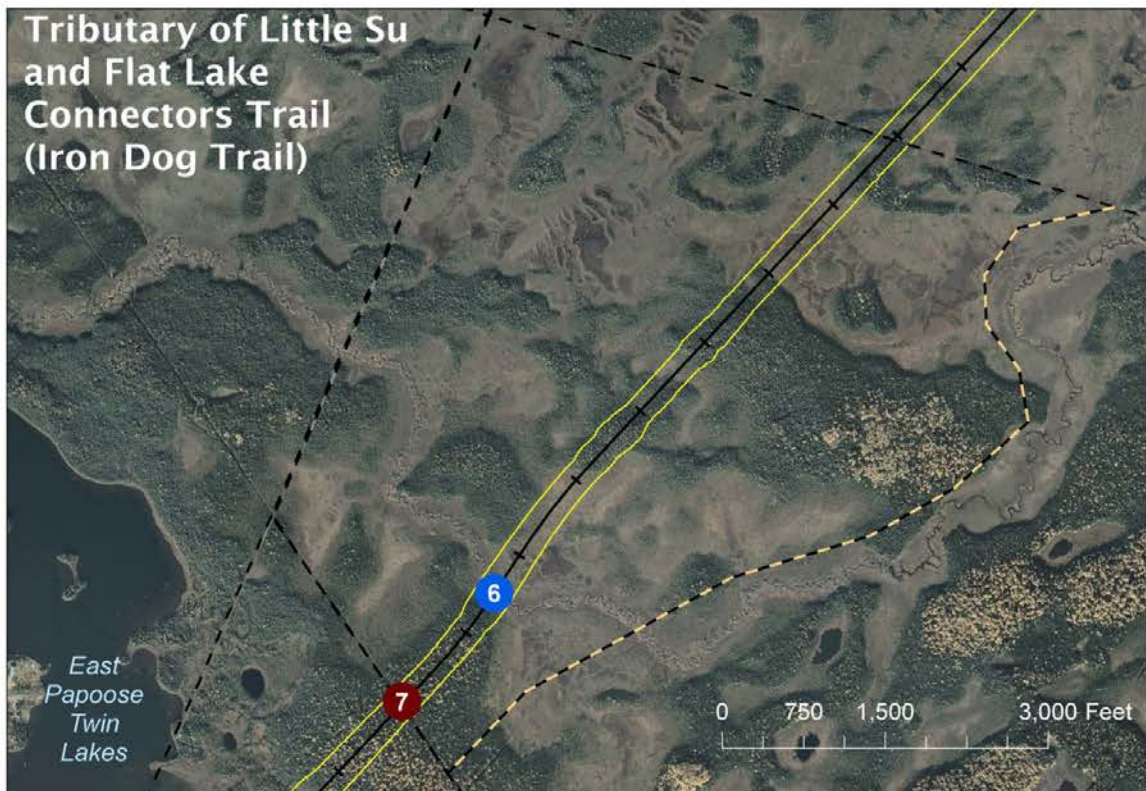


Rail Centerline

## Houston Lake Loop Trail and Outflow of Muleshoe Lake

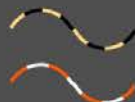


## Tributary of Little Su and Flat Lake Connectors Trail (Iron Dog Trail)



### Crossing Locations

Proposed Area of Potential Effect (APE)



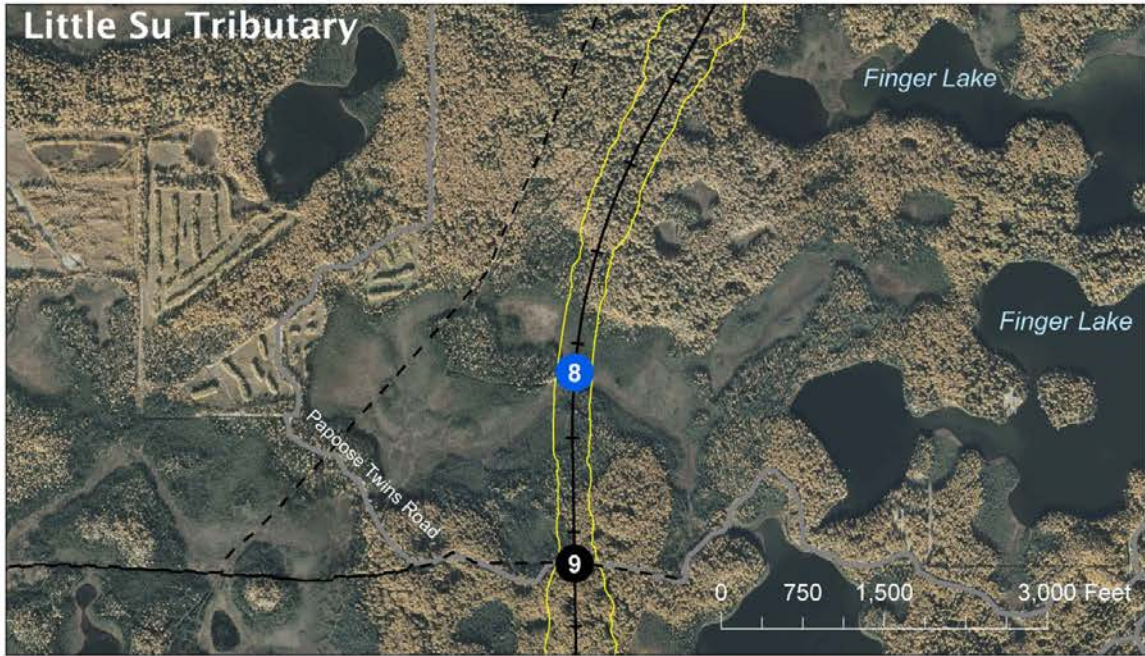
Proposed trail re-route

Contributing Trails

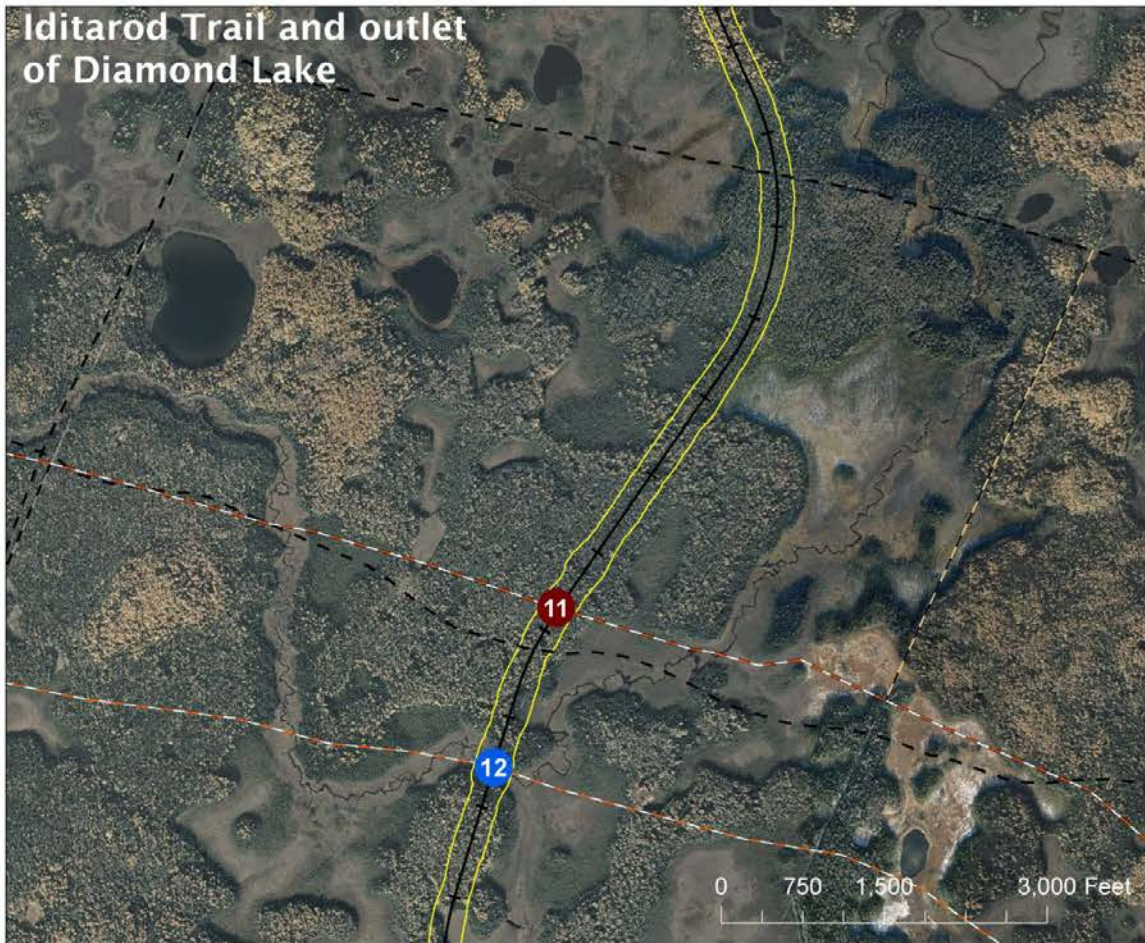
Trail

Rail Centerline

### Little Su Tributary



### Iditarod Trail and outlet of Diamond Lake



### Crossing Locations

Proposed Area of Potential Effect (APE)



Proposed trail re-route



Contributing Trails

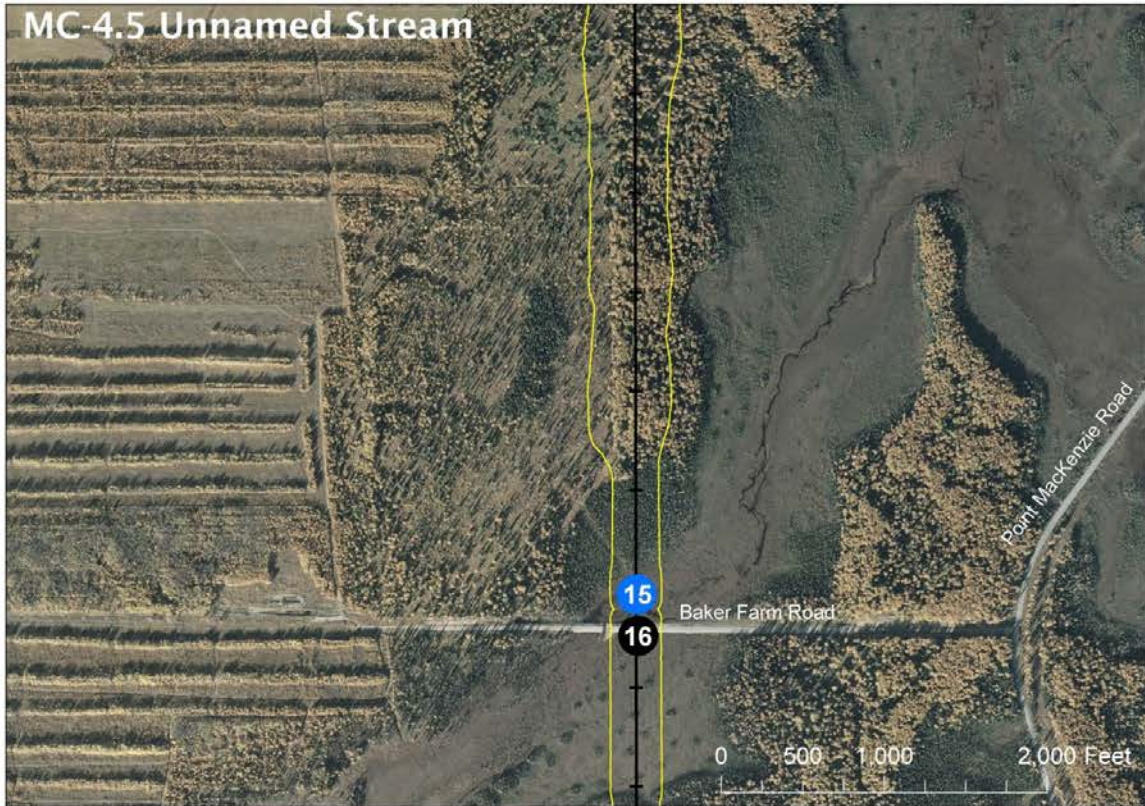


Trail

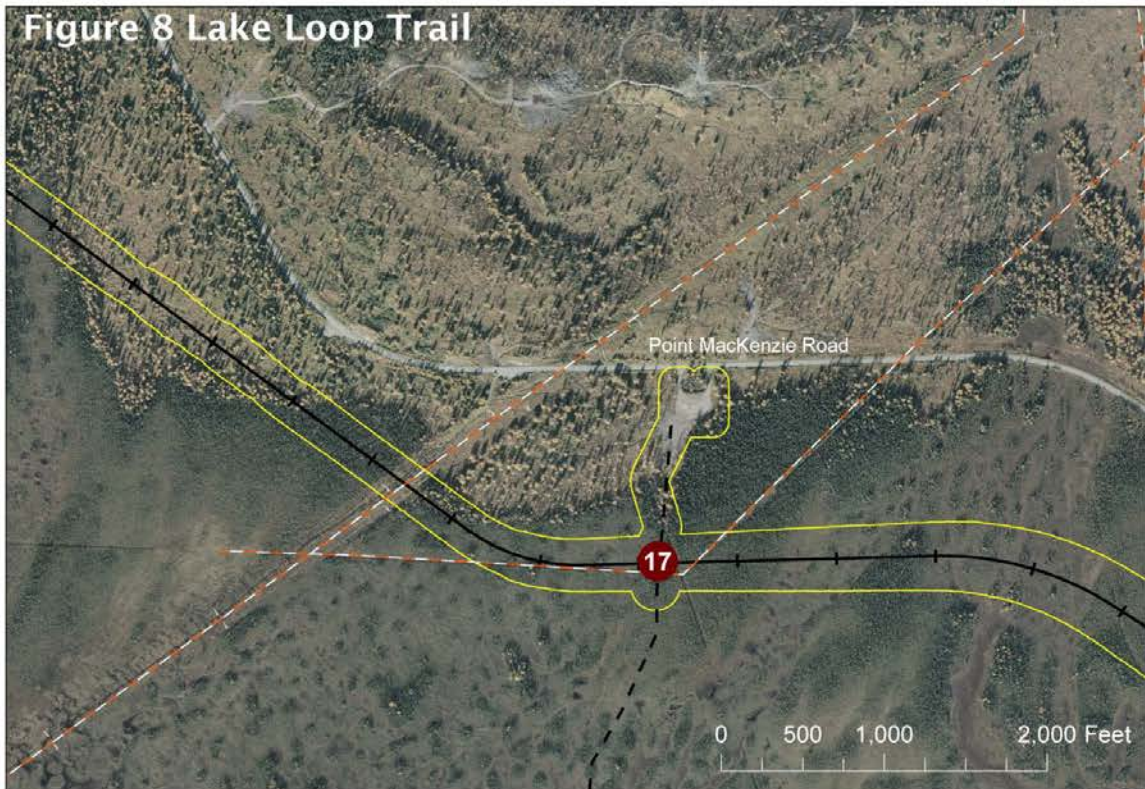


Rail Centerline

### MC-4.5 Unnamed Stream

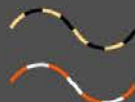


### Figure 8 Lake Loop Trail



### Crossing Locations

Proposed Area of Potential Effect (APE)



Proposed trail re-route

Contributing Trails



Trail



Rail Centerline



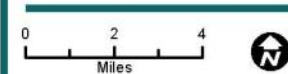
FIGURE 1



**Project Area Vicinity and Area of Potential Effect**

**LEGEND**

- Proposed Area of Potential Effect (APE)
- Proposed Alternative
- ARRC Track
- Highway
- Medium Rd
- Minor Rd
- Park or Refuge



Date: April 25, 2011  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: DNR, MSB, HDR Alaska, Inc., Hanson

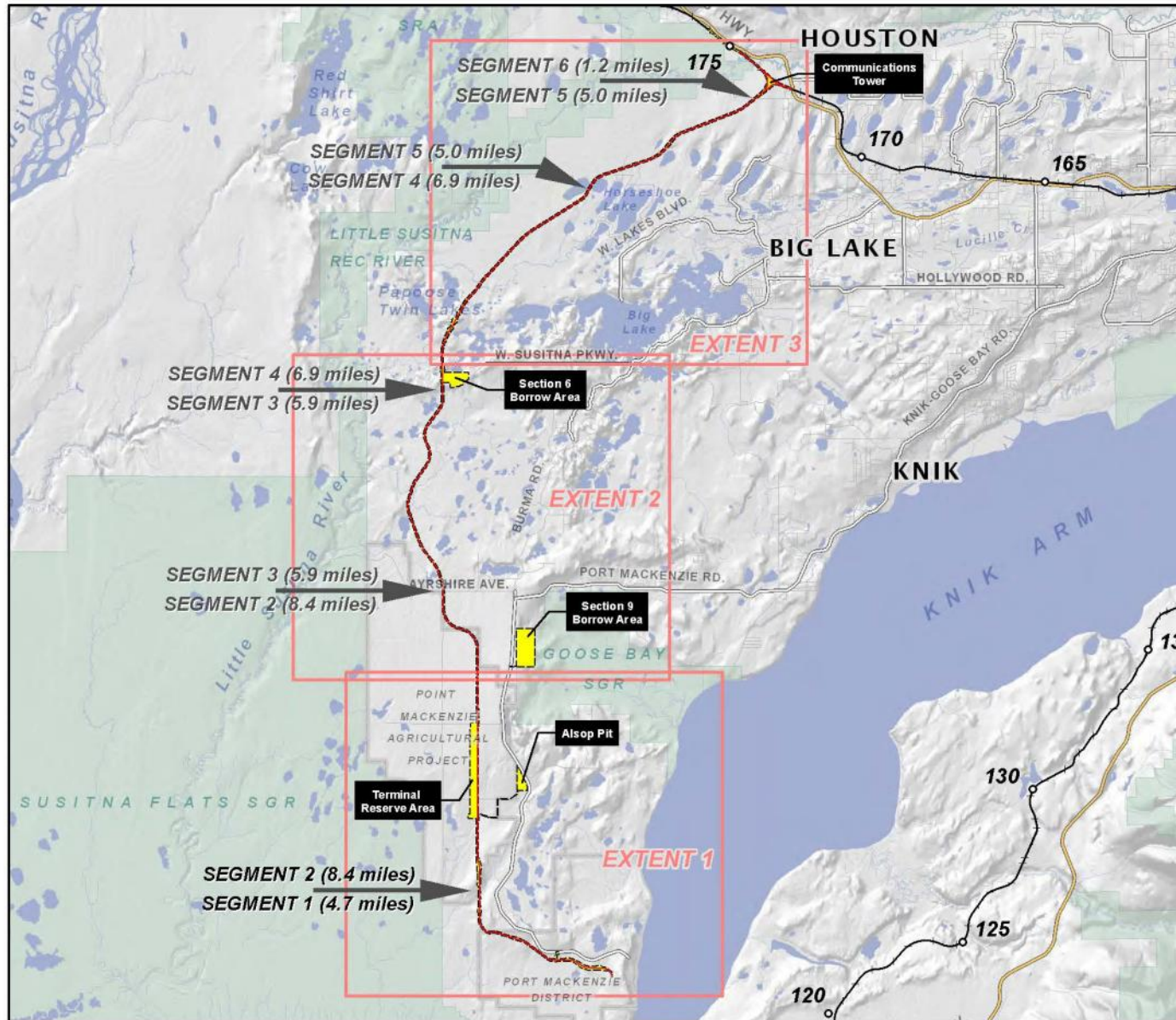


FIGURE 2

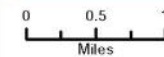


Area of Potential Effect  
USGS Base  
Extent 1

LEGEND

-  Proposed Area of Potential Effect (APE)
  -  Proposed Alternative
  -  Trail
  -  Proposed Contributing Trails, SRB&A 2010
- Separated Grade Crossings
-  Trail
  -  Multi-Purpose
- At-Grade Crossings
-  Road
  -  Trail

NOTE: Crossing locations are based from Mat-Su Borough GIS and field survey data and may not align with what is shown on the USGS base map.



Date: May 12, 2011  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: HDR Alaska, Inc., Hanson  
 Base map: USGS 1:63,360  
 ANCHORAGE (8-8), TYONEK (8-1)

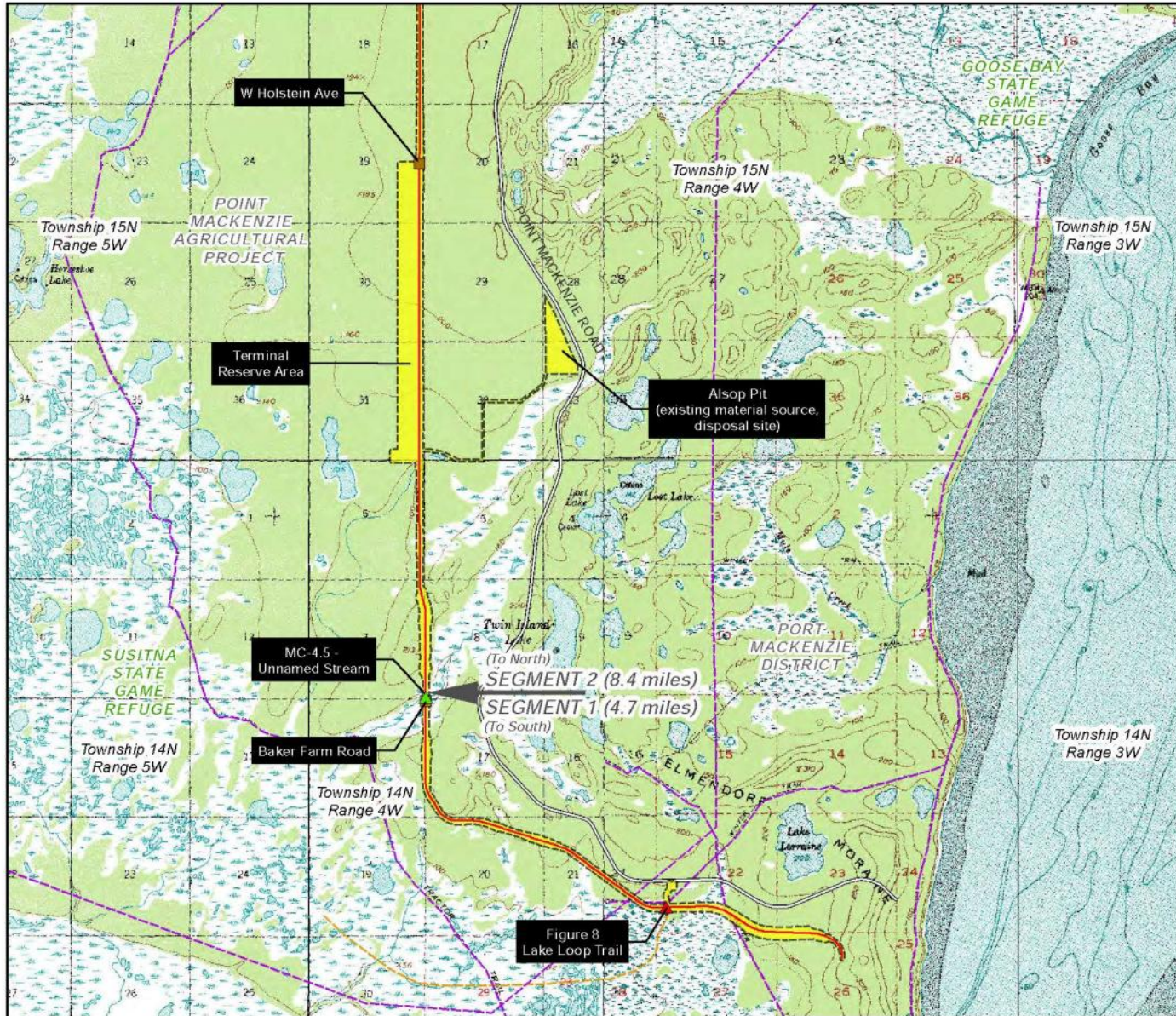


FIGURE 3

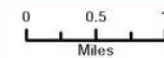


Area of Potential Effect  
USGS Base  
Extent 2

LEGEND

- Proposed Area of Potential Effect (APE)
- Proposed Alternative
- Trail
- Proposed Contributing Trails, SRB&A 2010
- Separated Grade Crossings**
  - Trail
  - Multi-Purpose
- At-Grade Crossings**
  - Road
  - Trail

NOTE: Crossing locations are based from Mat-Su Borough GIS and field survey data and may not align with what is shown on the USGS base map.



Date: May 12, 2011  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR/Alaska, Inc.  
 Sources: HDR/Alaska, Inc., Hanson  
 Base map: USGS 1:63,360  
 ANCHORAGE (8-B-C-8), TYONEK (8-1-C-1)

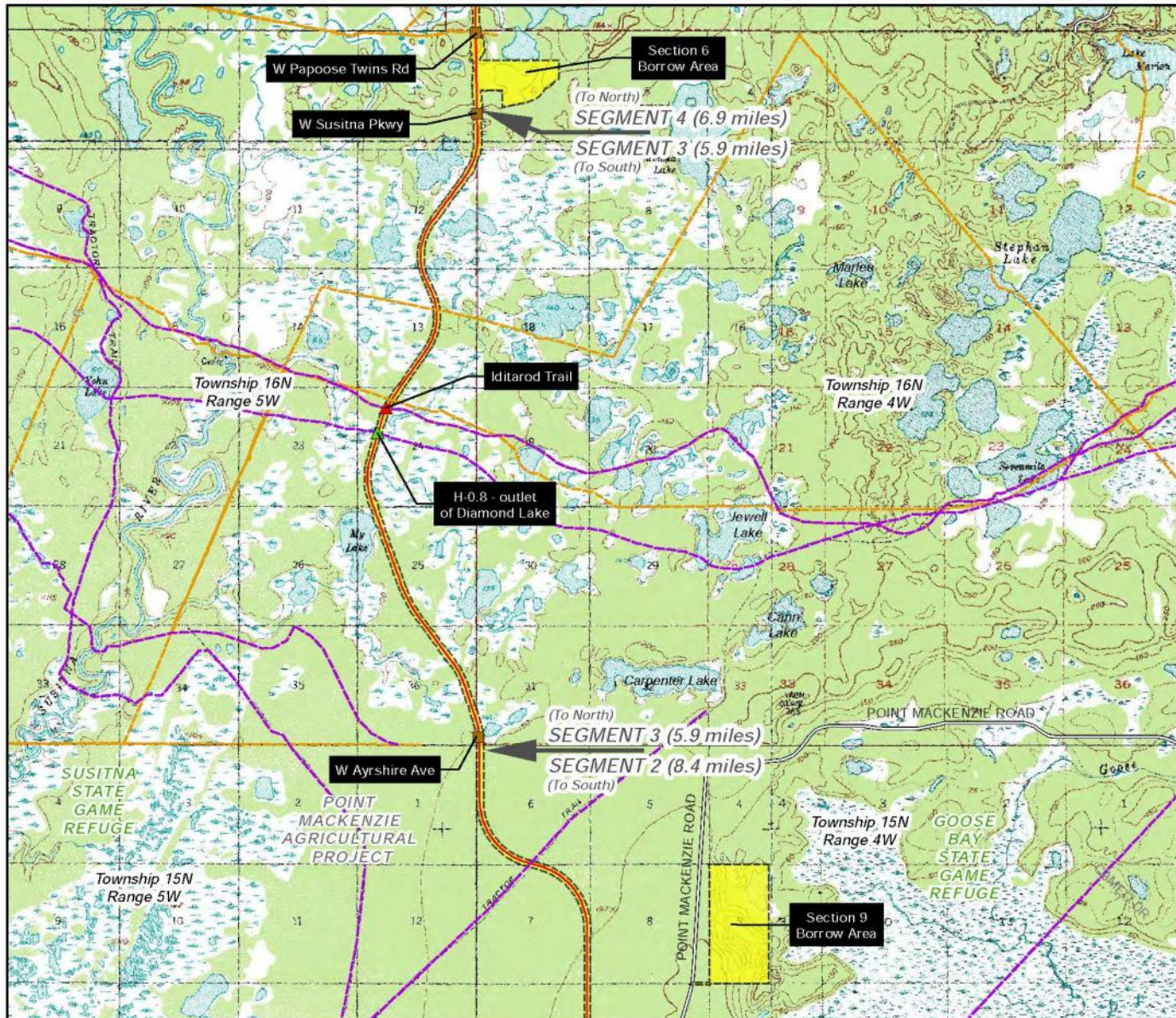


FIGURE 4

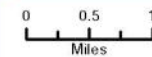


Area of Potential Effect  
USGS Base  
Extent 3

LEGEND

- Proposed Area of Potential Effect (APE)
- Proposed Alternative
- Trail
- Proposed Contributing Trails, SRB&A 2010
- Separated Grade Crossings
  - Trail
  - Multi-Purpose
- At-Grade Crossings
  - Road
  - Trail

NOTE: Crossing locations are based from Mat-Su Borough GIS and field survey data and may not align with what is shown on the USGS base map.



Date: May 12, 2011  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR/Alaska, Inc.  
 Sources: HDR/Alaska, Inc., Hanson  
 Base map: USGS 1:63,360  
 ANCHORAGE (C-8), TYONEK (C-1)

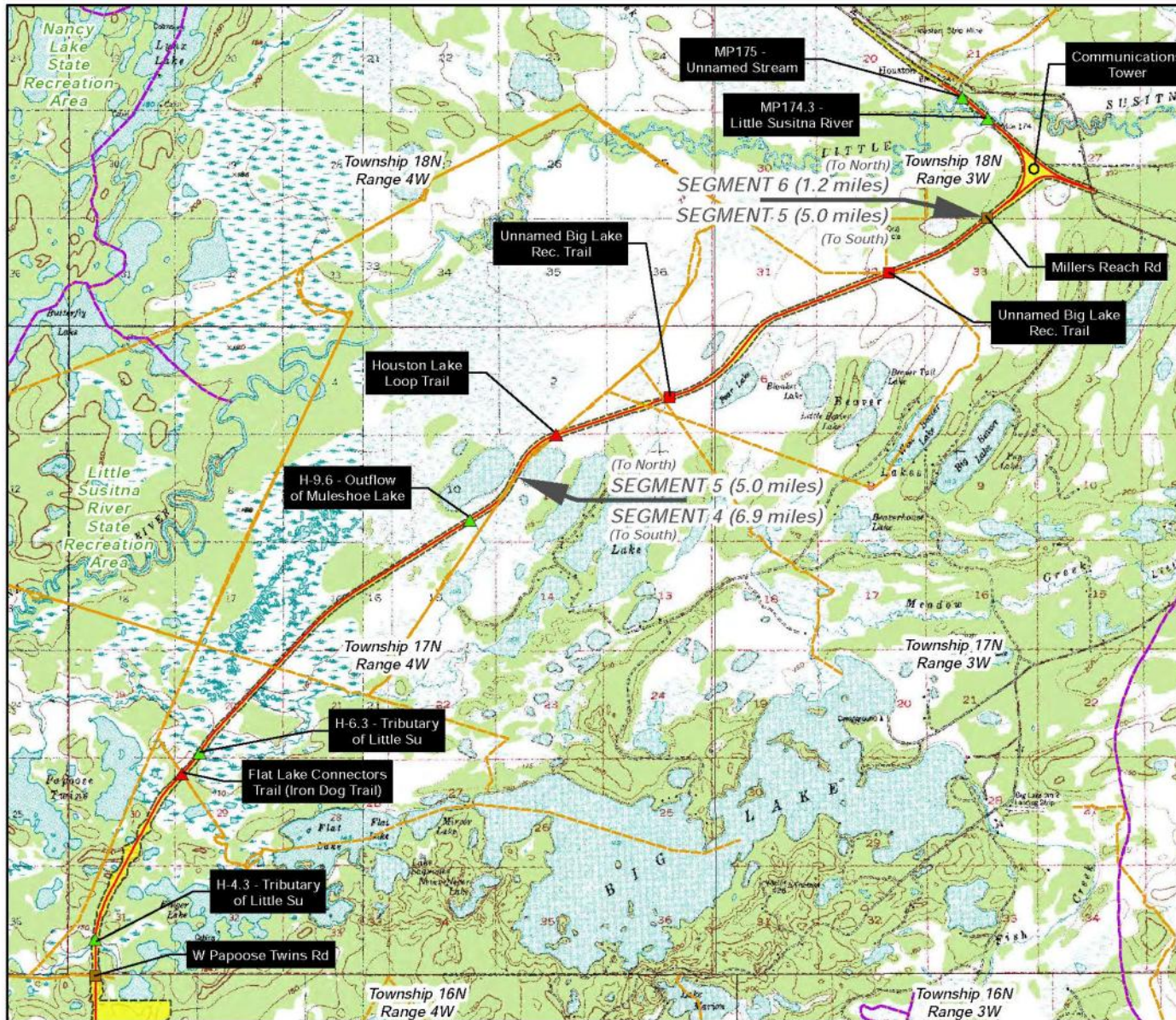


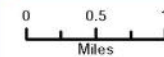
FIGURE 5



Area of Potential Effect  
Aerial Imagery  
Extent 1

LEGEND

- Proposed Area of Potential Effect (APE)
- Proposed Alternative
- Trail
- Proposed Contributing Trails, SRB&A 2010
- Highway
- Medium Rd
- Minor Rd
- Separated Grade Crossings**
  - Trail
  - Multi-Purpose
- At-Grade Crossings**
  - Road
  - Trail



Date: May 12, 2011  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: HDR Alaska, Inc., Hanson  
 Basemap: NRCS Imagery 2004

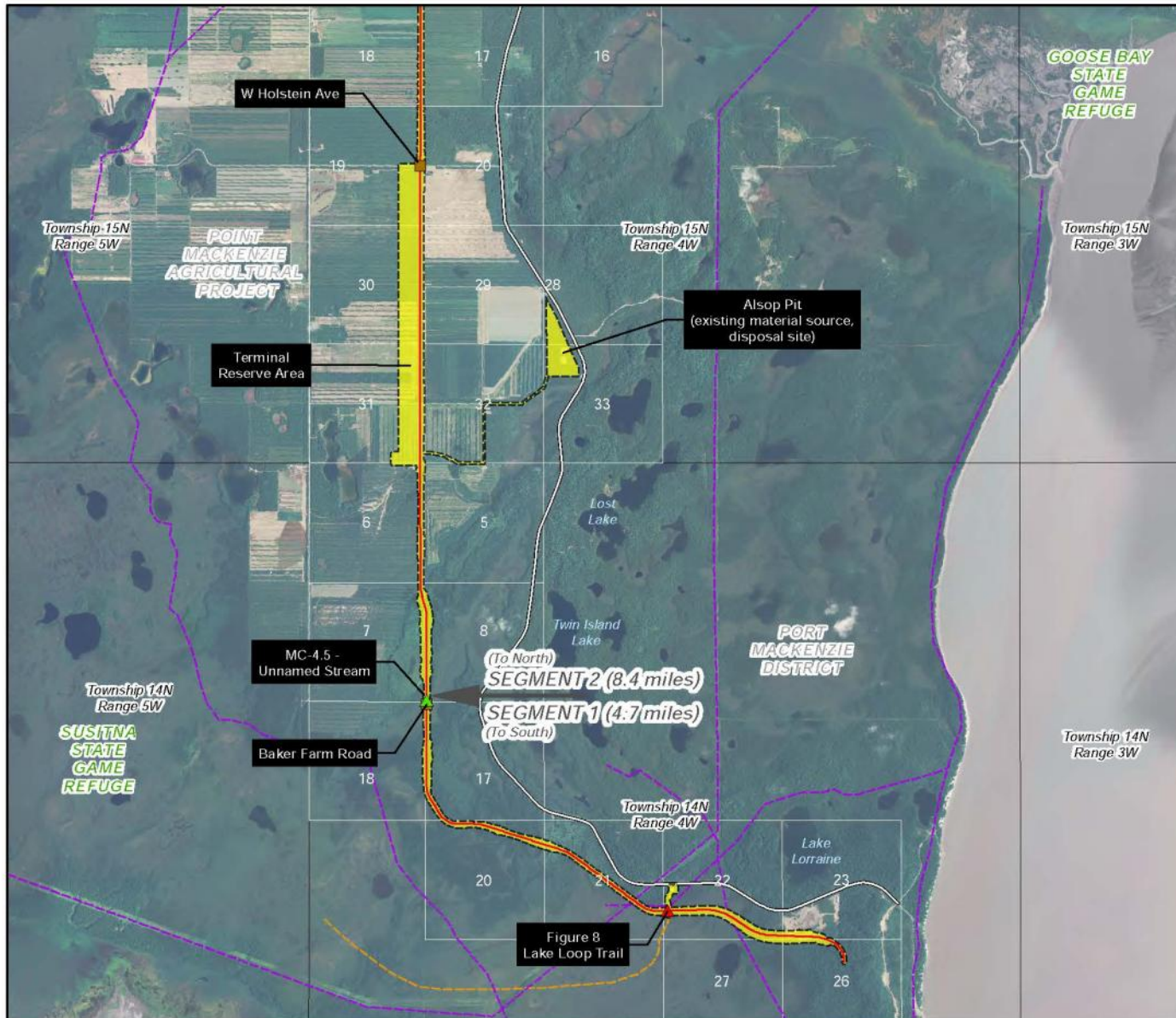


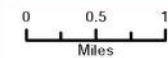
FIGURE 6



Area of Potential Effect  
Aerial Imagery  
Extent 2

LEGEND

- Proposed Area of Potential Effect (APE)
- Proposed Alternative
- Trail
- Proposed Contributing Trails, SRB&A 2010
- Highway
- Medium Rd
- Minor Rd
- Separated Grade Crossings**
  - Trail
  - Multi-Purpose
- At-Grade Crossings**
  - Road
  - Trail



Date: May 12, 2011  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR Alaska, Inc.  
 Sources: HDR Alaska, Inc., Hanson  
 Basemap: NRCS Imagery 2004

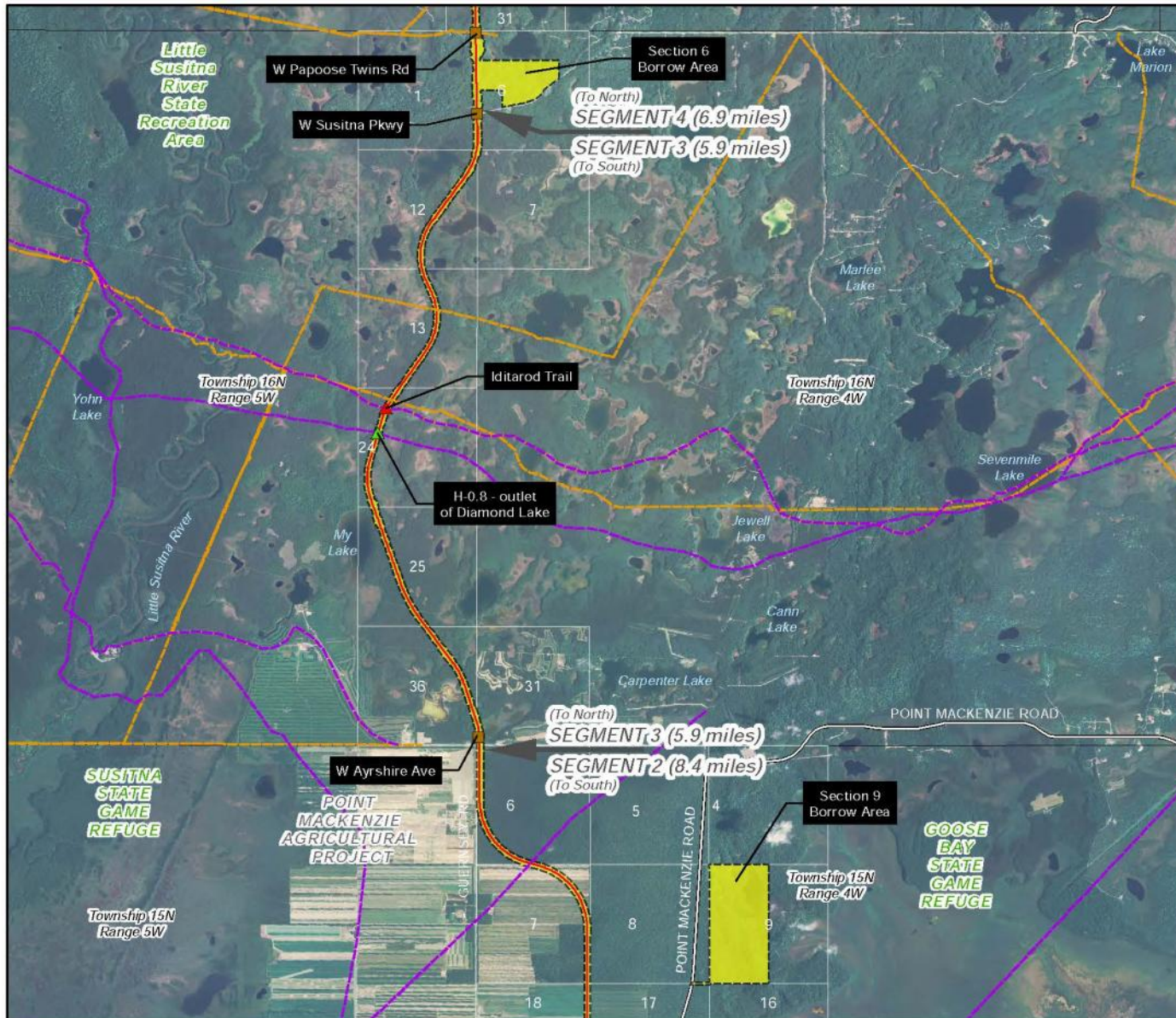


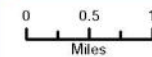
FIGURE 7



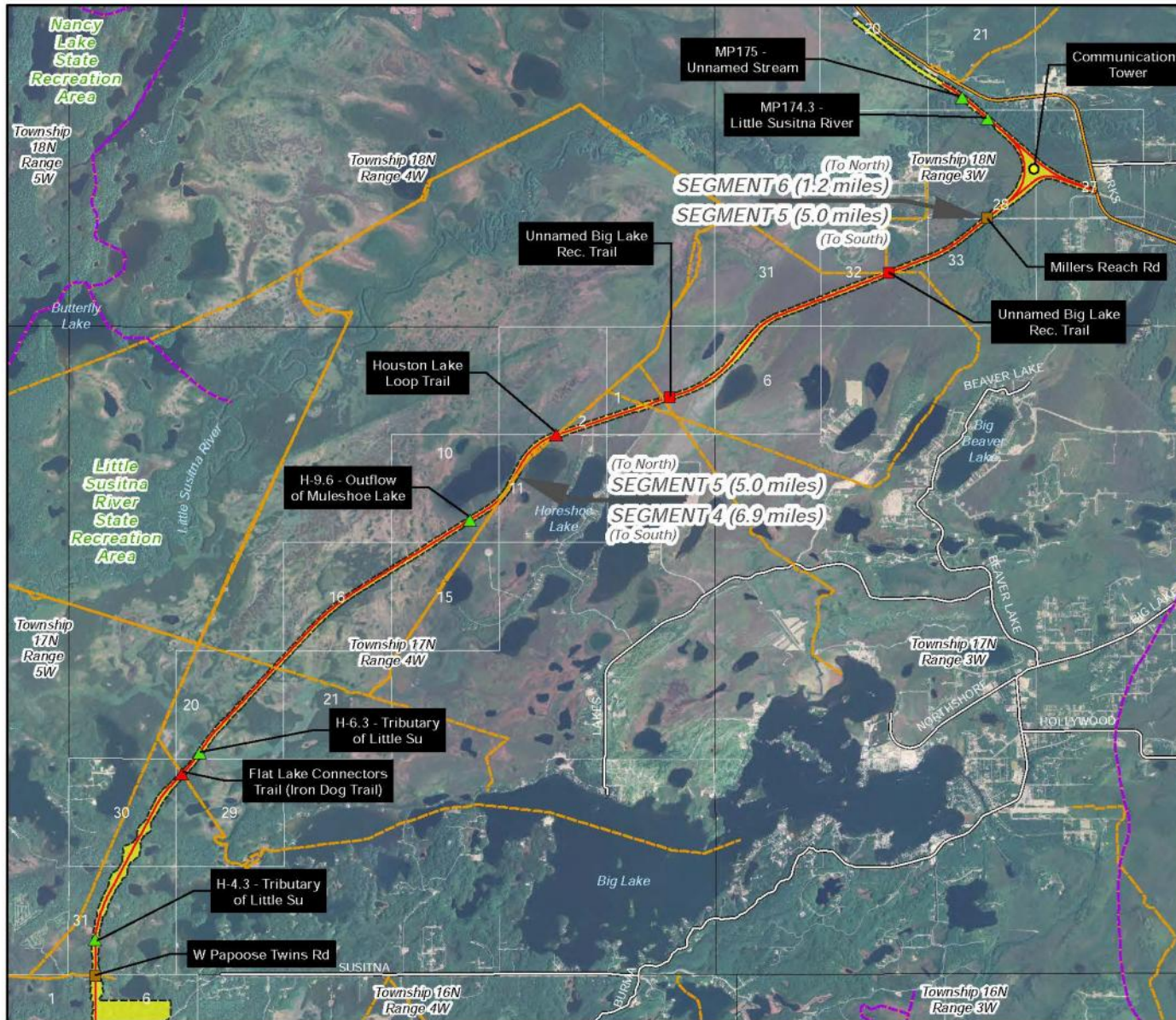
Area of Potential Effect  
Aerial Imagery  
Extent 3

LEGEND

- Proposed Area of Potential Effect (APE)
- Proposed Alternative
- Trail
- Proposed Contributing Trails, SRB&A 2010
- Highway
- Medium Rd
- Minor Rd
- Separated Grade Crossings**
- Trail
- Multi-Purpose
- At-Grade Crossings**
- Road
- Trail



Date: May 12, 2011  
 Projection: Alaska State Plane Zone 4, NAD 83  
 Author: HDR/Alaska, Inc.  
 Sources: HDR/Alaska, Inc., Hanson  
 Basemap: NRCS Imagery 2004





**Iditarod Dog Sledding Historic District (IDSHD)**

**Small Focus Group Workshop**

**Thursday, July 7, 2011**

**6:00-8:00 pm**

Big Lake Library, Big Lake, Alaska

**AGENDA**

- I. Welcome and Introductions
- II. Overall Goal of the IDSHD Small Focus Group Workshop
  - a. Develop and address the location and design of trail crossings to minimize or mitigate impacts to important characteristics of the IDSHD focusing on connectivity and use of the landscape.
- III. Brief Discussion of the Proposed Area of Potential Effect (APE)
- IV. Discussion of Comments/Issues Identified during the June 28<sup>th</sup> Workshop
  - a. Questions/Comments
- V. Workshop Wrap-up
- VI. Next Steps
  - a. IDSHD July 19 Workshop
  - b. MSB recreational trail user meeting
  - c. IDSHD Workshop Summary and Implementation Plan
- VII. Adjournment





**Iditarod Dog Sledding Historic District Focus Group Meeting**  
**Thursday, July 7, 2011**  
**6:00 – 8:00 pm**  
**Big Lake Public Library**  
**Big Lake, Alaska**

<i>First Name</i>	<i>Last Name</i>	<i>Address</i>	<i>Phone</i>	<i>Email</i>
Kelley	Griffin	8796 W Sunset 99623	373-1126	Kelleyg@mtaonline.net
Chas	St. George	591 E. Park Hwy #106 <sup>99654</sup>	352-225	stjorge@idland.com
Lesli	Schick	550 W. 7 <sup>th</sup> Ave. <sup>Su 900C, Anc. 99501</sup>	334-2679	Lesli.Schick@alaska.gov
Shimi	duVall	550 W. 7 <sup>th</sup> Ave. <sup>St. 99504 1310 Anc. 99501</sup>	269-8720	shina.duvall@alaska.gov
DOUG	GASEK	" "	269-8726	doug.gasek@alaska.gov
Frau	Seager-Boss	Mat-Su Borough	745-9859	fseagerboss@matsugov.us
BRIAN	LINDAMOND	327 W SHIP <sup>99581</sup> <del>ANCE</del> Ave	265-3095	lindamond@alaska.gov

*[Handwritten scribbles]*



Iditarod Dog Sledding Historic District Focus Group Meeting

Thursday, July 7, 2011

6:00 – 8:00 pm

Big Lake Public Library

Big Lake, Alaska

First Name	Last Name	Address	Phone	Email
Judy	Bitman	OHA/SHPO		
DAN	Mayfield	PO Box 520705 Big Lake AK 99652	223-2447	biglake trails@gmail.com
Bruce	Paulsen		745-9867	Bruce.Paulsen@netsugov.us
Barb	Hotchkiss	ARRC	265-2313	
Becca	Moxo	POB 652 Willow AK 99688	775-1972	BECCA-MOXO@YAHOO.COM
VERN	CHEKRESKI	ECHO LK DR B.L	892-6325	
TERRY	LANGHOLZ	9741 S. Doc Sued St WASILLA AK 99654	873-7533	tLangholz@gci.net
JON.	BRAUTIGAN	11053 S MALAMUTE RUE WASILLA	373-1103	JON_BRAUTIGAN@HOTMAIL
Angie	Walle			
Elaine	Martin	PO Box 879586 wasilla	357-5840	



**Iditarod Dog Sledding Historic District Focus Group Meeting**  
**Thursday, July 7, 2011**  
**6:00 – 8:00 pm**  
**Big Lake Public Library**  
**Big Lake, Alaska**

<i>First Name</i>	<i>Last Name</i>	<i>Address</i>	<i>Phone</i>	<i>Email</i>
Vern	Halter	Willow	495-1197	shalter@iditarod.com
Gene	Martin	7 mi KGB	357-5840	
GREG	BILL	IDITAROD P.O. BOX 870800 WASILLA 99687	746-1000	GBILL@IDITAROD.COM



## SMALL GROUP DISCUSSION SUMMARY

### Iditarod Dog Sledding Historic District Workshop

Tuesday, June 28, 2011

Willow Area Community Organization Center, Willow, Alaska

Discussions at the June 28th Iditarod Dog Sledding Historic District (IDSHD) Workshop primarily focused on access, safety, and connectivity concerns. This document summarizes comments on these topics recorded during the workshop to facilitate discussions during the Small Focus Group Workshop on July 7, 2011. The purpose of the Small Focus Group discussion is to review and clarify workshop comments and discuss options for addressing the issues.

- Recommendations for additional crossings or trails:
  - Separated grade crossing (bridge) for the historic Iditarod Trail between Crossings #10 and #11 (north of the actual Iditarod Race route)
  - Crossing for the Iron Dog trail [*is this Crossing #7?*]
  - Crossing for the existing trail used by mushers to avoid Iron Dog traffic (between Crossings #5 and #6)
  - Crossing for contributing trail between Crossings #13 and #14 [*is this trail in use?*]
  - Crossings at seismic and/or section lines in Big Lake area (Crossings #2 through #9)
  - Trail parallel to the proposed rail right-of-way
  
- Safety issues:
  - Recommend increasing clearance under bridges to more than 12 feet to account for snow depth and removal of snow from rail portions over bridges
  - Reroutes and combining trails (e.g., routing snowmachiners off of seismic lines onto Crossing #11) may “bottleneck” users and increase the potential for collisions and user conflicts between snowmachiners and mushers
  - Concerns about locations and/or types of crossings (e.g., Crossing #9 is currently proposed to be an at-grade crossing in a high vehicle traffic area)
  - Concerns about design (e.g., span width, crossing approaches)

Additional comments recorded during the June 28th Workshop did not pertain to the IDSHD or are outside the scope of these workshops. These comments are not included in this summary document and will be addressed as appropriate during later meetings with mushers and other recreational trail users in the project area.

# Types of Rail Crossings



Coastal Trail Culvert/Pedestrian Crossing

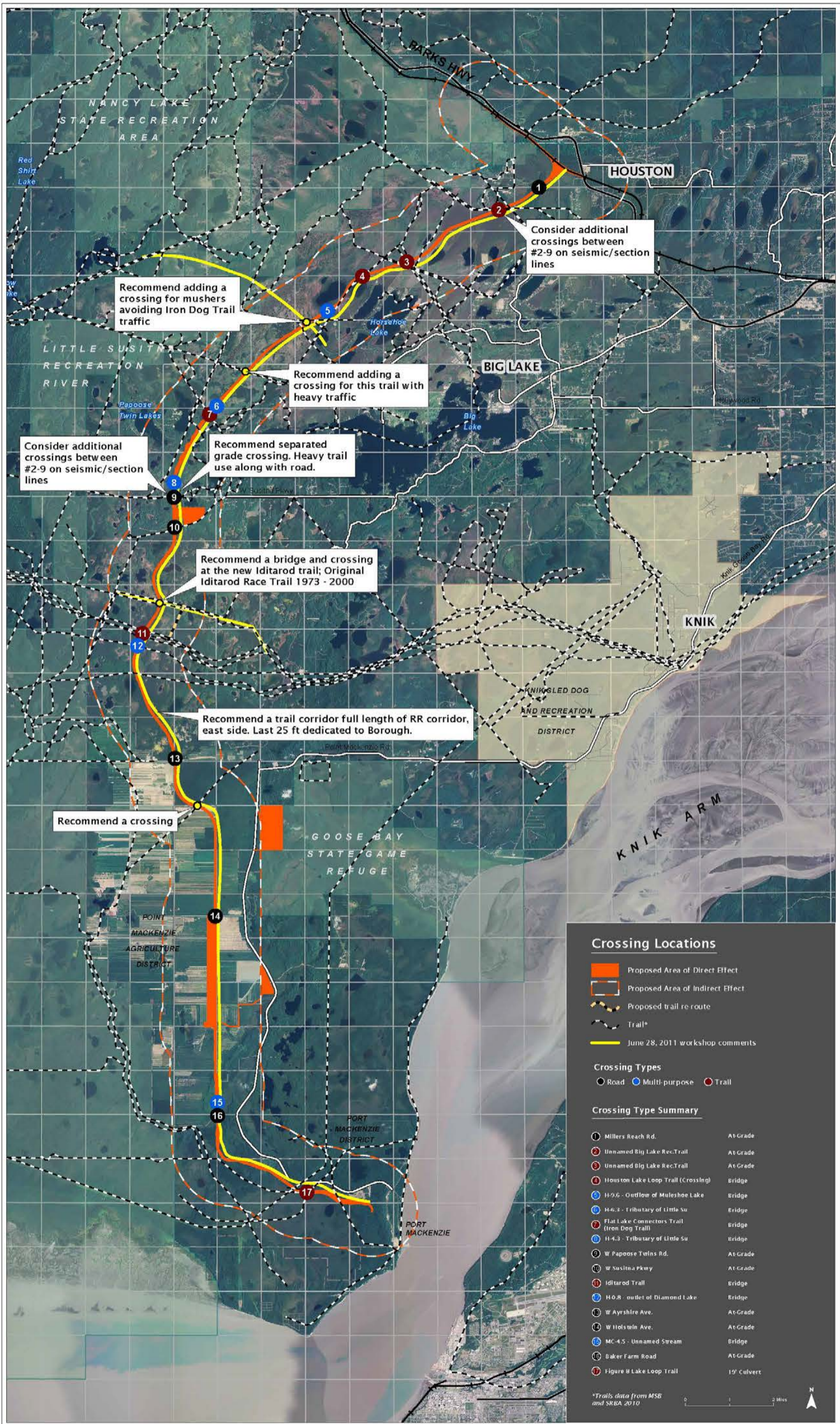


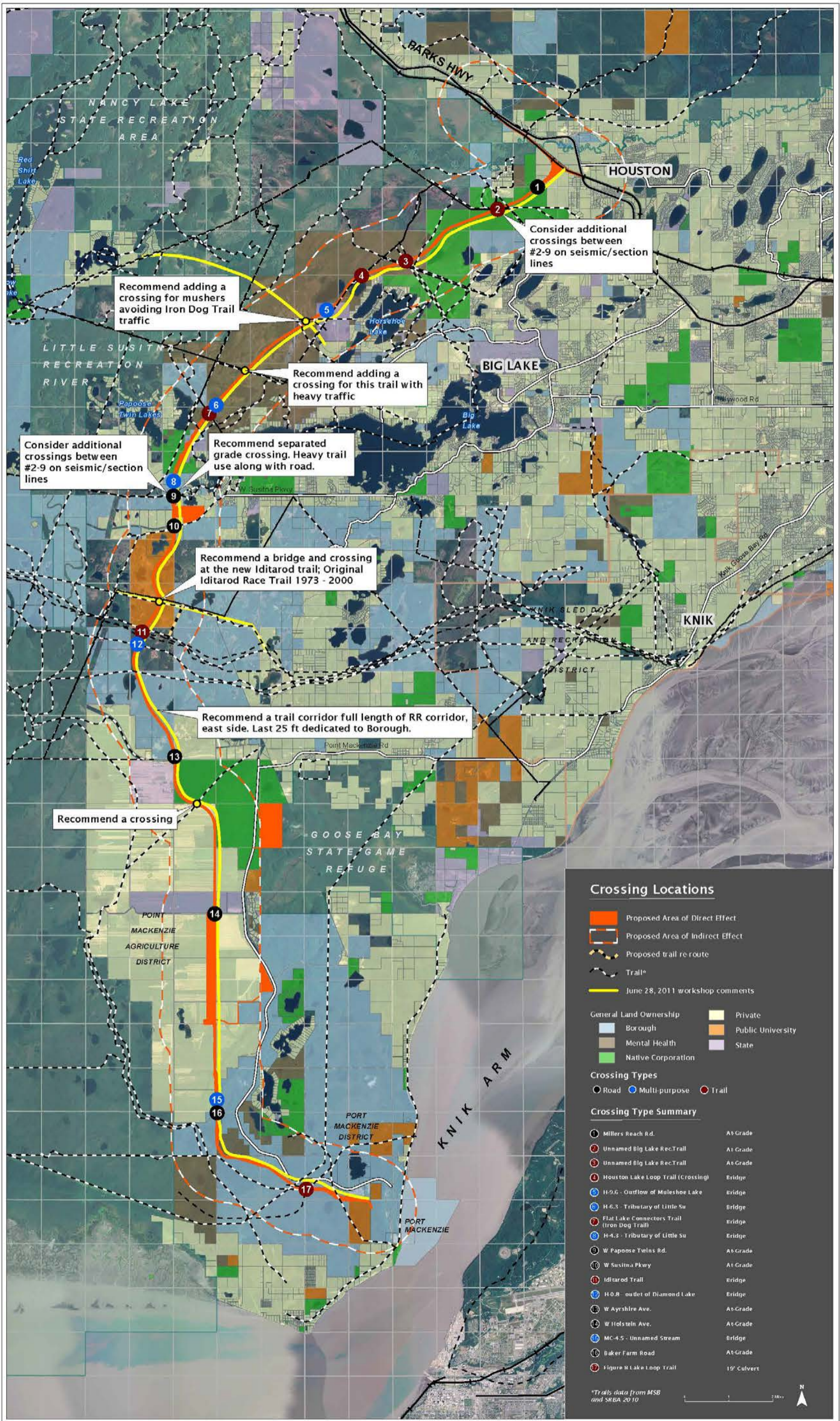
One-Span Multi-Purpose Crossing

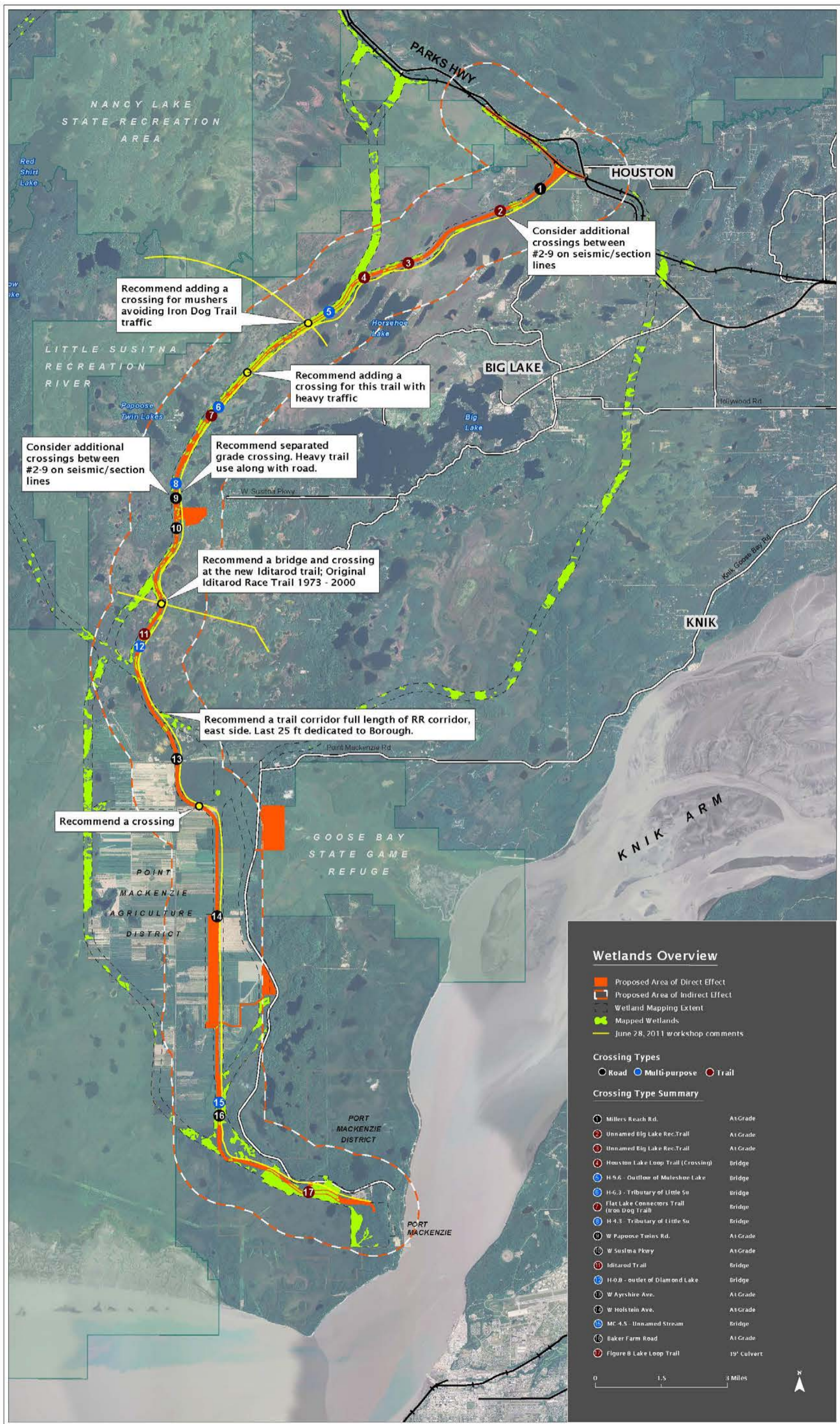


Triple-Span Multi-Purpose Crossing









### Wetlands Overview

- Proposed Area of Direct Effect
- Proposed Area of Indirect Effect
- Wetland Mapping Extent
- Mapped Wetlands
- June 28, 2011 workshop comments

### Crossing Types

- Road
- Multi-purpose
- Trail

### Crossing Type Summary

1	Millers Reach Rd.	At Grade
2	Unnamed Big Lake Rec.Trail	At Grade
3	Unnamed Big Lake Rec.Trail	At Grade
4	Houston Lake Loop Trail (Crossing)	Bridge
5	H-9.6 - Outflow of Muleshoe Lake	Bridge
6	H-6.3 - Tributary of Little Su	Bridge
7	Flat Lake Connectors Trail (Iron Dog Trail)	Bridge
8	H-4.3 - Tributary of Little Su	Bridge
9	W Papoose Twins Rd.	At Grade
10	W Susitna Pkwy	At Grade
11	Iditarod Trail	Bridge
12	H-0.8 - outlet of Diamond Lake	Bridge
13	W Ayrshire Ave.	At Grade
14	W Holstein Ave.	At Grade
15	MC-4.5 - Unnamed Stream	Bridge
16	Baker Farm Road	At Grade
17	Figure 8 Lake Loop Trail	19' Culvert







# PORT MACKENZIE

## RAIL EXTENSION

# MSB Trails

# Open House

The Matanuska-Susitna Borough is hosting a trail user meeting for the Port MacKenzie Rail Extension Project. The purpose of the meeting is to provide trail users the opportunity to provide feedback on proposed crossing and to ensure continued connectivity and use during, and after construction. We need to hear from you to make this happen. Please join us on:

**Thursday, October 27 from  
6 to 8pm at Houston High School**

Houston High School is located at 12501 W. Hawk Lane in Houston, Alaska. Come and learn more about the project and provide feedback. For more project information visit: [www.portmacrail.com](http://www.portmacrail.com).

---

## Have Questions?

### Contact:

HDR Alaska, Inc.  
Rosetta Alcantra, Public Involvement  
Phone: (907) 745-4129  
Email: [Rosetta.Alcantra@hdrinc.com](mailto:Rosetta.Alcantra@hdrinc.com)





**PORT MACKENZIE**  
RAIL EXTENSION

**MSB Trails Meeting**

October 27, 2011 -6:00 pm  
Houston High School, Houston, Alaska

Name	Organization	Phone	Email
Cathy Mayfield	BLT	227-5077	jdmayfield@ak.net
Dan Mayfield	Big Lake Trails	223-2447	biglaketrails@gmail.com
Steve Charles	Willow Trails	495-4368	scharles@mtaonline.net
Frau Seager-Boss	MSB Cultural Review	745-9859	Fseagerboss@mta.gov.us
Kyle Kidder	DNR-SCRO	269-8564	KYLE.KIDDER@ALASKA.GOV
ELIZABETH GROWER	HDR	644-2077	Elizabeth.Grower@hdrinc.com
Aneje Wade	KTC	373-3170	awade@kniktribe.org
Richard Plack	JR IDITAROD	745-5222	leaddog@mtaonline.net
Mike Donald	Big Lake TRAILS		
Cole Donald	with him ↑		
Mark Nordman	Iditarod Trail Comm	360-1814	mark.nordman@iditarod.com
RICHARD GAFFEY	Big Lake Trails	892 5280	PARTYCHIEF360@gmail.com
Todd Rinaldi	mat-SU Borough Parks/Rec/Trails AB	227-5037	tarinaldi@yahoo.com
Card Tyler	Aurora Dog MUDERS	892-8110	tylertm@gmail.com
CORINNE FURFUEH	Northern Lights 300 Dog Race	232-2019	CORINNE_FURFUEH@MAC.COM



### MSB Trails Meeting

October 27, 2011 - 6:00 pm  
Houston High School, Houston, Alaska

Name	Organization	Phone	Email
Shina duVall	DNR/SHPO	269-8720	shina.duvall@alaska.gov
Brad Sworts	MSB	746-7430	brad.sworts@mat.sugov.us
Chris Kosinski	(self) 6726 Nemna Place, Anch. AK	350 4145	CKOSINSKI@SCI.NET
Jack Campbell	Matsa Trails Council	376-4232	
Jamie C Smyth	Perserverance Springs Farm	232-2019	Cimsmyth@gmail.com
Vera Hatter	Assembly	495-1197	vhatter@mtaonline.net
McStanulescu	WTC	495-0912	-
TINA Owen	WTC	495-0912	-
Ellen Halverson		892-2535	ehalvers@provaak.org
Bruce Paulsen	MSB	745-9867	
Noreen Austermaul		892 8056	denner@mtaonline.net
Joseph Buzard	Houston Planning	892-1924	-







*Welcome!*

*Please Sign In*

Port MacKenzie Rail Extension Project

MSB Trails Open House

[www.portmacrail.com](http://www.portmacrail.com)





# Meeting Purpose

To demonstrate and explain the crossings, connectivity and constraints associated with the trails within the project area.





# Project Team

Alaska Railroad Corporation..... Applicant

Matanuska-Susitna Borough.....Project Sponsor

HDR Alaska, **Inc**.....Environmental Consultant

Hanson Alaska, LLC.....Engineering Consultant







# Project Schedule



- March 25, 2011 – Release of Final Environmental Impact Statement
- Spring 2011 – Permit Applications were submitted to State and Federal agencies and MSB for review and comments.
- Winter 2011 – Record of Decision
- Construction is scheduled to begin Winter 2011
- Fall 2015 – Ribbon Cutting





# Port MacKenzie Trail Crossing Cost Estimates

Feature Crossing	Structure	Approx. Station	Total Cost of Crossing
1. Millers Reach Road	At-Grade Road Crossing	5471+20	\$302,500.00
2. Utility easement/Big Lake Trail*	At-Grade Crossing	5416+50	\$30,000.00
3. Private property/Big Lake Trail*	At-Grade Crossing	5287+30	\$25,000.00
4. Houston Lake Loop Trail	Bridge	5235+00	\$1,600,000.00
5. Unnamed Tributary to the Little Susitna River (H 6.3)	Bridge	4394+70	\$2,200,000.00
6. Unnamed Tributary to the Little Susitna River (H 4.3)	Bridge	4288+30	\$1,600,000.00
7. West Papoose Twins Road	At-Grade Road Crossing	4270+80	\$50,000.00
8. West Susitna Parkway	At-Grade Road Crossing	3434+40	\$50,000.00
9. Iditarod Historic Trail	Bridge	3293+40	\$1,550,000.00
10. Outflow of Diamond Lake (H 0.8)	Bridge	3277+50	\$2,200,000.00
11. Aryshire Avenue	At-Grade Road Crossing	826+70	\$75,000.00
12. Holstein Avenue	At-Grade Road Crossing	618+00	\$50,000.00
13. Baker Farm Bridge (MC 4.5)	Bridge	380+70	\$1,600,000.00
14. Figure 8 Lake Loop Trail	Bridge	216+00	\$335,000.00

*\*If crossing is not approved by the landowner or easement holder, reroutes will be provided to the nearest crossing.*

**Total cost of crossings \$ 11,667,500.00**

[www.portmacrail.com](http://www.portmacrail.com)



# Public Outreach – Past, Present & Future

2007

- The Point MacKenzie Rail Extension Project corridor was evaluated in 2007 in the Preliminary Environmental and Alternatives Report (PEAR) a comprehensive report produced by the Railroad, contractors, and the Borough. In mid-January 2008, the MSB and the ARRC submitted the PEAR to the Surface Transportation Board (STB), the lead federal agency responsible for approving new rail line.
- Public Involvement activities for the PEAR occurred between September and December 2007. Over 361 comments were received.

2008

- On Feb. 12, 2008, the STB issued a Notice of Intent (NOI) to prepare an Environmental Impact Statement (EIS), which kicked off the formal environmental review process required under the National Environmental Policy Act (NEPA).
- The STB held scoping meetings:
 

•Knik – March 3	•Houston – March 6
•Big Lake – March 4	•Wasilla – March 10
•Willow – March 5	•Anchorage – March 11

2009

- The STB considered agency and public input received during the scoping process and on July 17, 2009 issued the final scope of study for the Draft EIS.
- The STB published the final scope of study in the Federal Register, placed it on the STB Web site and project Web sites, and mailed an announcement of the availability of the final scope of study to approximately 8,000 individuals, agencies, and other interested parties on the STB project mailing list.

2010

- The STB issued a Draft EIS on March 16, 2010.
- The STB and cooperating agencies held 6 public meetings on the Draft EIS
 

•Anchorage– April 6	•Houston – April 12
•Big Lake – April 7	•Willow– April 13
•Wasilla– April 8	•Knik– April 14
- The STB distributed the Draft EIS through hard copy and CD-ROM mailing to maximize public awareness of the availability of the document and to provide instructions on how to comment on the Draft EIS. The list included approximately 8,000 contacts at the time STB distributed the Draft EIS. STB received a total of approximately 162 written and oral comments during the Draft EIS comment period, which closed on May 10, 2010.

2011

- The STB released the Final Environmental Impact Statement on March 25, 2011. The Final EIS recommended the Mac Central and Houston South as the proposed route for the Point MacKenzie Rail Extension.
- ARRC prepared and submitted permit applications
- Winter 2010/2011 Big Lake Trails Group Meeting
- April - ARRC Open House
- Section 106 outreach
  - June, July – Musers meetings
  - October - MSB trails meeting
  - November – Final Musers meeting



# What We Heard

## (from past meetings)

### Recommendations for additional crossings or trails:

- Separated grade crossing (bridge) for the historic Iditarod Trail (south of the actual Iditarod Race route)
- Crossing for the Iron Dog Connector Trail (Big Lake Trail #5).
- Crossing for the Flat Lake Connector Trail used by mushers to avoid Iron Dog traffic on Big Lake Trail #5 (Iron Dog Connector Trail).
- Crossing for Iditarod Race Trail from Knik between the Historic Race Trail and West Susitna Parkway.
- Crossings at seismic and/or section lines in Big Lake area.
- Trail parallel to the proposed rail right-of-way.

### Safety issues:

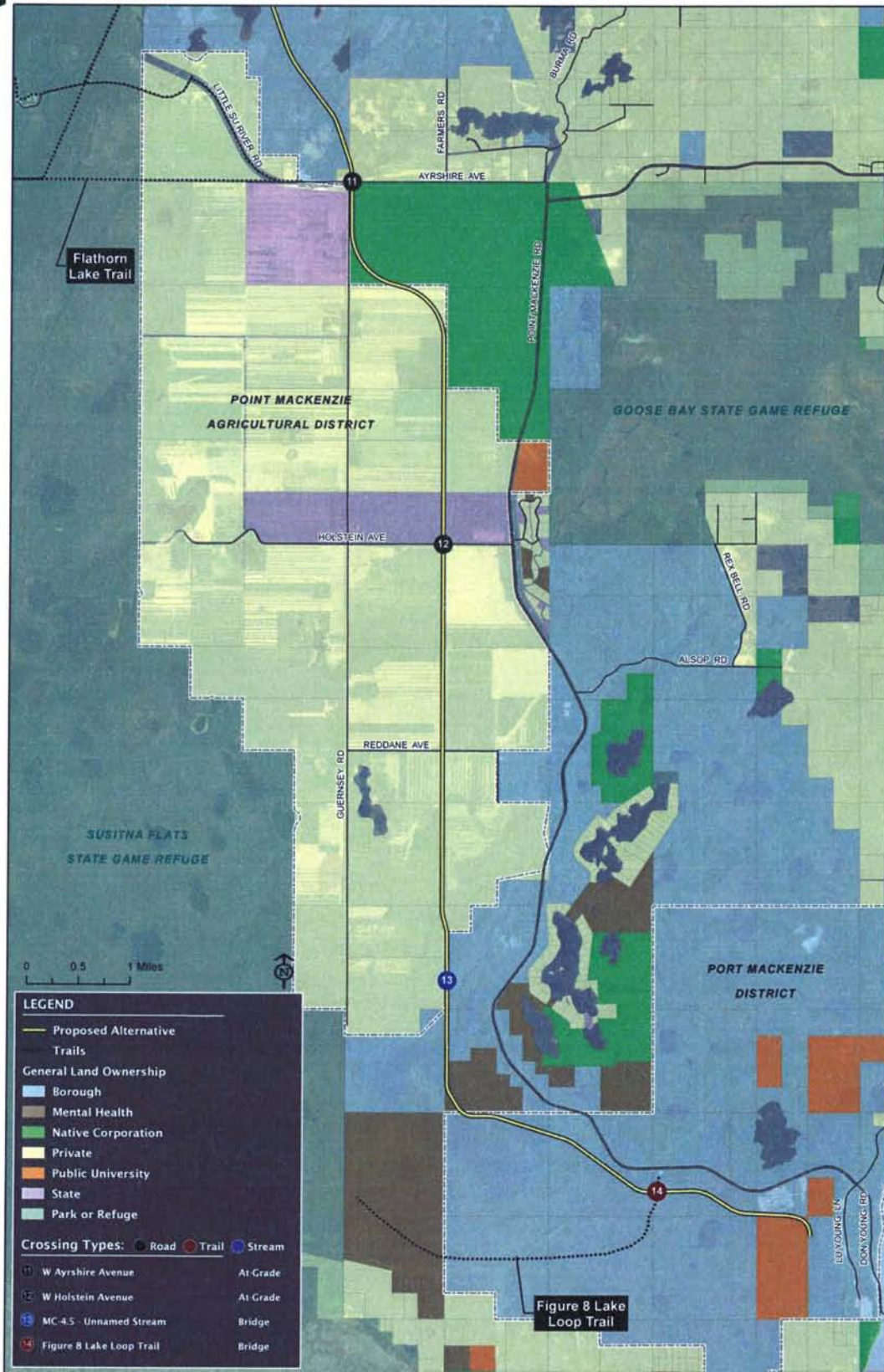
- Recommend increasing clearance under bridges from 12 feet to at least 14 feet to account for snow depth, removal of snow from rail portions over bridges and also to account for height of grooming equipment.
- Reroutes and combining trails may “bottleneck” users /wildlife and increase the potential for collisions and user conflicts.
- Concerns about locations and/or types of crossings (e.g., At-grade crossings on existing roadways)

### Other:

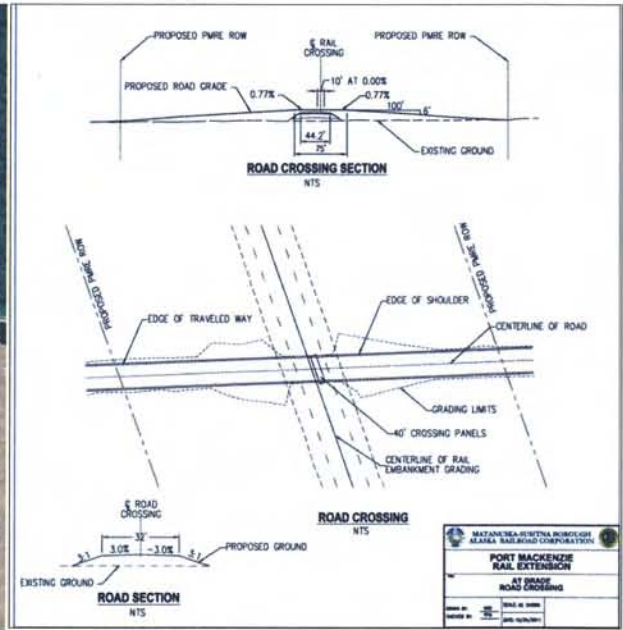
- Request the creation of a Special Use District similar to the one at Knik (Sled Dog and Recreation District) for protection of trails important to mushing.
- Take growth into account when planning crossings
- Provide information on where the ARRC cannot provide crossings and appropriate mitigation steps (e.g. VORTAC radar site)



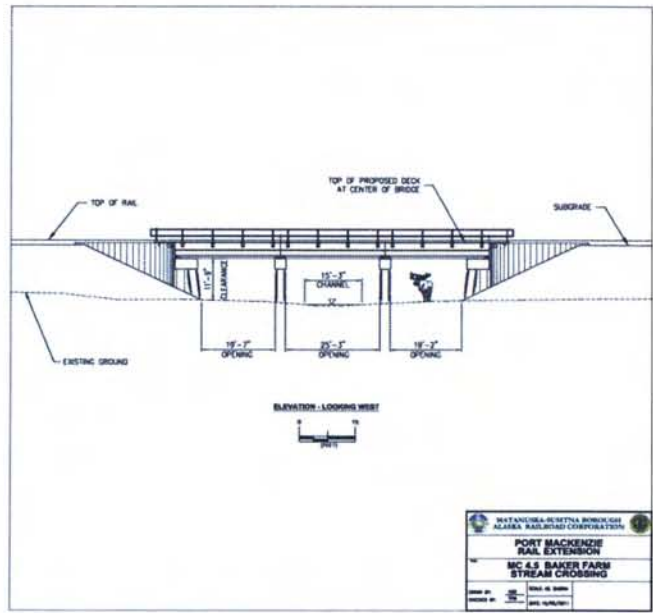
# Point MacKenzie Area



# Point MacKenzie

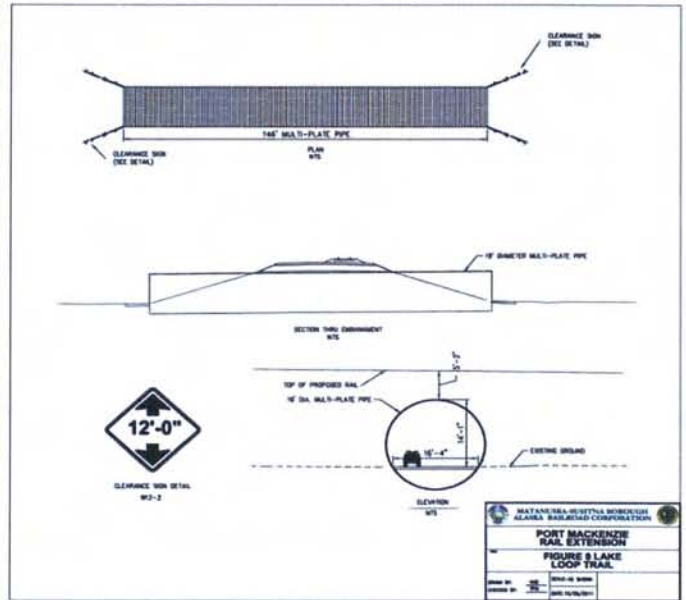


11. West Aryshire Avenue and 12. West Holstein Avenue  
Cost = \$125,000 (per crossing)



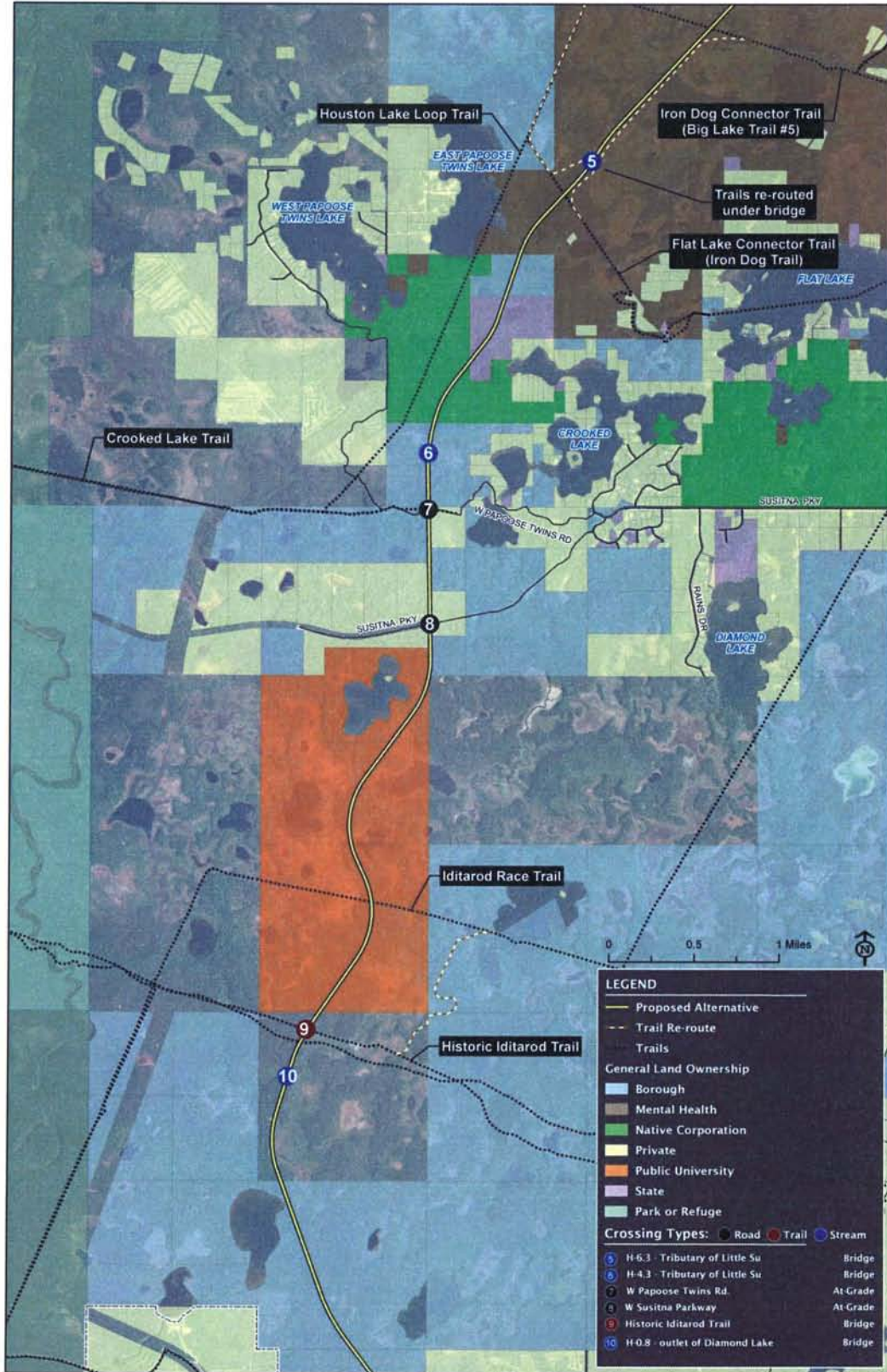
13. Baker Farm Stream Crossing (MC 4.5)  
Cost = \$1,600,000

# Point MacKenzie



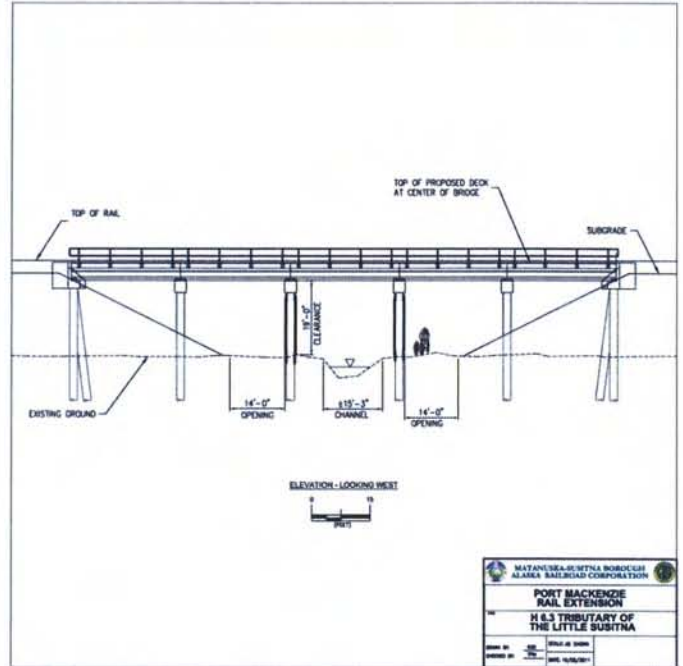
14. Figure 8 Lake Loop Trail  
Cost = \$ 335,000

# West Big Lake/Houston



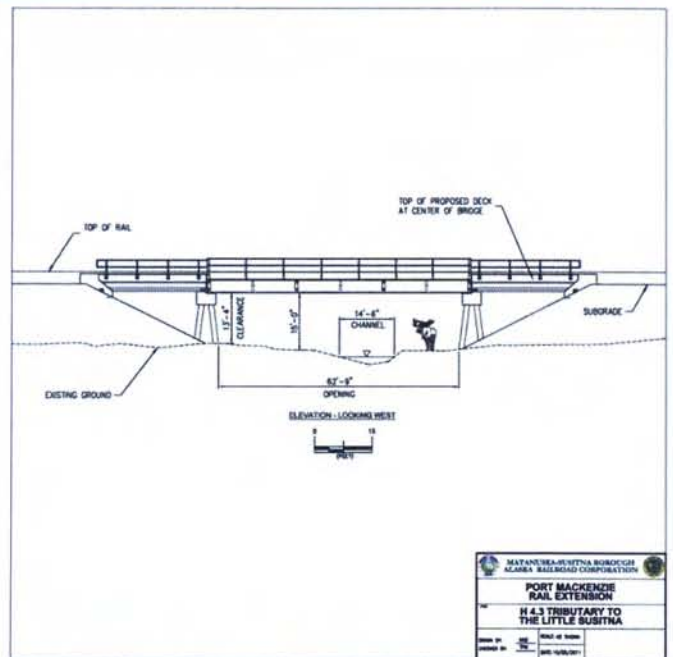


# West Big Lake/Houston



5. Unnamed Tributary to the Little Susitna River (H 6.3)

Cost = \$2,200,000

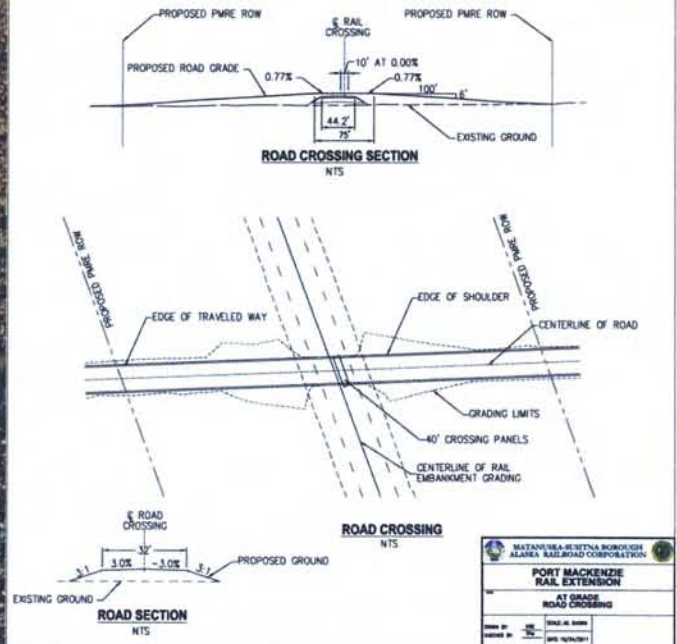
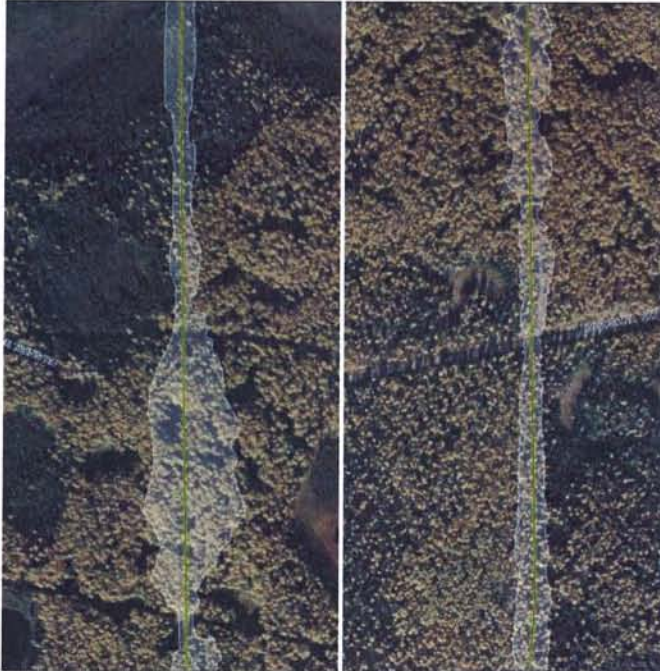


6. Unnamed Tributary to the Little Susitna River (H 4.3)

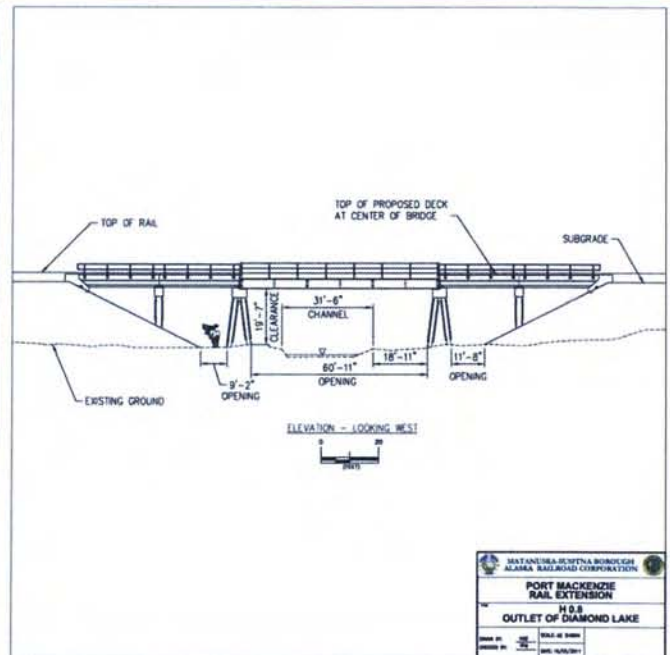
Cost = \$1,600,000



# West Big Lake/Houston

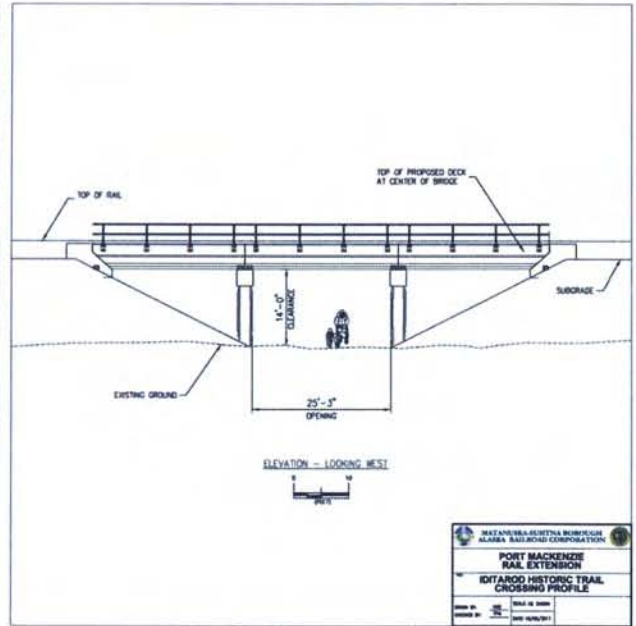


7. West Papoose Twins Road and 8. West Susitna Parkway  
Cost = \$100,000 (per crossing)



10. Outflow of Diamond Lake (H 0.8)  
Cost = \$2,200,000

# West Big Lake/Houston

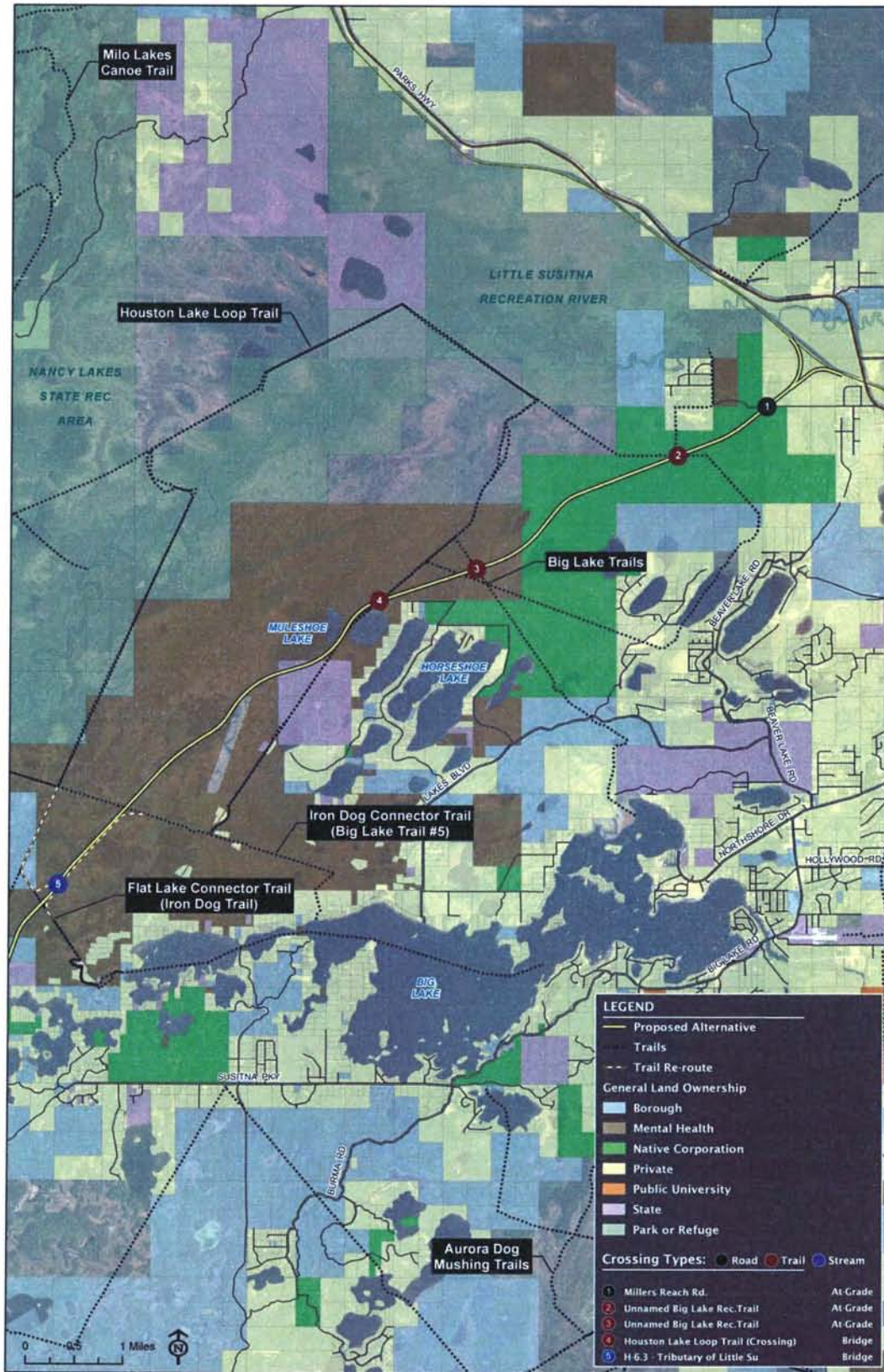


9. Iditarod Historic Trail  
Cost = \$1,550,000

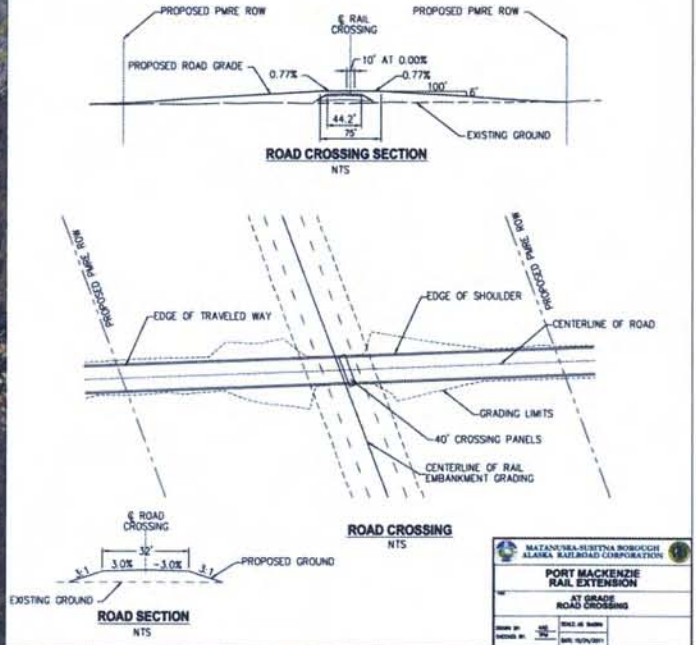


**Iditarod Race Trail Reroute**

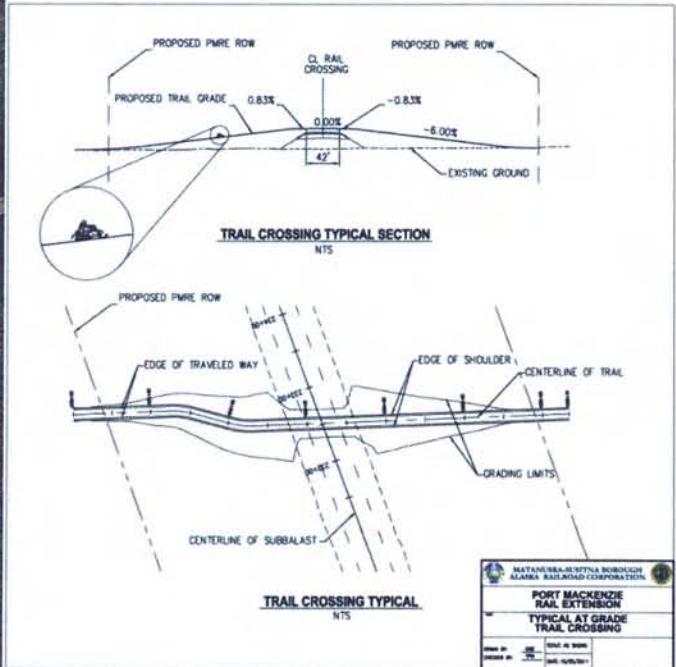
# North Big Lake/Willow



# North Big Lake/Willow

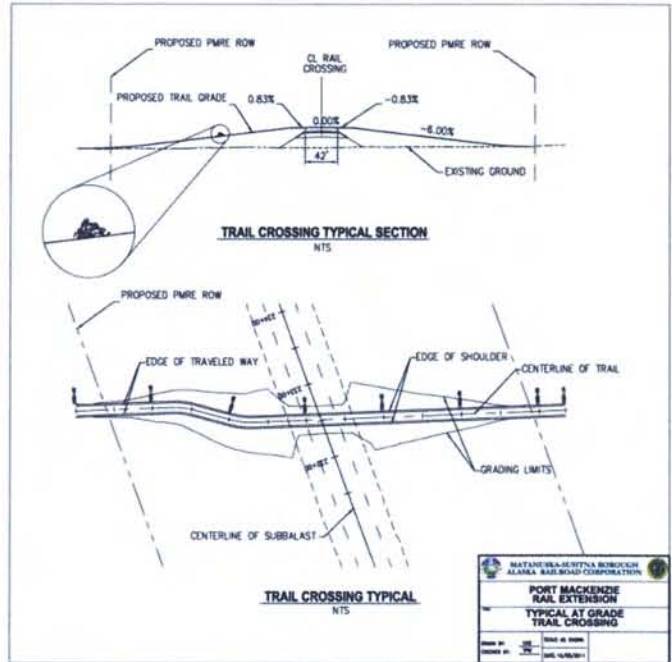


1. Millers Reach Road  
Cost = \$302,500

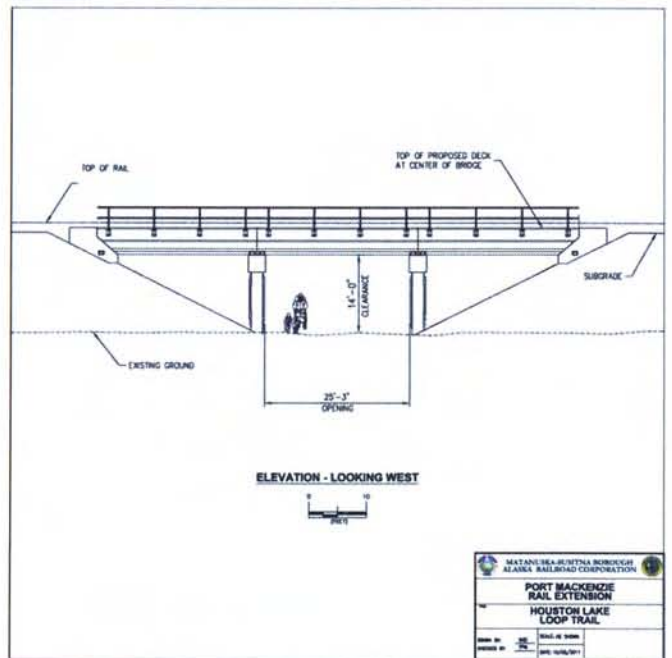
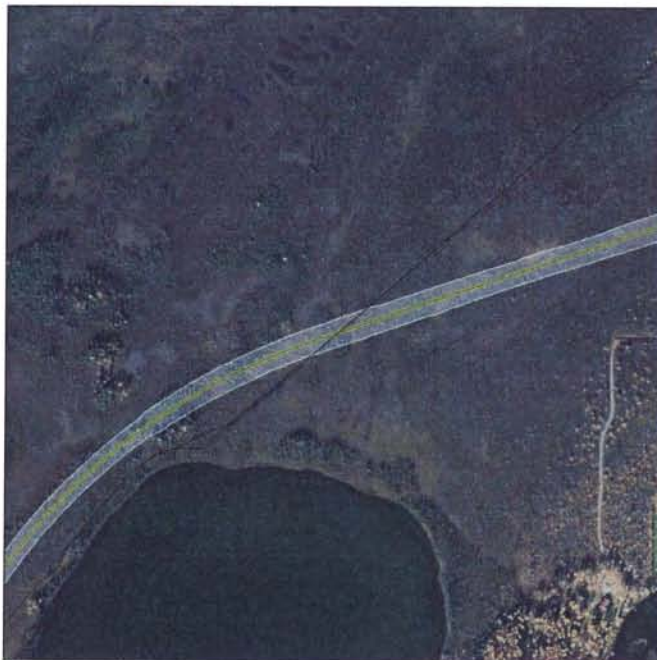


2. Utility Easement/Big Lake Trail  
Cost = \$30,000

# North Big Lake / Willow



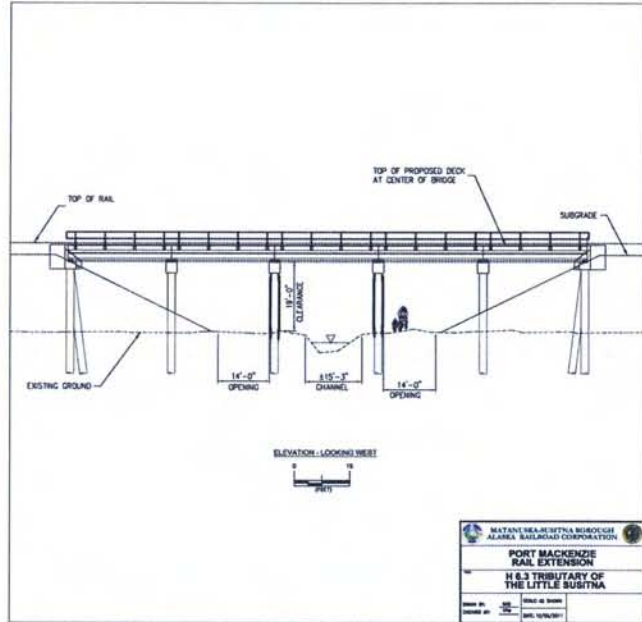
### 3. Private Property/Big Lake Recreation Trail Cost = \$25,000



### 4. Houston Lake Loop Trail Cost = \$1,600,000



# North Big Lake / Willow



5. Unnamed Tributary to the Little Susitna River (H 6.3)  
Cost = \$2,200,000



Flat Lake/Iron Dog Connector Reroute

# What's Next

- Update MSB trails plan to include new trails and reroutes created as part of the mitigation process.
- Construction schedule for new trail connections and upgraded trails.
- Compilation of and response to comments to be posted on Project website following meeting.
- One more mushers meeting to wrap up consultation for the Iditarod Dog Sledding Historic District.
- Record of Decision





# What signs are appropriate here?



## MSB Trail Meeting Comments and Responses - October 27, 2011

### 1. Point MacKenzie Access

Reference #	Comment	Response
1a.	Verify that the University of Alaska Access Rd. will be available for Public Use (not gated, drivable off end) so owners of Point MacKenzie properties can access their properties.	Point MacKenzie properties will be accessible by using either (1) the University of Alaska Access Road located along the section line immediately south of the Port MacKenzie Bi-Modal Bulk Facility or (2) the Figure 8 Lake Loop Trail crossing. Both access points are designed to accommodate vehicles; when using either access if additional trail clearing is necessary on Borough property a temporary permit can be issued by the MSB. Maps will be provided to interested parties upon request.

### 2. Historic Iditarod Trail (Project Crossing #9)

Reference #	Comment	Response
2a.	The Historic Trail needs improvement; it is too narrow for current and potential increased use.	Trail improvements are planned for the Historic Trail including trail widening in the vicinity of the rail embankment and reroutes.
2b.	The Historic Trail west of the crossing location near the Little Susitna washes out and is in ill repair, needs fixed (bridge needed).	Access to the 'race' trail will still be possible west of the alignment before the Historic Trail crosses the Little Susitna River.
2c.	The Historic Trail is not used as frequently as the race trail.	The Historic Trail will be improved in the vicinity of the crossing making it more likely to be used. To aid trail users, a kiosk identifying the user's location on a trails map will be installed to identify trail reroutes and show available trail connections.
2d.	Crossing signs are needed at this location	Proposed signs to be installed at the crossing include: <ul style="list-style-type: none"> <li>• Caution dog teams on trail</li> <li>• North/South arrows</li> <li>• Slow – Keep Right High Traffic Area</li> <li>• Clearance 14'</li> </ul>

## MSB Trail Meeting Comments and Responses - October 27, 2011

### 3. Iditarod Knik 'Race' Trail

Reference #	Comment	Response
3a.	Need a crossing here; this trail has hosted 20 Iditarod races and may have been used prior to being used for the race.	ARRC/MSB is working with the Alaska Department of Natural Resources (ADNR) and University of Alaska to determine if there are existing easements that cross the University's property at this location.
3b.	The Iditarod (Knik) 'race' trail is used more than the Historic Trail.	A crossing at this location will be provided if there is an existing easement or if the University of Alaska authorizes and requests a crossing at this location on their property.
3c.	The ARRC/MSB needs to get the ROW easement from the University of Alaska.	If it is determined that there is not an easement at this location the 'race' trail will be rerouted to the Historic Iditarod Trail which will be upgraded to account for a potential increase in users. It is outside of the scope of the Port MacKenzie Rail Extension Project to acquire trail easements.
3d.	Crossing signs needed at this location	If no easement exists, proposed signs include: <ul style="list-style-type: none"> <li>• Trail closed ahead</li> <li>• Slow, trail rerouted</li> </ul>

### 4. Flat Lake Connector Trail (Rerouted to multi use crossing H 6.3)

Reference #	Comment	Response
4a.	Need a crossing here; this is a well established trail.	Based on the engineering work completed, the height requirement threshold cannot be met for grooming equipment at this location; no more than 10 feet of clearance is achievable without interfering with the shallow underlying groundwater table. A new trail will be established at crossing H 6.3 with greater than 20' horizontal and 14' vertical clearances.
4b.	Why can't an at-grade crossing be done here? Cut trees for a wider field of view.	An at-grade crossing can't be used due to safety concerns; the available line of sight is minimal even if trees are cleared due to area topography.
4c.	Crossing signs needed at this location.	Proposed signs include: <ul style="list-style-type: none"> <li>• Trail closed ahead</li> <li>• Slow, trail rerouted</li> </ul>

## MSB Trail Meeting Comments and Responses - October 27, 2011

5. Reroute under Crossing 6.3 (Project Crossing #5)		
Reference #	Comment	Response
5a.	Too much traffic for one crossing.	This proposed crossing is a 5 span bridge with a northern and southern access span of over 20' in width. The spans will accommodate a trail on both the north and south side of the creek. The outside spans will be widened from 14' to over 20' to accommodate groomers and multiple users.
5b.	Need a wider turn radius for the reroutes coming under the crossing location.	The turn radius for the reroutes under both the north and south spans will be modified and designed for a 100' vehicle to turn and pass under the structure.
5c.	Mushers need 85' in length for 16 to 20 dog teams; curve is too tight.	The outside spans will be widened from 14' to over 20' to accommodate dog teams with sleds of this length. The trails approaching the crossing will also be designed with curves to accommodate this length.
5d.	Want wider openings at the crossing location (H 6.3); this crossing should be 25'3" wide like the other locations.	The design of reroutes will allow for sufficient line of sight for all user groups and traffic will be accommodated by two parallel trails one on each side of the creek. The trail groomer will be able to maintain access on the north and south spans of this crossing structure via existing trail connectivity.
5e.	Need a good line of sight coming under the crossing.	The turn radius for the reroutes under both the north and south spans will be modified and designed for a 100' vehicle to turn and pass under the structure. Trails will be aligned to provide increased site distance.
5f.	The stream at crossing H 6.3 tends to have overflow, making a trail crossing problematic.	Trail embankments will be designed to minimize impacts from overflow.
5g.	Crossing signs needed at this location.	Proposed signs include: <ul style="list-style-type: none"> <li>• Clearance signs</li> <li>• Slow – Keep Right High Traffic Area</li> <li>• Caution dog teams on trail</li> </ul>

## MSB Trail Meeting Comments and Responses - October 27, 2011

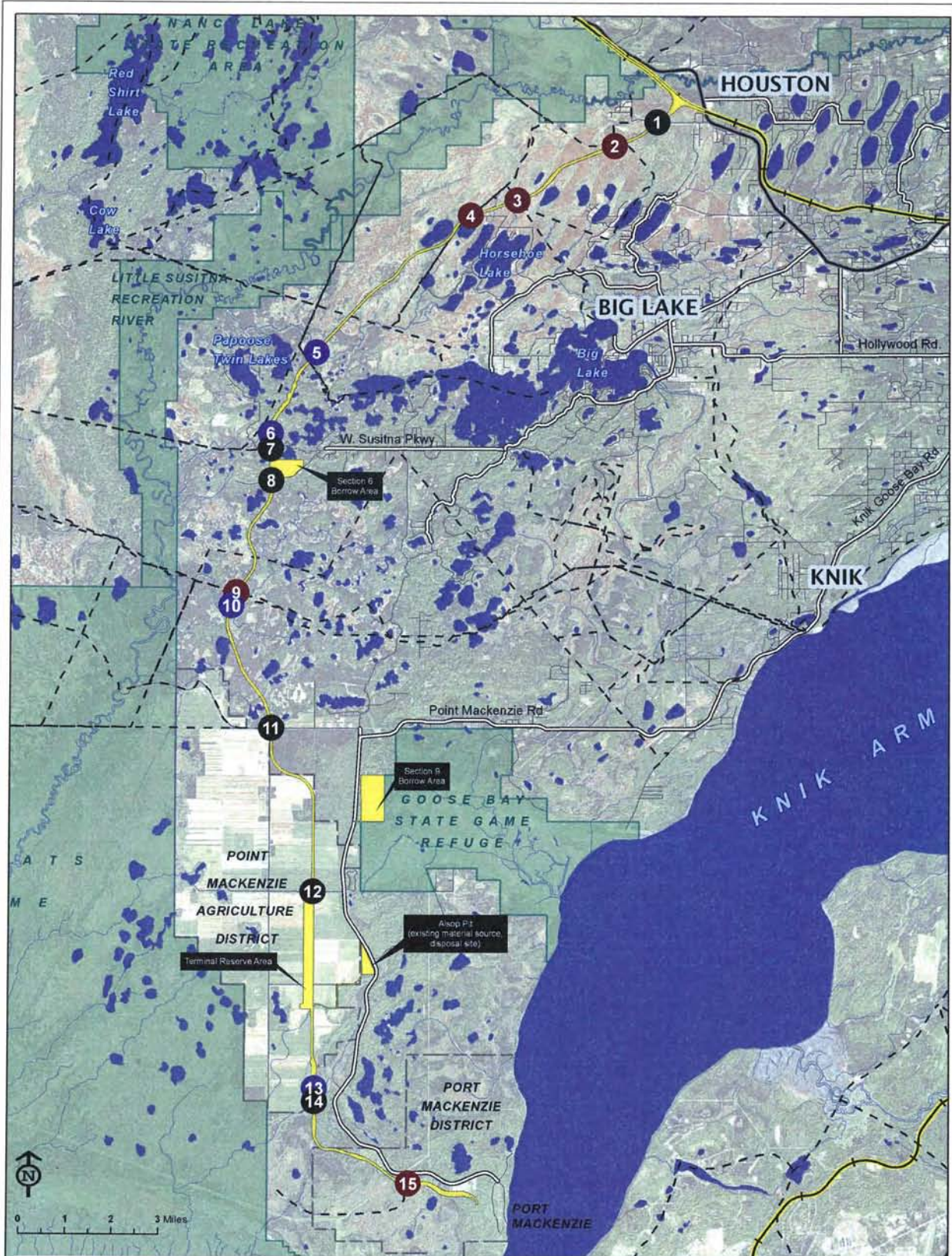
6. Iron Dog Connector (Reroute under Crossing 6.3)		
Reference #	Comment	Response
6a.	This crossing is needed; people will cross even if a designated crossing is not provided.	Due to FAA VORTAC issues, safety concerns, wetland and regulatory concerns, tree cover and topography, a crossing is not an option at this location.
6b.	Running snow machines here would be unsafe; snow machines would jump or side hill	A kiosk identifying the user's location with trails map and bulletin board will be installed to identify trail reroutes and available trail connections. Trail closed signs be posted. Iron Dog Race Director will be asked to work with us to provide adequate safety coverage at this location during race.
6c.	Can we work to get permission to have this crossing maintained?	Due to FAA VORTAC issues, safety concerns, wetland and regulatory concerns, tree cover and topography, a crossing is not an option at this location. It is not a question of obtaining permission.
6g.	Crossing signs needed at this location.	Proposed signs include: <ul style="list-style-type: none"> <li>• Trail closed ahead</li> <li>• Slow, trail rerouted</li> </ul>

### 7. General Trails Concerns

Reference #	Comment	Response
7a.	We asked for more crossings at the IDSHD Workshops, but now there are fewer crossings – bring back the crossings for the Iditarod and Iron Dog Race trails	We are working with DNR and UAF to determine if an easement exists at the Iditarod 'race' trail crossing coming from Knik. Due to area wetlands, topography, land ownership and the Big Lake VORTAC site, it has been necessary to concentrate efforts on improving crossing structures, signage and existing trail conditions instead of adding crossings. See comments related to the Iron Dog and Iditarod crossings for additional details.
7b.	What happened to the crossings in the Point MacKenzie area –more crossings were identified in that area at the IDSHD Workshops	There are still 5 crossings in the Point MacKenzie area, although not all were included on Open House maps. Baker Farm Road is an access controlled crossing provided as an emergency access to area farms. There will also be an electric utility crossing not identified which will be access controlled.
7c.	By eliminating crossings and relocating trails, you will be concentrating multiple user groups onto fewer trails, which will make the trails more dangerous.	Crossing structures have been modified to account for increased usage, and trails will be widened and upgraded in the vicinity of crossing structures and reroutes to accommodate multiple user groups.

## MSB Trail Meeting Comments and Responses - October 27, 2011

7d.	What happened to the trails paralleling the Railroad ROW? Those need to be included or trail users will just use the ROW and cross wherever they feel like it.	A parallel trail(s) along the Railroad ROW will not be provided; however, there is a north-south corridor using the existing trail system which will be mapped and posted at kiosks along the trail system. One area, between Crossings 7 and 10, demonstrates the biggest gap and we are working to find an alternative as the majority of the property between these crossings is owned by University of Alaska.
7e.	Why did they choose the alternative that was chosen? It would have been better for trail users if they had picked the western alternative (west of Red Shirt Lake).	The Willow (or western) Alternative was evaluated but not chosen as the preferred alternative in STB's Final Environmental Impact Statement (EIS) and will not likely be included as the alternative licensed by STB. As detailed in the EIS the Willow route had a greater amount of environmental impacts than the other routes.
7f.	The project team/ARRC/MSB is not listening to people's comments – the proposed crossings/alignment does not reflect what people have been saying at the meetings.	The ARRC and MSB have made efforts to address users concerns by providing improved crossing structures with increased vertical clearance (14') and bridging areas with wider spans to support multiple uses and users. Due to project constraints such as private property, wetlands and the FAA VORTAC site, some crossing locations have been eliminated. Reroutes are being provided when possible and information kiosks will be installed to inform the users of changes in connectivity. Total cost for crossings currently totals: \$11.7 million.
7g.	There needs to be more crossings between Crossings 4 and 5.	The land between crossings 4 and 5 does not present opportunities for crossing structures due to wetlands impacts, high water table, and the FAA VORTAC site (which requires that the rail embankment and passing trains do not block the VORTAC electronic signals); this reduces the possible height of the rail embankment, which eliminates the possibility of constructing a crossing structure in this area with 12' to 14' of clearance.
7h.	Consider standard snow machine trail signage as seen on Alaska State Parks SnoTRAC website, not road side signs!	The project team will consider signage as seen on the Alaska State Parks snoTRAC website to address user concerns.



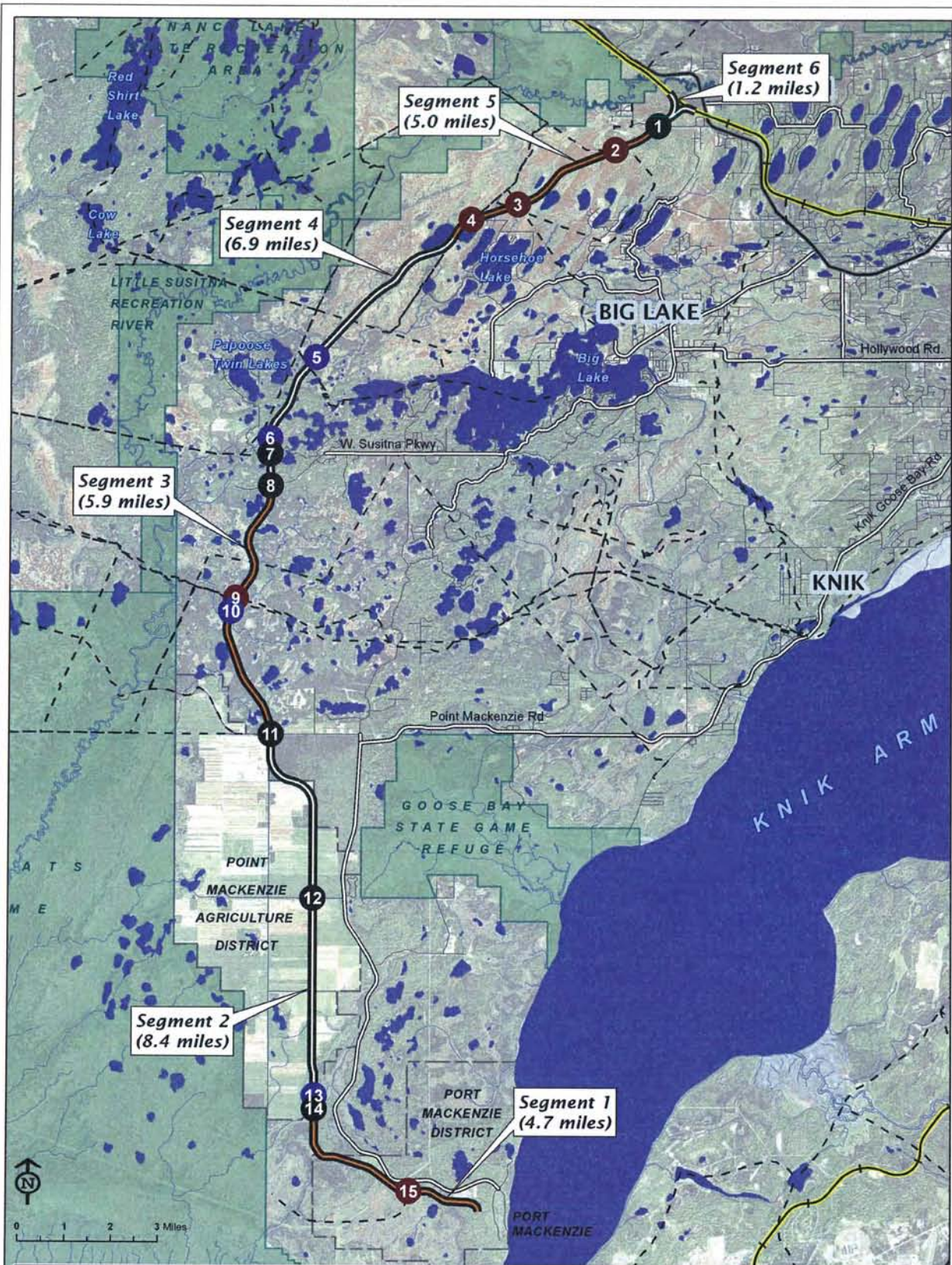
**Area of Potential Effect**

- Proposed Alternative
  - Area of Potential Effect
  - ARRC Track
  - Trail
- Crossing Types**
- Road
  - Stream
  - Trail

**Crossing Type Summary**

1	Millers Reach Rd.	At-Grade	9	Historic Iditarod Trail	Bridge
2	Unnamed Big Lake Rec Trail* <i>Utility Easement (contingent on utility approval)</i>	At-Grade	10	H-0.8 - outlet of Diamond Lake	Bridge
3	Unnamed Big Lake Rec Trail* <i>Private Drive (contingent on owner approval)</i>	At-Grade	11	W Ayrshire Avenue	At-Grade
4	Houston Lake Loop Trail (Crossing)	Bridge	12	W Holstein Avenue	At-Grade
5	H-6.3 - Tributary of Little Su	Bridge	13	MC-4.5 - Unnamed Stream	Bridge
6	H-4.3 - Tributary of Little Su	Bridge	14	Baker Farm Road	At-Grade
7	W Papoose Twins Rd.	At-Grade	15	Figure 8 Lake Loop Trail	Oversized Culvert
8	W Susitna Parkway	At-Grade			

\*If crossing 2 or 3 are not approved by landowner/easement holder, reroutes will be provided to the nearest crossing



Crossing Locations		Crossing Type Summary	
	Proposed Alternative	1	Millers Reach Rd. At-Grade
	ARRC Track	2	Unnamed Big Lake Rec.Trail* Utility Easement (contingent on utility approval) At-Grade
	Trail	3	Unnamed Big Lake Rec.Trail* Private Drive (contingent on owner approval) At-Grade
	Crossing Types	4	Houston Lake Loop Trail (Crossing) Bridge
		5	H-6.3 - Tributary of Little Su Bridge
		6	H-4.3 - Tributary of Little Su Bridge
		7	W Papoose Twins Rd. At-Grade
		8	W Susitna Parkway At-Grade
		9	Historic Iditarod Trail Bridge
		10	H-0.8 - outlet of Diamond Lake Bridge
		11	W Ayrshire Avenue At-Grade
		12	W Holstein Avenue At-Grade
		13	MC-4.5 - Unnamed Stream Bridge
		14	Baker Farm Road At-Grade
		15	Figure 8 Lake Loop Trail Oversized Culvert

\*If crossing 2 or 3 are not approved by landowner/easement holder, reroutes will be provided to the nearest crossing





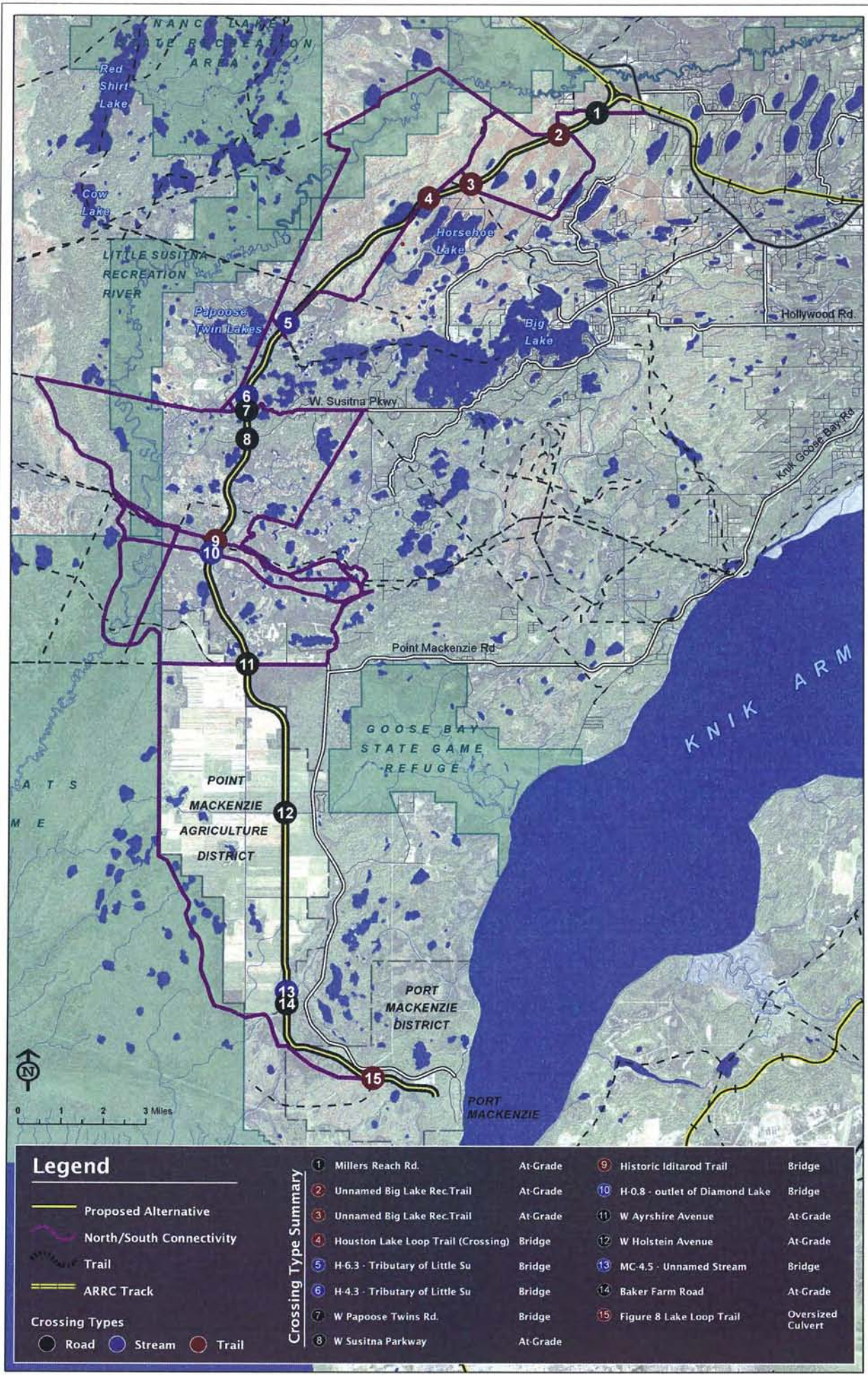
### Crossing Constraints

	Proposed Alternative		State
	ARRC Track		University
	Trail		Private
	VORTAC Site		Native Corporation
	FAA Area of Restriction		Mental Health
	Wetlands		
<b>Crossing Types</b>			
	Road		Stream
	Trail		

### Crossing Type Summary

1	Millers Reach Rd.	At-Grade	9	Historic Iditarod Trail	Bridge
2	Unnamed Big Lake Rec.Trail* <i>Utility Easement (contingent on utility approval)</i>	At-Grade	10	H-0.8 - outlet of Diamond Lake	Bridge
3	Unnamed Big Lake Rec.Trail* <i>Private Drive (contingent on owner approval)</i>	At-Grade	11	W Ayrshire Avenue	At-Grade
4	Houston Lake Loop Trail (Crossing)	Bridge	12	W Holstein Avenue	At-Grade
5	H-6.3 - Tributary of Little Su	Bridge	13	MC-4.5 - Unnamed Stream	Bridge
6	H-4.3 - Tributary of Little Su	Bridge	14	Baker Farm Road	At-Grade
7	W Papoose Twins Rd.	At-Grade	15	Figure 8 Lake Loop Trail	Oversized Culvert
8	W Susitna Parkway	At-Grade			

\*If crossing 2 or 3 are not approved by landowner/easement holder, reroutes will be provided to the nearest crossing

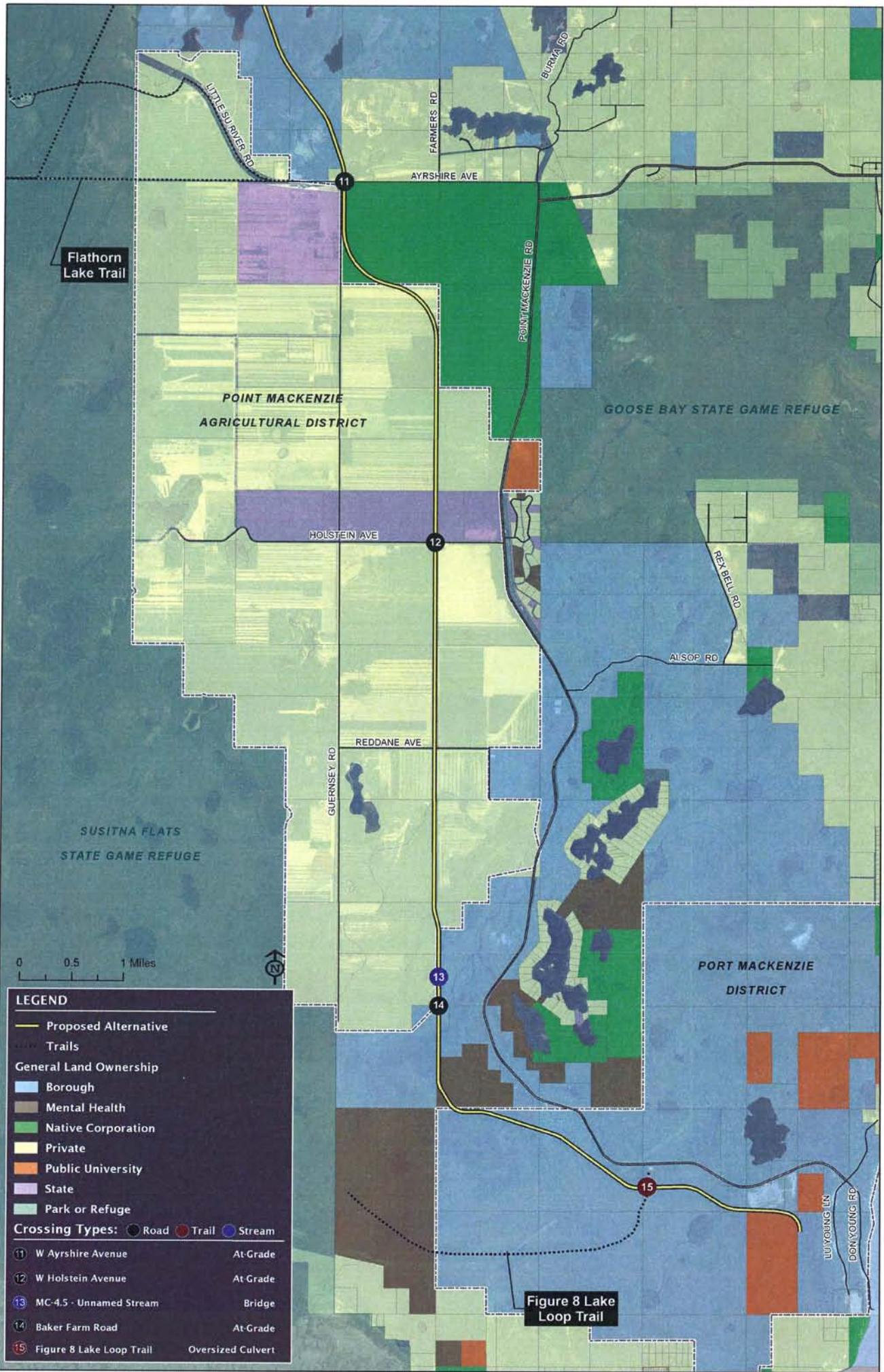


### Legend

- Proposed Alternative
  - North/South Connectivity Trail
  - ARRC Track
- Crossing Types**
- Road
  - Stream
  - Trail

### Crossing Type Summary

1	Millers Reach Rd.	At-Grade	9	Historic Iditarod Trail	Bridge
2	Unnamed Big Lake Rec.Trail	At-Grade	10	H-0.8 - outlet of Diamond Lake	Bridge
3	Unnamed Big Lake Rec.Trail	At-Grade	11	W Ayrshire Avenue	At-Grade
4	Houston Lake Loop Trail (Crossing)	Bridge	12	W Holstein Avenue	At-Grade
5	H-6.3 - Tributary of Little Su	Bridge	13	MC-4.5 - Unnamed Stream	Bridge
6	H-4.3 - Tributary of Little Su	Bridge	14	Baker Farm Road	At-Grade
7	W Papoose Twins Rd.	Bridge	15	Figure 8 Lake Loop Trail	Oversized Culvert
8	W Susitna Parkway	At-Grade			





# Section 106 Facts

## Project Description

The Matanuska-Susitna Borough (MSB) and the Alaska Railroad Corporation (ARRC) propose to construct a 32-mile rail line in the Susitna River Valley to connect the Borough's Port MacKenzie to the Alaska Railroad Corporation's (ARRC) existing mainline track just south of Houston. The proposed rail line would provide freight services between the Port and Interior Alaska.

On March 25, 2011, the Surface Transportation Board (STB) recommended the Mac Central and Houston South as the selected route in the Final Environmental Impact Statement for the Port MacKenzie Rail Extension.

The federal board approves all new rail line construction in the nation. The 32-mile rail extension will link the deepwater Port MacKenzie to the mainline of the Alaska Railroad. The STB finding finalizes a rigorous environmental impact study, field work and public involvement process that began in 2007. The STB issued the Record of Decision on November 21, 2011; it became effective on December 21, 2011.

## Project Schedule

March 2011:	Release of Final EIS
April 2011:	Permit Application Package Submittal
November/ December 2011:	Record of Decision Issued
Spring 2011– Spring 2013:	Right-of-Way Acquisition
2012–2015:	Construction

## General Contact Information

Borough Contact:  
Patty Sullivan, MSB Public Affairs Director  
Phone: (907) 745-9577  
Email: psullivan@matsugov.us

Railroad Contact:  
Stephenie Wheeler, ARRC Corporate  
Communications Officer  
Phone: (907) 265-2671  
Email: wheelers@akrr.com

## Purpose of the Iditarod Dog Sledding Historic District Workshop

The new rail line will cross several officially recognized recreational trails within the Iditarod Dog Sledding Historic District (IDSHD).

The Programmatic Agreement (PA)—a document that describes the actions that will be taken by the parties in order to meet their responsibilities under the National Historic Preservation Act (NHPA)—between the STB, Advisory Council on Historic Preservation (ACHP), and Alaska State Historic Preservation Officer (SHPO) determined that the Iditarod Dog Sledding Historic District is eligible for listing in the National Register of Historic Places at the national level of significance between 1967 and 1978.

To date, ARRC, in consultation with the Working Group (ARRC, MSB, and Knik Tribal Council) and SHPO, has conducted two of the three planned IDSHD meetings: an initial workshop (6/28/11) and a focus group meeting (7/7/11). In addition, the MSB hosted an open house for all trail users in the Project area (10/27/11). Based on comments and feedback from a variety of trail users, the Project Team (ARRC and MSB) has identified design changes, modifications, and/or refinements to the Project and is drafting a Workshop Summary and Implementation Plan, as required in PA.

During the final IDSHD workshop (1/11/12), the Project Team will present a summary of comments provided by participants at previous IDSHD workshops and the trails open house and proposed resolutions to those comments. Based on comments received during this workshop, ARRC will develop a Workshop Summary and Implementation Plan in consultation between the Working Group, STB, and SHPO. ARRC will submit this document to appropriate consulting parties\* (e.g., STB, SHPO, workshop participants) for a 30-day review and comment period. ARRC will incorporate comments, as appropriate, and will submit a final Workshop Summary and Implementation Plan to appropriate parties and post the document on the project website.

\* A consulting party can be any person or group that has demonstrated a legal/economic relation to a project or demonstrated interest in the project's effects on historic properties (or cultural resources eligible for listing on the National Register).

## Section 106 Contact Information

Alaska State Historic Preservation Office Contact:  
Shina duVall, Archaeologist/Review and Compliance  
Phone: (907) 269-8720  
Email: Shina.duVall@alaska.gov

HDR Alaska, Inc. Contact:  
Elizabeth Grover, Cultural Resources Specialist  
Phone: (907) 644-2077  
Email: Elizabeth.Grover@hdrinc.com



### Iditarod Dog Sledding Historic District Meeting

Wednesday, January 11, 2012

6:00 pm - 8:00 pm

Houston Middle School, Houston, Alaska

Name	Address	Phone	Email
JESSICA MANIFOLD	2525 C Street, Suite 305	644-2073	jessica.manifold@hdrine.com
ELIZABETH GROVER	" "	644-2077	Elizabeth.Grover@hdrine.com
JUDITH BITTNER			judy.bittner@alaska.gov
Mark V Nordman		360-1814	mark.k.nordman@iditarod.com
Scott Lavene	FOR 2356 Palmer <sup>91648</sup> 373-7568	373-1102	Jon_Brautigam@hotmail.com
JON BRAUTIGAN			mtaonline@gmail.com
TERRY LANGHEILZ	9741 S. Dec St Wasilla	373-7533	
Kelley Griffin	8796 W Sunset Ave 99623	373-1126	Kelleyg@mtaonline.net
Bruce Paulsen	4950 W. Greenward WASILLA	745-9867	Bruce.Paulsen@mtasugov.us
GREG BILL	P.O. Box 870800	746-1000	GBILL@IDITAROD.COM
Annie Wade	951 E. Bergard Rd Wasilla <sup>99654</sup>	373-3170	awade@kniktribe.org
Heather Ralston	2225 E. Grizzly Bear Cir. <sup>Wasilla 99151</sup>	232-4112	ldywif@qci.net
Kyle Kidder	550 W 7th Anch AK	269-8564	Kyle.Kidder@ALASKA.GOV
Lesli Schick	" "	334-2679	lesli.schick@alaska.gov
Shina duVall	550 W. 7th Ave St. 1310	269-8720	shina.duvall@alaska.gov



**Iditarod Dog Sledding Historic District Meeting**

Wednesday, January 11, 2012

6:00 pm - 8:00 pm

Houston Middle School, Houston, Alaska

Name	Address	Phone	Email
Summer Rickman	550 W 7 <sup>th</sup> Ave Suite 1310	907-269-8717	Summer.rickman@alaska.gov
Roscaz Arcandra	742 S. Alaska Palmer	644-2074	roscaz.arcandra@ndcinc.com
Zoe Meade	742 S Alaska Palmer	644-2074	zoe.meade@helvine.com
Barb Hotchkiss	ARRC	265-2315	
Mark Buser	Box 520997 B.L.	355 7898	buserdog@mtaonline.net
Kevin Keele	Bum rto #700 BLM-AD A/AD #9507	267.1207	kkeeled@blm.gov



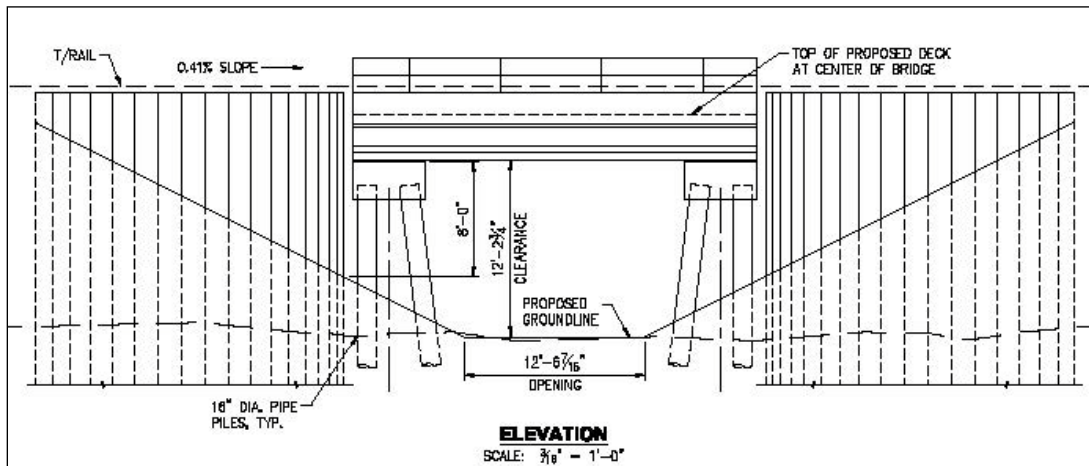
## **Iditarod Dog Sledding Historic District (IDSHD) Workshop Workshop Summary and Implementation Plan Comment Resolution Summary**

In accordance with the Section 106 Programmatic Agreement for the Port MacKenzie Rail Extension (PMRE) project, the Alaska Railroad Corporation (ARRC) hosted two of three planned Iditarod Dog Sledding Historic District (IDSHD) workshops in June and July 2011. At the request of IDSHD workshop participants, the Matanuska-Susitna Borough (MSB) hosted a larger trail user meeting in October 2011. ARRC and MSB, hereafter referred to collectively as the Project Team, have worked closely to identify design changes, modifications, and/or refinements to the project that will address concerns and mitigate impacts identified during those meetings. This document summarizes meeting/workshop participants' comments and concerns, and provides the Project Team's proposed resolution. In January 2012, ARRC will host a final IDSHD Workshop, and this document will serve as the basis for discussing how mushers' (and other trail users') comments and concerns have been addressed. Following that meeting, ARRC will prepare a *Workshop Summary and Implementation Plan* for submittal to the Surface Transportation Board (STB), State Historic Preservation Officer (SHPO), and appropriate Section 106 consulting parties.

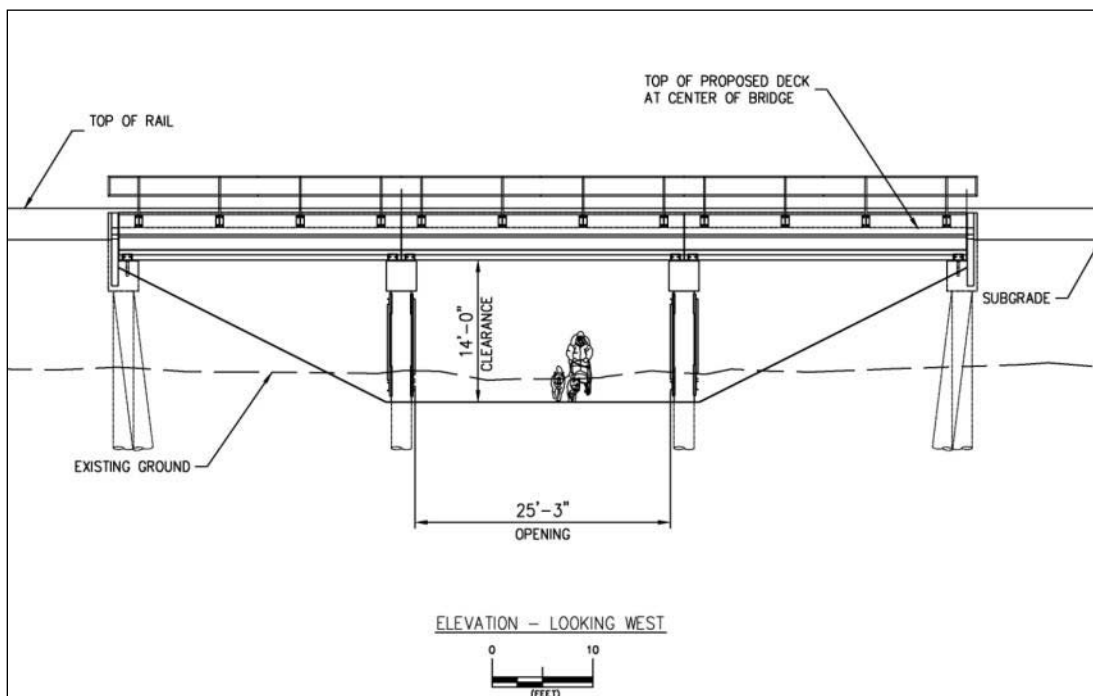
1. *The height (vertical clearance) of crossings needs to be increased to more than 12' to account for snow depth, accommodate equipment for grooming of trails, and enhance safety. The existing clearance is as low as 10' for some crossings.*

**Proposed Resolution:** The Project Team has redesigned bridges/crossings with the vertical clearance increased to a minimum of 14', whenever possible. Table 1 summarizes previously and currently proposed trail and road crossings for the PMRE project. Vertical clearances will range from 12' to over 18'. Only two crossing structures, located south of the Agricultural District, will have a vertical clearance less than 14'. One of these crossings (Crossing 14), which will be a three-span bridge providing between approximately 12' vertical by approximately 25' horizontal clearance, is adjacent to the Baker Farm Road at-grade crossing (Crossing 15) and provides an alternative crossing option in the vicinity (Table 1). The other crossing is the Figure 8 Lake Loop trailhead crossing (Crossing 16), which will be accommodated by a 19'-6" plate pipe, providing between 10' and 14' vertical by 16' horizontal clearance (Table 1). A bridge crossing in this location is not practicable due to the poor soil conditions and high-railroad embankment through the area. Alternative access for over-height vehicles is provided by the access road from Lu Young Lane, south of the Bulk Material Bi-modal Facility (BMBF).

Increasing the vertical clearance under a bridge also increases the footprint of the embankment approaching the bridge, which often increases wetlands impacts. The design team has tried to strike a reasonable balance between the needs of trail users and our obligations to minimize impact to wetlands under Federal law. Exhibit 1 shows a comparison of the previously proposed Historic Iditarod Trail crossing and the currently proposed crossing that has been refined based on comments from trail users. As illustrated in this example, the vertical clearance was increased, based on comments from workshop participants, from approximately 12' to 14'.



**June 2011 Planned Crossing**



**December 2011 Planned Crossing**

**Exhibit 1: Historic Iditarod Trail crossing design before and after the IDSHD Workshops<sup>1</sup>**

<sup>1</sup> Please note that all exhibits/figures in this document are based on 60% design and are not final.



2. The length of a sled dog team with 16 to 20 dogs (approximately 85') needs to be considered during design of trail approaches to crossings to ensure curves are not too tight. (This comment was made in reference to the rerouted trails, specifically Crossing H 6.3 [Project Crossing 5]).

**Proposed Resolution:** The Project Team has been working with Sustainable Design Group to develop standards for sight distance needs and crossing approaches that will accommodate a 100' long vehicle/dog team, which requires a minimum 60' turn radius. The design standard for trail approaches to crossings includes gentle curves with turn radii between 75' and 90'. This turn radius will allow for a 100' long dog team to turn and safely pass through the structure while allowing adequate line of sight for trail users prior to passage (Exhibit 2).

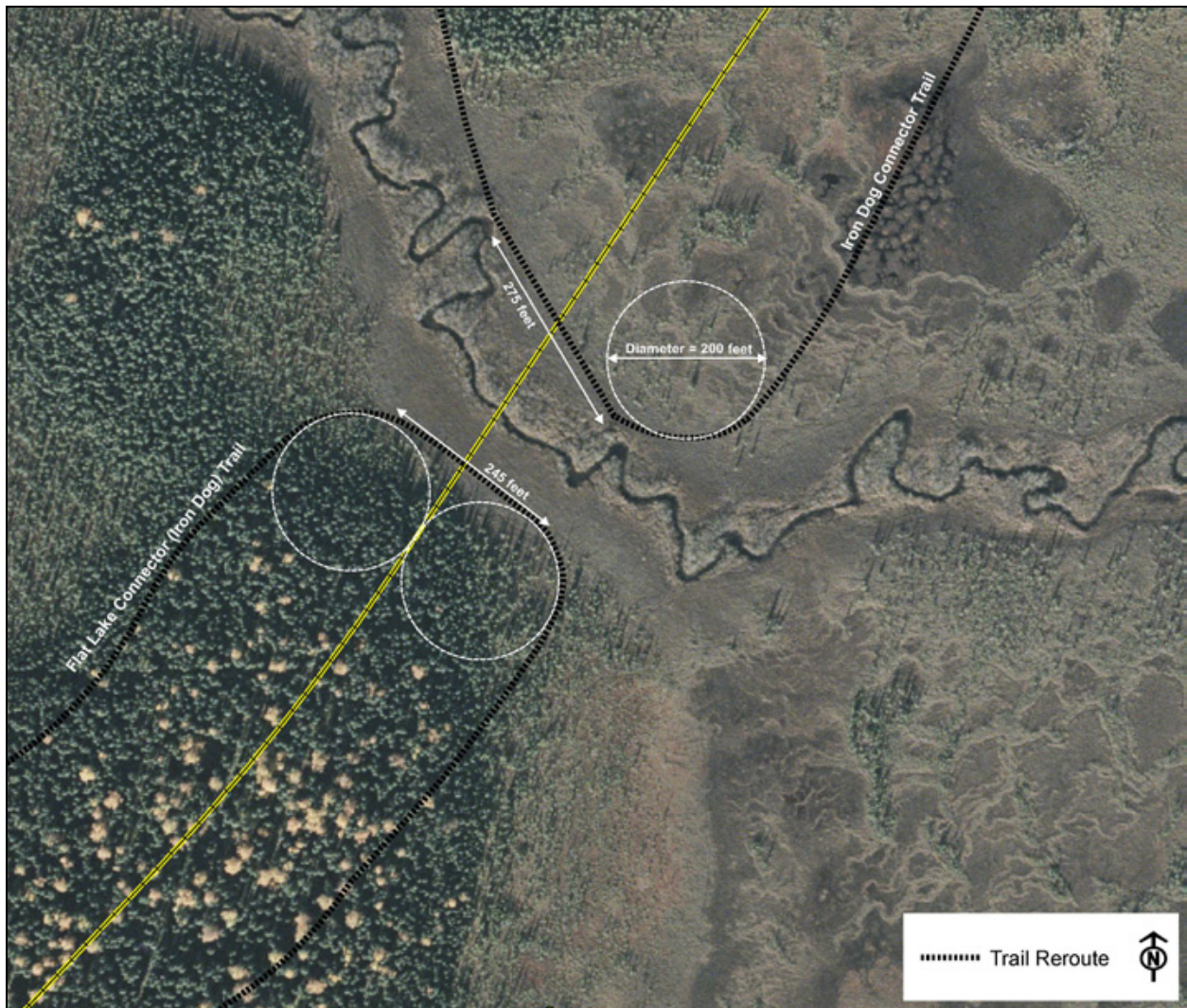


Exhibit 2: Approach example

- 3. Some crossings are single span bridges and do not provide mushers with adequate visibility to see if anything is approaching from the other direction. The length of these crossings needs to be increased. Trail approaches to crossings also need adequate line-of-sight.*

**Proposed Resolution:** The Project Team eliminated the 28' single-span concrete ballast deck (CBD) bridges that were originally proposed for some trails due to the sight distance concerns. Bridges are now a minimum of three spans, and the spans designated specifically for existing and or rerouted trails will generally provide a minimum of 20' of horizontal clearance. Adequate visibility/line-of sight will also be provided with relatively straight trail approaches to these longer bridges with higher vertical clearance (minimum of 14' whenever possible). The December 2011 design for the Historic Iditarod Trail crossing is typical of design changes undertaken by the Project Team to address Workshop participants' concerns (see Exhibit 1). Exhibit 2 illustrates typical design changes to approaches to address line of sight issues for trail reroutes.

- 4. Will trail crossings be turned into road crossings as growth and development increases? Planning for at-grade road crossings (e.g., West Susitna Parkway and Ayrshire Road) should take population growth and resulting development into consideration, as conflicts between users could increase.*

**Proposed Resolution:** The proposed designs for the various trail crossings are not adequate for roadways, and the trail crossings will not be turned into road crossings. The Project Team reviewed the MSB Long Range Transportation Plan (LRTP) and included sufficient road crossings to address those needs, with parallel trails separated from the road. The Project Team also considered future growth during design and right-of-way (ROW) acquisition for roads such as West Susitna Parkway and Ayrshire Road, which may grow from two to four lane roads at some point in the future. The proposed crossings at Millers Reach Road, West Papoose Twins Road, West Susitna Parkway, Ayrshire Avenue and West Holstein Avenue will include separated crossing panels on either side of the roadway to enhance safe passage for the various user groups (Table 1).

- 5. Easements do not exist but are needed for some trails. Also, with population increases, more and more trails could be sold into private ownership/development.*

**Proposed Resolution:** MSB Community Development and Land Management will continue to work with trail users and agencies to obtain easements for existing trails in the project area that do not currently have legal easements including the Iditarod Race Trail, the Houston Lake Loop Trail and Big Lake Trail #2. MSB will also work with agencies to try to secure easements or other legal instrument for recognized trails that are proposed to be rerouted as part of the project. These trails include the Iron Dog Connector Trail and Flat Lake Connector (Iron Dog) Trail.

- 6. Provide a map to workshop participants showing where crossings cannot be located (e.g., VORTAC or wetland restrictions or other design concerns).*

**Proposed Resolution:** A map showing restrictions/constraints limiting crossing locations was available at the MSB Trail User Open House (10/27/11). This map was posted on the

project website ([http://www.portmacrail.com/maps/Constraints\\_v2%20web.jpg](http://www.portmacrail.com/maps/Constraints_v2%20web.jpg)) and is included in this document (see Figure 1 in Attachment 1).

The Project Team continues to work with the Federal Aviation Administration (FAA) regarding project and construction constraints associated with the FAA VORTAC radar site, which controls the approach for all Anchorage air traffic. FAA defined a three-mile radius where construction restrictions will apply (see Figure 1 in Attachment 1). Within this restricted area, the rail embankment and passing trains cannot block the VORTAC electronic signals. These restrictions include minimizing the height and/or geometry of the rail embankment and associated crossing structures in the vicinity of the VORTAC. This constraint has resulted in the loss of one previously planned crossing as the height over the Outflow of Muleshoe Lake cannot meet the 14' of vertical clearance required. It also prevents grade separation of the Iron Dog Connector Trail to the west of the VORTAC site.

Other project concerns include area wetlands and land ownership. Wetland complexes between Crossings 4 and 6 have made adding crossings difficult. Adding a grade separated crossing in an area like the Iron Dog Connector Trail (Big Lake Trail #5) is not possible because raising the embankment high enough to provide 12' to 14' of clearance for trail users would also increase the fill footprint of the embankment over several miles. In consultation with the Project Team, the United States Corps of Engineers and the Environmental Protection Agency requested that there be no increases in the project footprint in wetlands areas to accommodate a widened embankment area. As a result, the Project Team has worked to keep the rail embankment as low as possible in wetlands areas to minimize impact. In the case of the Iron Dog Connector Trail, adding an at-grade crossing would increase the impact to wetlands as the crossing would require additional embankment at the approaches to the proposed railroad crossing.

Land ownership and lack of legal easements are also challenges. The Project Team must consider property ownership when planning crossing locations, so as to not promote trespass or provide a crossing that may not be usable into the foreseeable future. As identified in the above referenced constraints map, this is a concern between Crossings 1 and 3 and Crossings 7 and 10. The MSB is currently in discussions with UA and MHLT to attempt to establish easements for some of the currently used trails in the project area (e.g., at the Iditarod Race Trail).

- 7. More crossings in the heavily used area between Crossings 2 and 10 are needed to maintain connectivity. Limiting the number of crossings and relocating and/or combining trails in this area may "bottleneck" multiple user groups onto fewer trails, which will make the trails more dangerous by increasing the potential for collisions and user conflicts (e.g., between snow machiners or other users and mushers).*

**Proposed Resolution:** The Project Team has tried to strike a reasonable balance between the needs of trail users, our obligations to property owners and State and Federal agencies, and safety considerations. We continue to investigate inclusion of additional grade-separated crossings between Crossings 2 and 10 and are working with user groups, agencies, and landowners to find a solution.

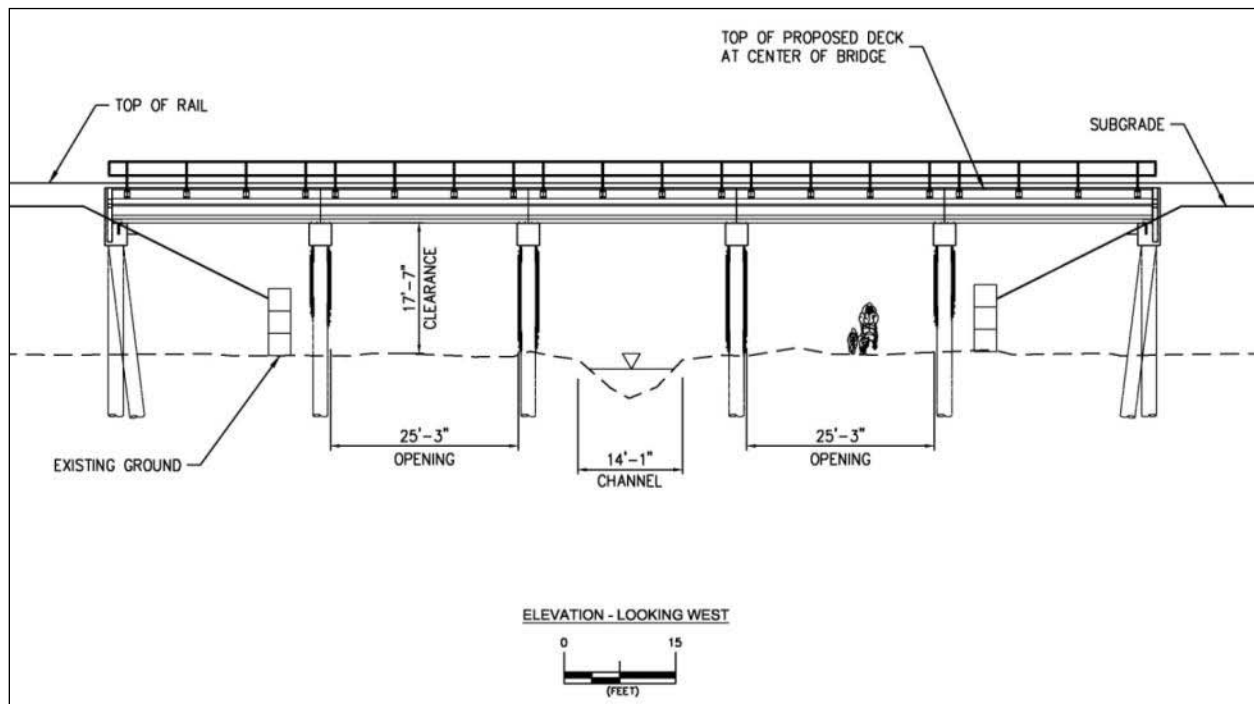
However, this area does not present good opportunities for separated grade crossing structures (e.g., bridges) because of safety concerns (e.g., visibility issues caused by topography and vegetation), regulatory issues related to wetlands impacts, design issues associated with a high water table in the area, property ownership, existing property/easement constraints, and construction restrictions associated with the FAA VORTAC radar site. Comment 6 describes

the latter constraints (see also Figure 1 in Attachment 1). The Project Team has been working with FAA regarding construction restrictions associated with the VORTAC site.

Safety is a significant concern. Much of the terrain in the area between Crossings 2 and 10 is undulating with patches of trees making it difficult for users to see an oncoming train. A train cannot stop quickly, and it generally takes one mile or more for a train to come to a stop. Sight distance for train operators is also a potential safety issue in areas where a curve in the tracks, dense vegetation, and topography may affect visibility. In addition, some users may be wearing helmets and listening to headphones while traveling in the area, or be operating loud vehicles, thus impeding their ability to hear a train coming.

8. *Provide a crossing for the Iron Dog Connector Trail (Big Lake Trail #5 in MSB Trails Plan)*

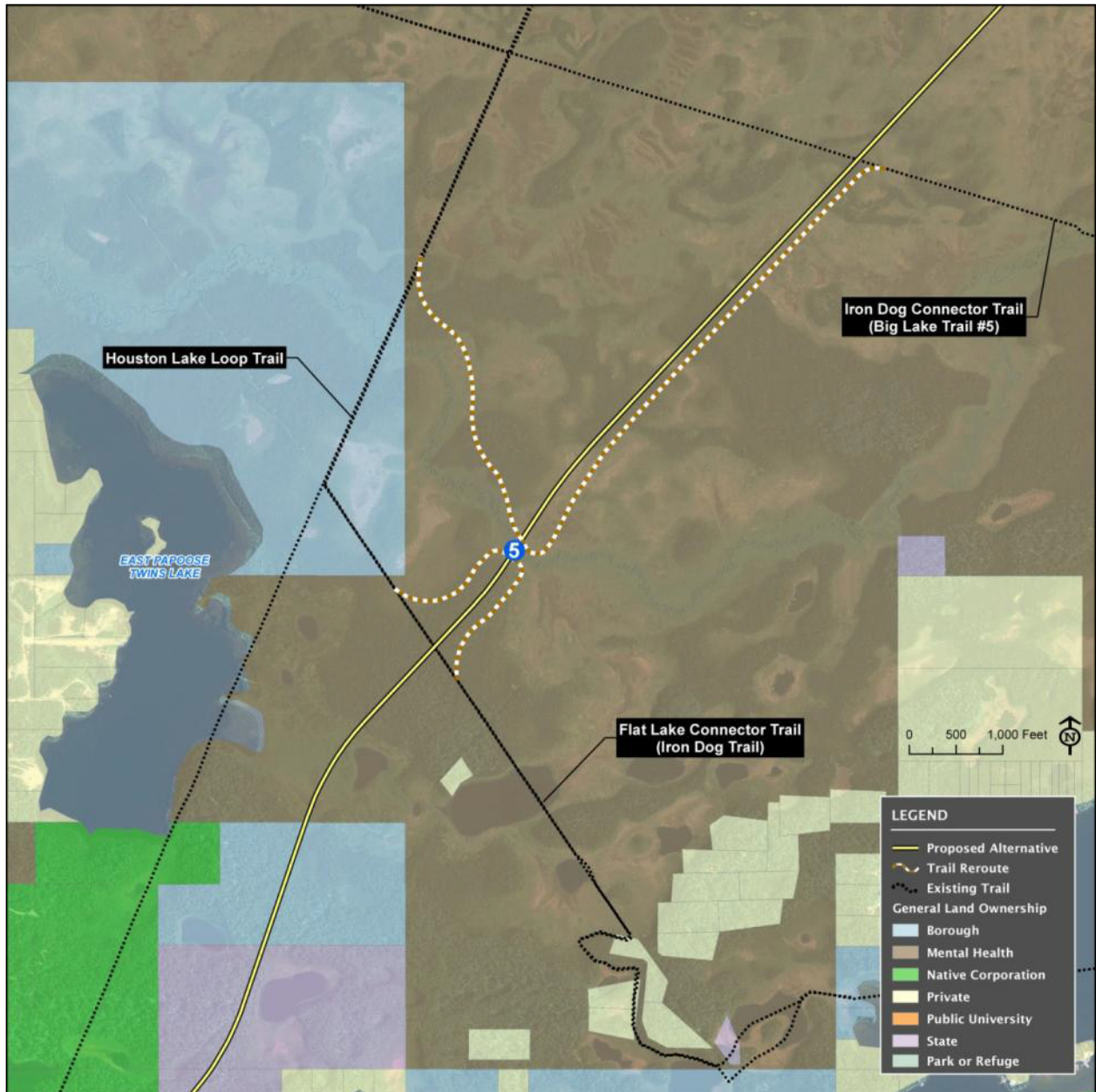
**Proposed Resolution:** The Iron Dog Connector Trail is identified in MSB trail plans as a winter use only trail as it traverses portions of wetlands and lakes in the project area. Due to safety concerns, wetland and regulatory concerns, tree cover, and topography (see Comment 5), a crossing is not a good option at its current location. Therefore, the Project Team proposes to reroute the trail to the northern usable span of a five-span bridge with approximately 25' of horizontal clearance and 17' of vertical clearance (Crossing 5; Exhibit 3). The rerouted trail will be reconnected with the Houston Lake Loop Trail north of the rail embankment (see Exhibit 4).



**Exhibit 3: Crossing 5 (H 6.3) profile view**

The Project Team is working with MHLT to acquire easements for the rerouted trail. If MSB can not acquire an easement for the reroute, the Project Team will notify Workshop participants and provide a revised proposal for continued access and connectivity.

The MSB has met with the Iron Dog Race Director regarding relocation of the Iron Dog Connector Trail to crossing Houston 6.3 (Crossing 5). The Iron Dog Race Director will present the reroute proposal to the Iron Dog Race Board of Directors. MSB will contact the Race Director again to determine the outcome of this meeting, and will work with the Race Director and/or Race officials to provide adequate safety coverage at this location during races.



**Exhibit 4: Proposed Iron Dog Connector Trail Reroute and Crossing (Crossing 5)**

9. *Provide a crossing for the Flat Lake Connector Trail (official Iron Dog trail; between Crossings 5 and 6). This is a well established trail with a legal easement that needs an at-grade crossing. Improvements should include cutting trees at the crossing to allow a wider field of view for approaches.*

**Proposed Resolution:** As discussed under Comment 1, workshop participants requested a minimum height of 14' to allow for snow depth, accommodate equipment for grooming of trails, and enhance safety. Based on the engineering work completed, this minimum height cannot be achieved at this location; no more than 10' of clearance is possible without interfering with the shallow underlying groundwater table. An at-grade crossing cannot be used due to safety concerns, since the available line of sight is minimal due to area topography even if trees are cleared.

To meet trail user crossing needs and allow for a safe crossing, the Project Team proposes to reroute the trail to a multi-use crossing structure, Houston 6.3 (Crossing 5; Exhibits 3 and 4; see Comments 8 and 10). This five-span bridge will have approximately 25' of horizontal clearance for three spans and over 17' of vertical clearance (see Comments 8 and 10). This trail will be routed under the southern span and reconnected with the existing trail west of the rail embankment.

MHLT issued a six year revocable license issued by the MHLT for the Flat Lake Connector Trail. MSB is working with MHLT to acquire the same or better legal rights for the rerouted Flat Lake Connector and other connecting trails currently used on MHLT lands. If MSB can not acquire an easement for the reroutes, the Project Team will notify Workshop participants and provide a revised proposal for continued access and connectivity.

10. *Incorporate safety considerations into design for the Houston 6.3 crossing (Crossing 5). Design should account for increased traffic caused by funneling multiple trails into a single crossing and should include wider/higher openings under the bridge, a wider turn radius for the reroutes approaching the crossing location, and a good line of sight approaching and extending under the crossing.*

**Proposed Resolution:** The original crossing design at this location was to construct a three-span bridge with 12'-7.5" of vertical clearance, two spans with 14' horizontal clearance, and a center span with over 25' of horizontal clearance. The design carried forward to the MSB Trail User Open House (10/27/11) where MSB presented a five-span crossing structure with increased vertical clearance but similar horizontal clearances. Concerns of trail users attending the MSB Trail User Open House (10/27/11) were addressed by upgrading the crossing structure to provide additional horizontal clearances. The improved structure will accommodate a trail on both the north and south side of the creek, each with 25' horizontal clearance to accommodate multiple users and grooming equipment with a vertical clearance of more than 17'. Exhibit 3 (see Comment 8 above) shows the most recent proposed design for Crossing 5.

The design of reroutes will allow for sufficient line of sight for all users and traffic will be accommodated by two parallel trails, one on each side of the creek. The trail groomer will be able to maintain access on the north and south spans of this crossing structure via existing trail connectivity.

As discussed under Comment 2, the Project Team has been working with Sustainable Design Group to develop standards for sight distance needs and crossing approaches that will accommodate a 100' long vehicle/dog team, which requires a minimum 60' turn radius. The design standard for trail approaches to crossings includes gentle curves with turn radii between

75' and 90' (see Exhibit 2). This turn radius will allow for a 100' dog team to turn and safely pass through the structure while allowing adequate line of sight for trail users prior to passage. Trails will be aligned to provide increased site distance. Trail embankments will be designed to minimize impacts from overflow.

*11. Provide a crossing for the Iditarod Race Trail and obtain an easement from the University of Alaska for this trail should one not currently exist. This trail has reportedly hosted 20 Iditarod races and is used more frequently/regularly than the Historic Iditarod Trail. A separated grade crossing (bridge) is preferred, but if a bridge is not possible, an at-grade crossing would be better than no crossing at all.*

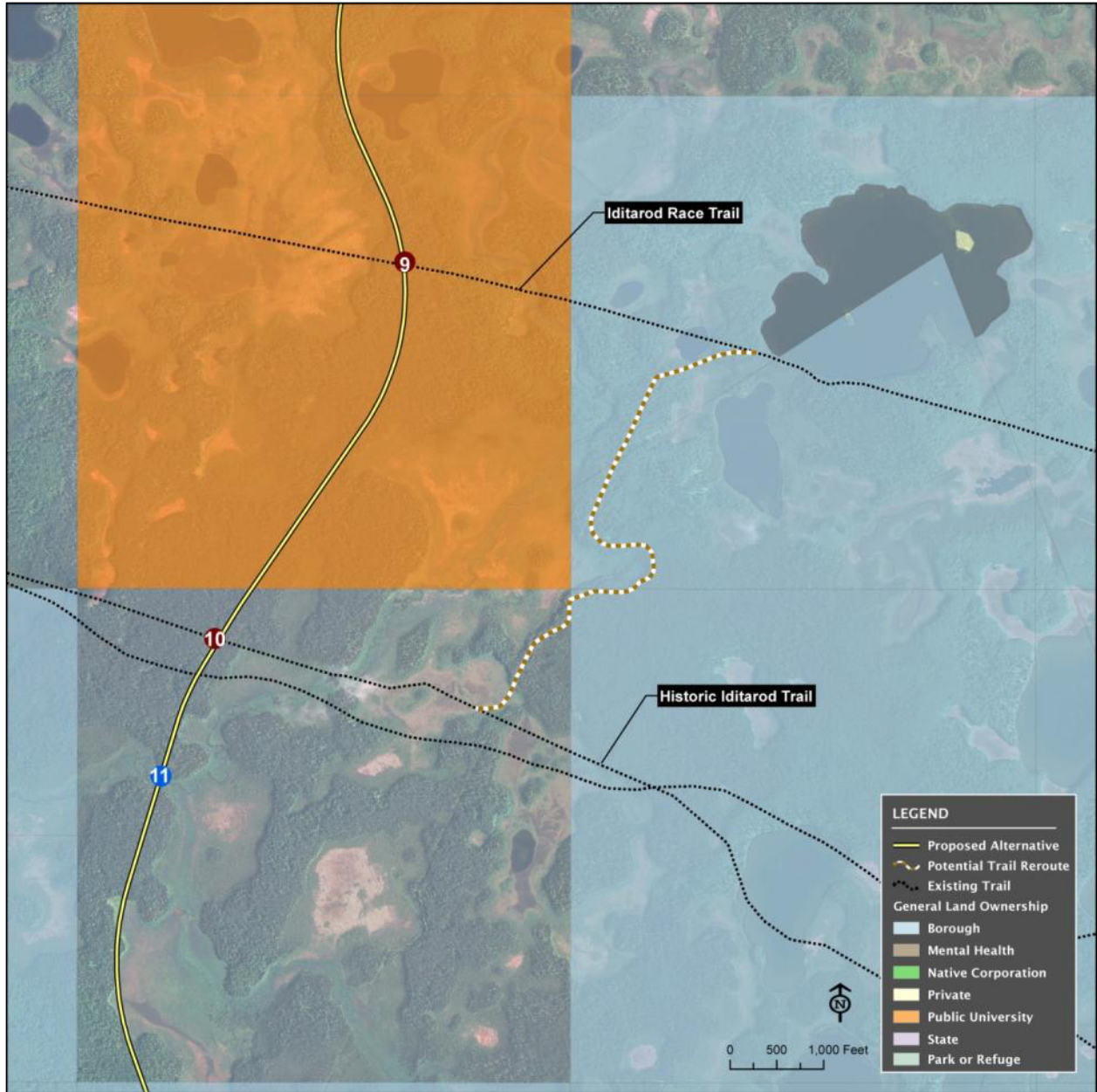
**Proposed Resolution:** Approaches to a crossing for the Iditarod Race Trail would be on University of Alaska land. The MSB is working with UA to secure an easement at this location if no easement currently exists. The crossing (Crossing 9) would be a typical three-span bridge with a minimum 14' vertical clearance, and approximately 25' of horizontal clearance in the center span (see Exhibit 1 as an example and Table 1). Approach improvements would include widening the trail in the vicinity of the crossing.

If negotiations with UA to secure an easement are unsuccessful, a crossing would not be provided for the Iditarod Race Trail as it would encourage trespassing on UA property. The Race Trail would be rerouted to the Historic Iditarod Trail, which would be upgraded/improved to accommodate the potential increase in users (Exhibit 5; see Comment 12).

*12. Provide a separated grade crossing (bridge) and trail improvements for the Historic Iditarod Trail (Crossing 10). The Historic Trail is not used as frequently as the Iditarod Race Trail and needs improvement if it is to be regularly used. It is currently too narrow for current and potential increased use and the portion of the trail west of the crossing location near the Little Susitna washes out and is in ill repair.*

**Proposed Resolution:** The Project Team would provide a separated grade crossing for the Historic Iditarod Trail. Original crossing designs for this trail included a single span crossing with 12' of vertical and 14' of horizontal clearance. Based upon comments received at the previous workshops, the crossing structure was redesigned to include a three-span bridge with over 25' of horizontal clearance and 14' of vertical clearance (see Exhibit 1 under Comment 1 and Table 1 for a comparison of early and current crossing design). Although only the central span of the bridge will be passable for trail users, the north and south spans increase visibility for potential oncoming traffic.

The Historic Trail will be improved where it passes beneath the rail embankment. As discussed under Comments 2 and 10, the Project Team has been working with Sustainable Design Group to develop standards for sight distance needs and crossing approaches that will accommodate a 100' long vehicle/dog team, which requires a minimum 60' turn radius. The design standard for trail approaches to crossings includes gentle curves with turn radii between 75' and 90' (see Exhibit 5). This turn radius will minimally allow for a 100' dog team to turn and safely pass through the structure while allowing adequate line of sight for trail users prior to passage.



**Exhibit 5: Possible Iditarod Race Trail Reroute and Crossings**



13. *Add a crossing for the trail between current Crossings 12 and 13 (formerly Crossings 13 and 14) that is shown on various maps.*

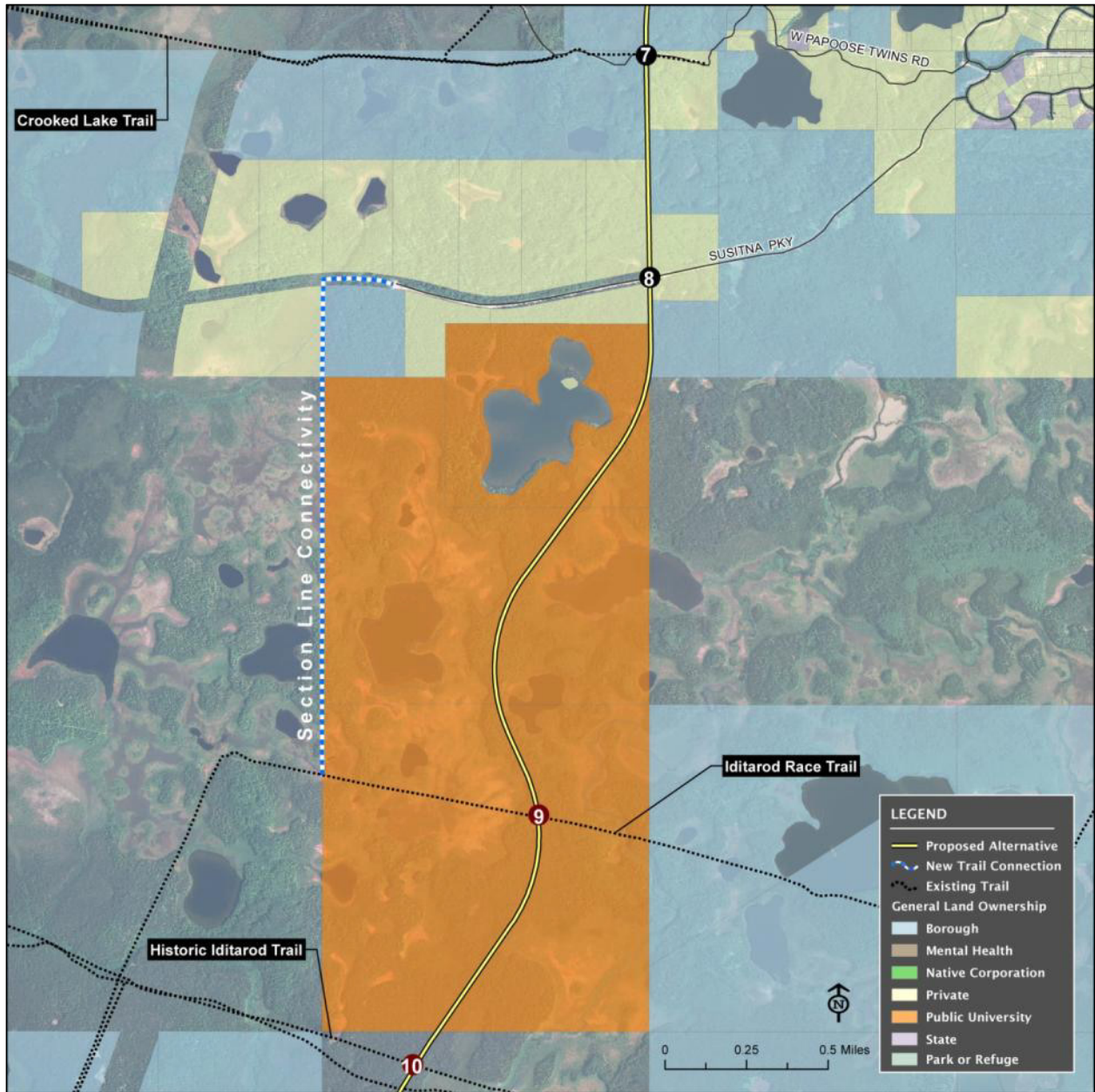
**Proposed Resolution:** This former trail is on agricultural land and no longer exists, although it appears on old maps and was included in the Stephen R. Braund & Associates (SRB&A) 2010 report prepared during the National Environmental Policy Act (NEPA) process. Per consultation with Workshop participants, the crossing for the trail between Crossings 12 and 13 is not needed as this trail is no longer in use. No other trail users have identified a need for a crossing at this location. An at-grade crossing at Baker Farm Road (Crossing 15) has been added to project crossing maps; however, this crossing is not currently planned for continual use. A summary of crossings is included in Table 1 and illustrated on Figure 2 of Attachment 1.

14. *Provide north-south trail along one or both sides of the proposed ROW to help maintain connectivity. Even if there is not a dedicated trail, people would still likely travel along this area.*

**Proposed Resolution:** Trail reroutes, enhanced crossing structures and strategies to maintain connectivity in a safe manner are all measures the Project Team has and continues to work toward as this project moves forward. The proposed railroad alignment is orientated predominately north-south, and therefore, the Project Team believes that the project primarily affects east-west connectivity, not north-south connectivity. We have been working diligently with local trails groups/users, regulatory agencies and other interested parties to maintain east-west connectivity.

Most of the existing trails identified both in the Final Environmental Impact Statement (FEIS) and trails workshops that provide north-south movement will remain intact. The Project Team believes that the existing trail system west of the proposed rail corridor provides a north-south corridor (Figure 3 of Attachment 1). However, one area of concern for north-south connectivity has been identified between Crossings 7 and 10 (Exhibit 6). This area demonstrates the biggest gap in connectivity and is owned primarily by UA. The Project Team has developed an alternative corridor for north-south travel in this area that may work for trail users and property owners (see Exhibit 6).

Providing additional north-south corridors along one or both sides of the rail line has many challenges, some of which include: FAA VORTAC issues, safety concerns, wetland and regulatory concerns, tree cover, topography and cost of constructing additional bridge structures. Current bridge and at grade crossing structures total over \$11 million dollars; mitigation measures such as signage, trail reroutes additional brushing activities and strategic plantings to deter moose have not been factored into these estimated costs. Based upon current north-south connectivity (Figure 3 of Attachment 1) and the varying issues identified above, a new trail(s) paralleling the proposed ROW for the project is not planned.



**Exhibit 6: Connectivity between Crossings 7 and 10**

15. Adequate signage needs to be provided to warn trail users. Consider standard snow machine/dog sledding trail signage as seen on Alaska State Parks Snowmobile Trail Advisory Committee (SnoTRAC) website, not road side signs.

**Proposed Resolution:** Appropriate signage would be placed at key points on trails, trail reroutes, and at crossings and could include: trail direction (e.g., North/South arrows), “Caution - Dog Teams on Trail,” “High Traffic Area,” “Clearance 14’,” “Slow – Keep Right,” “Trail Closed Ahead,” or “Slow – Trail Rerouted.”

Examples of signage presented at the MSB Trail User Open House (10/27/11) are provided in Exhibit 7, and are posted on the project website (<http://www.portmacrail.com/library.html> ). To

address user concerns, the Project Team will develop signage that is generally consistent with the Alaska State Parks SnoTRAC guidelines provided on its website ([http://www.dot.state.ak.us/stwdplng/projectinfo/project\\_pages/winter\\_trans/pdf/AppendixC.pdf](http://www.dot.state.ak.us/stwdplng/projectinfo/project_pages/winter_trans/pdf/AppendixC.pdf)).



**Exhibit 7: Signage examples presented at the MSB Trail User Open House (10/27/11)**

Kiosks with trail maps will be placed at key locations and will aid trail users by identifying trail reroutes and showing available trail connections. An example of a kiosk located on the Willow trails system is illustrated in Exhibit 8. A proposed design for the kiosks is illustrated in Exhibit 9.



Exhibit 8: Willow Trail System Kiosk

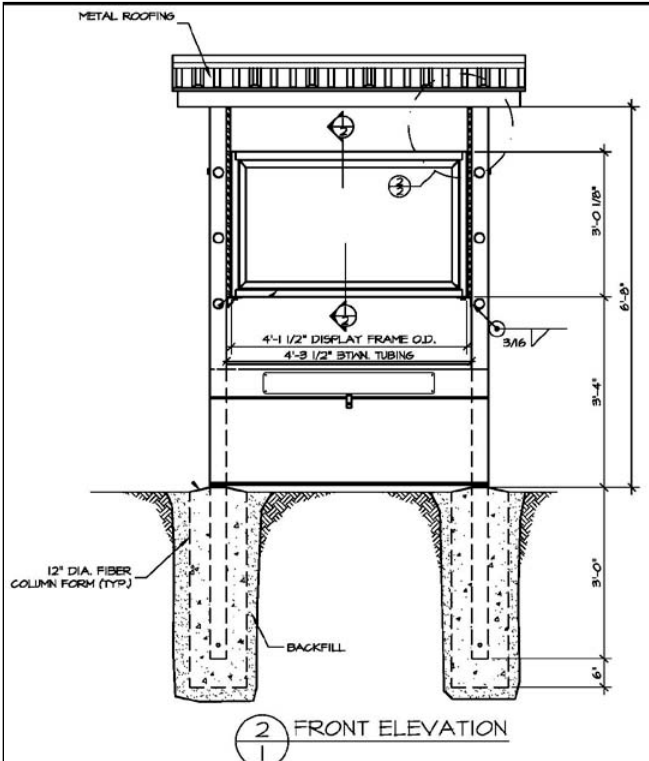


Exhibit 9: Kiosk schematic (Department of Natural Resources)

16. *Would the Iditarod Dog Sledding Historic District (IDSHD) work the same way as the Knik Sled Dog and Recreation Special Land Use District (SPUD) in the MSB? If a SPUD was created for the IDSHD area, would this require future development in the area to go through a formal review process? Consider a special use district similar to the Knik SPUD to create a formal procedure/process for review of projects planned in the IDSHD area and/or to solidify/formalize designation of the IDSHD for the National Register of Historic Places.*

**Proposed Resolution:** The Knik SPUD was implemented to preserve and protect the existing community of Knik and the homestead lifestyle (includes outdoor, historical, traditional recreational and agricultural uses), protect legal trails, and to provide for planning and growth in the Knik area. The Knik SPUD ordinance implements land use regulations to ensure compatibility between various land uses and encourage developers to recognize and dedicate legal trail ROWs where appropriate. For example, developers are required to identify legal and known trails that will be impacted by their project and work with MSB and the community to protect or relocate those trails. Any new SPUD or expansion of an existing SPUD would have to be initiated by a Community Council and approved by the MSB Assembly.

The IDSHD was found eligible for the National Register of Historic Places (NRHP) as a result of Section 106 (National Historic Preservation Act) review carried out as part of the NEPA process. A NRHP nomination is currently underway for the Iditarod National Historic Trail which may include the portion of the trail in the project area. As required under Section 106, development using Federal funds or requiring a Federal permit and/or oversight would require potential developers to go through the Section 106 review process. This review process includes identifying historic properties (i.e., cultural resources eligible for the NRHP) in the project area, assessing effects to historic properties from the project, and resolving adverse affects. Consultation (i.e., seeking, discussing, and considering the views of interested parties, and seeking agreement when feasible) occurs at all stages of the process. As the IDSHD has been found eligible for the National Register, Federal agencies would be required to consider the impact of any projects in the area on the IDSHD and avoid, minimize or mitigate any potential adverse effects. Listing and/or nomination of the IDSHD for the NRHP would provide no additional protection or review requirements than are currently in place. However, the Project Team and SHPO will continue to consult to refine the boundaries of the IDSHD.

17. *Relative to the planned third workshop with the mushers group, participants would prefer to postpone that meeting until after MSB's larger trail user meeting. Participants believe that the input of other trail users is necessary as the mushers are not the only users of the trails in the IDSHD.*

**Proposed Resolution:** A larger trail user meeting, sponsored by MSB and held on October 27, 2011, utilized an open house format and included participants from multiple trail user groups. Comments/questions from participants were compiled, responded to, and posted on the project website (<http://www.portmacrail.com/library.html>) following the meeting.

18. *Provide better/more detailed information on each crossing for the MSB Trail User Open House (10/27/11).*

**Proposed Resolution:** Maps/graphics providing details for crossings were available at the MSB Trail User Open House (10/27/11). These maps/graphics include proposed crossing design schematics and aerial maps showing relocations and approaches for each crossing and

are posted on the project website (<http://www.portmacrail.com/library.html> ). Detailed information regarding crossings is also included in Table 1.

19. *When the project was in the earlier phases of the EIS, public meetings participants were told by MSB and ARRC that trails in the MSB trails plan would have continued connectivity. Is this still the case?*

**Proposed Resolution:** Although certain east-west trails through the area will change, east-west connectivity will be maintained. The Project Team has been working with various trail user groups to identify and address access/connectivity concerns. We believe we have addressed the major concerns to insure continued access for mushers as well as other users, consistent with the need to maintain connectivity across the rail corridor in the IDSHD. In fact, the project will enhance connectivity by resolving multiple right-of-way issues on existing trails, ensuring that they will be maintained for the long-term.

Per the FEIS, "Where the proposed rail line would cross an officially recognized trail, ARRC has stated it would provide public access by a grade-separated crossing. Alternatively, the trail could be relocated by ARRC to avoid crossing the rail line." The Project Team has maintained throughout the EIS process that we will not provide crossings for "unofficial trails." Per the FEIS, an unofficial trail is any trail "that is not specifically established within currently adopted plans by ADNR and/or MSB or is established within these plans at the time of construction or ROW conveyance (whichever occurs first), and whose location is not provided for by recorded ROW or easement. ARRC does not propose to provide crossings for unofficial trails. Unofficial trails would be blocked, and ARRC's trespassing regulations would prohibit the public from crossing of the ROW without first obtaining approval from ARRC."

The Project Team has stated in IDSHD Workshops that trails that cannot be maintained in their current location will be rerouted. If there is a legal easement for the trail, such as the Historic Iditarod Trail, the trail will be maintained in its current location or the Project Team will work with regulatory agencies and or property owners to attempt to obtain similar legal access and easements for any rerouted trail (e.g., Flat Lake Connector Trail).

20. *How will the final decision be made on the crossings?*

**Proposed Resolution:** The Project Team is compiling as much information as possible to make informed decisions about the crossings. The IDSHD Workshops and MSB Trail User Open House address crossings concerns of all trail users, including mushers. ARRC will host a final IDSHD Workshop focusing on how mushers' (and other trail users') recommendations for additional trails and crossings, as well as design recommendations, were considered/responded to by the Project Team. As stipulated in the Section 106 Programmatic Agreement (PA; Stipulation V, Treatment of the Iditarod Dog Sledding Historic District), the Project Team's proposed project design changes, modifications, and/or refinements to avoid, minimize, or mitigate adverse effects on historic properties (i.e., the IDSHD) are included in this section of the draft *Workshop Summary and Implementation Plan* [Note: this draft document is only one section of a larger document], will be presented at the final IDSHD Workshop. As stipulated in the PA, ARRC will refine the *Workshop Summary and Implementation Plan*, in consultation with the Working Group, STB and SHPO, within 60 days of the final Workshop. ARRC will submit the revised document to PA Signatories, Invited Signatories, and other interested consulting parties (e.g., dog mushers and other trail users). Following a 30-day review and comment period, ARRC will incorporate comments as appropriate. ARRC will provide a final *Workshop*

*Summary and Implementation Plan* PA Signatories, Invited Signatories, and other interested consulting parties and will post the document on the project website.

*21. Moose will also use the dog musher crossings. Funneling wildlife and trail users onto fewer crossings/trails would increase conflicts.*

**Proposed Resolution:** Moose often go up and over the railroad embankment, although they may also use the crossings. On many trails, they are unlikely to be present due to the presence of human users. The longer bridges, with wider horizontal and vertical openings, will improve visibility for trail users and reduce the potential for wildlife conflicts. Moose conflicts on the existing trail system are not common and the number of moose encounters with dog teams, snowmachines or other user groups is not expected to increase as a result of this project. The length and width of the crossing structures provides ample space for moose and other wildlife to move away from the rail embankment.

[This page intentionally left blank]



**Table 1: Crossing Comparison Summary**

Crossing # Crossing Name	Crossing Type	Original Design Description	Revised Design Description	Comment/Status
<b>Crossing 1</b> Millers Reach Road	At-Grade, Road	<ul style="list-style-type: none"> <li>• 32' road width</li> <li>• 40' crossing panels<sup>2</sup></li> <li>• \$150,000</li> </ul>	<ul style="list-style-type: none"> <li>• 72' crossing width</li> <li>• 32' road surface</li> <li>• 10' separation, 10' trail (north and south)</li> <li>• 80' of crossing panels for road and trail</li> <li>• \$300,000</li> </ul>	Crossing lights
<b>Crossing 2</b> Utility easement/Big Lake Trail	At-Grade	<ul style="list-style-type: none"> <li>• 14' utility corridor width</li> <li>• 20' crossing panels</li> <li>• \$75,000</li> </ul>	No change	Trails groups/users will be required to work with easement/property owner to ensure continued use and access.
<b>Crossing 3</b> Private property/Big Lake Trail	At-Grade	<ul style="list-style-type: none"> <li>• 32' gravel drive width</li> <li>• 40' crossing panels</li> <li>• \$150,000</li> </ul>	No change	Trails groups/users will be required to work with easement/property owner to ensure continued use and access.
<b>Crossing 4</b> Houston Lake Loop Trail	Bridge	<ul style="list-style-type: none"> <li>• 146' multi-plate culvert (19" diameter)</li> <li>• 14' vertical clearance</li> <li>• 16'-3" horizontal clearance</li> <li>• \$335,000</li> </ul>	<ul style="list-style-type: none"> <li>• 3-span bridge</li> <li>• 14' vertical clearance</li> <li>• 25'-3" horizontal clearance (center span)</li> <li>• \$1,600,000</li> </ul>	
<b>Crossing 5</b> Unnamed Tributary to the Little Susitna River (H 6.3)  Flat Lake/Iron Dog Connector Reroute	Bridge	<ul style="list-style-type: none"> <li>• 3-span bridge</li> <li>• 12'-7.5" vertical clearance</li> <li>• 14' horizontal clearance (opening 1)</li> <li>• 26' horizontal clearance (opening 2)</li> <li>• 14' horizontal clearance (opening 3)</li> <li>• \$1,600,000</li> </ul>	<ul style="list-style-type: none"> <li>• 5-span bridge</li> <li>• 17'-7" vertical clearance</li> <li>• 25'-3" horizontal clearance (openings 2-4)</li> <li>• Opening 3 is a stream channel</li> <li>• \$2,200,000</li> </ul>	
<b>Crossing 6</b> Unnamed Tributary to the Little Susitna River (H 4.3)	Bridge	<ul style="list-style-type: none"> <li>• Single span bridge</li> <li>• 17' vertical clearance</li> <li>• 45' horizontal clearance</li> <li>• \$1,200,000</li> </ul>	<ul style="list-style-type: none"> <li>• 5-span bridge</li> <li>• 14'-6" vertical clearance</li> <li>• 25'-3" horizontal clearance (opening 2-4)</li> <li>• Opening 3 is a stream channel</li> <li>• \$2,200,000</li> </ul>	
<b>Crossing 7</b> West Papoose Twins Road	At-Grade, Road	<ul style="list-style-type: none"> <li>• 32' road width</li> <li>• 40' crossing panels</li> <li>• \$150,000</li> </ul>	<ul style="list-style-type: none"> <li>• 72' crossing width</li> <li>• 32' road surface</li> <li>• 10' separation, 10' trail (north and south)</li> <li>• 80' of crossing panels for road and trail</li> <li>• \$300,000</li> </ul>	Trail would be separated from road and would have separate panels
<b>Crossing 8</b> West Susitna Parkway	At-Grade, Road	<ul style="list-style-type: none"> <li>• 32' road width</li> <li>• 40' crossing panels</li> <li>• \$150,000</li> </ul>	<ul style="list-style-type: none"> <li>• 72' crossing width</li> <li>• 32' road surface</li> <li>• 10' separation, 10' trail (north and south)</li> <li>• 80' of crossing panels for road and trail</li> <li>• \$300,000</li> </ul>	Crossing lights

<sup>2</sup> A crossing panel is an element that is placed on both sides and between the rails at a wheeled vehicle crossing. The purpose of the crossing panel is to provide a surface level with the top of the rail. The crossing panel allows wheeled vehicles to pass over the rails. The crossing panels may be constructed from metal, concrete or plastic.

<b>Crossing # Crossing Name</b>	<b>Crossing Type</b>	<b>Original Design Description</b>	<b>Revised Design Description</b>	<b>Comment/Status</b>
<b>Crossing 9</b> Iditarod Race Trail	Bridge	<ul style="list-style-type: none"> <li>• Single span bridge</li> <li>• 13'-4" vertical clearance</li> <li>• 14' horizontal clearance</li> <li>• \$900,000</li> </ul>	<ul style="list-style-type: none"> <li>• 3-span bridge</li> <li>• 14' vertical clearance</li> <li>• 25'-3" horizontal clearance (center span)</li> <li>• \$1,600,000</li> </ul>	Pending discussions with UA
<b>Crossing 10</b> Iditarod Historic Trail	Bridge	<ul style="list-style-type: none"> <li>• Single span bridge</li> <li>• 12'-6" vertical clearance</li> <li>• 12'-6" horizontal clearance</li> <li>• \$900,000</li> </ul>	<ul style="list-style-type: none"> <li>• 3-span bridge</li> <li>• 14' vertical clearance</li> <li>• 25'-3" horizontal clearance (center span)</li> <li>• \$1,600,000</li> </ul>	
<b>Crossing 11</b> Outflow of Diamond Lake (H 0.8)	Bridge	<ul style="list-style-type: none"> <li>• 3-span bridge</li> <li>• 18' vertical clearance</li> <li>• 60' horizontal clearance (center span)</li> <li>• \$1,600,000</li> </ul>	<ul style="list-style-type: none"> <li>• 5-span bridge</li> <li>• 18'-5" vertical clearance</li> <li>• 21'-11" horizontal clearance (opening 2 and 4)</li> <li>• 60' horizontal clearance (opening 3; stream channel)</li> <li>• \$2,400,000</li> </ul>	
<b>Crossing 12</b> Ayrshire Avenue	At-Grade, Road	<ul style="list-style-type: none"> <li>• 32' road width</li> <li>• 40' crossing panels</li> <li>• \$150,000</li> </ul>	<ul style="list-style-type: none"> <li>• 72' crossing width</li> <li>• 32' road surface</li> <li>• 10' separation, 10' trail (north and south)</li> <li>• 80' of crossing panels for road and trail</li> <li>• \$300,000</li> </ul>	Crossing lights
<b>Crossing 13</b> Holstein Avenue	At-Grade, Road	<ul style="list-style-type: none"> <li>• 32' road width</li> <li>• 40' crossing panels</li> <li>• \$150,000</li> </ul>	<ul style="list-style-type: none"> <li>• 72' crossing width</li> <li>• 32' road surface</li> <li>• 10' separation, 10' trail (north and south)</li> <li>• 80' of crossing panels for road and trail</li> <li>• \$300,000</li> </ul>	Crossing lights
<b>Crossing 14</b> Baker Farm Bridge	Bridge (stream)	<ul style="list-style-type: none"> <li>• 3-span bridge</li> <li>• 11'-9" to 12' vertical clearance</li> <li>• 19'-7" horizontal clearance (opening 1)</li> <li>• 25'-3" horizontal clearance (opening 2)</li> <li>• 19'-2" horizontal clearance (opening 3)</li> <li>• \$1,600,000</li> </ul>	No change	
<b>Crossing 15</b> Baker Farm Road	At-Grade, emergency access route	<ul style="list-style-type: none"> <li>• 32' road width</li> <li>• 40' crossing panels</li> <li>• \$150,000</li> </ul>	No change	Emergency access route
<b>Crossing 16</b> Figure 8 Lake Loop Trail	Culvert	<ul style="list-style-type: none"> <li>• 146' multi-plate culvert (19" diameter)</li> <li>• 9' to 14' vertical clearance</li> <li>• 16'-3" horizontal clearance</li> <li>• \$335,000</li> </ul>	No change	A trail embankment and parking lot improvements are planned for the Figure 8 Loop Trail

## **Attachment 1: Document Figures**

[This page intentionally left blank]

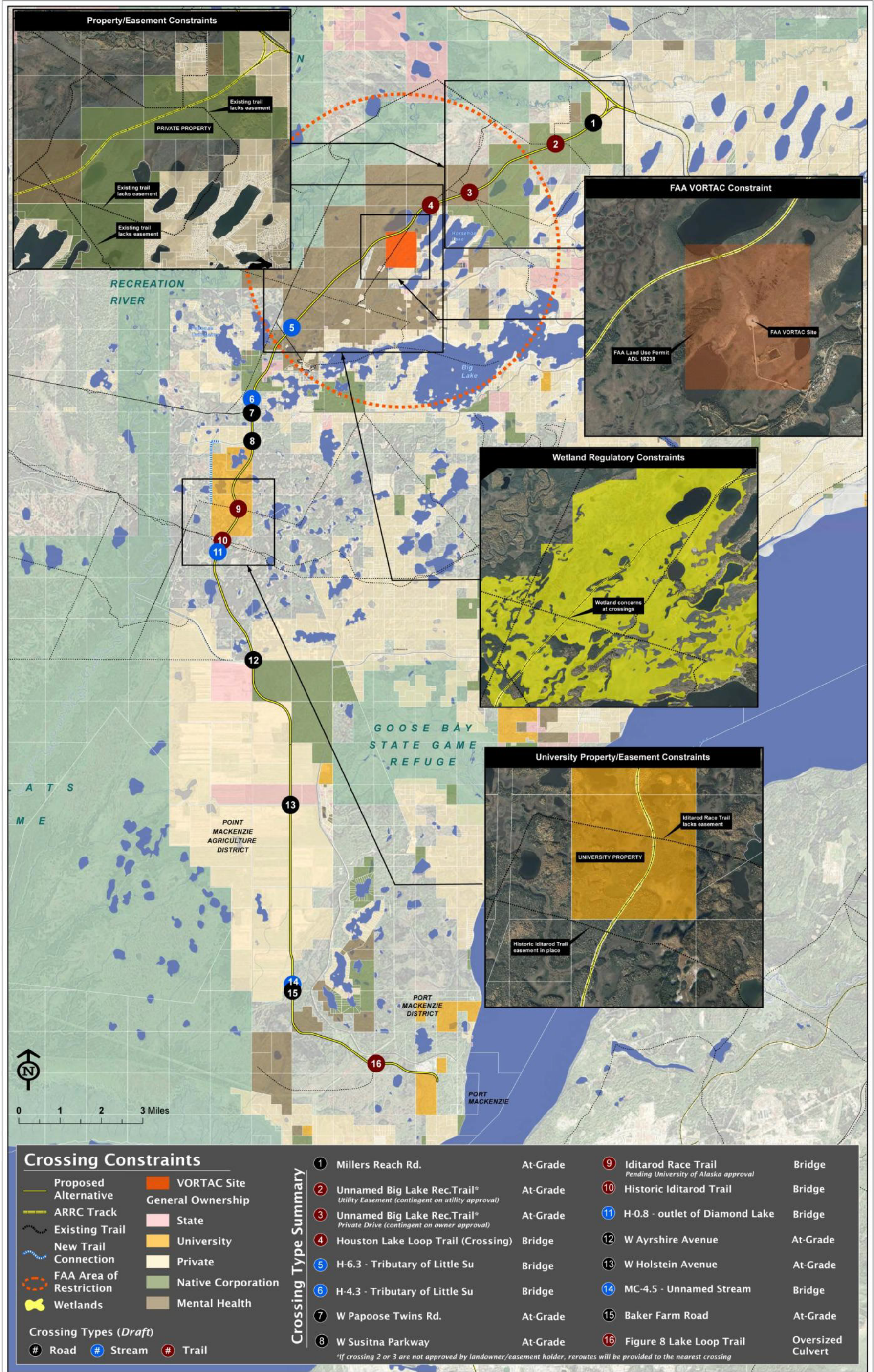


Figure 1: Crossing constraints

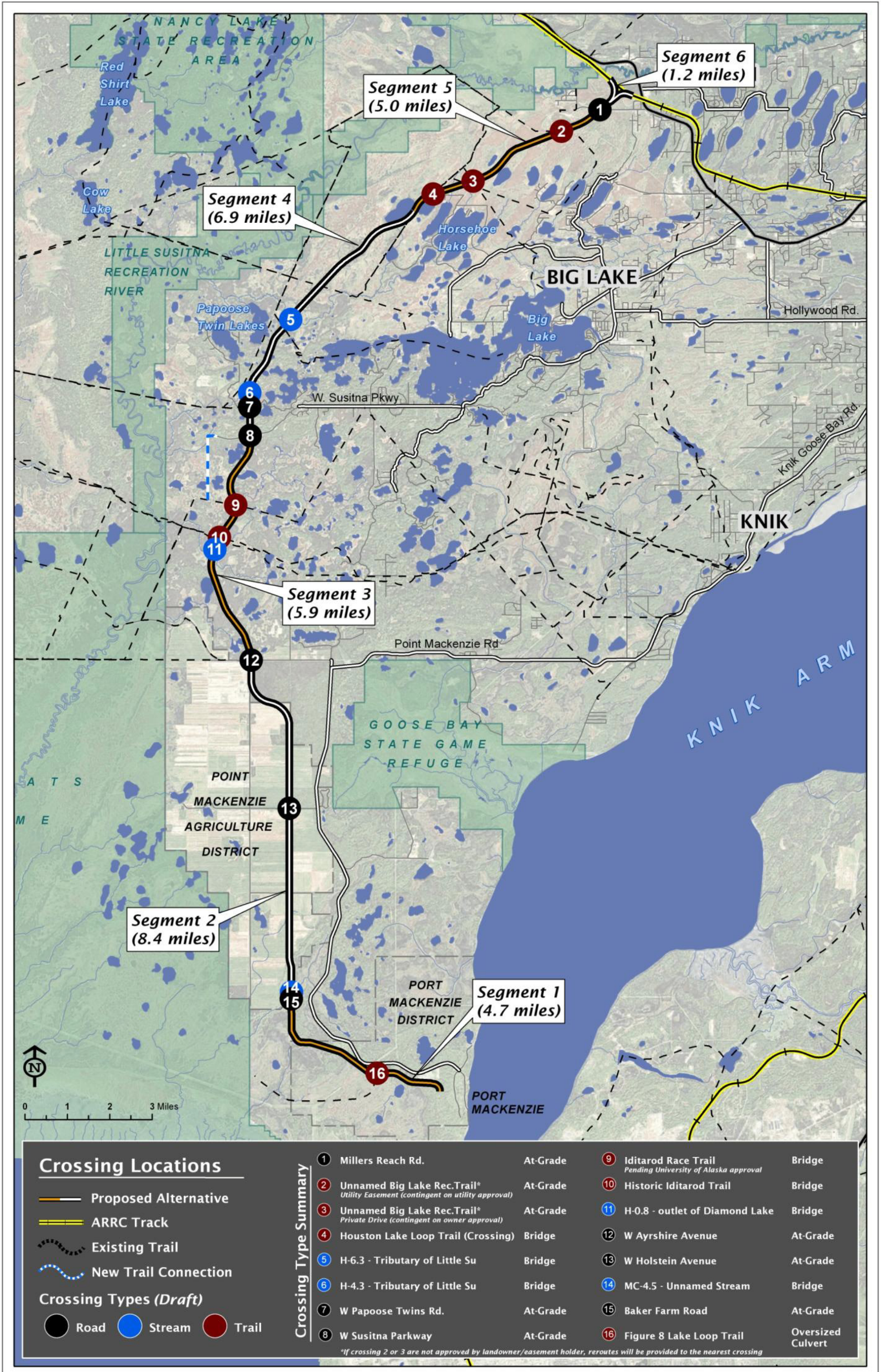


Figure 2: Crossing locations

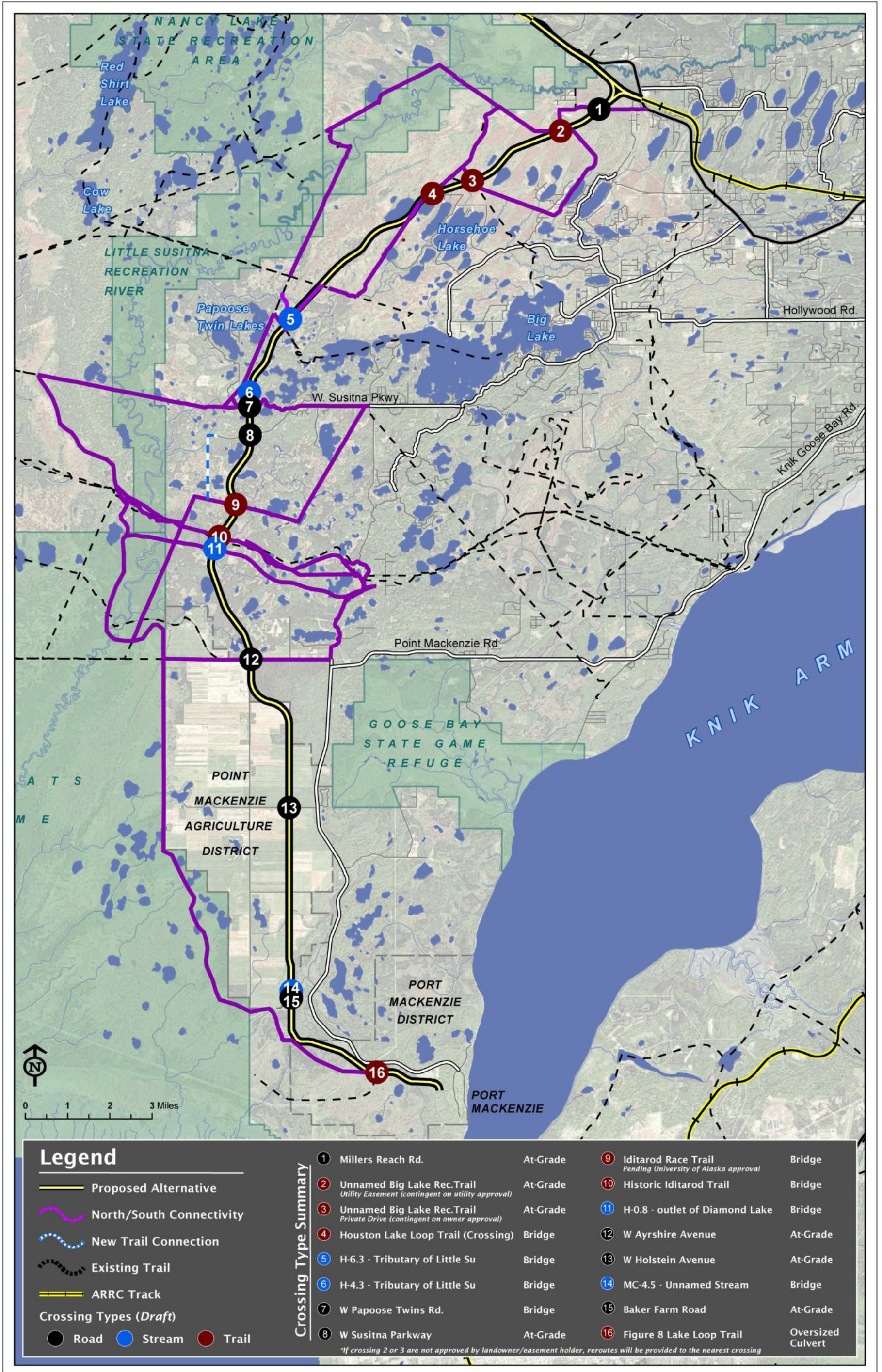


Figure 3: Trail connectivity



## WELCOME to the Iditarod Dog Sledding Historic District (IDSHD) Workshop

January 11, 2012

Houston Middle School  
Houston, Alaska

[www.portmacrail.com](http://www.portmacrail.com)



## Workshop Purpose:

- Present/discuss proposed resolutions to comments received to date
- Create understanding - the Project Team has heard, considered, and addressed participant comments when possible
- Discuss next steps

[www.portmacrail.com](http://www.portmacrail.com)







## Workshop Ground Rules

- Please show mutual respect, courtesy, and patience
- Please don't interrupt anyone while he/she is speaking
- Please remain quiet so others can hear
- Please leave the room for side discussions
- Please refrain from addressing the audience or asking for audience participation.
- Please turn off cell phones or set them to vibrate
- Listen carefully, understand concerns raised by all parties, and be open to new ideas
- **Be positive! Be problem-solvers!**



## Crossing Vertical Clearance

### Comment 1:

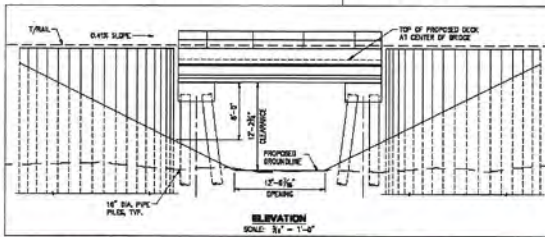
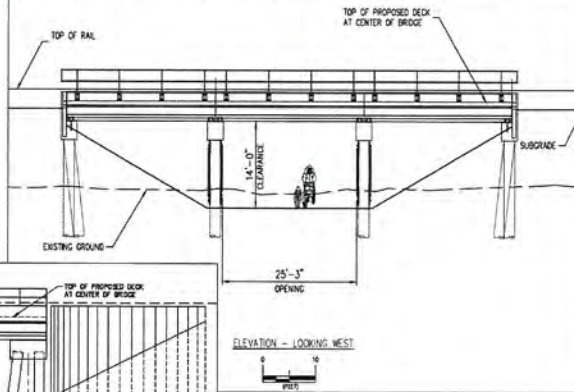
The height (vertical clearance) of crossings needs to be increased to more than 12' to account for snow depth, accommodate equipment for grooming of trails, and enhance safety. The existing clearance is as low as 10' for some crossings.



**Proposed Resolution:**

Vertical clearance increased to a minimum of 14' whenever possible

Revised Historic Iditarod Crossing design following the IDSHD Workshops



Early Historic Iditarod Crossing Design (June 2011)



## Approach Curvature

**Comment 2:**

The length of a sled dog team with 16 to 20 dogs (approximately 85') needs to be considered during design of trail approaches to crossings to ensure curves are not too tight. (This comment was made in reference to the rerouted trails, specifically Crossing H 6.3 [Project Crossing 5]).



**Proposed Resolution:**

Design standard for approaches:

- Gentle curves - turn radii between 75' and 90'
- Allow 100' long dog team to turn and safely pass through the structure
- Allow adequate line of sight.



Approach example

## Line of Sight

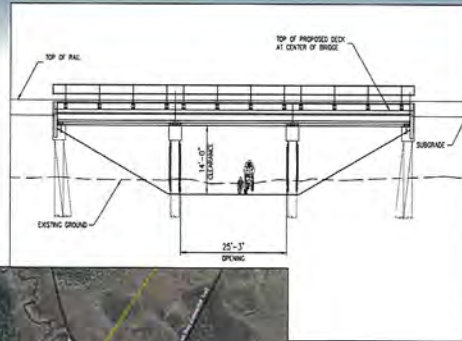
**Comment 3:**

Some crossings are single span bridges and do not provide mushers with adequate visibility to see if anything is approaching from the other direction. The length of these crossings needs to be increased. Trail approaches to crossings also need adequate line-of-sight.

**Proposed Resolution:**

Redesigned bridges:

- Minimum of 3-spans
- Generally provide 20' horizontal clearance (minimum)
- Adequate line-of-sight provided by relatively straight approaches



## Future Expansion

**Comment 4:**

Will trail crossings be turned into road crossings as growth and development increases? Planning for at-grade road crossings (e.g., West Susitna Parkway and Ayrshire Road) should take population growth and resulting development into consideration, as conflicts between users could increase.



### **Proposed Resolution:**

The proposed designed trail crossings:

- Not adequate for roadways
- Will not be turned into road crossings
- Will include separated crossing panels on either side of select roadways to enhance safe passage

## **Easements**

### **Comment 5:**

Easements do not exist but are needed for some trails. Also, with population increases, more and more trails could be sold into private ownership/development.

### **Proposed Resolution:**

MSB Community Development and Land Management will continue to:

- Work with trail users/agencies to obtain easements for existing trails that do not currently have legal easements
- Work with agencies to try to secure easements for legal trails that are proposed to be rerouted

## **Constraints**

### **Comment 6:**

Provide a map to workshop participants showing where crossings cannot be located (e.g., VORTAC or wetland restrictions or other design concerns).

**Proposed Resolution:**

The constraints map (right) is:

- Posted on the Project website
- Included in the comment summary document
- Presented at workshops/open house

Constraints include:

- FAA VORTAC radar site (3 mile radius)
- Wetlands
- Land ownership
- Lack of legal easements



## Crossing Quantity

**Comment 7:**

More crossings in the heavily used area between Crossings 2 and 10 are needed to maintain connectivity. Limiting the number of crossings and relocating and/or combining trails in this area may “bottleneck” multiple user groups onto fewer trails, which will make the trails more dangerous by increasing the potential for collisions and user conflicts (e.g., between snow machiners or other users and mushers).



## **Proposed Resolution:**

Strike a reasonable balance between:

- Needs of trail users
- Obligations to property owners and State and Federal agencies
- Safety considerations

Continue to investigate inclusion of additional grade-separated crossings between Crossings 2 and 10:

- Safety concerns (e.g., visibility issues caused by topography and vegetation)
- Regulatory issues related to wetlands impacts
- Design issues associated with a high water table in the area
- Property ownership
- Existing property/easement constraints
- Construction restrictions associated with the FAA VORTAC radar site



# Iron Dog Connector

## Comment 8:

Provide a crossing for the Iron Dog Connector Trail (Big Lake Trail #5 in MSB Trails Plan).

## Proposed Resolution:

- Propose a reroute of trail
- A crossing is not a good option at this location due to:
  - Safety concerns
  - Wetland/regulatory concerns
  - Tree cover and topography
- Project Team working with the following regarding the rerouted trail:
  - MHLT to acquire easements
  - Iron Dog Race Board of Directors



## Flat Lake Connector

### Comment 9:

Provide a crossing for the Flat Lake Connector Trail (official Iron Dog trail; between Crossings 5 and 6). This is a well established trail with a legal easement that needs an at-grade crossing. Improvements should include cutting trees at the crossing to allow a wider field of view for approaches.

### Proposed Resolution:

- Participants requested 14' vertical clearance for separated grade crossings
  - 10' vertical clearance is possible without interfering with the shallow underlying groundwater table
- At-grade crossing would have safety concerns
  - Available line-of-sight is minimal
- Propose reroute of trail to Crossing 5
- MSB will attempt to acquire easements for trail reroutes prior to construction.

## Crossing 5

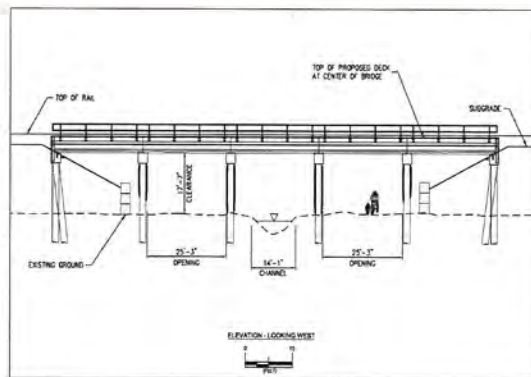
### Comment 10:

Incorporate safety considerations into design for the Houston 6.3 crossing (Crossing 5). Design should account for increased traffic caused by funneling multiple trails into a single crossing and should include wider/higher openings under the bridge, a wider turn radius for the reroutes approaching the crossing location, and a good line of sight approaching and extending under the crossing.

### Proposed Resolution:

The improved structure will provide:

- Trail on both the north and south side of the creek
- 25'+ horizontal clearance to accommodate multiple users and grooming equipment
- 17'+ vertical clearance
- Sufficient line-of-sight
- Approaches with gentle curves (75'-90' radii)



Crossing 5 (H 6.3) profile view.

## Iditarod Race Trail

### Comment 11:

Provide a crossing for the Iditarod Race Trail and obtain an easement from the University of Alaska for this trail should one not currently exist. This trail has reportedly hosted 20 Iditarod races and is used more frequently/regularly than the Historic Iditarod Trail. A separated grade crossing (bridge) is preferred, but if a bridge is not possible, an at-grade crossing would be better than no crossing at all.

### Proposed Resolution:

- MSB is working with UA to secure an easement at this location
- If easement secured, crossing would include:
  - Typical three-span bridge
  - Minimum 14' vertical clearance
  - Approximately 25' of horizontal clearance (center span)
  - Approach improvements
- Reroute to Historic Iditarod Trail if easement cannot be secured

# Historic Iditarod Trail

## Comment 12:

Provide a separated grade crossing (bridge) and trail improvements for the Historic Iditarod Trail (Crossing 10). The Historic Trail is not used as frequently as the Iditarod Race Trail and needs improvement if it is to be regularly used. It is currently too narrow for current and potential increased use and the portion of the trail west of the crossing location near the Little Susitna washes out and is in ill repair.

## Proposed Resolution:

- A separated grade crossing for the Historic Iditarod Trail will be provided.
- Crossing would include:
  - Typical three-span bridge
  - Minimum 14' vertical clearance
  - Approximately 25' of horizontal clearance (center span)
  - Approach improvements
  - Approaches with gentle curves (75'-90' radii)



## Additional Crossing

### Comment 13:

Add a crossing for the trail between current Crossings 12 and 13 (formerly Crossings 13 and 14) that is shown on various maps.

### Proposed Resolution:

- Although it appears on old maps, this former trail is on agricultural land and is no longer used.
- Per consultation with Workshop/Open House participants, this crossing is not needed
- No crossing is proposed in this location
- Baker Farm Road at-grade crossing (Crossing 14) has been added to Project maps



## North-South Connectivity

### Comment 14:

Provide north-south trail along one or both sides of the proposed ROW to help maintain connectivity. Even if there is not a dedicated trail, people would still likely travel along this area.



**Proposed Resolution:**

- Proposed railroad alignment is orientated predominately north-south
- Project primarily affects east-west connectivity
- Project Team working with users to maintain east-west connectivity
- Existing north-south trails identified in the FEIS will remain intact
- Gap in north-south connectivity between Crossings 7 and 10 addressed by alternative corridor



## Signage

**Comment 15:**

Adequate signage needs to be provided to warn trail users. Consider standard snow machine/dog sledding trail signage as seen on Alaska State Parks Snowmobile Trail Advisory Committee (SnoTRAC) website, not road side signs.



**Proposed Resolution:**

- Appropriate signage placed at key points on trails, trail reroutes, and crossings
- Project Team will develop signage that is consistent with the Alaska State Parks SnoTRAC guidelines
- Kiosks with trail maps placed at key locations to aid users by identifying trail reroutes and connections



## SPUD/National Register

**Comment 16:**

Would the Iditarod Dog Sledding Historic District (IDSHD) work the same way as the Knik Sled Dog and Recreation Special Land Use District (SPUD) in the MSB? If a SPUD was created for the IDSHD area, would this require future development in the area to go through a formal review process? Consider a special use district similar to the Knik SPUD to create a formal procedure/process for review of projects planned in the IDSHD area and/or to solidify/formalize designation of the IDSHD for the National Register of Historic Places.





**Proposed Resolution:**

- Knik SPUD was implemented to:
  - preserve and protect the existing community of Knik and the homestead lifestyle (e.g., outdoor, historical, traditional recreational and agricultural uses),
  - protect legal trails
  - provide for planning and growth in the Knik area
- Knik SPUD ordinance implements land use regulations to
  - ensure compatibility between various land uses
  - encourage developers to recognize and dedicate legal trail ROWs where appropriate
- Any new SPUD/expansion of an existing SPUD would have to be initiated by a Community Council and approved by the MSB Assembly

- The IDSHD has already been found eligible for the National Register
  - Federal agencies would be required to consider the impact of any projects in the area on the IDSHD and avoid, minimize or mitigate any potential adverse effects.
  - Listing and/or nomination of the IDSHD for the NRHP would provide no additional protection or review requirements than are currently in place.
- The Project Team and SHPO will continue to consult to refine the boundaries of the IDSHD



## Trail User Input

### Comment 17:

Relative to the planned third workshop with the mushers group, participants would prefer to postpone that meeting until after MSB's larger trail user meeting. Participants believe that the input of other trail users is necessary as the mushers are not the only users of the trails in the IDSHD.

### Proposed Resolution:

A larger trail user meeting, sponsored by MSB, was held on October 27, 2011.

- Included participants from multiple trail user groups
- Comments/questions from participants were compiled, responded to, and posted on the project website

[www.portmacrail.com](http://www.portmacrail.com)



## Information

### Comment 18:

Provide better/more detailed information on each crossing for the MSB Trail User Open House (10/27/11).

### Proposed Resolution:

Proposed crossing design schematics and aerial maps showing relocations and approaches for each crossing are posted on the Project website

[www.portmacrail.com](http://www.portmacrail.com)



## Connectivity

### Comment 19:

When the project was in the earlier phases of the EIS, public meetings participants were told by MSB and ARRC that trails in the MSB trails plan would have continued connectivity. Is this still the case?

### Proposed Resolution:

- Project Team working with user groups, agencies, and land owners to maintain connectivity
- Project Team will not provide crossings for "unofficial trails."
- Trail with legal easements will be maintained in their current location
- Trails that cannot be maintained in their current location will be rerouted
  - Project Team will work with regulatory agencies and or property owners to attempt to obtain similar legal access and easements for any rerouted trail

## Decision-making

### Comment 20:

How will the final decision be made on the crossings?

### Proposed Resolution:

ARRC will:

- Refine the *Workshop Summary and Implementation Plan* within 60 days of the final Workshop
- Submit this document to PA Signatories, Invited Signatories, and other interested consulting parties.
- Incorporate comments, as appropriate, following a 30-day comment period
- Submit a final *Workshop Summary and Implementation Plan* to PA Signatories, Invited Signatories, and other interested consulting parties and post the document on the project website

## Moose/User Conflicts

### Comment 21:

Moose will also use the dog musher crossings. Funneling wildlife and trail users onto fewer crossings/trails would increase conflicts.

### Proposed Resolution:

Improve visibility for trail users and reduce the potential for wildlife conflicts:

- Longer bridges
- Wider horizontal and vertical openings
- Increased length and width provides ample space for moose and other wildlife to move away from the rail embankment

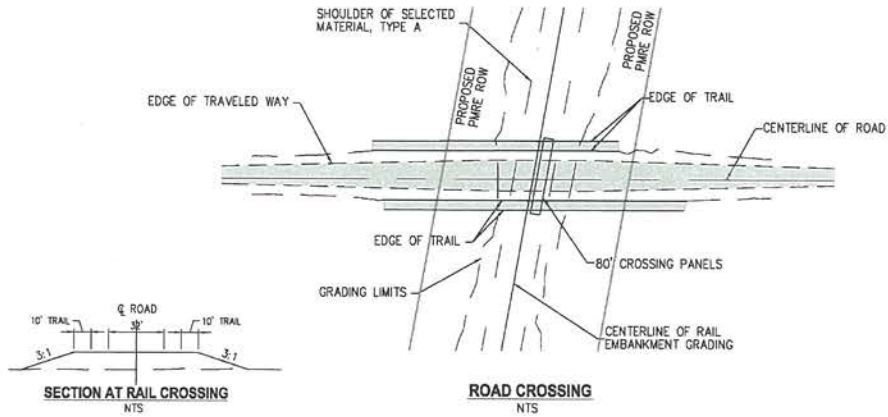


## Next Steps

- ARRC will develop a *Workshop Summary and Implementation Plan* in consultation between the Working Group, STB and SHPO
- ARRC will submit this document to appropriate parties (e.g., STB, SHPO, Workshop participants)
- Reviewing parties provide comments to ARRC (30-day comment period)
- ARRC will incorporate comments, as appropriate
- ARRC will submit a final *Workshop Summary and Implementation Plan* to appropriate parties and post the document on the project website



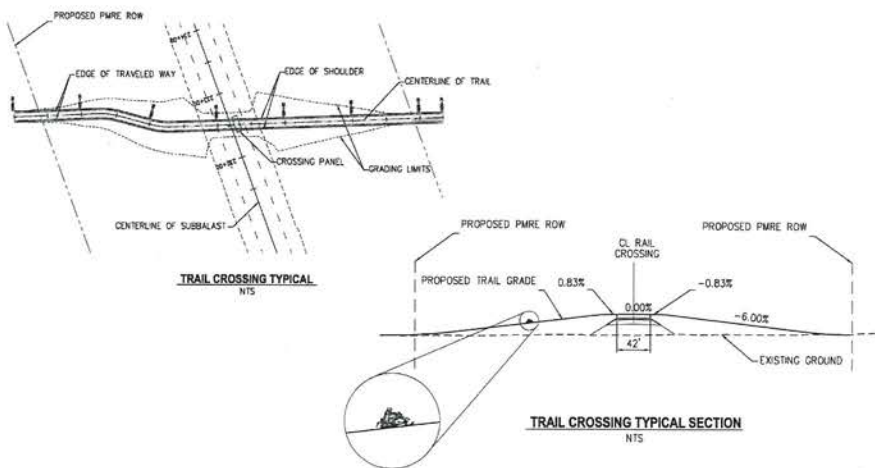
## Crossing 1: Millers Reach Road



[www.portmacrail.com](http://www.portmacrail.com)



## Crossing 2: Big Lake Trail #1 MEA Utility Corridor Crossing

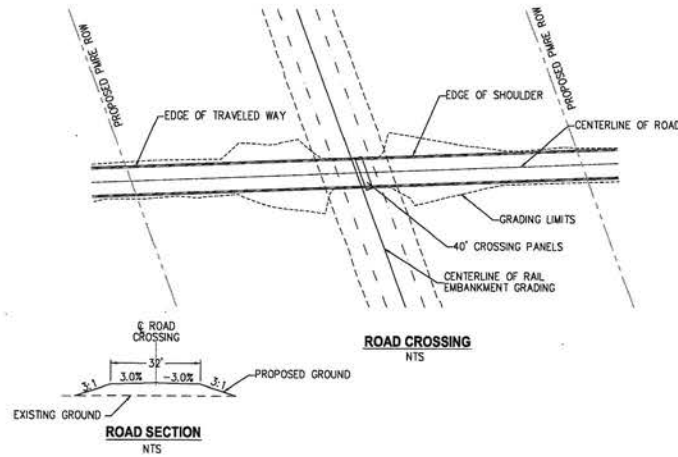


[www.portmacrail.com](http://www.portmacrail.com)





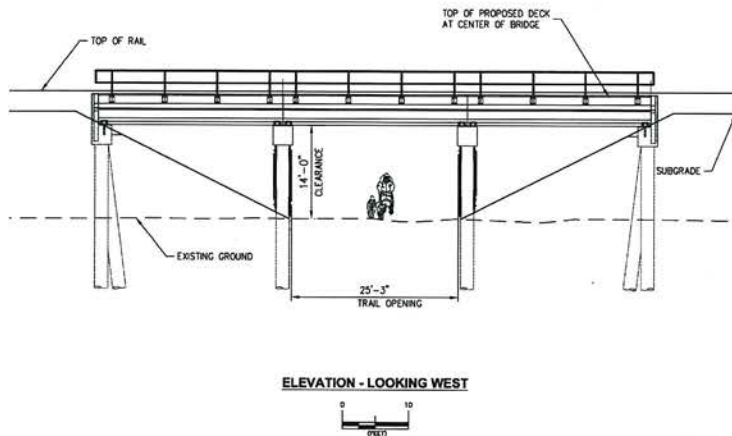
### Crossing 3: Big Lake Trail #2 MHLT Gravel Access Road



[www.portmacrail.com](http://www.portmacrail.com)



### Crossing 4: Houston Lake Loop Trail



ELEVATION - LOOKING WEST

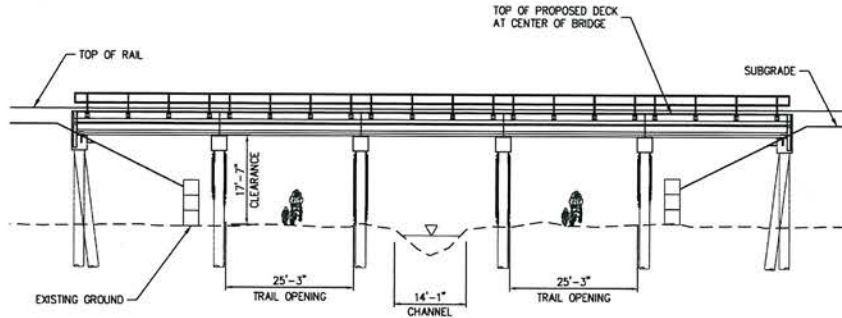


[www.portmacrail.com](http://www.portmacrail.com)





### Crossing 5: Houston 6.3 Iron Dog and Flat Lake Connectors



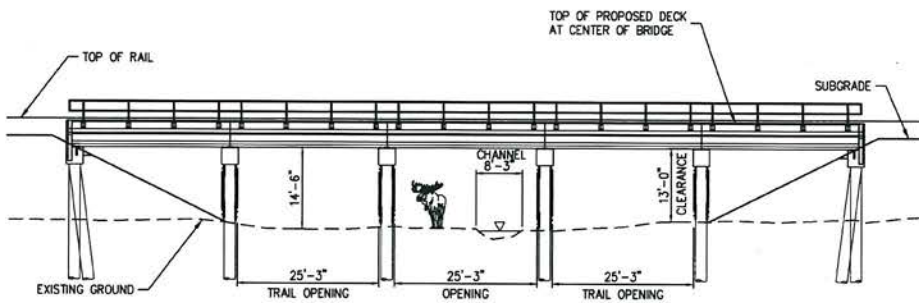
ELEVATION - LOOKING WEST



[www.portmacrail.com](http://www.portmacrail.com)



### Crossing 6: Houston 4.3 (multi-use) Tributary of the Little Susitna River



ELEVATION - LOOKING WEST



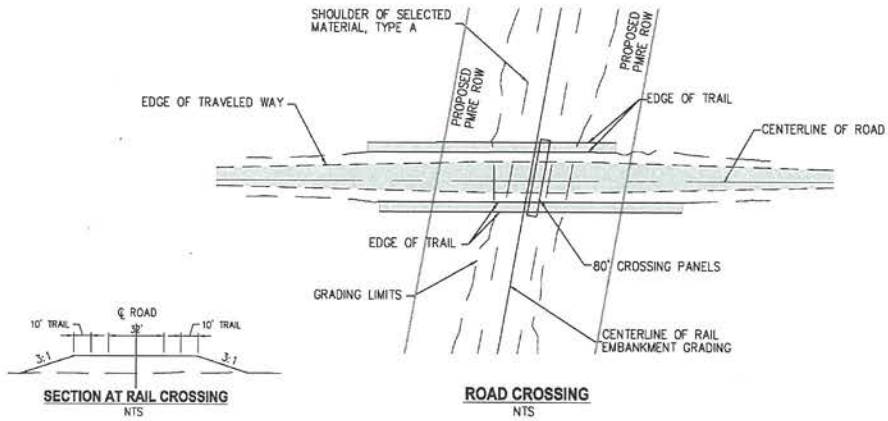
[www.portmacrail.com](http://www.portmacrail.com)







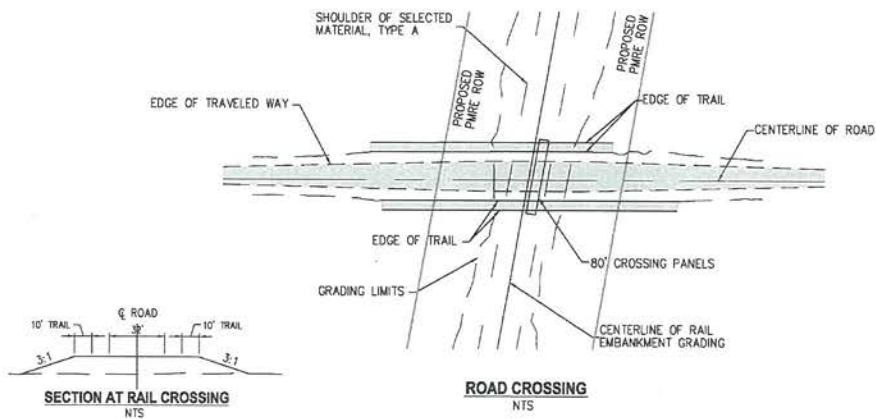
# Crossing 7: West Papoose Twins Road



[www.portmacrail.com](http://www.portmacrail.com)



# Crossing 8: West Susitna Parkway

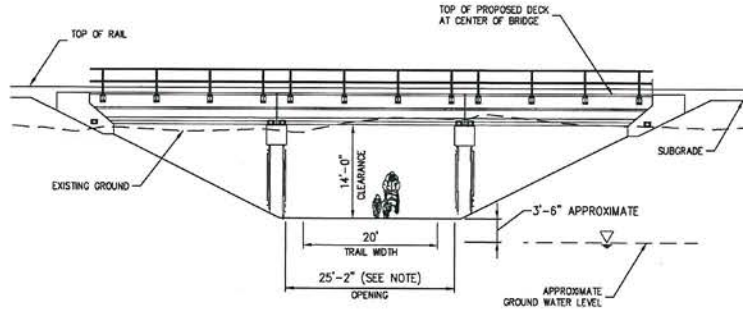


[www.portmacrail.com](http://www.portmacrail.com)





### Crossing 9: Iditarod Race Trail



ELEVATION - LOOKING WEST



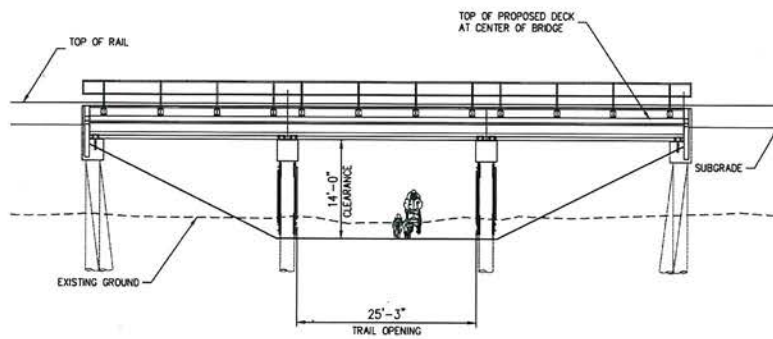
NOTE:

OPENING WIDTH IS PERPENDICULAR TO BRIDGE.  
EFFECTIVE OPENING/TRAIL WIDTH IS 20' DUE TO SKEW OF TRAIL AT BRIDGE LOCATION

[www.portmacrail.com](http://www.portmacrail.com)



### Crossing 10: Iditarod Historic Trail



ELEVATION - LOOKING WEST

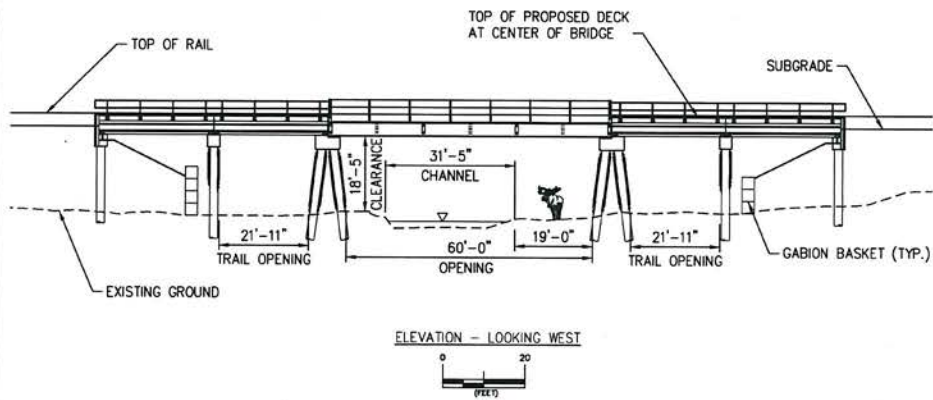


[www.portmacrail.com](http://www.portmacrail.com)





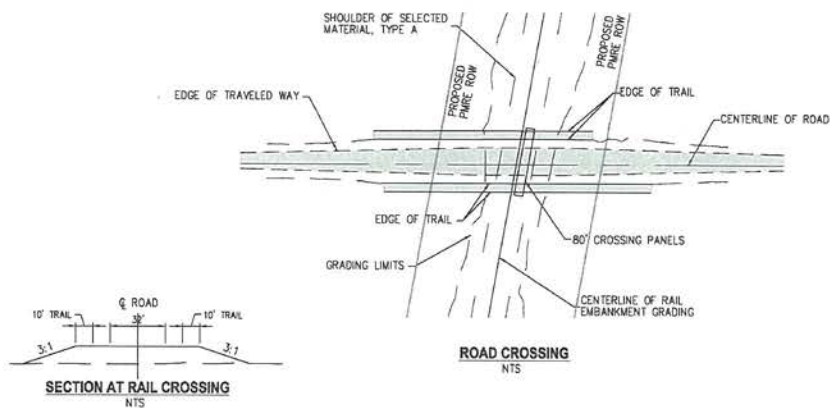
### Crossing 11: Houston 0.8 (multi-use) Outlet of Diamond Lake



[www.portmacrail.com](http://www.portmacrail.com)



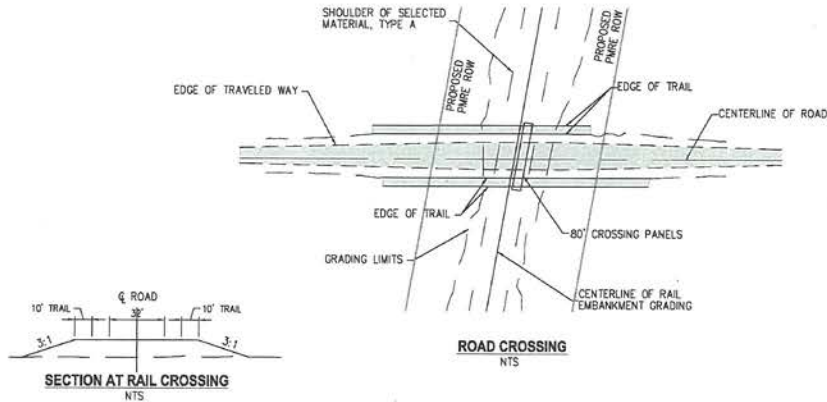
### Crossing 12: Ayrshire Avenue



[www.portmacrail.com](http://www.portmacrail.com)



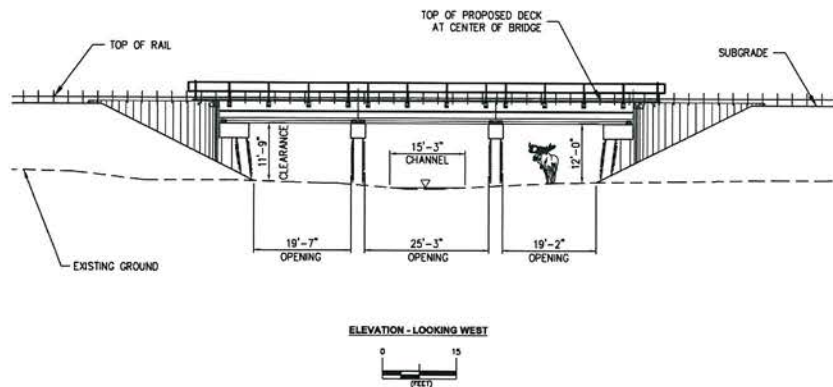
### Crossing 13: Holstein Avenue



[www.portmacrail.com](http://www.portmacrail.com)



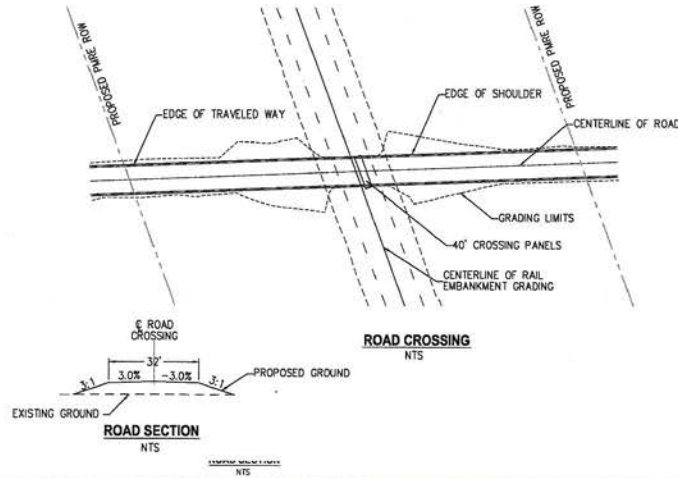
### Crossing 14: Baker Farm Bridge



[www.portmacrail.com](http://www.portmacrail.com)



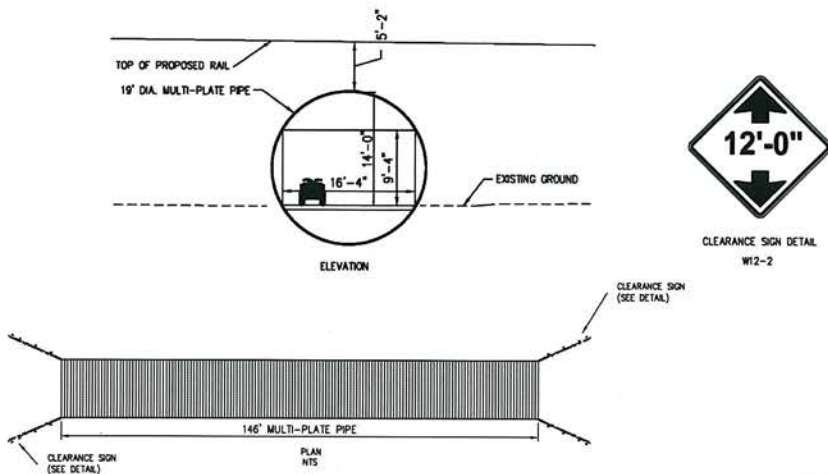
### Crossing 15: Baker Farm Road



[www.portmacrail.com](http://www.portmacrail.com)



### Crossing 16: Figure 8 Lake Loop Trail



[www.portmacrail.com](http://www.portmacrail.com)





# MEA Utility Corridor Crossing Possible Trail Reroute



## Public Outreach – Past, Present & Future

2007

- The Point MacKenzie Rail Extension Project corridor was evaluated in 2007 in the Preliminary Environmental and Alternatives Report (PEAR) a comprehensive report produced by the Railroad, contractors, and the Borough. In mid-January 2008, the MSB and the ARRC submitted the PEAR to the Surface Transportation Board (STB), the lead federal agency responsible for approving new rail line.
- Public Involvement activities for the PEAR occurred between September and December 2007. Over 351 comments were received.

2008

- On Feb. 12, 2008, the STB issued a Notice of Intent (NOI) to prepare an Environmental Impact Statement (EIS), which kicked off the formal environmental review process required under the National Environmental Policy Act (NEPA).
- The STB held scoping meetings:
  - Knik – March 3
  - Big Lake – March 4
  - Willow – March 5
  - Houston – March 6
  - Wasilla – March 10
  - Anchorage – March 11

2009

- The STB considered agency and public input received during the scoping process and on July 17, 2009 issued the final scope of study for the Draft EIS.
- The STB published the final scope of study in the Federal Register, placed it on the STB Web site and project Web site, and mailed an announcement of the availability of the final scope of study to approximately 8,000 individuals, agencies, and other interested parties on the STB project mailing list.

2010

- The STB issued a Draft EIS on March 16, 2010.
- The STB and cooperating agencies held 6 public meetings on the Draft EIS:
  - Anchorage – April 6
  - Houston – April 12
  - Big Lake – April 7
  - Willow – April 13
  - Wasilla – April 8
  - Knik – April 14
- The STB distributed the Draft EIS through hard copy and CD-ROM mailing to maximize public awareness of the availability of the document and to provide instructions on how to comment on the Draft EIS. The list included approximately 6,800 contacts at the time STB distributed the Draft EIS. STB received a total of approximately 142 written and oral comments during the Draft EIS comment period, which closed on May 30, 2010.

2011

- The STB released the Final Environmental Impact Statement on March 25, 2011. The Final EIS recommended the Mac Central and Houston South as the proposed route for the Point MacKenzie Rail Extension.
- ARRC prepared and submitted permit applications.
- Winter 2010/2011 Big Lake Trails Group Meeting
- April – ARRC PMRE Open House
- Section 106 Programmatic Agreement executed June 15, 2011.
- Section 106 outreach
  - June 28, July 7 – Iditarod Dog Sledding Historic District (IDSHD) workshops
  - October 27 – MSB trails meeting.
- The STB issued the Record of Decision on November 23, 2011; it became effective on December 21, 2011.

2012

- January – final IDSHD workshop



## What We Heard (from past meetings)

### Recommendations for additional crossings or trails:

- Separated grade crossing (bridge) for the historic Iditarod Trail (south of the actual Iditarod Race route)
- Crossing for the Iron Dog Connector Trail (Big Lake Trail #5).
- Crossing for the Flat Lake Connector Trail used by mushers to avoid Iron Dog traffic on Big Lake Trail #5 (Iron Dog Connector Trail).
- Crossing for Iditarod Race Trail from Knik between the Historic Race Trail and West Susitna Parkway.
- Crossings at seismic and/or section lines in Big Lake area.
- Trail parallel to the proposed rail right-of-way.

### Safety issues:

- Recommend increasing clearance under bridges from 12 feet to at least 14 feet to account for snow depth, removal of snow from rail portions over bridges and also to account for height of grooming equipment.
- Reroutes and combining trails may “bottleneck” users /wildlife and increase the potential for collisions and user conflicts.
- Concerns about locations and/or types of crossings (e.g., At-grade crossings on existing roadways)

### Other:

- Request the creation of a Special Use District similar to the one at Knik (Sled Dog and Recreation District) for protection of trails important to mushing.
- Take growth into account when planning crossings
- Provide information on where the ARRC cannot provide crossings and appropriate mitigation steps (e.g. VORTAC radar site)







# Connectivity

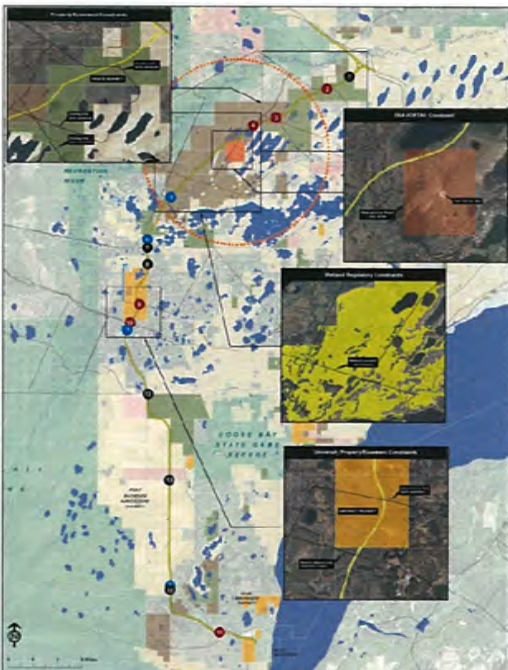


**Legend**

Proposed Alternative	New Rail Connection	Existing Trail	ABC Trail	Crossing Types (Bridges)	Road	Stream	Trail
Crossing Types (Culverts)	Crossing Types (Trestles)	Crossing Types (Overpasses)	Crossing Types (Underpasses)	Crossing Types (Tunnels)	Crossing Types (Other)	Crossing Types (Other)	Crossing Types (Other)



# Constraints



**Crossing Constraints**

Wetlands	Riparian Area	Wildlife Habitat	Other Constraints
Wetlands	Riparian Area	Wildlife Habitat	Other Constraints



# Crossings





# 2012 Alaska Railroad Program of Projects (POP)

Your input is important to the Alaska Railroad as we pursue a program of capital improvements during 2012. Please write legibly (printing is appreciated). Attach additional sheets if necessary.

COMMENT FORM

[Empty rectangular box]

Name  
*Kelley Griffin*

Phone  
*907-373-1126*

Address  
*8796 W Sunset Ave Wasilla*




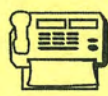
Email  
*Kelleyg@mtaonline.net*

Your Comments: *99623*

*To Clarify Comment # 14  
The issue of a trail on both sides of the tracks, North/South is not an issue of North/South connectivity. It is to keep users of "dead end" trails from crossing the tracks at unauthorized places. It allows users to travel to a crossing.  
It is all about East/West connectivity*

*Rec. 1/11/12*

The Alaska Railroad welcomes your input. Please send your written comments:

	Capital Projects - Public Comment Alaska Railroad Corporation P.O. Box 107500 Anchorage, AK 99510-7500		ARRC's TTY/TTD 265-2620 or voice 265-2494 Alaska Relay TTY 800-770-8973 or voice 1-800-770-8255
	<a href="mailto:public_comment@akrr.com">public_comment@akrr.com</a>		Fax (907) 265-2365